

**WILSONVILLE CITY HALL  
DEVELOPMENT REVIEW BOARD PANEL A**

**MONDAY, MARCH 11, 2013 - 6:30 P.M.**

I. Call To Order:

II. Chairman's Remarks:

III. Roll Call:

Mary Fierros Bower  
Lenka Keith  
Simon Springall

Ken Ruud  
Jerry Greenfield  
Council Liaison Susie Stevens

IV. Citizen's Input:

V. City Council Liaison's Report:

VI. Consent Agenda:

A. Approval Of Minutes Of February 11, 2013 Meeting

Documents: [February 11, 2013 Minutes.pdf](#)

VII. Public Hearing:

A. Resolution No. 250

**Wilsonville Family Fun Center: Ben Altman, SFA Design Group - representative for Wilsonville Land Partnership and Darren Harmon, Wilsonville Family Fun Center - Owner and Applicant.** Modify condition PDB3 in case file DB12-0071 - Stage II

Development Plan to address specific notice and process related issues for the 25 special all-night events for a zip line attraction. The site is located at 28855 SW Parkway Avenue on Tax Lots 100 and 109, Section 14D; T3S R1W; Clackamas County; Wilsonville, Oregon. Staff: Amanda Hoffman and Blaise Edmonds.

Documents: [Fun Center 3.11.2013 DRB Packet.pdf](#)

B. Resolution No. 248

**Old Town Single Family: Mark and Darla Britcliffe - owner/ applicant.** The applicant is requesting approval of a Site Design Review for two (2) single-family dwellings with attached accessory dwellings ~~and a Type A Tree Removal Permit for two trees~~. The subject parcels are located at 9445 9155 and 9185 SW 4<sup>th</sup> Street on Tax Lots 500 and 501, Section 23AC; T3S-R1W; Clackamas County; Wilsonville, Oregon. Staff: Amanda Hoffman

Case Files: DB13-0002 - Site Design Review  
~~TR13-0006 - Type A Tree Removal Permit~~

Documents: [Old Town Houses SR . Exhibits.pdf](#)

C. Resolution No. 249

**Boones Ferry Pointe - Carls Jr Restaurant and Multi-Tenant Commercial Building:**





**VI. Consent Agenda:**

- A. Approval of minutes from February 11, 2013 meeting**

**Wilsonville City Hall  
29799 SW Town Center Loop East  
Wilsonville, Oregon**

**Development Review Board – Panel A  
Minutes–EXCERPT Resolution No. 243  
February 11, 2013 6:30 PM**

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**I. Call to Order**

Acting Chair Lenka Keith called the meeting to order at 6:30 p.m.

**II. Chair’s Remarks**

The Conduct of Hearing and Statement of Public Notice were read into the record.

**III. Roll Call**

Present for roll call were: Lenka Keith, Mary Fierros Bower, Simon Springall, Ken Ruud, and Jerry Greenfield. Councilor Liaison Susie Stevens arrived after Roll Call.

Staff present: Blaise Edmonds, Barbara Jacobson, Nancy Kraushaar, Michael Wheeler and Amanda Hoffman.

New Development Review Board (DRB) Panel A members Ken Ruud, Jerry Greenfield and Simon Springall briefly described their reasons for joining the Board.

**Chair Keith** noted Councilor Liaison Susie Stevens was not expected until after 7 p.m.

**IV. Citizens’ Input** This is an opportunity for visitors to address the Development Review Board (DRB) on items not on the agenda. There were no comments.

**V. City Council Liaison Report**

This agenda item was addressed following the public hearing on Resolution No. 243.

**Councilor Stevens** reported that on February 4, 2013, City Council:

- Approved amendments to the Parking Ordinance to allow motor homes and trailers to park overnight on the street for one night, providing travelers extra time to unload their vehicles after returning from a trip.
- Discussed numerous possible uses for the Visitor’s Information Center building located across from Goodwill during work session. Discussion about the issue would continue.
- Approved the hiring of a consultant to work on ways to streamline Smart Transit operations in the city, including evaluating Dial-a-Ride routes and every aspect of the public transit system to make it more efficient. Prior to her joining the Council, state and federal grant money was awarded to the City for the Transit Integration Project, which was now ready to be implemented.
- Awarded an engineering contract to Wallace Engineering to begin work on the rebuilding of the Memorial Park parking lot. The project is expected to come before one of the DRB panels for review.

Councilor Stevens noted the next City Council meeting was scheduled for February 21, 2013. She welcomed the new DRB members.

The Board continued to Public Hearing Item B. Resolution No. 244 at this time.

**VI. Election of 2013 Chair and Vice-Chair:**

- Chair

**Simon Springall nominated Mary Fierros Bower for the 2013 DRB-Panel A Chair. Lenka Keith seconded the nomination. Mary Fierros Bower was unanimously elected as 2013 DRB-Panel A Chair.**

- Vice-Chair

**Mary Fierros Bower nominated Lenka Keith for the 2013 DRB-Panel A Vice-Chair. Ken Ruud seconded the nomination. Lenka Keith was elected as the 2013 DRB-Panel A Vice-Chair by a 4 to 0 to 1 vote with Lenka Keith abstaining.**

## **VII. Consent Agenda:**

- A. Approval of minutes of November 15, 2012 meeting

*-Note: Due to a lack of quorum to approve minutes from the November 15, 2012 minutes in the normal fashion, staff has attained signatures of approval from all attendees. The board is asked to recognize those signatures as valid and therefore adopt those minutes as approved*

**Simon Springall moved to adopt the November 15, 2012 DRB-B minutes as approved. Mary Fierros Bower seconded the motion, which passed unanimously.**

## **VIII. Public Hearings:**

- A. **Resolution No. 243. Wilsonville Family Fun Center: Ben Altman, SFA Design Group – representative for Wilsonville Land Partnership and Darren Harmon, Wilsonville Family Fun Center – Owner and Applicant.** The applicant is requesting approval a Modified Stage I Master Plan, Stage II Development Plans, Site Design Review, Waiver and Type B Tree Removal permit for Wilsonville Family Fun Center to allow a 74’ support tower for a zip line ride and minor associated site modifications. The site is located at 29111 SW Town Center Loop West on Tax Lots 100 and 109, Section 14D; T3S R1W; Clackamas County; Wilsonville, Oregon. Staff: Amanda Hoffman

Case Files: DB12-0070 – Modified Stage I Master Plan  
DB12-0071 – Stage II Development Plan  
DB12-0072 – Site Design Review  
DB12-0073 – Waiver  
TR13-0002 – Type B Tree Removal Permit

**Chair Keith** called the public hearing to order at 6:38 p.m. and read the conduct of hearing format into the record. All Board members declared for the record that they had visited the site. No board member, however, declared a conflict of interest, bias, or conclusion from a site visit. No board member participation was challenged by any member of the audience.

**Amanda Hoffman, Assistant Planner,** announced that the criteria applicable to the application were stated on page 2 of the Staff report, which was entered into the record. Copies of the report were made available to the side of the room.

**Ms. Hoffman** presented the Staff report via PowerPoint, entered into the record as Exhibit A3, with these key additional comments:

- She entered the letter submitted by Holland Partner Group dated February 11, 2013 into the record as Exhibit D1. The letter had been emailed to the Board members earlier that day.
- The Family Fun Center proposed a zip line tower of 74 ft, which would be located east of Interstate 5 south of the new Jory Trail apartment complex and north of Town Center Loop West.

- She noted the applications regarding the request, including the requested waiver to the 35-ft height limit in the PDC-TC Zone for the 74-ft zip line tower. The Type B Tree Removal Permit would allow removal of two trees for additional equipment to be located next to the tower.
- The tower is proposed to be 20 inches in diameter and tan in color to match the building.
- A photo simulation of the tower indicated the lines that run from the tower down to the beginning of the chair lift and the support guide wires that extend east into the parking lot. The simulation also depicted how the tower would appear in relation to the trees around the site. The photo simulation illustrated the view seen from the bank across Town Center Loop approximately 325 ft away.
  - Another slide simulated how the site would appear from a viewpoint further east along Town Center Loop toward the edge of the bank parking lot.
  - Another slide simulated what the site would look like with the apartment complex in the background, showing the trees behind the apartment complex. Photos at the bottom of the slide showed how the site appears today and what it would look like with approval of the application.
- She displayed a height comparison showing different places in the city with elements at the height limit of the proposed tower or higher. The Wilsonville Business Center towers were approved in 1988 with heights of 130-ft, 100-ft, and 80-ft. The 76 Station sign was approved at 75 ft in 2010. The movie theater tower was approved at 54 ft.
- The Applicant's argument in support of the request is a slim design, much like a cell tower, taller trees to the north that provide a good screening and buffering to the 74-ft tower. Tree heights to the north vary, but can be more than 100 ft with some adjacent trees being 90-ft tall.
- The entertainment use was compatible with the Town Center zoning. The Applicant would allow the Tualatin Valley Fire and Rescue to use the tower for safety training as well.
- The tower's height was determined by the clearance needed for the chair above the existing entertainment building. An illustration showing the dimensions was presented that indicated how the Applicant determined that 74 ft was necessary.
- When the Family Fun Center was approved in 1992, City Council restricted operation of the Fun Center batting cages to 10 p.m. due to the noise the facility created for residents of the Thunderbird Mobile Home Park. City Council also prohibited outdoor music and paging in 1992.
- Exhibit D1, the letter from Holland Partners, identified three requests: including a 10 p.m. restriction on operation of the zip line; the need for trees to provide additional screening, which was discussed with the Applicant; and addressing the outdoor music and paging, which was currently an ongoing issue even though City Council had prohibited it. These issues would need to be discussed with the Applicant.
- Otherwise, Staff recommended approval with conditions, including the 10 p.m. restriction for the zip line. She noted additional slides with more details about the tree issue were available.

**Simon Springall** asked how the tree screening and noise issue would be negotiated.

**Ms. Hoffman** replied it would be difficult to provide screening anytime soon. With a 74-ft tower, trees would take a number of years to grow tall enough to provide screening. At that point in time, it is unclear what the Family Fun Center's plans will be regarding its featured attractions. Trees may or may not help the situation. Planting a few trees to provide for potential screening in the future, or how tall the trees to be planted must be needed to be discussed with the Applicant. The Board needed to decide if the Applicant would be required to plant the trees.

**Mary Fierros Bower** asked about the zip line ride, how and how many times an hour it would operate; where people would queue up for the ride, etc.

**Ms. Hoffman** replied that some questions would have to be addressed by the Applicant who had more detailed information. She understood people would get on the ride near the bumper boats inside the gates



and go up with their backs facing the tower. Once riders reach the tower, they would come back down facing forward.

**Ken Ruud** stated that the outdoor music and paging issue was interesting. He visited the site and asked about the speaker locations. He noted some speaker systems would be needed to give people instructions about the rides. He confirmed that paging referred to paging specific people over the intercom and asked

**Ms. Hoffman** answered yes, according to the condition. She could not find that this requirement had been overturned at any point.

**Simon Springall** asked to see the additional slides about the tree screening.

**Ms. Hoffman** first displayed a slide highlighting the City Council conditions, which were included in the Staff report.

- Next, she presented the additional slides indicating two areas where an additional one or two trees might be placed. The trees would likely need to be evergreen.
- One area with deciduous trees created a gap, providing very little screening. She displayed the view from the apartment complex with deciduous, not evergreen, trees and a straight view toward the zip line tower's proposed location. Another area was at the end of the apartment complex where the windows had a sight line right through a gap.
- She was concerned that the areas available to provide trees were on the Holland property and would require negotiation between the Family Fun Center and Holland to plant new trees.

**Chair Keith** asked the height of the apartment building's top floor.

**Blaise Edmonds, City Manager, Current Planning Director** replied the peak of the building is about 45 ft, so each story is approximately 9 ft high.

**Mr. Springall** noted Staff's recommendation was to approve with the recommended conditions of approval. He asked what exactly the recommended conditions are.

**Ms. Hoffman** replied the approval would include all the conditions listed on Pages 4, 5, 6, and 7 of the Staff report. She noted that under the conditions of approval, the Building Division and Natural Resources did not have comments. She had missed including the table from engineering which stated that department did not have any comments either.

- She clarified that the trees discussed in the additional slides were not in Staff's conditions, but were mentioned in Exhibit D1, the letter Staff received from Holland Partner Group, which she read into the record.

**Mr. Springall** believed the conditions outlined in Holland's letter made sense. The 10 p.m. restriction was already in effect by Code, and while there seems to be a precedent of playing music, it made sense that the volume should be low to avoid disturbing neighbors. He asked if the ideas of adjusting the speaker placement and keeping the volume low could be expressed in Code.

**Chair Keith** stated that should be addressed with the Applicant.

**Ms. Hoffman** stated the problem was that the music playing is prohibited by the City Council decision and, therefore, could not be allowed by the Board.

**Mr. Ruud** confirmed that the issue regarded enforcement versus anything discussed at DRB and would be completely different than the subject application.

**Ms. Hoffman** added that after hearing from the Applicant, they could discuss having Holland meet with the Family Fun Center to negotiate the type of trees that would be placed and determine what was reasonable.

**Chair Keith** called for the Applicant's presentation.

**Ben Altman, SFA Design Group, 9020 SW Washington Square Dr, Portland, OR 97223** representing the Fun Center introduced Fun Center General Manager Darren Harmon to respond to any specific operational questions. He thanked Ms. Hoffman for her extra help with the somewhat unique application being a request for a height waiver for a specific use.

- The Applicant has reviewed the Staff report and generally agreed with the findings and conclusions except for some minor adjustments on one of the conditions.
- He noted the application also included a Master Plan update that provides for future anticipated improvements and modernization of the Fun Center over the next several years. These improvements would be subject to subsequent design review for the specific actions when that stage is reached.
  - One key improvement involved removing the batting cage and expanding the entertainment building to create more indoor space. The batting cages are outdated and not one of the more popular activities.
  - The Applicant also anticipated expanding the building to the north where the patio and large umbrellas currently exist, to expand the dining room area. These revisions would return to the Board with specific applications at some future date. The Applicant believed these expansions were consistent with the general recreational entertainment characteristics of the Fun Center that have already been approved within the context of the Town Center.
- In the present Application for the zip line, the primary issue the Applicant sought to address was the height, requesting a waiver for the height to exceed the 35-ft standard. This was a unique request because the design of the facility requires distance and height to make it work. The only way this could be achieved was to provide the start point inside by the boat pond and extend the cables and the tower outside into the parking lot.
- He understood someone on the Board had a question about the original intent of the walled in area versus the parking lot. He explained the general intent of the walled area in the original application was to control the entry points to the entertainment options or features, providing some type of crowd control area so visitors come into the building and then back out to the entertainment area.
  - Although the facility required that the tower be extended outside, the access remains inside the wall, which was consistent with the original layout. If there was a concern about the design setting precedence, he noted this had never been an issue during his time in Wilsonville, as the City reviews every application on a case-by-case basis and considers circumstances for anything similar that might come up in the future. At this point, the Applicant did not anticipate a similar structure. The other planned improvements were mostly with the building itself and activities that would take place inside.
  - The other controlling aspect was the City's landscaping and parking requirement. The Applicant was right at the threshold for the parking requirements, and the landscaping requirements were fulfilled. There was not much room on the site to add more parking, so the Applicant was locked into the remaining area unless more property could be acquired, but none was available.
  - If any issue were to arise, it would come back for review by the Board.
- Another issue raised was the fact that the tower is not lighted. While an FAA beacon would be installed on top as required by Federal Aviation, the Applicant has considered, but decided against lighting. Lighting was not needed for safety or security because access to the zip line was internal to the center like all other events and activities that take place at the facility. Participants on the ride would only be outside for a few seconds. Existing ambient lighting from the parking lot, apartments

to the north, and street lights would be sufficient to light the area. Anyone in a zip line chair would be looking down at the lighted parking lot.

- The only safety issue he could think of was if a cable were to get hung up, which would prompt a need for fire and rescue, who would have spotlight provisions.
- Working with the City's lighting standard, the Applicant did not want to push the limits and add unnecessary lighting.
- One issue raised by Holland included outdoor music.

**Darren Harmon, General Manager, Family Fun Center**, understood there was a 100-ft limit from the wall for the placement of amplified sound devices. The sound device was facing north now because previously, the area to the north was unoccupied. When Brenner (Daniels) came by, the Applicant had already removed all the northward-facing sound devices as well as those located on the golf course.

**Mr. Altman** stated the Applicant could work with Staff if an enforcement issue existed or a misunderstanding about that condition.

- **Mr. Harmon** had spoken with Holland Group about planting additional trees; however, not much space is available onsite to do much. The Applicant acknowledged the need to replace the two trees being removed, and the Applicant was willing to work with Holland to some extent, even if it involved working on Holland Group's side of the wall to provide additional screening.
  - The Applicant was concerned about setting limits to ensure the Applicant was not doing what Holland Group should have done in light of the fact that their facility was recently built knowing that the Fun Center was adjacent to their property. Holland Group could have planted more trees for screening themselves; however, the Applicant would not mind planting a couple trees to fill gaps in a few areas.
- The Applicant understood the 10 p.m. limit addressed the noise sensitivity of the residential area to the north, and did not have a problem with shutting the zip line down at 10 p.m. in general. However, historically, the Fun Center has had special event nights like Grad Night and Boy Scouts/Girl Scouts nights during which the center operates for extended hours in the summer. The Fun Center would like to maintain the extended hours during these events, especially now that the zip line would be a new attraction.
  - The Applicant understood that the sound of the bats hitting the balls was the main concern with the batting cages. The zip line would not have the same type of noise with exception of occasional screams.
  - The Applicant requested some flexibility for special event nights with extended hours beyond 10 p.m. The extended hour activities would take place approximately 20 to 25 nights during the year. The Applicant recognized the sensitivity regarding noise to the north. However, if the neighbors know about the events ahead of time and the events do not take place every night, the Applicant hoped that the extended hours would be allowed.

**Chair Keith** inquired about safety in the area of the parking lot; was there a way to prevent items like cell phones from dropping into the parking lot and on people's heads.

**Mr. Harmon** replied that the company that makes the zip line equipment also makes chair lifts for ski resorts; therefore, they do not have such procedures. The Fun Center would like to put something in place to address the issue. Participants on some of the current rides are required to empty their pockets and put their belongings in a secure box and remove flip flops. This would likely be part of the procedures for the zip line. As soon as an incident occurs, the Fun Center generally writes a procedure addressing the issue. Falling items was expected to be one of the procedures that the Fun Center would need to put into place.

**Mr. Ruud** asked if installing the zip line would put the Applicant over the parking requirement.

**Mr. Altman** replied the Applicant was still be within the minimum range. Currently, the facility had a couple parking spaces over the limit. While two spaces were being removed to put the tower in, they would remain within the required range by two or three spaces.

**Jerry Greenfield** acknowledged the contribution the Family Fun Center has made to Wilsonville for nearly two decades. He had concerns, but stated that the Applicant had addressed them very well.

**Ms. Fierros Bower** asked how much noise the ride would generate, how often it would go up and down and how fast.

**Mr. Harmon** replied that the ride is not very fast and would take approximately 45 seconds from beginning to end. The thrill was not the same as a typical zip line; however, the ride would be thrilling enough that people would want to go on the ride multiple times and bring their children. Passengers on the ride would be in a chair lift with double seatbelts controlled by a computer. The seatbelts, once engaged, would not reengage unless a signal was received from the ride's base.

**Mr. Springall** asked for a characterization of any mechanical noises associated with the ride.

**Mr. Harmon** responded that the ride is mostly quiet as an electric motor does the lifting. The area with the motor would be screened because it would be located on the golf course where people put. The stopping mechanisms have baffles, so the air brakes would not be audible. The zone in front of Holland's Building 6 was the only area where ride participants would be seen. Riders would face the freeway when going up on the lift chair, so voices would project that direction.

**Mr. Rudd** inquired about the car track's and bumper boats' hours of operation during special events.

**Mr. Harmon** replied that those attractions operate all night. Grad Nights involve 14 or 15 nights from late May to mid-June. The Fun Center might operate all night during these nights, depending on what schools attend. Fundraisers and church nights were also all night events that could last until 3:00 a.m.

**Chair Keith** asked how many back-to-back runs can be made on the ride in an hour.

**Mr. Harmon** stated he hopes to have at least 20 to 25 maximum runs in an hour.

**Mr. Altman** noted that getting in and out of the chair would likely take longer than taking the ride.

**Mr. Harmon** added the zip line would be one of the 11 attractions for kids to choose from during the late event nights. Protecting the Grad Nights was his primary concern. These events have a maximum of 600 students in the park spread across all the available attractions, except the batting cages due to safety reasons and the noise restriction. He did not anticipate a significant number of zip line rides taking place at the overnight events.

**Mr. Greenfield** understood the Applicant was already aware of the critical nature of the parking situation and that the Board would consider that closely. He asked if the Applicants had estimated the anticipated increase in their patronage.

**Mr. Harmon** stated if the Fun Center added the 25,000 square feet by removing the batting cages, more than likely, 12,000 square feet would be a footprint for an attraction housing 10 people. Right now, the Applicant had no plans for modifications, but wanted to do two things at once [update the Master Plan] because they were coming before the Board with the subject application.



- The Applicant knew the batting cages would eventually go away, and more indoor space is needed to stay competitive since many new entertainment facilities are very large and indoors. The newness of the Family Fun Center has worn off after 20 years and the Applicant would like to appeal to peoples' desire to play inside. As a result, they were considering a motion-based theater or miniature bowling alley. However, this would take up a large footprint without necessarily generating more parking or traffic. The aim was to allow guests to come inside when the park is slow during the winter, when people tend to go to indoor facilities.

**Mr. Greenfield** asked whether the expansion of Bullwinkle's would have a greater impact.

**Mr. Harmon** replied that part of the restaurant would likely be removed anyway to make room for something right now. It was not a 40-ft expansion of the restaurant because the facility needed to expand the game room.

**Mr. Altman** stated this would be addressed when the Applicant returned for those specific applications. Essentially, the parking requirement is driven by square footage relationships, and the Applicant would work with Staff because the Fun Center does not have a normal building footprint. There would be some tradeoffs of existing space for future or new space, and the Applicant would have to determine those net numbers.

**Mr. Greenfield** believed he had read the margin was more like fifty parking spaces being available.

**Mr. Altman** said he did not remember that part, but believed plenty of parking existed for now.

**Chair Keith** called for public testimony in favor of, opposed and neutral to the application. Seeing none, she closed the public hearing at 7:25 p.m.

**Councilor Susie Stevens** arrived at 7:25 p.m.

**Ms. Feirros Bower** inquired whether the Board wanted to add verbiage about having the Family Fun Center negotiate with Holland Group regarding the specifics of adding additional trees.

**Mr. Ruud** did not believe that was something to be addressed with the current application. Holland knew they were building next to an existing amusement park and those choosing to live there were aware of the surroundings.

**Mr. Springall** agreed nothing specific needed to be added to the application; however, the Applicant stated he would be happy to negotiate with Holland about the placement of additional trees. The Applicant and Holland could come up with a reasonable solution without the Board making it a condition.

**Mr. Ruud** agreed the two parties should be left to negotiate, and the negotiations should not be required.

**Chair Keith** did not believe any screening would be effective for many years.

**Mr. Ruud** asked about the Staff's recommendation on noise. He had asked about the bumper boats and cars because they tend to be attractions that generate noise from excited participants. If additional people were on the zip line, he was curious how much added noise would be generated. With no existing time limit restrictions on the existing rides, he asked if restrictions should be imposed for the new ride.

**Mr. Edmonds** stated when this project was reviewed years ago, there was always ambient noise from the freeway. The Applicant had been conditioned to conduct a sound study a year later and the Fun Center

was found to be within the State decibel requirements for being next to a residential area. It was difficult to distinguish the ambient noise of cars and trucks on the freeway, which is noisier when wet, from the noises coming from the batting cages and go karts, which are on a lower plane. The Applicant was conditioned to construct a 10-ft concrete wall to provide a noise buffer. The wall was pretty effective, but at that time, City Council was concerned about the paging system and loud music. He did not have the evidence in front of him to verify Mr. Harmon's testimony about sound equipment being a certain distance from the wall.

- He noted that during his years with the City, the Applicant has operated a very good business and has had very few complaints. In the beginning, there were concerns about sound and complaints about litter being thrown over the wall, but the Applicant has always been responsive and tried to be a good neighbor to the previous Thunderbird Mobile Park residents. He did not believe those efforts would stop with the new residents.

**Mr. Springall** inquired about Staff's recommendation that the park impose a 10 p.m. limit on the ride; however, remaining open late during special events appeared to be an important part of the Applicant's business.

**Mr. Edmonds** replied that traditionally Staff honors previous DRB and Council decisions. The condition was imposed on the batting cages due to the piercing sound of pinging metal bats. The Board could decide if the Applicant's proposal was reasonable and adjust that condition.

**Mr. Springall** asked if specifying that a 25-night maximum per year be allowed for extended hours would be acceptable.

**Mr. Edmonds** explained the hearing would need to be reopened if the Board wanted to discuss that proposal with the Applicant.

**Chair Keith** asked if the issue could be reassessed after the ride had been operating.

**Mr. Springall** noted the Applicant would not be able to operate the new ride during Grad Night and the zip line would be the big appeal.

**Ms. Jacobson** advised that the Board could make a change, or approve the recommendation in the Staff report as presented. The Applicant could return or be invited to return after some period of time to offer testimony as to why that issue should be reopened. If the Board made the decision to open the park for the specified nights, it would be a binding decision.

**Ms. Fierros Bower** supported that proposal.

**Mr. Ruud** inquired whether the Applicant could give neighboring properties advance notice seven or twenty-one days prior to those nights with extended hours.

**Mr. Edmonds** stated that because the late nights are consecutive, there would have to be advance notice of the grouping of the upcoming late nights.

**Chair Keith** reopened the hearing at 7:36 p.m.

**Mr. Springall** asked the Applicant if changing the condition to allow 25 nights per year of extended zip line operation would be acceptable if advanced notice of those nights was given to the neighbors to the north.

**Mr. Harmon** asked it be no less than 25 nights because right now, the Fun Center was pushing that number due to the Grad Nights and a few other events already scheduled. He and the Jory Trail property manager have spoken often and he has invited the manager to contact him with any problems. He agreed to 25 nights and would notify the neighbors in advance of those nights. He noted that Holland has a newsletter that is sent to their residents. A calendar could be used to notify about the June nights. The remaining late nights would be sprinkled throughout the year

**Mr. Springall** clarified the proposed 25-night limit would apply to the zip line because the other operations of the Fun Center would be unaffected.

**Mr. Ruud** noted the line of sight and noise would not be an issue when the facility operates after 10 p.m. because of the blocking; however line of sight and the potential for screaming and more noise could be an issue with the zip line on special event nights.

**Mr. Altman** reminded that the batting cages were not operating beyond 10 p.m. at all due to the prior condition.

**Mr. Harmon** added the music does not operate on Grad Nights either; it did when no neighbors were to the north. No overnight music was played when the Thunderbird Mobile Park was there, nor was the instructions announced for go karts. Now that residents live adjacent to the Fun Center, the facility would resume its prior operations. Go kart safety instructions could be verbally given while students are on the bus.

**Chair Keith** asked whether all night operations would be allowed for the 25 nights per year.

**Mr. Harmon** asked that a time limit not be placed on those nights because closing times vary.

**Mr. Edmonds** confirmed that no City Ordinance limits business operations at night; however a noise ordinance does exist; but it was difficult to administer. Determining what residents here is difficult because Staff must stand behind the wall as they did not have permission to stand on someone's third story balcony at 3 a.m. Administrating the noise ordinance is based on complaints. If enough complaints are received about noise, Code Enforcement would contact Mr. Harmon and determine how to mitigate it.

**Mr. Springall** commented that the noise would still be subject to the City's noise ordinance in all cases.

**Chair Keith** confirmed with Mr. Edmonds that there was nothing to stop the neighbors from filing a formal noise complaint. If the zip line is operated 25 nights a year, the neighbors could always object to the noise if there was a problem and return to negotiate shorter hours.

**Mr. Ruud** added it would not be valid if the ride did not comply with the noise ordinance.

**Mr. Springall** did not believe the noise of the zip line could be significantly greater than the noise of the go karts, which are pretty loud and run all night. He asked if any noise complaints had been received about the go karts or the water.

**Mr. Altman** explained that even at night, the freeway generates ambient noise that could be heard over the go karts.

**Mr. Harmon** stated that before the residents moved in, he listened from the third floor balcony of the apartments and could hear the occasional go kart, but mostly the freeway. The sound barrier erected along

the freeway has become a tunnel for the freeway sound instead of minimizing it. Sound study technicians in the past have been unable to get a baseline because the freeway is so loud.

**Chair Keith** believed the ride should be allowed to operate at extended hours 25 nights a year, and if it became a problem, it could be addressed at that point.

**Chair Keith** closed the hearing at 7:44 p.m.

**Mr. Greenfield** noted that given the two decade-long relationship, he considers the Fun Center to be a partner with the City.

**Jerry Greenfield moved to amend the Staff report to include Exhibit A3, Staff's PowerPoint, and Exhibit D1, the letter dated February 11, 2013 from Holland Partner Group. Chair Keith seconded the motion, which passed unanimously.**

**Chair Keith moved to adopt Resolution No. 243 accepting the Staff report with the modification of Condition PDB 3 to allow zip line operations 24 hours a day for a maximum of 25 nights a year with notification to surrounding neighbors and property owners. The motion was seconded by Ken Ruud and passed unanimously.**

**Chair Keith** read the rules of appeal into the record.

The Board returned to the Council Liaison Report at this time.

- B. Resolution No. 244. Ron Tonkin Gran Turismo: LRS Architects. – Representative for RTGT Properties LLC DBA Ron Tonkin Gran Turismo – Applicant/Owner.** The applicant is requesting approval of a Stage II Final Plan, Site Design Review, Type C Tree Removal Plan and Master Sign Plan Modification for remodel and expansion of an existing automobile dealership. The site is located on Tax Lots 200 and 300, Section 02A; T3S R1W, City of Wilsonville, Washington County, Oregon. Staff: Michael Wheeler

Case Files:           DB12-0060 – Stage II Final Plan  
                          DB12-0061 – Site Design Review  
                          DB12-0063 – Type C Tree Removal Plan  
                          DB12-0068 – Master Sign Plan Modification

This agenda item was addressed following the City Council Liaison Report.

**Chair Keith** called the public hearing to order at 7:54 p.m. and read the conduct of hearing format into the record.

**Mary Fierros Bower** declared a conflict of interest because the Applicant's representative, LRS Architects, was her employer. She stepped down from the dais.

All remaining board members declared for the record that they had visited the site. No board member, however, declared a conflict of interest, bias, or conclusion from a site visit. No board member participation was challenged by any member of the audience.

**Michael Wheeler, Associate Planner**, announced that the criteria applicable to the application were stated on page 2 of the Staff report, which was entered into the record. Copies of the report were made available to the side of the room.



Mr. Wheeler presented the Staff report via PowerPoint, the exhibits of which were included in the meeting packet, with the following key comments:

- The displayed exhibits could be referenced on pages 12 and 13 of the Staff report.
- The application's four components included a Stage II Final Plan for a proposed remodeling to expand the existing automobile sales and service facility; Site Design Review to consider the design of the improvements, circulation, landscaping, required on-street parking; Type C Tree Removal Plan for the removal of 12 trees, and modifications to an existing Master Sign Plan for the proposed improvements.
- He noted two corrections to the Staff report:
  - The last sentence of Finding A28 on Page 20 of 45 was corrected to state, "...property line (~~Exhibits~~ *See Conditions* PFA 8, PFA 20 and PFA 21)."
  - The first sentence of Finding D7 on Page 36 of 45 was corrected to state, "Master Sign plan (Case File DB42 05-0006;..."
- The application received several comments from City divisions as well as Tualatin Valley Fire and Rescue (TVFR), which were included in the Staff report, and where appropriate, conditions of approval were recommended for the application.
- The application was to increase the floor area of an existing facility to approximately 22,500 square feet. Parking will be reconfigured. Circulation will be largely the same, but the improvements to the facility would be more modern and better for the environment. As a result, the proposal included an expansion of the showroom facility at the west end of the existing building, an enlargement of the service area to the east, outside waste and recyclable storage that would be covered, and an enclosed, locked storage area for fuels and other fluids.
- He displayed maps showing surrounding parcels in the vicinity of the existing facility and site of the proposed improvements off Parkway Avenue with Elligsen Road to the south and Interstate 5 to the west.
- The rendering on the cover page depicted the prominence of the proposed architectural alterations to the structure.
- All the exhibits discussed had been included in the record and He reviewed the items addressed in the Staff report with these key comments:
  - The site includes a loop drive with two access points on Parkway Avenue, which would remain in place following some modifications to the site in the form of demolitions. Parking east of the loop drive is very limited, and circulation through the drive was expanded in the northeast area. Most parking in the past had been into opposing bay areas on the driveway that takes drivers back out to Parkway Avenue.
    - The Applicant's revisions called for new parking to the south and east, which would be screened from view and would be reserved for employee parking or service parking that would be used during the day and vacated at night.
  - The Utility Plan illustrated the extent of paving improvements that would be made in replacing the driveway system and adding the parking. The surface would be pervious rather than impervious and would funnel the runoff through the layer of proposed pavement into catch basins directing it to a treatment facility along the south edge of the property and finally to the street through a weep hole at the curb. The northern portion of the site was always served by storm drainage that flows to the north along the north property line and out to an existing storm drainage system flowing to the west.
  - The Applicant proposed to remove 12 trees in the process of constructing the improvements, three of which were in the vicinity of the front street trees. The area would be graded and built up for a terrace to display vehicles. The 12 trees will be removed or relocated and then mitigated with the Applicant planting 31 trees, which would enhance the landscape significantly.

- The Applicant intends to meet the requirement that inventory stored onsite be screened from view offsite. A stretch of landscaping would act as a screen to buffer the view from the west. Plantings along the east boundary would serve the same purpose for the public site owned by TVFR.
- The Demolition Site Plan showed the removal of asphalt, replacement of the parking and resulting circulation.
- Two areas of expansion were proposed, one to the showroom in the southwest corner of the building, and a service-related expansion in the southeast vicinity. A plaza or terrace would be built up at the southwest corner of the building for the display of vehicles and a canopy would enhance its appearance. Two other minor areas of improvement were the fluid storage on the east side of the property, and the covered waste and recycling facility accessible to hauling utilities directly south of the fluid storage.
- Current elevations of the structure, proposed demolition changes and the existing location of the Grand Prix Import sign on the south wall of the facility were displayed. In the proposed remodel, the storefronts would be rebuilt and enhanced, and the canopies would be extended along the southwest corner and on the west side of the building. The signage would be relocated farther west.
- Code requires that when changes are made to electrical lighting facilities beyond a certain threshold, all must be brought up to current Code. The City adopted the Dark Sky Ordinance about three years ago to prevent lights from shining on the stars and clouds; to be available only when needed and then diminished or shut off by a certain time at night. The Applicant chose one of two methods for implementing the outdoor lighting ordinance. The proposed lighting levels were displayed. The Applicant was not proposing as much illumination as allowed; therefore, the lighting levels would meet the Code requirements.
- The Applicants provided a color and materials board, which was represented in the prospective drawing and another, more elevated drawing to the southwest. [of the southwest elevation] The Applicant could provide more detail about the materials and their use.
- One graphic showed all the components involved with the modifications to the Master Site Plan.
- He reviewed the existing and proposed placement of the signage shown on Exhibit A401. The proposed signage, would be placed farther west on the fascia, as well as the service sign. The new monument sign would consist of the pair of monuments approximately 15 feet tall and be placed just off the terrace that would be built along Parkway Avenue.
- He reviewed Staff's four minor areas of concern with the application, noting that conditions of approval had been recommended for each area to bring the proposal into full compliance with the criteria, except that regarding the exterior display.
  - In the site design, the curb line for the parking spaces along the east edge and the three spaces along the south edge were five feet from the east and south property lines. Code requires a minimum of seven feet of landscaping from the property line when parking overhangs the curb, to avoid interfering with potential sidewalks. Because the Applicant only has five feet, wheel stops must be placed in each pair of parking spaces to prevent vehicles from hanging over the curb. Otherwise, the Applicant could redesign the parking areas to move cars away from the east and south property lines to comply with the requirement.
  - The monument sign was being moved from its former location and rebuilt into what appeared to be a pair of monument signs. The proposed sign area would comply with the approved sign area amounts of the previous approval. Only one monument sign was approved in the Master Site Plan.
  - Staff assumed that the area beneath the canopy on the west side of the building would be used for outdoor display. An exterior sales display opportunity is built into the Code; however, the area must be fully covered. In the prospective drawings, the placement of vehicles for display on the plaza would not comply. Because the Code governs compliance, and not a condition of approval, Staff simply noted the requirement.

- Staff disagreed with the Applicant's calculation of four bicycle parking spaces as a minimum is required for each type of use specified in the Parking Standards, resulting in a total of eight bicycle parking spaces being required. The Applicant has proposed one area for bicycle parking, but other areas might be available to split the eight required spaces up.
- He concluded, stating that Staff recommended approval of the four components of the application with conditions.

**Chair Keith** called for the Applicant's presentation.

**Daniel Drake, LRS Architects, 720 Northwest Davis, Portland, Oregon 97209**, thanked Staff for their presentation and for working with the Applicant.

- He addressed the architectural portion of the building's exterior, noting the expansion of the existing automotive facility to increase some presence along the frontage, beautifying it with new metal panels and a two-story jewel box. The Applicant also wanted the opportunity to do some exterior display, as noted by the City Code, up to five percent and allowable by 10 percent of the DRB. Elements of glass and light metals were used to lighten up the building. The terracing was also extended around the corner and brought down through the plaza area and landscaping was added to soften the edge. The building would open from the inside via a couple doors to create a plaza area in the summertime to allow customers to interact on the outside, such as during private dinner functions.
- The service bay was enlarged by creating a new service area. In doing so, the parking had to be reconfigured. As a result of working with Staff, some impervious surface was removed, and a pervious paving area was created to control a lot of the storm water onsite and create less impact to the City's sewer system. Landscaping would also be increased, which would also help absorb storm water. The service area would be screened to reduce the impact of the visual implications commonly associated with an auto dealership in the neighborhood. Employee parking would be vacated at night as well as customer parking.
- Through the requirements of the Dark Skies Ordinance, LED lighting would be used to reduce energy impacts on the facility and also reduce the light level at night and the impact to the city.
- The Applicant would like to work with Staff to address the vehicle overhang in the parking areas on the east and south property lines by considering either low rubber bumpers for low exotic cars or to relocate the curb westward to obtain the seven-foot clearance from the property line.
- Staff's comments about the bicycle parking had been incorporated into the drawing, and the Applicant would address those comments on their resubmittal.
- The Applicant reviewed the previous conditions of approval for the facility as far as the building sign and site sign through the Master Sign Plan. The Applicant was working to incorporate the dual totems onto a single pylon to create a more uniform, one sign approach. The Applicant ensured the sign would remain within the area and height requirements based on the previous Master Sign Plan, and therefore, was requesting a modification to the plan.
- A foam board exhibit was displayed to more clearly illustrate that vehicles could be accommodated underneath the canopy overhang. The exhibit was a modification of Exhibitb41. He noted the staging area might be moved further away from the building; consequently the overhang might need to be increased about two feet to accommodate complete overhead coverage. The Applicant would like to work with Staff and the owner to determine that increase and ensure the overhang would still comply with the allowable percentage area of exterior display, which would be 10 percent as approved through the DRB.
- He concluded by offering to answer any questions.

**Chair Keith** asked if any lighting intrusion impact was anticipated on hotel property.

**Mr. Drake** replied the Applicant hired a lighting designer at MFIH to go through the site. The LEDs have incorporated cutoff shields, so light would cut off at the property line. Additional landscaping would be

added to the existing landscaping along the south property line as noted in one of the exhibits included in the packet. Additional lighting would be used through the canopies within the incorporated seeding/landscaped tiered area to shine down onto the walkways for safety purposes. Impervious pavers were being incorporated in that area as well. This added lighting would ensure people are stepping in correct areas.

**Chair Keith** called for public testimony in favor of, opposed or neutral to the application. There was none.

**Simon Springall** stated he was slightly confused about the exterior sales limit, whether it was 5 percent or 10 percent and if the application was in compliance.

**Mr. Wheeler** deferred the question to the Applicant as he was unaware where the numbers were specifically calculated in the Applicant's narrative. He stated that the displayed illustration seemed to be a revision of an earlier drawing in which two vehicles were depicted beneath the canopy, which would satisfy the requirement of outdoor display of merchandise. The vehicle on the west side of the building had not appeared to be beneath the canopy, which was likely the reason for caution. He did not know whether the canopies would exceed five percent of the total floor area.

**Mr. Drake** apologized for not having the exact number for percentages; however, the canopies' area was below five percent. The Applicant should still be below five percent if the lower canopy on the west side had to be extended two feet. He clarified that the DRB has the authority to increase the maximum to 10 percent. He was not 100 percent sure the Applicant would not need to increase that limit, but he would get the exact number for Staff. Looking at the size of the buildings for the canopies, it appeared the proposal would be below five percent, but that would need to be verified. If the DRB approved the 10 percent, the Applicant could work with Staff. He emphasized the Applicant was not proposing to increase the percentage any more, except for the two feet to cover the vehicles as requested.

**Jerry Greenfield** asked if the Applicant planned to extend the canopies to the north.

**Mr. Drake** answered no. The canopy on the west side would be extended two feet as Staff requested.

**Blaise Edmunds** entered the foam board exhibit displayed by the Applicant into the record as Exhibit B42.

**Mr. Drake** clarified the new exhibit only showed that vehicles were able to fit under the canopy; no canopies had been extended.

**Mr. Ruud** confirmed the Applicant would work on adding the required bicycle spaces and would work with Staff on extending the vehicle overhang beyond curb, and the exterior sales display limits.

**Mr. Drake** agreed the Applicant was willing to work with Staff on all the noted items. The Applicant had no issues with the conditions.

**Mr. Greenfield** complimented the Applicant's design, adding it was a handsome plan overall.

**Chair Keith** believed the proposal would be a great improvement. She confirmed there were no further questions and closed the public hearing at 8:32 p.m.



**Chair Keith moved to amend the Staff report adding Exhibit B42, the foam board displayed by the Applicant showing that vehicles could fit under the west canopy of the building, and including the following corrections:**

- The last sentence of Finding A28 on Page 20 of 45 should state, "...property line (*See Conditions* PFA 8, PFA 20 and PFA 21)."
- The first sentence of Finding D7 on Page 36 of 45 should state, "Case File DB05-0006;..."

**Ken Ruud seconded the motion, which passed unanimously.**

**Simon Springall moved to adopt Resolution No. 244. The motion was seconded by Jerry Greenfield and passed unanimously.**

**Chair Keith** read the rules of appeal into the record.

**IX. Board Member Communications:**

- A. Meeting notes from January 28, 2013 DRB Panel B Meeting

**X. Staff Communications:**

There were none.

**XI. Adjournment**

The meeting adjourned at 8:37 p.m.

Respectfully submitted,

By Paula Pinyerd of ABC Transcription Services, Inc. for  
Shelley White, Planning Administrative Assistant

**VII. Public Hearing:**

**A. Resolution No. 250. Wilsonville Family Fun Center: Ben Altman, SFA Design Group – representative for Wilsonville Land Partnership and Darren Harmon, Wilsonville Family Fun Center – Owner and Applicant.** Modify condition PDB3 in case file DB12-0071 – Stage II Development Plan to address specific notice and process related issues for the 25 special all-night events for a zip line attraction. The site is located at 28855 SW Parkway Avenue on Tax Lots 100 and 109, Section 14D; T3S R1W; Clackamas County; Wilsonville, Oregon. Staff: Amanda Hoffman and Blaise Edmonds.

**DEVELOPMENT REVIEW BOARD  
RESOLUTION NO. 250**

**A RESOLUTION MODIFYING CONDITION OF APPROVAL PDB3 IN RESOLUTION NO. 243 FOR THE WILSONVILLE FAMILY FUN CENTER. THE SITE IS LOCATED ON TAX LOTS 100 & 109, SECTION 14D, T3S-R1W, CLACKAMAS COUNTY, OREGON. BEN ALTMAN, SFA DESIGN GROUP – REPRESENTATIVE FOR DARREN HARMON, WILSONVILLE LAND PARTNERSHIP – OWNER/APPLICANT.**

**RECITALS**

WHEREAS, this Resolution modifies Condition of Approval PDB3 in Resolution No. 243 which Resolution shall remain in effect but for the modification set forth herein; and

WHEREAS, the Planning Staff prepared a staff report on the above-captioned subject dated February 28, 2013; and

WHEREAS, said planning staff report was duly considered by the Development Review Board Panel A at a regularly scheduled meetings conducted on February 11, 2013 at which time exhibits, together with public testimony were entered into the public record; and

WHEREAS, the Development Review Board, upon the Applicant's request, voted to amend Condition PDB3 to allow for extended hours of operation for the Soaring Eagle Zip Line ride ("Zip Line") on up to 25 nights during any given calendar year and such amendment was incorporated into the staff report and adopted as part of Resolution No. 243; and

WHEREAS, following approval of Resolution No. 243 and following further discussions with City staff over concerns raised about the unknown and unstudied noise levels of the Zip Line operations and their impact on newly constructed adjoining residences, the Applicant voluntarily agreed to request that the DRB modify Resolution No. 243 to remove the DRB modification to Condition PDB3 concerning the hours of Zip Line operations and reinstate the original staff recommended 10 pm curfew, as set forth in the original Condition PDB3; and

WHEREAS, the Applicant has agreed to return to the original condition PDB3 in consideration of the agreement of the City's Planning Director to issue the Applicant a Class 1 Temporary Use Permit, waiving the application fee therefor, which will allow the Applicant the right to operate the Zip Line beyond 10 pm on up to 25 nights during 2013, subject to Applicant's Zip Line operations being in compliance with the City's Noise Ordinance, understanding that issuance of said Temporary Use Permit does not create any exception to compliance with the City Noise Ordinance; and

WHEREAS, the Planning Director has further agreed that the Applicant may continue to make annual applications for a similar Class 1 Temporary Use Permits in future years and the application fee will be waived; and

WHEREAS, Staff, the Applicant and other interested parties, if any, have had an opportunity to be heard on the subject of this modification.

NOW, THEREFORE, BE IT RESOLVED that the Development Review Board of the City of Wilsonville does hereby adopt the staff report, dated February 28, 2013, as amended and attached hereto as Exhibit A4, with recommendations contained therein including the revised PDB3, and authorizes the Planning Director to enforce condition PDB3 consistent with staff's original recommendation but subject to the Planning Director's discretionary issuance of a Temporary Use Permit to allow for extended Zip Line operation hours on up to 25 days during the calendar year, as described above and does hereby amend Resolution No. 243 with this Resolution No. 250. Except as set forth herein, Resolution No. 243 shall otherwise remain in full force and effect.

ADOPTED by the Development Review Board of the City of Wilsonville at a regular meeting thereof this 11<sup>th</sup> day of March 2013 and filed with the Planning Administrative Assistant on \_\_\_\_\_. This Resolution is final on the 15th calendar day after the postmarked date of the written notice of decision per *WC Sec 4.022(.09)* unless appealed per *WC Sec 4.022(.02)* or called up for review by the City Council in accordance with *WC Sec 4.022(.03)*.

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Mary Fierros Bower, Chair - Panel A  
Wilsonville Development Review Board

Attest:

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Shelley White, Planning Administrative Assistant

**EXHIBIT A4**

**DEVELOPMENT REVIEW BOARD MEETING  
STAFF REPORT**

<b>Meeting Date:</b> March 11, 2013 <b>Date of Staff Report:</b> Feb 28, 2013	<b>Subject:</b> Revisit condition PDB-3 to address specific notice and process related issues for the 25 special all-night events.  <b>Staff Members:</b> Blaise Edmonds, Manager of Current Planning and Amanda Hoffman, Assistant Planner
<b>Action Required:</b>	Modify Condition PDB3 of Resolution 243.
<input checked="" type="checkbox"/> Resolution – Public Hearing <input type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Consent Agenda	

**Staff Recommendation:** See the proposed condition PDB3 at the end of this staff report.

**PROJECT / ISSUE RELATES TO:** DB12-0071 Stage II Final Plan. The applicant proposes to install a 74 foot monopole to support a zip line amusement park ride.  
  
**OWNER:** Wilsonville Land Partnership (Family Fun Center)

**ISSUE BEFORE DRB:** Revisit condition PDB-3 to address specific notice and process related issues for the 25 special all-night events for a zip line attraction.

In Resolution No. 243 the DRB approved condition PDB3 as modified to read:

**PDB3.** The operation of the zip line amusement ride shall be prohibited between the hours of 10 p.m. and the regular opening time with the exception of 25 nights a year where the zip line is allowed to run 24 hours a day with prior notification to adjacent neighbors and property owners. (See Finding B19).

**Finding B19 in Exhibit A1: Subsection 4.118 (.03) E. Other Requirements or Restrictions:**

**B19. Review Criteria:** “Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may adopt other requirements or restrictions, inclusive of, but not limited to, the following:” Listed 1. through 12.

**Finding:** “These criteria will be satisfied by Condition of Approval PDB 3.”

**Details of Finding 19:** “Under 92PC05, Condition #30, staff finds that City Council made an additional condition of approval regarding the operation of the batting cage. Because of the noise sensitivity of the adjacent residential to the north the hours of operation were restricted. The previous condition prohibited operation of the batting cage between 10 p.m. and the regular opening time. Staff finds that the zip line amusement ride is a similar type-use related to noise and will be located much closer to the residential than the batting cage. Limiting the operation time will give assurance that this amusement ride will not be a nuisance to adjacent residents regarding noise. Therefore, staff finds that the operation of the zip line amusement ride shall be prohibited between the hours of 10 p.m. and the regular opening time. (See Condition of Approval PDB 3 and Exhibit A2)”

## **EXECUTIVE SUMMARY:**

On February 11, 2013 the Family Fun Center’s zip line “soaring eagle” proposal was heard by the Development Review Board (DRB) Panel A. Staff’s recommendation and condition PDB3 included limiting the operation of the zip line to 10 p.m. based on the previous approvals of the Family Fun Center and the sensitivity that the previous residents of Thunderbird Mobile Home Club had regarding the issue of noise as well as the presence of new multi-family housing at Jory Trail at the Grove.

Staff received a letter from Holland Partner Group (Exhibit D1 of Exhibit A1), owner of Jory Trail at the Grove that was included in the public record identifying three concerns they had regarding the proposal, however they were generally in support of the project. One of the three issues they raised was a request that the zip line operation to be limited to 10 p.m.

During the hearing, the applicant raised the issue of certain special event nights when they would like to operate the zip line for extended hours and potentially all night on “grad nights”. The Development Review Board listened to the testimony and discussed noise in great length. In testimony to the DRB, the applicant requested to operate the zip line attraction 24 hours a day for not more than 25 days a year. According to the applicant, many nights would be less than 24 hours depending on the event. The DRB approved the zip line attraction and amended the proposed condition of approval limiting the operation to 10 p.m. and allowed 25 nights with prior notice to surrounding neighbors and property owners.

There are concerns with the approved condition of approval PDB3 because it does not provide an adequate level of specificity on notice and it does not provide an opportunity for evaluation of potential impacts of the attraction to the residents and community, including remedy and could result in long-term complaint/enforcement issues for the city and the applicant for the following reasons.

- The condition does not provide any protocols or guidelines for notification. There are no details regarding lead time, who receives notice, what the notice is required to include, or other details that may assist residents with an understanding of when the events will occur. Language could be added to strengthen this issue.

- The city cannot rely on the Noise Ordinance for enforcement in its current form due to a specific exemption found in WC Section 6.204(3.f.) which states:

*Sounds caused by the following are exempt from the prohibitions set out in 6.204 and are in addition to the exemptions specifically set forth in 6.204(2).*

*(f.) Other Outdoor Events. Outdoor gatherings, public dances, shows, sporting events, and other similar outdoor events, provided that any necessary permit has been obtained from the appropriate permitting authority.*

Approval of this development permit will be the only chance the City has to ensure that processes are set up for monitoring, evaluation and adjustment of the activity if necessary in the future. Staff met with the applicant and came up with the following agreement.

1. The applicant agreed to return to the Development Review Board on March 11<sup>th</sup> to re-address the language in the above condition.
2. The applicant agreed to toll the 14-day City Council call-up to March 18<sup>th</sup>, the first City Council meeting following the March 11<sup>th</sup> DRB meeting.
3. The applicant will work with the staff to re-write the condition to address the review process and public notice.

Staff would propose to the DRB in a new revised condition PDB-3 that the “special events” should be processed annually via a Class II administrative review which:

1. Provides for public notice of the calendar of special events.
2. Will allow for the potential neighborhood impacts of the attraction to be evaluated and monitored over time and the hours of operation adjusted, if necessary.
3. Will allow for the noise ordinance to be enforced as the special event permits would be issued annually and the permit would be modifiable or revocable.

**PROPOSED REVISED CONDITION PDB3: Bold/underline = new words.**

1. Proposed revised condition PDB3 to read:

**PDB3:** The operation of the Zip Line amusement ride shall be prohibited between the hours of 10 pm and the regular opening time with the following exception: **Pursuant to Section 4.030 of the City of Wilsonville Code (“Code”), the Family Fun Center (“Applicant”) will be granted a Class 1 Temporary Use Permit for the year 2013 to allow the Applicant to operate its new Soaring Eagle Zip Line ride (“Zip Line”) after 10 pm for a maximum of 25 days during 2013. Except on those 25 identified dates, the Zip Line must cease operations between the hours of 10 pm and the regular opening time. Issuance of the Temporary Use Permit is subject to the following conditions:**

1. **Applicant will provide the City with all known proposed dates for late night Zip Line operations at least two (2) weeks in advance of the first date for a late night Zip Line operations. If all dates are not known at the time this notice is provided, or if**

**the dates should change, Applicant will provide the City with notice of added dates or revised dates at least 48 hours in advance of those late night Zip Line operations dates.**

2. **The City may, but shall not be obligated, to provide notice of the late night Zip Line operations dates to nearby residents and property owners who may be impacted by the late night Zip Line operations.**
3. **The Temporary Use Permit is subject to compliance with the City's Noise Ordinance found in Section 6.204 of the Code, as it may be amended from time to time, it being understood by the Applicant that issuance of said Temporary Use Permit does not create any exception to compliance with the City Noise Ordinance.**
4. **No fee will be charged by the City for issuance of the Temporary Use Permit described herein.**
5. **Subject to compliance with the City's Noise Ordinance, Applicant may apply for a Class 1 Temporary Use Permit for up to 25 days of late night Zip Line operations annually.**

**Staff comment:** The above condition may result in noise complaints requiring public resources to help mitigate the issues.

2. The Board may also retain the original staff recommended Condition of Approval PDB3 that reads:

**PDB3.** “The operation of the zip line amusement ride shall be prohibited between the hours of 10 p.m. and the regular opening time with the exception of 25 nights a year where the zip line is allowed to run 24 hours a day with prior notification to adjacent neighbors and property owners.”

**Staff comment:** The original staff condition limiting the zip line use to a 10:00 p.m. curfew is clear and objective and it does not require administrative reviews, protocols or guidelines for notifications.

#### **EXHIBITS:**

Exhibit A1 – Amended and Adopted Staff Report in Resolution 243.



**Exhibit A1**

**STAFF REPORT  
WILSONVILLE PLANNING DIVISION  
*Family Fun Center*  
DEVELOPMENT REVIEW BOARD PANEL 'A'  
QUASI-JUDICIAL PUBLIC HEARING  
AMENDED AND ADOPTED FEBRUARY 11, 2013  
ADDED LANGUAGE ***BOLD ITALICS UNDERLINED*****

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**HEARING DATE:** February 11, 2013  
**DATE OF REPORT:** February 4, 2013

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**APPLICATION NOS.:**

- A. DB12-0070: *Stage I Master Plan Modification*
- B. DB12-0071: *Class 3 Stage II*
- C. DB12-0072: *Class 3 Site Design Review*
- D. DB12-0073: *Height Waiver*
- E. TR13-0002: *Type B Tree Removal*

**APPLICANT:** Ben Altman, SFA Design Group

**OWNER:** Wilsonville Land Partnership

**REQUEST:** SFA Design Group on behalf of the Family Fun Center, proposes to develop Tax Lot 100, 109 in Section 14D; T3S R1W; Clackamas County, Oregon. The applicant proposes to install a 74 foot monopole to support a zip line.

**LOCATION:** Approximately 5.93-acres between two parcels located at 29111 SW Town Center Loop W. (**See Vicinity Map on Page 2**).

**LEGAL DESCRIPTION:** Tax Lots 100 and 109 in Section 14D; T3S R1W; Clackamas County, Oregon.

**LAND USE DESIGNATION:** Wilsonville Comprehensive Plan Map  
Designation: *Commercial*

**ZONING DESIGNATION:** Wilsonville Zone Map Classification: *Planned Development Commercial-Town Center (PDC-TC)- (Service Commercial)*

**STAFF REVIEWER:** Amanda Hoffman, Assistant Planner



## **SUMMARY:**

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- **Request A – Stage I Master Plan Modification (DB12-0070):**

As demonstrated in findings A1 through A14, with conditions of approval referenced therein, the proposed Stage II Final Development Plan modifications **meet** the City criteria in Subsections 4.118, 4.131, 4.155, 4.171, 4.177 and 4.140.

- **Request B – Stage II (DB12-0071):**

As demonstrated in findings B1 through B34, with conditions of approval referenced therein, the proposed Site Design Review plans (architecture and landscaping) **meet** the City criteria in Subsections 4.118, 4.131 and 4.140.

- **Request C – Class 3 Site Design Review (DB12-0072):**

As demonstrated in findings C1 through C22, the proposed Master Sign Plan modification with proposed conditions referenced herein **meets** the City criteria in Subsections 4.156, 4.400, and 4.421.

- **Request D-Height Waiver (DB12-0073):**

As demonstrated in findings D1 through D2, the proposed waiver with proposed conditions referenced herein **meets** the City criteria in Subsections 4.400, and 4.421.

- **Request E-Type B Tree Removal (TR13-0002):**

As demonstrated in findings E1 through E11, the proposed waiver with proposed conditions referenced herein **meets** the City criteria in Subsections 4.600.

## ***CONCLUSIONS AND RECOMMENDATIONS:***

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Staff has reviewed the applicant's analysis of compliance with the applicable criteria. Staff finds that the analysis satisfactorily demonstrates compliance with the City's Comprehensive Plan and the Planning and Land Development Ordinance. The Staff report adopts the applicant's responses as Findings of Fact, except as noted in the Conclusionary Findings, and modified by proposed Conditions of Approval. Based on the Findings of Fact and information included in this Staff Report, and information received from a duly advertised public hearing, Staff recommends that the Development Review Board approve the proposed applications (DB12-0070, DB12-0071, DB12-0072, DB12-0073 and TR13-0002), together with the following conditions:

***CONDITIONS OF APPROVAL FOR REQUESTS ‘A’ – ‘E’:***

The application and supporting documents are hereby adopted for approval with the following conditions:

<b>PD</b> = <b>Planning Division Conditions</b>	<b>A. DB12-0070:</b> Modification to the Stage I Master Plan
<b>BD</b> = <b>Building Division Conditions</b>	<b>B. DB12-0071:</b> Stage II
<b>NR</b> = <b>Natural Resources Conditions</b>	<b>C. DB12-0072:</b> Site Design Review
<b>FD</b> = <b>Tualatin Valley Fire and Rescue</b>	<b>D. DB12-0073:</b> Waiver
	<b>E. TR13-0002:</b> Type B Tree Removal Permit

**Planning Division Conditions:**

***Request A – DB12-0070: Stage I Modification to Master Plan***

**PDA 1.** On the basis of findings A1 through A14, this action approves the Stage I Master Plan modification to install a zip line and associated 74 foot support tower as well as other concept plans to expand the use in the future, as submitted with this application, approved by the Development Review Board, and stamped “Approved Planning Division”, unless altered by a subsequent Board approval, or with minor revisions approved by the Planning Director under a Class I administrative review process.

**PDA 2.** Should the operations of this project by either the owner or future tenants fail to meet any performance standards of Subsection 4.135(.05) of the City’s Development Code, the property owner and/or future tenant(s) shall seek approval from the Planning Division for the City of Wilsonville.

**Building Division Conditions:**

***Request A-D – DB12-0070-73***

*No comments*

**Natural Resources Division Conditions:**

***Request A-D - DB12-0070-73***

*No comments*

**Planning Division Conditions:**

***Request B – DB12-0071: Stage II***

**PDB 1.** The approved final plan and stage development schedule shall control the issuance of all building permits and shall restrict the nature, location and design of all uses. Minor changes in an approved preliminary or final development plan may be approved by the Planning Director through the Class I Administrative Review Process if such changes are consistent with the purposes and general character of the development plan. All other modifications, including extension or revision of the stage development schedule, shall be processed in the same manner as the original application and shall be subject to the same procedural requirements.

**PDB 2.** The applicant/owner shall provide the general contractor for the proposed project with a copy of the approved plans and conditions of approval adopted by the City.

**PDB 3.** The operation of the zip line amusement ride shall be prohibited between the hours of 10 p.m. and the regular opening time **with the exception of 25 nights a year where the zip line is allowed to run 24 hours a day with prior notification to adjacent neighbors and property owners.** (See Finding B19)

**Planning Division Conditions:**

***Request C – DB12-0072: Class 3 Site Design Review***

**PDC 1.** The applicant/owner shall develop the Site Design Review Plans in substantial compliance with the plans approved by the DRB, unless altered with Board approval, or minor revisions are approved by the Planning Director under a Class I administrative review process. (See Finding C3)

**PDC 2.** All equipment shall be inconspicuous and designed to be screened from off-site view. This includes, to the greatest extent possible, private utilities such as natural gas and electricity. The City reserves the right to require further screening of the equipment and utilities if they should be visible from off-site after occupancy is granted. (See Finding B28 and C17)

**Planning Division Conditions:**

***Request D – DB12-0073: Height Waiver***

No conditions

**Tualatin Valley Fire & Rescue Conditions:**

***Request D – DB12-0073: Waiver***

**FDD 1. Training**-The manufacturer or designated representative shall provide the fire district with on-site training surrounding emergency operation procedures.

**Planning Division Conditions:**

***Request E – TR13-0002: Type B Tree Removal Permit***

**PDE 1.** The applicant/owner shall allow Planning Division staff access to the subject properties as necessary for tree related observations including verifying information provided by the application, observe site conditions, and verifying the terms and conditions of tree removal permits are followed. (See Finding E1)

**PDE 2.** This approval for removal applies only to the two (2) trees identified in the Applicant’s submitted materials. All other trees on the property shall be maintained unless removal is approved through separate application. The applicant shall replace one Callery Pear tree onsite and shall pay into the City’s tree fund for the other tree.

**PDE 3.** Trees planted as replacement of the two (2) removed trees shall be staked, fertilized and mulched, and shall be guaranteed by the permit grantee or the grantee’s successors-in-interest for two (2) years after the planting date. A “guaranteed” tree that dies or becomes diseased during that time shall be replaced. (See Finding E8)

**PDE 4.** All trees to be planted shall consist of nursery stock that meets requirements of the American Association of Nurserymen (AAN) American Standards for Nursery Stock (ANSI Z60.1) for top grade. Tree shall be approximately two inch (2”) caliper.

**PDE 5.** Solvents, building material, construction equipment, soil, or irrigated landscaping, shall not be placed within the drip line of any preserved tree, unless a plan for such construction activity has been approved by the Planning Director or Development Review Board based upon the recommendations of an arborist.

**PDE 6.** Before and during development, land clearing, filling or any land alteration the applicant shall erect and maintain suitable tree protective barriers meeting the specifications shown in Drawing Number: R-1155 of the City’s Public Works Standards.

**PDE 7.** Following performance required by Conditions Numbered PDE1 through PDE6, above, the Applicant shall submit evidence of completion of all required improvements within 30 days of completion to the Planning Division staff. Such evidence shall be either: a) dated photographs of each required improvement, sufficient in detail to enable confirmation of compliance with required conditions; or, b) request on-site inspection of all improvements by the Planning Division staff by doing one of the following:

- i. Send a request for inspection via email to [planning@ci.wilsonville.or.us](mailto:planning@ci.wilsonville.or.us), identifying the case-file number of this action (TR13-0002) and the date of completion; or,
- ii. Send a request for inspection via regular U.S. Mail, identifying the case-

file number of this action (TR13-0002) and the date of completion. Send such mail to Planning Division; City of Wilsonville; 29799 SW Town Center Loop East; Wilsonville, OR 97070; or,

- iii. Phone such request to the Planning Division staff at 503-682-4960, identifying the case-file number of this action (TR13-0002) and the date of completion.

**MASTER EXHIBITS LIST:**

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The following exhibits are hereby entered into the public record by the Development Review Board as confirmation of its consideration of the application as submitted. This is the master exhibits list that includes exhibits for Planning Case Files DB12-0070, DB12-0071, DB12-0072, DB12-0073, and TR13-0002.

**A. Staff's Written and Graphic Materials:**

**A1. Staff Report:**

- Findings of Fact for Requests A through E
- Proposed Conditions of Approval for requests A through E
- Conclusionary Findings for requests A through E

**A2. Additional Conditions of Approval for 92 PC 05**

**A3. Staff's Powerpoint**

**B. Applicant's Written and Graphic Materials:**

**B1. Applicant's Narrative; dated 1/4/2013**

*(NOTEBOOK SUBMITTED UNDER SEPARATE COVER)*

**B 1.2 Application Form; dated 11/29/2012**

**B 1.3 FAA Beacon Lighting cut-sheet; dated 1/7/2013**

**B 1.4 Color and Materials; dated 1/7/2013**

**B2. Plan Set (full size):**

Sheet Title
Surrounding Building Height Exhibit
Preliminary Site Plan
Preliminary Assembly-Complete Ride

**C. Development Review Team Correspondence and Materials:**

- C1.** Memo from D. Walters, Building Plans Examiner; dated 1/18 /2013
- C2.** Memo from Steve Adams, Deputy City Engineer; dated 1/16/2013
- C3.** Letter from D. DeBois; Tualatin Valley Fire & Rescue; dated 1/25/2013
- C4.** Table from City Public Works Department; dated 1/24/2013
- C5.** Letter from Oregon Department of Aviation; dated 1/15/2013

**D. General Correspondence:**

- D1.** Letters (neither For nor Against): **Holland Partners; dated 2/11/2013**
- D2.** Letters (In Favor): None submitted
- D3.** Letters (Opposed): None submitted



**FINDINGS OF FACT:**

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**1. Statutory Timeline:**

The statutory 120-day time limit applies to this application. The application was received on November 29, 2012. On December 10, 2012, staff conducted a completeness review within the statutorily allowed 30-day review period, and, on January 7, 2013, the applicant submitted new materials. On January 9, 2013, the application was deemed complete. The City must render a final decision for the request, including any appeals, by May 9, 2013.

**2. Adjacent land uses:**

Compass Direction	Zone:	Existing Use:
North:	PDR	Jory Trail Apartments
East:	PDC-TC	Napa Auto Parts
South:	PDC-TC	Commercial services
West:	n/a	Interstate 5

**3. Comprehensive Plan and Zone Maps:** The site has a Comprehensive Plan designation of Commercial and is zoned Planned Development Commercial-Town Center (PDC-TC)-(Service Commercial)

**4. Previous Planning Approvals:**

- 03DB32: Stage I Modification, Stage II Final, Site Design Review for bldg. addition
- 03DB15: One-year TUP for tent
- 02DB17: One-year TUP for tent
- 01DB21: One-year TUP for tent
- 00DB27: One-year TUP for tent
- 99DB19: One-year TUP for tent
- 98DB10: One-year Temporary Use Permit for tent
- 95SR02: Sign Review
- 95DR07: Parking lot improvements
- 94DR07: Review condition of approval
- 94DR14: Site Design Review for building addition
- 92DR32: Wilsonville Family Fun Center, Site Design Review
- 92PC05: Stage I, Stage II for Family Fun Center

**5. Natural Characteristics:** The subject property is of generally flat terrain. Vegetation is limited to existing landscape and trees throughout the site, within the existing parking lot and along the site’s southern street frontage.

**6. Streets:** The subject property is located on the north side of SW Town Center Loop W. Access to the property is by Town Center Loop W.

7. **Review Procedures:** The required public notices have been sent and all proper notification procedures have been satisfied. In addition, staff sent notifications to apartment dwellers in the Jory Trail Apartments facing the proposed development.

## GENERAL INFORMATION

### *Section 4.008 Application Procedures-In General*

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville's development review process.

**Finding:** These criteria are met.

**Details of Finding:** The application is being processed in accordance with the applicable general procedures of this Section.

### *Section 4.009 Who May Initiate Application*

**Review Criterion:** "Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply."

**Finding:** This criterion is satisfied.

**Details of Finding:** The application has been submitted on behalf of Wilsonville Land Partnership, dba Wilsonville Family Fun Center, Darren Harmon, General Manager, with the help of the consultant firm, SFA Design Group, specifically Ben Altman. The property owner, through signature on the development application, has given approval for submission of the application. This provision is met.

### *Subsection 4.010 (.02) Pre-Application Conference*

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** Pre-application conference was held on October 11, 2012 in accordance with this subsection.

### *Subsection 4.011 (.02) B. Lien Payment before Application Approval*

**Review Criterion:** "City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application."

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

#### ***Section 4.014 Burden of Proof is on the Applicant***

**Review Criterion:** “The burden of proving that the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case. In the case of an appeal, the burden of proof rests with the appellant.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant has provided the necessary findings of fact for approval with conditions of the requested development applications in accordance with this Section.

#### ***Subsection 4.035 (.04) A. General Site Development Permit Submission Requirements***

**Review Criteria:** “An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code.” Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

#### ***Section 4.110 Zoning-Generally***

**Review Criteria:** “The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192.” “The General Regulations listed in Sections 4.154 through 4.199 shall apply to all zones unless the text indicates otherwise.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The subject properties are designated Commercial on the Comprehensive Plan map and zoned Planned Development Commercial-Town Center on the zoning map. The site is situated within an area identified on the Town Center Master Plan as being in the Service Commercial (SC) overlay zone. In 1992, the Stage I Master Plan and Stage II Site Development Plans (Resolution 92PC14) were approved by the Planning Commission. The Planning Commission and City Council approvals allow the recreational uses within the SC (Service Commercial) overlay zone. These approvals further amended the Town Center Master Plan to include privately operated recreational centers as a recommended use in the SC overlay zone.

**REQUEST A**  
**DB12-0071: STAGE I MASTER PLAN MODIFICATION**  
**CONCLUSIONARY FINDINGS**

***Planned Development Regulations***

***Subsection 4.140 (.01) Purpose of Planned Development Regulations***

**A1. Review Criterion:** The proposed revised Stage I Master Plan shall be consistent with the Planned Development Regulations purpose statement.

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant asserts the application is consistent with the purpose statement and staff concurs.

***Subsection 4.140 (.02) Lot Qualifications for Planned Developments***

**A2. Review Criterion:** “Planned Development may be established on lots which are suitable for and of a size to be planned and developed in a manner consistent with the purposes and objectives of Section 4.140.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The properties owned by the applicant subject to this application are of sufficient size to be developed in a manner consistent the purposes and objectives of Section 4.140.

**A3. Review Criteria:** “Any site designated for development in the Comprehensive Plan may be developed as a Planned Development, provided that it is zoned “PD.” All sites which are greater than two (2) acres in size, and designated in the Comprehensive Plan for commercial, residential, or industrial use shall be developed as Planned Developments, unless approved for other uses permitted by the Development Code.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The subject properties are greater than 2 acres, are designated for commercial development in the Comprehensive Plan, and are zoned Planned Development Commercial-Town Center. The properties have been and continue to be developed as a planned development in accordance with this subsection.

***Subsection 4.140 (.03) Ownership Requirements for Submitting Planned Development Application***

**A4. Review Criterion:** “The tract or tracts of land included in a proposed Planned Development must be in one (1) ownership or control or the subject of a joint application by the owners of all the property included.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The land included in the current application is under the single

ownership of Wilsonville Land Partnership, dba Wilsonville Family Fun Center and an authorized representative, Darren Harmon, has signed the application.

***Subsection 4.140 (.04) Professional Design Team Required for Planned Developments***

**A5. Review Criteria:** “The applicant for all proposed Planned Developments shall certify that the professional services of the appropriate professionals have been utilized in the planning process for development. One of the professional consultants chosen by the applicant shall be designated to be responsible for conferring with the planning staff with respect to the concept and details of the plan.”

**Finding:** These criteria are satisfied.

**Details of Finding:** As can be found in the applicant’s submitted materials, appropriate professionals have been involved in the planning and permitting process. Ben Altman of SFA Design Group has been designated the coordinator for the planning portion of the project.

***Subsection 4.140 (.05) Planned Development Permit Process***

**A6. Review Criteria:** “All parcels of land exceeding two (2) acres in size that are to be used for residential, commercial or industrial development, shall, prior to the issuance of any building permit:

1. Be zoned for planned development;
2. Obtain a planned development permit; and
3. Obtain Development Review Board, or, on appeal, City Council approval.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The subject property is greater than 2 acres, is designated for commercial development in the Comprehensive Plan, and is zoned Planned Development Commercial-Town Center. The property has been and continues to be developed as a planned development in accordance with this subsection.

***Subsection 4.140 (.06) Stage I Master Plan Consistent with Comprehensive Plan***

**A7. Review Criteria:** “The planning staff shall prepare a report of its findings and conclusions as to whether the use contemplated is consistent with the land use designated on the Comprehensive Plan.” “The applicant may proceed to apply for Stage I - Preliminary Approval - upon determination by either staff or the Development Review Board that the use contemplated is consistent with the Comprehensive Plan.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed project, as found elsewhere in this report, complies with the Planned Development Commercial-Town Center zoning designation, which implements the Comprehensive Plan designation of ‘Commercial’ for this property. All other applicable Development Code criteria

that implement the Comprehensive Plan are being met, or will be met as conditions of approval.

***Subsection 4.140 (.07) Stage I Master Plan Application Requirements and Hearing Process***

**A8. Review Criteria:** This subsection establishes that the Development Review Board shall consider a Stage I Master Plan after completion or submission of a variety of application requirements.

**Finding:** These criteria are satisfied.

**Details of Finding:** Review of the proposed revised Stage I Master Plan has been scheduled for a public hearing before the Development Review Board in accordance with this subsection and the applicant has met all the applicable submission requirements as follows:

- The properties affected by the revised Stage I Master Plan are under the sole ownership of Wilsonville Land Partnership, dba Wilsonville Family Fun Center and an authorized representative, Darren Harmon, has signed the application.
- The application for a revised Stage I Master Plan has been submitted on a form prescribed by the City.
- The professional design team and coordinator have been identified. See Finding A5.
- The applicant has stated the various uses involved in the Master Plan and their locations.
- Boundary survey and topography plans were submitted with 92DR32.
- A tabulation of the land area to be devoted to various uses has been provided within the narrative.
- A project phasing plan has been provided, see applicant's narrative and the specific timeframe for completion is between 1-7 years. Buildings in the Master Plan area have already been constructed. The applicant is and will be requesting Stage II Master Plan for all new development on the site concurrent with this application. See Request B.
- Any necessary performance bonds will be required.
- Site Design Review for the new development is requested with this application. See Request C.
- A Waiver is requested for height. See Request D.
- A Type B Tree Removal is requested for removal of two trees. See Request E.

***Section 4.023 Expiration of Development Approvals***

**A9. Review Criterion:** "Except for Specific Area Plans (SAP), land use and development permits and approvals, including both Stage I and Stage II Planned Development approvals, shall be valid for a maximum of two years, unless extended as provided in this Section."

**Finding:** This criterion is satisfied.

**Details of Finding:** It is understood the requested approval will expire after two (2) years unless extended.

*Standards Applying to Commercial Development in Any Zone*

*Subsection 4.116 (.01)-(.10)*

**A10. Review Criterion:** “Commercial developments shall be planned in the form of centers or complexes as provided in the City’s Comprehensive Plan. As noted in the Comprehensive Plan, Wilsonville’s focus on centers or complexes is intended to limit strip commercial development.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The subject site is an established Family Fun Center. It does not result in strip commercial development. The applicant is proposing a series of modernization improvements to the Family Fun Center. The improvements will be phased over the next 1-7 years. They are all consistent with the overall entertainment function provided by the Fun Center.

*Subsection 4.131.05 (.01) Purpose of Planned Development Commercial-Town Center*

**A11. Review Criterion:** “The purpose of this zoning is to permit and encourage a Town Center, adhering to planned commercial and planned development concepts...”

**Finding:** This criterion is satisfied.

**Details of Finding:** The proposed uses are consistent with the purpose of the PDC-TC zone as they include entertainment uses. The prior approvals allowed for outdoor activities as being consistent with the allowed entertainment function being provided.

*Subsection 4.131.05 (.03) Example of uses that are typically recommended in PDC-TC Zone*

**A12. Review Criteria:** Uses that are typically recommended: Listed A. through E.

**Finding:** These criteria are satisfied.

**Details of Finding:** The existing uses include the following listed in this subsection: Entertainment under A.

*Subsection 4.135 (.04) Block and Access Standards in PDC-TC Zone*

**A13. Review Criterion:** “The PDC-TC zone shall be subject to the same block and access standards as the PDC zone, Section 4.131(.02) and (.03).”

**Finding:** This criterion is satisfied.

**Details of Finding:** The previously approved block spacing and access is not being changed. The Stage I Master Plan area will continue to be in compliance with these standards including adequate connectivity.

***SUMMARY FINDINGS FOR DB12-0070 – Stage I Modification to a Final Development Plan:***

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- A14.** The applicant’s response findings demonstrate that the proposed Stage I Modification to a Final Development Plan should be approved.

<p style="text-align: center;"><b>REQUEST B DB12-0071: STAGE II CONCLUSIONARY FINDINGS</b></p>
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**Planned Development Regulations**

***Subsection 4.140 (.01) Purpose of Planned Development Regulations***

- B1. Review Criterion:** The proposed Stage II Final Plan shall be consistent with the Planned Development Regulations purpose statement.  
**Finding:** This criterion is satisfied.  
**Details of Finding:** Based on the information provided by the applicant in their narrative, staff is of the professional opinion that the purpose of the planned development regulations is met by the proposed Stage II Final Plan.

***Subsection 4.140 (.02) Lot Qualifications for Planned Developments***

- B2. Review Criterion:** “Planned Development may be established on lots which are suitable for and of a size to be planned and developed in a manner consistent with the purposes and objectives of Section 4.140.”  
**Finding:** This criterion is satisfied.  
**Details of Finding:** The lot of the subject development site is of sufficient size to be developed in a manner consistent with the purposes and objectives of Section 4.140.
- B3. Review Criteria:** “Any site designated for development in the Comprehensive Plan may be developed as a Planned Development, provided that it is zoned ‘PD.’ All sites which are greater than two (2) acres in size, and designated in the Comprehensive Plan for commercial, residential, or industrial use shall be developed as Planned Developments, unless approved for other uses permitted by the Development Code.”  
**Finding:** These criteria are satisfied.  
**Details of Finding:** The development site is greater than 2 acres, is designated for commercial development in the Comprehensive Plan, and is zoned Planned Development Commercial-Town Center. The property will be developed as a component of a planned development in accordance with this subsection.



***Subsection 4.140 (.03) Ownership Requirements for Submitting Planned Development Application***

**B4. Review Criterion:** “The tract or tracts of land included in a proposed Planned Development must be in one (1) ownership or control or the subject of a joint application by the owners of all the property included.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The land included in the proposed Stage II Final Plan is under the single ownership of Wilsonville Land Partnership, dba Wilsonville Family Fun Center, Darren Harmon, General Manager. The property owner, through signature on the development application, has given approval for submission of the application.

***Subsection 4.140 (.04) Professional Design Team Required for Planned Developments***

**B5. Review Criteria:** “The applicant for all proposed Planned Developments shall certify that the professional services of the appropriate professionals have been utilized in the planning process for development. One of the professional consultants chosen by the applicant shall be designated to be responsible for conferring with the planning staff with respect to the concept and details of the plan.”

**Finding:** These criteria are satisfied.

**Details of Finding:** As can be found in the applicant’s submitted materials, appropriate professionals have been involved in the planning and permitting process. Ben Altman of SFA Design Group has been designated the coordinator for the planning portion of the project.

***Subsection 4.140 (.05) Planned Development Permit Process***

**B6. Review Criteria:** “All parcels of land exceeding two (2) acres in size that are to be used for residential, commercial or industrial development, shall, prior to the issuance of any building permit:

1. Be zoned for planned development;
2. Obtain a planned development permit; and
3. Obtain Development Review Board, or, on appeal, City Council approval.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The subject property is greater than 2 acres, is designated for commercial development in the Comprehensive Plan, and is zoned Planned Development Commercial-Town Center. The property has been and continues to be developed as a planned development in accordance with this subsection.

***Stage II Final Plan Submission Requirements and Process***  
***Subsection 4.140 (.09) A. Timing of Submission***

**B7. Review Criterion:** “Unless an extension has been granted by the Development Review Board, within two (2) years after the approval or modified approval of a preliminary development plan (Stage I), the applicant shall file with the City Planning Department a final plan for the entire development or when submission in stages has been authorized pursuant to Section 4.035 for the first unit of the development”

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant is submitting a Stage II Master Plan for a portion of the proposed modifications, concurrently with a revised Stage I Master Plan.

***Subsection 4.140 (.09) B. Determination by Development Review Board***

**B8. Review Criterion:** “the Development Review Board shall determine whether the proposal conforms to the permit criteria set forth in this Code, and shall approve, conditionally approve, or disapprove the application”.

**Finding:** This criterion is satisfied.

**Details of Finding:** The Development Review Board is considering all applicable permit criteria set forth in the Planning and Land Development Code and staff is recommending the Development Review Board approve the application with conditions of approval.

***Subsection 4.140 (.09) C. Conformance with Stage I and Additional Submission Requirements***

**B9. Review Criteria:** “The final plan shall conform in all major respects with the approved preliminary development plan, and shall include all information included in the preliminary plan plus the following:” listed 1. through 6.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant states, and staff concurs, that the Stage II plans substantially conform to the proposed revised Stage I Master plan. The applicant has provided the required drawings and other documents showing all the additional information required by this subsection.

***Subsection 4.140 (.09) D. Stage II Final Plan Detail***

**B10. Review Criterion:** “The final plan shall be sufficiently detailed to indicate fully the ultimate operation and appearance of the development or phase of development.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant has provided sufficiently detailed information to indicate fully the ultimate operation and appearance of the development, including a detailed site plan, elevation drawings, and material information.

***Subsection 4.140 (.09) E. Submission of Legal Documents***

**B11. Review Criterion:** “Copies of legal documents required by the Development Review Board for dedication or reservation of public facilities, or for the creation of a non-profit homeowner’s association, shall also be submitted.”

**Finding:** This criterion is satisfied.

**Details of Finding:** No additional legal documentation is required for dedication or reservation of public facilities.

***Subsection 4.140 (.09) I. and Section 4.023 Expiration of Stage II Approval***

**B12. Review Criterion:** This subsection and section identify the period for which Stage II approvals are valid.

**Finding:** This criterion is satisfied.

**Details of Finding:** The Stage II Approval, along with other associated applications, will expire two (2) years after approval, unless an extension is approved in accordance with these subsections.

***Subsection 4.140 (.09) J. 1. Planned Development Permit Requirements: Conformance with Comprehensive Plan and other Applicable Plans and Ordinances***

**B13. Review Criteria:** “The location, design, size and uses, both separately and as a whole, are consistent with the Comprehensive Plan, and with any other applicable plan, development map or Ordinance adopted by the City Council.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The property is part of the Wilsonville Town Center. The location, design, and size are typical of the commercial zone and the surrounding development within the Wilsonville Town Center. To staff’s knowledge, the location, design, size, and uses are consistent with other applicable plans, maps, and ordinances, or will be by specific conditions of approval. The applicant has applied for a Waiver to Height, see Request D.

***Subsection 4.140 (.09) J. 2. Planned Development Permit Requirements: Traffic Concurrency***

**B14. Review Criteria:** “That the location, design, size and uses are such that traffic generated by the development at the most probable used intersection(s) can be accommodated safely and without congestion in excess of Level of Service D, as defined in the Highway Capacity Manual published by the National Highway Research Board, on existing or immediately planned arterial or collector streets and will, in the case of commercial or industrial developments, avoid traversing local streets. Immediately planned arterial and collector streets are those listed in the

City's adopted Capital Improvement Program, for which funding has been approved or committed, and that are scheduled for completion within two years of occupancy of the development or four year if they are an associated crossing, interchange, or approach street improvement to Interstate 5." Additional qualifiers and criteria listed a. through e.

**Finding:** These criteria are satisfied.

**Details of Finding:** A traffic impact study waiver was submitted for the proposed development and was subsequently approved by the City of Wilsonville Engineering Division.

***Subsection 4.140 (.09) J. 3. Planned Development Permit Requirements: Facilities and Services Concurrency***

**B15. Review Criteria:** "That the location, design, size and uses are such that the residents or establishments to be accommodated will be adequately served by existing or immediately planned facilities and services."

**Finding:** These criteria are satisfied.

**Details of Finding:** Facilities and services, including utilities, are available and sufficient to serve the proposed development.

***Subsection 4.140 (.09) L. Adherence to Approved Plan and Modification Thereof***

**B16. Review Criteria:** "The applicant shall agree in writing to be bound, for her/himself and her/his successors in interest, by the conditions prescribed for approval of a development. The approved final plan and stage development schedule shall control the issuance of all building permits and shall restrict the nature, location and design of all uses. Minor changes in an approved preliminary or final development plan may be approved by the Director of Planning if such changes are consistent with the purposes and general character of the development plan. All other modifications, including extension or revision of the stage development schedule, shall be processed in the same manner as the original application and shall be subject to the same procedural requirements."

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDB 1.

**Details of Finding:** Condition of Approval PDB 1 ensures adherence to approved plans except for minor revisions by the Planning Director.

***Comprehensive Plan and Zoning: Planned Development Commercial***

**B17. Review Criterion:** This subsection lists a number of requirements for commercial development such as setback, lot size, lot coverage, and street frontage requirements.

**Finding:** This criterion is satisfied.

**Details of Finding:** The subject property contains one zoning district – PDC-TC. The Comprehensive Plan identifies the subject property as Commercial.

***Subsection 4.118.03(B): Waivers***

**B18. Review Criteria:** “Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may” waive a number of standards as listed in A. through E.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is seeking a height waiver to the Planned Development Regulations of 35 feet for a 74’ tower. See Request D.

***Subsection 4.118 (.03) E. Other Requirements or Restrictions***

**B19. Review Criteria:** “Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may adopt other requirements or restrictions, inclusive of, but not limited to, the following:” Listed 1. through 12.

**Finding:** These criteria will be satisfied by Condition of Approval PDB 3.

**Details of Finding:** Under 92PC05, Condition #30, staff finds that City Council made an additional condition of approval regarding the operation of the batting cage. Because of the noise sensitivity of the adjacent residential to the north the hours of operation were restricted. The previous condition prohibited operation of the batting cage between 10 p.m. and the regular opening time. Staff finds that the zip line amusement ride is a similar type-use related to noise and will be located much closer to the residential than the batting cage. Limiting the operation time will give assurance that this amusement ride will not be a nuisance to adjacent residents regarding noise. Therefore, staff finds that the operation of the zip line amusement ride shall be prohibited between the hours of 10 p.m. and the regular opening time. (See Condition of Approval PDB 3 and Exhibit A2)

***Subsection 4.118 (.04) Effect of Determination of Compliance and Conditions of Approval on Development Cost***

**B20. Review Criteria:** “The Planning Director and Development Review Board shall, in making their determination of compliance in attaching conditions, consider the effects of this action on availability and cost. The provisions of this section shall not be used in such a manner that additional conditions, either singularly or cumulatively, have the effect of unnecessarily increasing the cost of development. However, consideration of these factors shall not prevent the Board from imposing conditions of approval necessary to meet the minimum requirements of the Comprehensive Plan and Code.”

**Finding:** These criteria are satisfied.

**Details of Finding:** It is staff’s professional opinion that the determination of compliance or attached conditions do not unnecessarily increase the cost of development, and no evidence has been submitted to the contrary.

## *Parking and Loading*

### *Subsection 4.155 (.02) General Parking Provisions*

- B21. Review Criteria:** This subsection lists a number of general provisions for parking.  
**Finding:** These criteria are satisfied.  
**Details of Finding:** The parking lot and access drives already exist for the Family Fun Center.

### *Subsection 4.155 (.03) B. 8. Parking Minimum and Maximum*

- B22. Review Criteria:** “Tables 5, below, shall be used to determine the minimum and maximum parking standards for various land uses. The minimum number of required parking spaces shown on Tables 5 shall be determined by rounding to the nearest whole parking space.”  
**Finding:** These criteria are satisfied.  
**Details of Finding:** The applicant has submitted summary findings with regard to parking. The applicant notes that 275 off-street parking spaces presently exist on the site. The applicant has proposed the reduction of two parking spaces. Based on the most recent approval, 03DB32, 222 parking spaces are required for the Family Fun Center. With the reduction of 2 spaces, the site will be providing 273 spaces. This standard is satisfied by the proposed off-street parking.

## *Natural Features*

### *Section 4.171 Protection of Natural Features and Other Resources*

- B23. Review Criteria:** This section provides for the protection of a number of natural features and other resources including: general terrain preparation, hillsides, trees and wooded areas, high voltage powerline easements and rights of way and petroleum pipeline easements, earth movement hazard areas, soil hazard areas, historic resources, and cultural resources.  
**Finding:** These criteria are satisfied.  
**Details of Finding:** The site was developed as a Family Fun Center in 1994. Cultivated landscape materials have been installed, but no natural features remain, as a result. The majority of existing trees are being preserved as part of the development. See Arborists Report in Exhibit B1-Applicant’s Narrative.

## *Public Safety and Crime Prevention*

### *Subsection 4.175 (.01) Design to Deter Crime and Ensure Public Safety*

- B24. Review Criteria:** “All developments shall be designed to deter crime and insure public safety.”  
**Finding:** These criteria are satisfied.

**Details of Finding:** According to the applicant the development has been designed to deter crime and insure public safety. The site has existing surveillance. No new lighting is proposed besides the FAA obstruction beacon at the top of the tower. See Exhibit B1.3 for details of the FAA obstruction beacon.

***Subsection 4.175 (.02) Addressing and Directional Signing***

**B25. Review Criteria:** “Addressing and directional signing shall be designed to assure identification of all buildings and structures by emergency response personnel, as well as the general public.”

**Finding:** These criteria are satisfied.

**Details of Finding:** All the buildings are clearly visible from the adjacent right-of-way from which they can be accessed. The address signs are existing and are easy to identify.

***Subsection 4.175 (.03) Surveillance and Police Access***

**B26. Review Criterion:** “Areas vulnerable to crime shall be designed to allow surveillance. Parking and loading areas shall be designed for access by police in the course of routine patrol duties.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The parking and loading areas are easily assessable and no areas of particular vulnerability to crime have been identified warranting additional surveillance.

***Subsection 4.175 (.04) Lighting to Discourage Crime***

**B27. Review Criterion:** “Exterior lighting shall be designed and oriented to discourage crime.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The lighting on the site is existing, the applicant does not propose any new lighting besides the FAA obstruction beacon at the top of the tower. See Exhibit B1.3.

**Landscaping Standards**

***Subsection 4.176 (.01) Purpose of Landscape, Screening, and Buffering***

**B28. Review Criteria:** “This Section consists of landscaping and screening standards and regulations for use throughout the City. The regulations address materials, placement, layout, and timing of installation. The City recognizes the ecological and economic value of landscaping and requires the use of landscaping and other screening or buffering to:” Listed A. through K.

**Finding:** These criteria will be satisfied by Condition of Approval PDC 2.

**Details of Finding:** In complying with the various landscape standards in Section 4.176 the applicant does not propose any new landscaping. Adequate screening is proposed around the mechanical equipment of the tower. The applicant proposes a chain-link fence with green slats. See Exhibit B1.4-Colors & Materials.

**Section 4.177 Street Improvement Standards**

***Subsection 4.177 (.01) (A)-(B)***

**B29. Review Criteria:** “Except as specifically approved by the Development Review Board, all street and access improvements shall conform to the Transportation Systems Plan and the Public Works Standards.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The site abuts SW Town Center Loop W. Street improvements currently exist along the frontage of the site. SW Town Center Loop W is classified by the City’s Transportation System Plan (TSP) as a Major Arterial.

***Subsection 4.177 (.01) E. Access Drives and Travel Lanes***

**B30. Review Criteria:** This subsection sets standards for access drives and travel lanes.

**Finding:** These criteria are satisfied.

**Details of Finding:** Access points are existing to the development and include two (2) driveway approaches on SW Town Center Loop W.

***Subsection 4.177 (.01) F. Corner or Clear Vision Area***

**B31. Review Criteria:** “A clear vision area which meets the Public Works Standards shall be maintained on each corner of property at the intersection of any two streets, a street and a railroad or a street and a driveway. However, the following items shall be exempt from meeting this requirement:” Listed a. through e.

**Finding:** These criteria are satisfied.

**Details of Finding:** Clear vision areas and vertical clearance have been reviewed by the City Engineering Division to assure compliance with the Section 4.177.

***Sections 4.199.20 Outdoor Lighting***

**B32. Review Criterion:** This section states that the outdoor lighting ordinance is applicable to “Installation of new exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas” and “Major additions or modifications (as defined in this Section) to existing exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas.” In addition the exempt luminaires and lighting systems are listed.

**Finding:** These criteria are satisfied.



**Details of Finding:** The applicant is not proposing any new lighting besides the required FAA obstruction beacon at the top of the tower. Spec sheets are included as Exhibit B1.3

*Sections 4.300-4.320 Underground Installation of Utilities*

**B33. Review Criteria:** These sections list requirements regarding the underground installation of utilities.

**Finding:** These criteria are satisfied.

**Details of Finding:** There are no existing overhead facilities that require undergrounding as part of this development. All new utilities associated with the development are proposed to be installed underground.

*SUMMARY FINDINGS FOR DB12-0071 – Stage II Final Development Plan:*

**B34.** The applicant’s response findings demonstrate that the proposed Stage II Final Development Plan should be approved.

**REQUEST C  
DB12-0072: SITE DESIGN REVIEW  
CONCLUSIONARY FINDINGS**

The applicant is requesting approval of Site Design Plans to install a 74’ support tower for a Zip Line amusement ride for the Wilsonville Family Fun Center. The details of the proposal are found beginning on page 1 of the compliance narrative (Exhibit B1).

*Site Design Review*

*Subsection 4.400 (.01) Excessive Uniformity, Inappropriateness of Design, Etc.*

**C1. Review Criteria:** “Excessive uniformity, inappropriateness or poor design of the exterior appearance of structures and signs and the lack of proper attention to site development and landscaping in the business, commercial, industrial and certain residential areas of the City hinders the harmonious development of the City, impairs the desirability of residence, investment or occupation in the City, limits the opportunity to attain the optimum use in value and improvements, adversely affects the stability and value of property, produces degeneration of property in such areas and with attendant deterioration of conditions affecting the peace, health and welfare, and destroys a proper relationship between the taxable value of property and the cost of municipal services therefor.”

**Finding:** These criteria are satisfied.

**Details of Finding:** It is staff’s professional opinion that the proposed development will not result in excessive uniformity, inappropriateness or poor design, and the

proper attention has been paid to site development. The 74 foot tower will be painted a tan color with green slats in the fencing for screening of mechanical equipment on the ground to blend it with the backdrop of large conifer trees.

***Subsection 4.400 (.02) Purposes of Objectives of Site Design Review***

**C2. Review Criterion:** “The City Council declares that the purposes and objectives of site development requirements and the site design review procedure are to:” Listed A through J.

**Finding:** These criteria are satisfied.

**Details of Finding:** It is staff’s professional opinion that the applicant has provided sufficient information demonstrating compliance with the purposes and objectives of site design review. Among the information provided is a written response to these purposes and objectives on pages 31 through 32 of the applicant’s compliance narrative. (Exhibit B1)

***Section 4.420 Site Design Review-Jurisdiction and Power of the Board***

**C3. Review Criteria:** The section states the jurisdiction and power of the Development Review Board in relation to site design review including the application of the section, that development is required in accord with plans, and variance information.

**Finding:** These criteria will be satisfied by Condition of Approval PDC 1.

**Details of Finding:** A condition of approval has been included to ensure construction, site development, and landscaping are carried out in substantial accord with the Development Review Board approved plans, drawings, sketches, and other documents. No building permits will be granted prior to development review board approval. No variances are requested from site development requirements.

***Subsection 4.421 (.01) Site Design Review-Design Standards***

**C4. Review Criteria:** “The following standards shall be utilized by the Board in reviewing the plans, drawings, sketches and other documents required for Site Design Review. These standards are intended to provide a frame of reference for the applicant in the development of site and building plans as well as a method of review for the Board. These standards shall not be regarded as inflexible requirements. They are not intended to discourage creativity, invention and innovation. The specifications of one or more particular architectural styles is not included in these standards.” Listed A through G.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided sufficient information demonstrating compliance with the standards of this subsection. Among the information provided is a written response to these standards on page 33 of their compliance narrative. (Exhibit B1)

*Subsection 4.421 (.02) Applicability of Design Standards to Various Site Features*

**C5. Review Criteria:** “The standards of review outlined in Sections (a) through (g) above shall also apply to all accessory buildings, structures, exterior signs and other site features, however related to the major buildings or structures.”

**Finding:** These criteria are satisfied.

**Details of Finding:** Design standards have been applied to the proposed structure, and other site features.

*Subsection 4.421 (.03) Objectives of Section 4.400 Serve as Additional Criteria and Standards*

**C6. Review Criteria:** “The Board shall also be guided by the purpose of Section 4.400, and such objectives shall serve as additional criteria and standards.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The purposes and objectives in Section 4.400 are being used as additional criteria and standards. See Finding C2 above.

*Subsection 4.421 (.05) Site Design Review-Conditions of Approval*

**C7. Review Criterion:** “The Board may attach certain development or use conditions in granting an approval that are determined necessary to insure the proper and efficient functioning of the development, consistent with the intent of the Comprehensive Plan, allowed densities and the requirements of this Code.”

**Finding:** These criteria are satisfied.

**Details of Finding:** Under 92PC05, Condition #30, staff finds that City Council made an additional condition of approval that the operation of the batting cage was prohibited between the hours of 10 p.m. and the regular opening time. (See Finding B19 above, Condition of Approval PDB 3 and Exhibit A2)

*Subsection 4.421 (.06) Color or Materials Requirements*

**C8. Review Criterion:** “The Board or Planning Director may require that certain paints or colors of materials be used in approving applications. Such requirements shall only be applied when site development or other land use applications are being reviewed by the City.”

**Finding:** This criterion is satisfied.

**Details of Finding:** It is the professional opinion of staff that the proposed coloring is appropriate for the proposed development and no additional requirements are necessary.

***Section 4.430 Design of Trash and Recycling Enclosures***

**C9. Review Criteria:** “The following locations, design and access standards for mixed solid waste and recycling storage areas shall be applicable to the requirements of Section 4.179 of the Wilsonville City Code.” Listed (.02) A. through (.04) C.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is not proposing any new trash enclosures. The existing enclosures were reviewed with the previous applications listed above.

***Section 4.440 Site Design Review-Procedures***

**C10. Review Criteria:** “A prospective applicant for a building or other permit who is subject to site design review shall submit to the Planning Department, in addition to the requirements of Section 4.035, the following:” Listed A through F.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has submitted the required additional materials, as applicable.

***Section 4.442 Time Limit on Approval***

**C11. Review Criterion:** “Site design review approval shall be void after two (2) years unless a building permit has been issued and substantial development pursuant thereto has taken place; or an extension is granted by motion of the Board.

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant has indicated that they will pursue development within two (2) years and it is understood that the approval will expire after 2 years if a building permit hasn’t been issued unless an extension has been granted by the board.

***Subsection 4.450 (.01) Landscape Installation or Bonding***

**C12. Review Criterion:** “All landscaping required by this section and approved by the Board shall be installed prior to issuance of occupancy permits, unless security equal to one hundred and ten percent (110%) of the cost of the landscaping as determined by the Planning Director is filed with the City assuring such installation within six (6) months of occupancy. "Security" is cash, certified check, time certificates of deposit, assignment of a savings account or such other assurance of completion as shall meet with the approval of the City Attorney. In such cases the developer shall also provide written authorization, to the satisfaction of the City Attorney, for the City or its designees to enter the property and complete the landscaping as approved. If the installation of the landscaping is not completed within the six-month period, or within an extension of time authorized by the Board, the security may be used by the City to complete the installation. Upon

completion of the installation, any portion of the remaining security deposited with the City shall be returned to the applicant.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is not proposing any new landscaping with this application.

### **Parking**

#### ***Subsection 4.155 (.02) Provision and Maintenance of Off-Street Parking***

**C13. Review Criteria:** This subsection lists general provisions for parking, A. through O.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is proposing a minor modification to the existing parking area by removing two parking spaces for the proposed tower.

#### ***Subsection 4.155 (.03) B. 1.-3. Landscaping of Parking Areas***

**C14. Review Criteria:** “Parking and loading or delivery areas shall be landscaped to minimize the visual dominance of the parking or loading area, as follows:” Listed 1. through 3.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is not proposing any new landscaping with this proposal.

### **Landscaping**

#### ***Subsection 4.176 (.02) B. Landscape Standards and Compliance with Code***

**C15. Review Criterion:** “All landscaping and screening required by this Code must comply with all of the provisions of this Section, unless specifically waived or granted a Variance as otherwise provided in the Code. The landscaping standards are minimum requirements; higher standards can be substituted as long as fence and vegetation-height limitations are met. Where the standards set a minimum based on square footage or linear footage, they shall be interpreted as applying to each complete or partial increment of area or length”

**Finding:** This criterion is satisfied.

**Details of Finding:** No waivers or variances to landscape standards have been requested. Thus all screening must comply with standards of this section.

#### ***Subsection 4.176 (.03) Landscape Area and Locations***

**C16. Review Criteria:** “Not less than fifteen percent (15%) of the total lot area, shall be landscaped with vegetative plant materials. The ten percent (10%) parking area landscaping required by section 4.155.03(B)(1) is included in the fifteen percent

(15%) total lot landscaping requirement. Landscaping shall be located in at least three separate and distinct areas of the lot, one of which must be in the contiguous frontage area. Planting areas shall be encouraged adjacent to structures. Landscaping shall be used to define, soften or screen the appearance of buildings and off-street parking areas. Materials to be installed shall achieve a balance between various plant forms, textures, and heights. The installation of native plant materials shall be used whenever practicable.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is not proposing any new landscaping with this project, therefore all landscaping exists and has been reviewed by previous approvals.

#### ***Subsection 4.176 (.04) Buffering and Screening***

**C17. Review Criteria:** “Additional to the standards of this subsection, the requirements of the Section 4.137.5 (Screening and Buffering Overlay Zone) shall also be applied, where applicable.

C. All exterior, roof and ground mounted, mechanical and utility equipment shall be screened from ground level off-site view from adjacent streets or properties.

D. All outdoor storage areas shall be screened from public view, unless visible storage has been approved for the site by the Development Review Board or Planning Director acting on a development permit.

E. In all cases other than for industrial uses in industrial zones, landscaping shall be designed to screen loading areas and docks, and truck parking.

F. In any zone any fence over six (6) feet high measured from soil surface at the outside of fenceline shall require Development Review Board approval.”

**Finding:** These criteria will be satisfied by Condition of Approval PDC 2.

**Details of Finding:** Consistent with the proposed Stage II Final Plan, adequate screening is proposed around the mechanical equipment of the tower. The applicant proposes a chain-link fence with green slats. (See also Finding B28 under Request B and Exhibit B1.4- Colors and Materials)

#### ***Section 4.177 Street Improvement Standards***

**C18. Review Criteria:** This section establishes standards for sidewalks and pathways.

**Finding:** These criteria are satisfied.

**Details of Finding:** The design of access improvements and improvements within the street right-of-way were approved under previous applications.

#### ***Section 4.178 Sidewalk and Pathway Standards***

**C19. Review Criteria:** This section establishes standards for sidewalks and pathways.

**Finding:** These criteria are satisfied.

**Details of Finding:** The sidewalks and pathways already exist on the site, the applicant is not proposing any new infrastructure.

***Section 4.179 Mixed Solid Waste and Recyclables Storage***

**C20. Review Criterion:** This section establishes standards for mixed solid waste and recyclables storage in new multi-family residential and non-residential buildings.

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant does not propose any new mixed solid waste or recyclables storage.

**Outdoor Lighting**

***Section 4.199.20 Applicability of Outdoor Lighting Standards***

**C21. Review Criterion:** This section states that the outdoor lighting ordinance is applicable to “Installation of new exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas” and “Major additions or modifications (as defined in this Section) to existing exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas.” In addition the exempt luminaires and lighting systems are listed.

**Finding:** This criterion is satisfied.

**Details of Finding:** The applicant is not proposing any new lighting besides the required FAA obstruction beacon at the top of the tower. Spec sheets are included as Exhibit B1.3

***SUMMARY FINDINGS FOR DB12-0072 – Site Design Review:***

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**C22.** The applicant has borne the burden of proof in demonstrating that the proposed Site Design Review plans should be approved.

**REQUEST D  
DB12-0073: HEIGHT WAIVER  
CONCLUSIONARY FINDINGS**

***Subsection 4.118 (.03) A. Waiver of Typical Development Standards***

**D1. Review Criteria:** This subsection establishes that “notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purpose and objectives of Section 4.140, and based on findings of fact supported by the record” may waive a number of typical development standards including height requirements.

**Finding:** These criteria are satisfied.

**Details of Finding:** This request includes a waiver to the 35 foot height limitation of the PDC-TC Zone. The applicant is proposing a 74 foot support tower for a Zip Line to maintain safe vertical clearance under the travel of the line. The proposed 74 foot tower will have a similar visual impact to typical cell towers. (See Exhibit B2 for Surrounding Building Height Plan Sheet) However, this tower will have a backdrop of various tall trees, ranging from 10 to over 60 feet in height. The tower is setback 210 feet from Town Center Loop W and 130 feet from the adjacent apartments to the north. The tower is a support structure, with technical design requirements that determine its ultimate height of 74 feet. This is a mono-pole design, which minimizes its profile and visual image. It is consistent with the overall entertainment function of the Fun Center. Surrounding development includes 2-3 story buildings, including the apartments to the north, the theater to the south, has a tall architectural glass tower measuring 53 feet and the Capital Realty Building to the southeast measures 48 feet.

***Subsection 4.140 (.01) B. Purpose and Objectives of Planned Development Regulations***

**D2. Review Criteria:** This subsection establishes the purpose of the Planned Development Regulations which are as follows:

- To take advantage of advances in technology, architectural design, and functional land use design:
- To recognize the problems of population density, distribution and circulation and to allow a deviation from rigid established patterns of land uses, but controlled by defined policies and objectives detailed in the comprehensive plan;
- To produce a comprehensive development equal to or better than that resulting from traditional lot land use development.
- To permit flexibility of design in the placement and uses of buildings and open spaces, circulation facilities and off-street parking areas, and to more efficiently utilize potentials of sites characterized by special features of geography, topography, size or shape or characterized by problems of flood hazard, severe soil limitations, or other hazards;
- To permit flexibility in the height of buildings while maintaining a ratio of site area to dwelling units that is consistent with the densities established by the Comprehensive Plan and the intent of the Plan to provide open space, outdoor living area and buffering of low-density development.
- To allow development only where necessary and adequate services and facilities are available or provisions have been made to provide these services and facilities.
- To permit mixed uses where it can clearly be demonstrated to be of benefit to the users and can be shown to be consistent with the intent of the Comprehensive Plan.



- To allow flexibility and innovation in adapting to changes in the economic and technological climate.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant states the following on page 23 of the compliance narrative:

“The Fun Center has already been determined to be a compatible and complimentary use within the Town Center. The color scheme (tan) is subtle so as to not draw undue attention to the tower and detract from the overall attractiveness of the Town Center. The applicant has decided the tower will not be lighted, except for the required FAA obstruction beacon.

Similar to a cell tower, the Zip Line Tower’s height is a matter of functional requirements. We believe the tower is consistent with the overall recreational and entertainment purposes of the Fun Center and its height must be considered in this context.

The Zip Line only adds to the overall recreation and entertainment functions provided at the Fun Center and thereby contributes to the stable environment within the Town Center. It is designed to minimize visual impact, while maintaining the functional requirements for the ride, which necessitates height to create the drop for the zip line. The vertical monopole design results in a smaller footprint with limited visual impact as compared to a bulkier commercial building of similar height. The result is maximum efficient use of limited commercial land.

The applicant’s design team has carefully coordinated the design with the Deputy Fire Marshal and the City’s Building Department to ensure that adequate fire and emergency response capabilities support the proposed 74 foot tower. It is anticipated that special training and practice rescue operations will be coordinated with the Fire District.”

Staff concurs that these points show the proposed height waiver for the tower meets the purpose and objectives of the planned development regulations.

<p><b>REQUEST E TR13-0002: TYPE B TREE REMOVAL CONCLUSIONARY FINDINGS</b></p>
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***Section 4.610.10 Guidelines and Limitations on Tree Removal***

**E1.** This section limits tree removal to, among other criteria, when tree removal is necessary for construction, when trees are diseased, become a nuisance, hazard, or interfere with the healthy growth of other trees. The two Callery pear trees that are

proposed for removal are necessary for construction of the zip line tower. This reason falls within the limits set by this subsection. (See Exhibit B2-Arborist Report)

**Section 4.610.20 (.02) Type A Tree Removal Criteria**

**E2.** This subsection stipulates that if a request does not meet the criteria in Subsection 4.610.20 (.01) to be reviewed as a Type A application, it may be submitted as a Type B application. The request involves the removal of trees planted and preserved as a condition of development and, therefore it does not meet the criteria to be reviewed as a Type A application. It is therefore being reviewed as a Type B application. This provision is satisfied.

**Subsection 4.610.20 (.03) and Subsection 4.610.30 (.02) Submittal Requirements**

**E3.** As indicated in the table below the Applicant has either submitted the required documentation, or has been granted a waiver under Subsection 4.610.30 (02) H. The requirements of these subsections are thus satisfied.

Requirement	Submitted	Waiver Granted		Condition of Approval	Not Applicable	Additional findings/notes
		Info Already Available to	Info Not Necessary for			
Statement why removal is necessary	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Description of trees (common name, d.b.h.)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Name of person removing (if known)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Time of removal (if known)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Map showing location of tree(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Arborist's Report (health and	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

<b>condition, species, common name, d.b.h.)</b>						
<b>Tree protection information</b>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
<b>Replacement tree description (species, size, number, cost)</b>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
<b>Copy of CC&amp;R's</b>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

Additional findings:

***Subsection 4.610.30 (.03) Review Process for Type B Tree Permits***

**E4.** This subsection stipulates that Type B Permits shall be reviewed under the standards of Class II Administrative Review and the requirements of the Tree Preservation and Protection subchapter. This application has been reviewed according the standards and processes referenced in this subsection. This provision is satisfied.

**Section 4.620.00 Tree Relocation, Mitigation, or Replacement**

***Subsection 4.620.00 (.01) Tree Replacement Required within One Year***

**E5.** This subsection requires a Type B Tree Removal Permit grantee to replace or relocate each removed tree having six inches (6”) or greater d.b.h. within one year of removal. Two (2) trees are proposed for removal. The Applicant is proposing to replant one (1) tree in the grass area at the western entrance (shared with Les Schwab’s). The applicant states, “there is no other space available” for replanting and therefore will pay into the City’s tree fund for the other required mitigation tree. A condition of approval ensures the requirements of this subsection are met.

***Subsection 4.620.00 (.02) Basis for Determining Replacement***

**E6.** This subsection requires that removed trees be replaced on a basis of one (1) tree replanted for each tree removed. It also requires all replacement trees measure two inches (2”) caliper. One (1) tree is being replaced onsite and will be two inch (2”) caliper. The applicant will be paying into the City’s tree fund for the other tree. The provisions of this subsection are satisfied.

***Subsection 4.620.00 (.03) A. Replacement Tree Requirements-Comparable Characteristics***

**E7.** This subsection identifies the requirements for replacement trees including: having characteristics similar to removed trees; being appropriately chosen for the site from an approved tree species list provided by the City, and being of state Department of Agriculture Nursery Grade No. 1 or better. The applicant does not state what type of tree they will use as a replacement tree. Staff recommends the applicant plant a Callery Pear to be consistent with the plantings that are already on the site. A condition of approval ensures the requirements of this subsection are met.

***Subsections 4.620.00 (.03) B. and C. Replacement Tree Requirements-Tree Care and Guarantee***

**E8.** These subsections require replacement trees be staked, fertilized and mulched, and be guaranteed by the permit grantee or the grantee’s successors-in-interest for two (2) years after the planting date. A “guaranteed” tree that dies or becomes diseased during the two (2) year period is required to be replaced. A condition of approval ensures the requirements of these subsections are met.

***Subsection 4.620.00 (.3) D. Replacement Tree Requirements- Encouragement of Diversity of Species***

**E9.** This subsection encourages a diversity of tree species to be planted. Two of the same variety are being removed. The applicant has not stated what type of tree they are proposing for replacement, however staff has recommended the same variety which will maintain substantially similar diversity of species on the property. This provision would be satisfied by a conditional of approval.

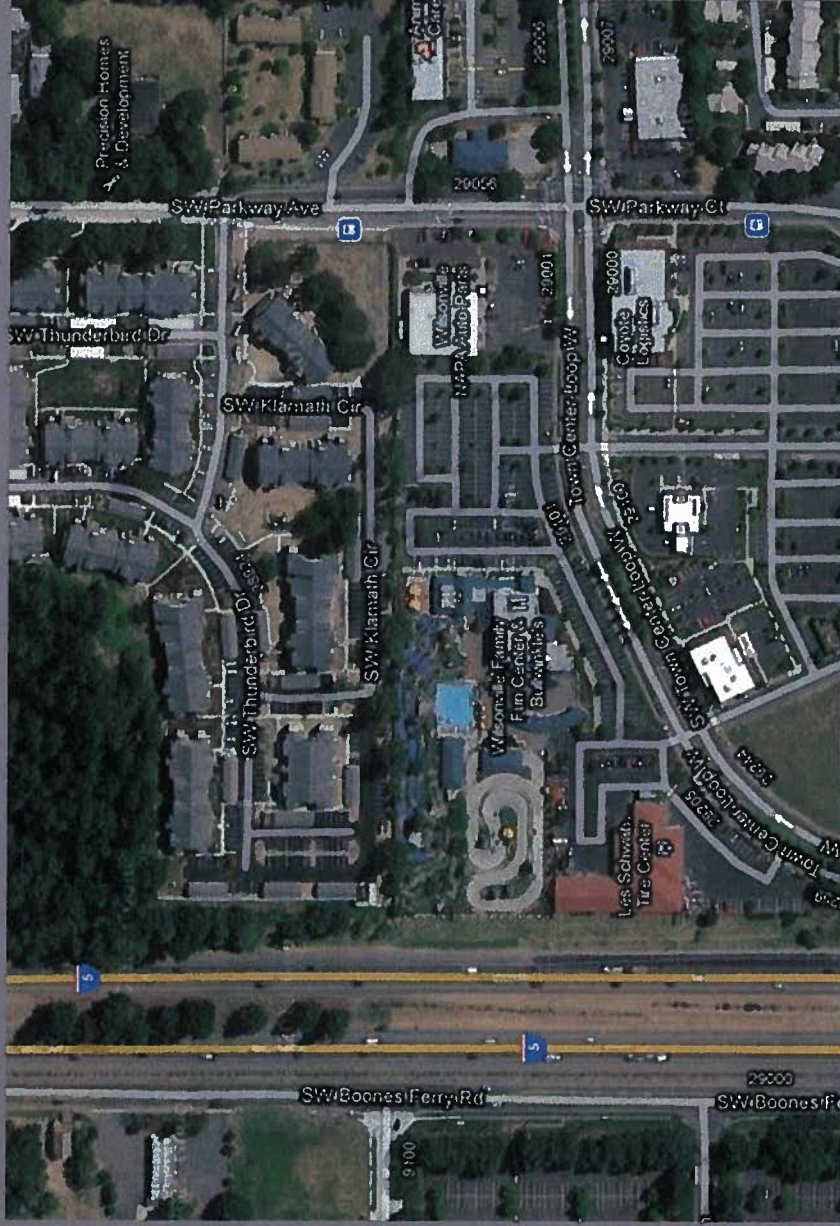
***Subsection 4.620.00 (.04) Additional Requirements for Replacement Trees***

**E10.** This subsection requires replacement trees consist of nursery stock that meets requirements of the American Association of Nurserymen (AAN) American Standards for Nursery Stock (ANSI Z60.1) for top grade. A condition of approval ensures the requirements of these subsections are met.

***Subsection 4.620.00 (.05) Replacement Tree Location- Review Required***

**E11.** This subsection requires the City to review tree replacement plans in order to provide optimum enhancement, preservation and protection of wooded areas. It requires that to the extent feasible and desirable, trees be replaced on site and within the same general area as the removed trees. Two trees are proposed to be removed. The landscape island is not large enough to accommodate replacement trees after the construction of the zip line tower and therefore the applicant has proposed the replacement tree in a different area of the site and to pay into the City’s tree fund. The requirement of this subsection is met.

# Family Fun Center-Zip Line



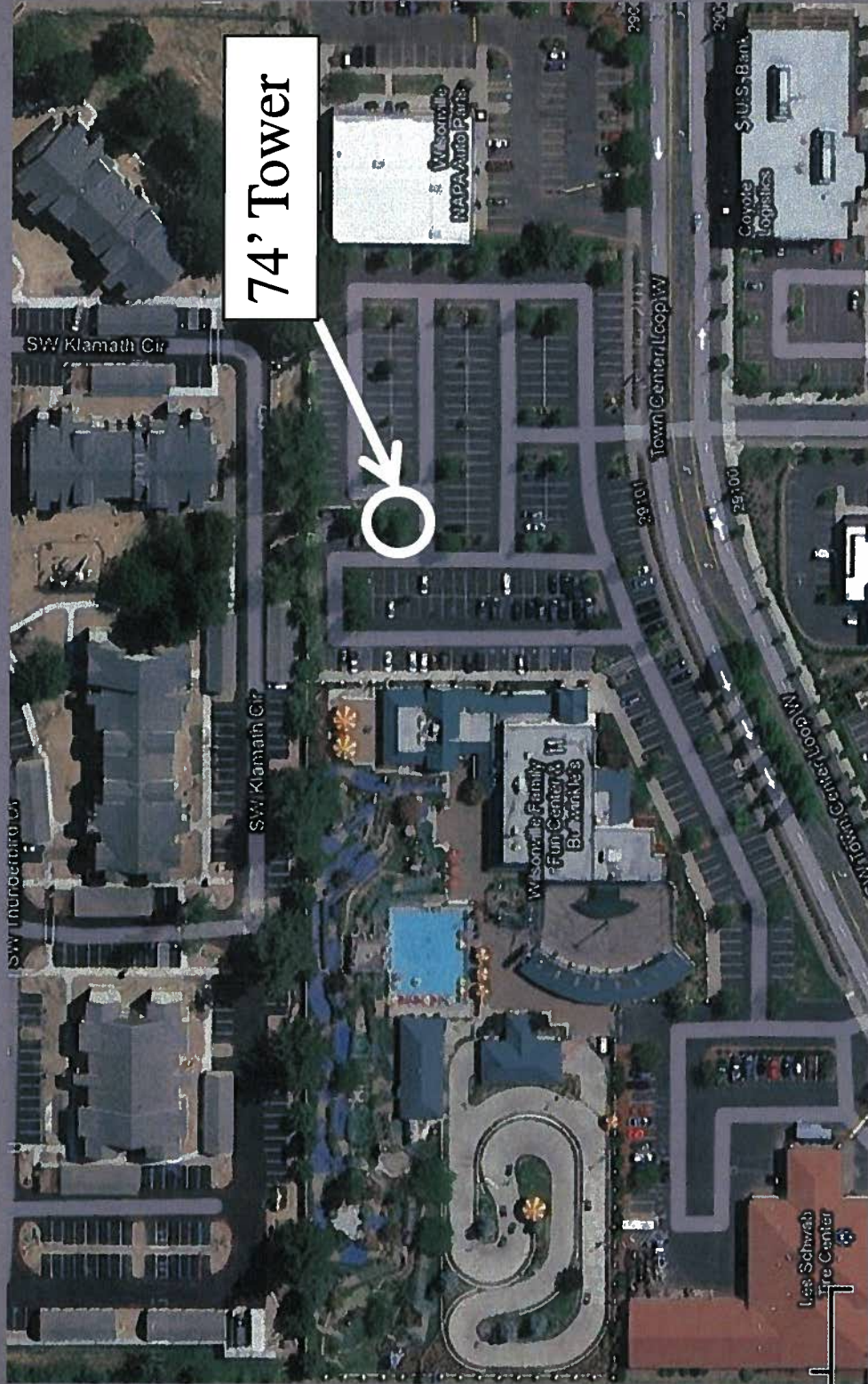
- DRB Panel A Public Hearing
- February 11, 2013
- Presented by: Amanda Hoffman, Assistant Planner



City of Wilsonville  
DB12-0070-73 et al.  
Exhibit A3



# Project Overview





# Visual

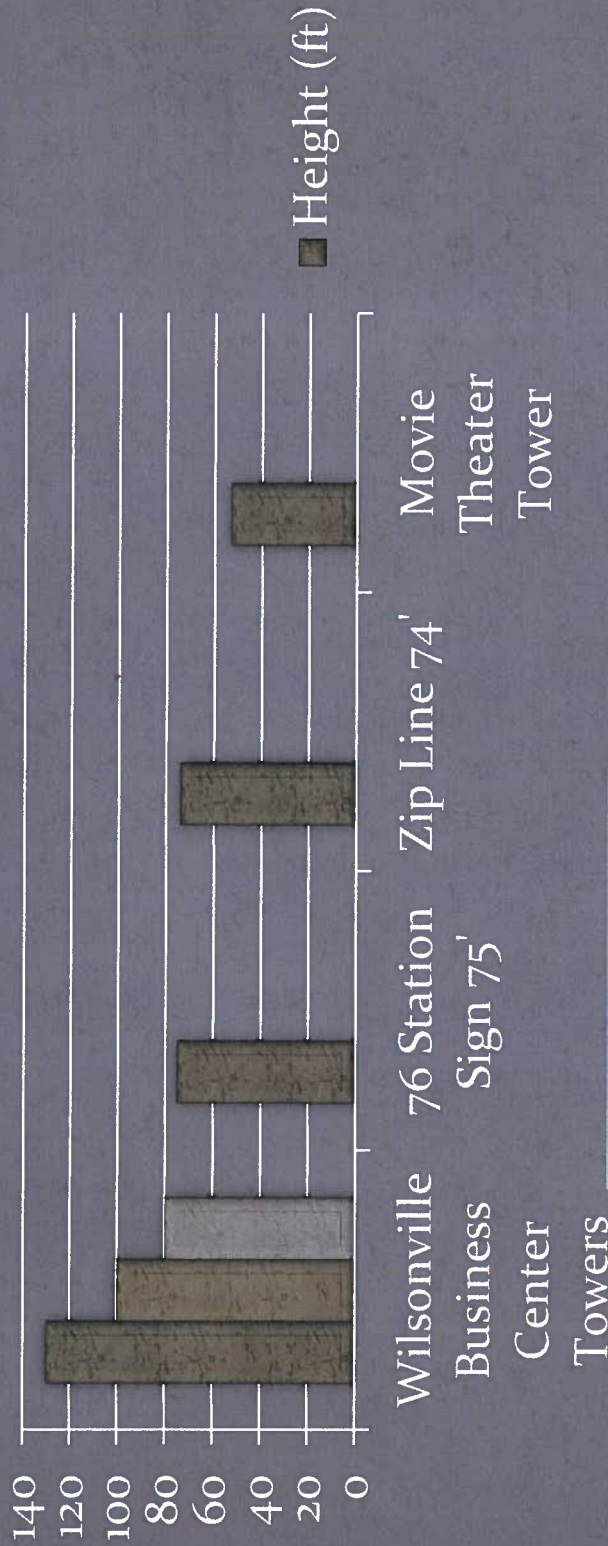








# Height Comparison

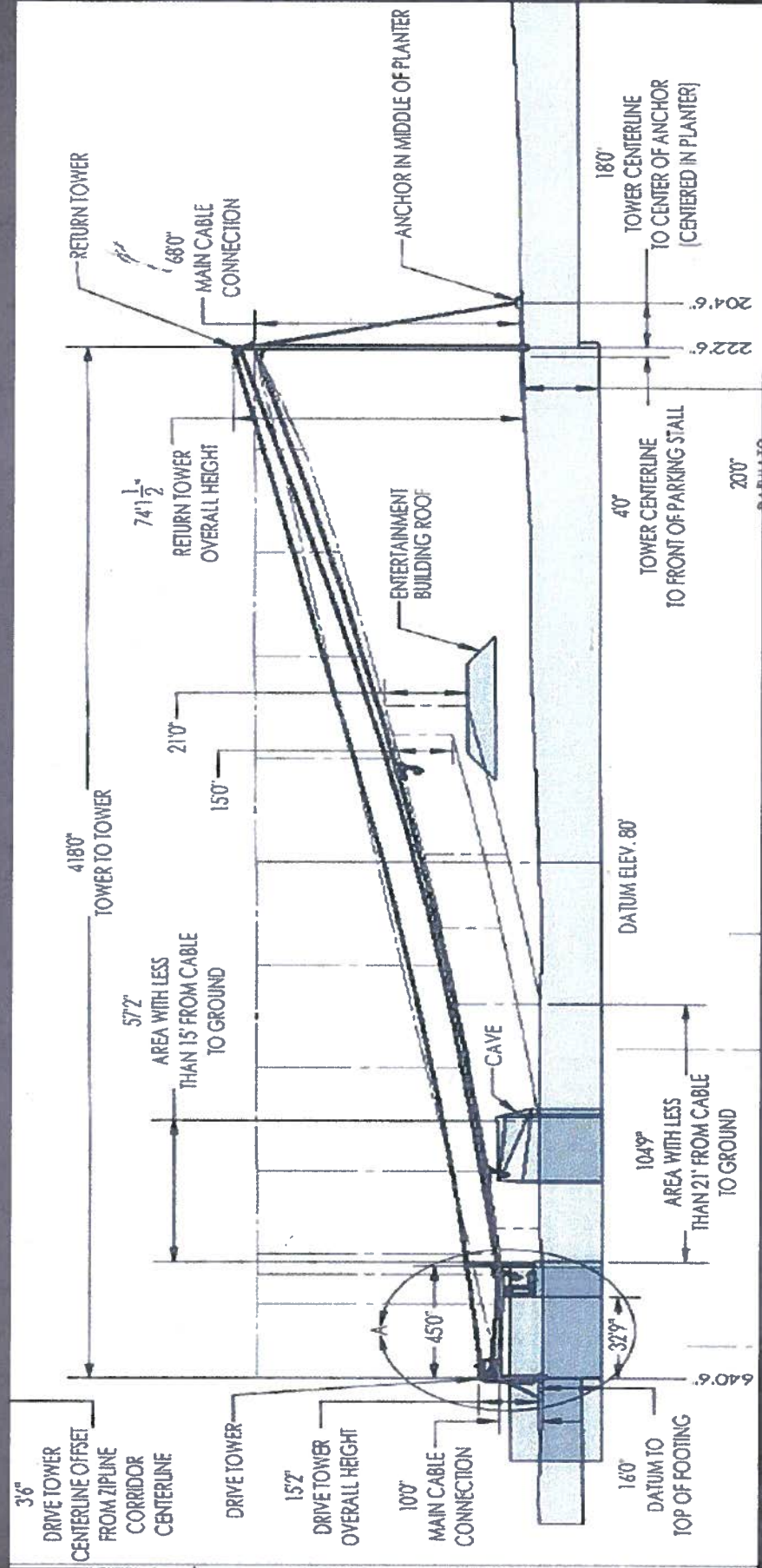


# Applicant's Arguments in Support of Requests


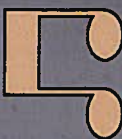


- Slim design (cell tower-like)
- Taller trees to the North
- Color (tan)
- Entertainment use is compatible with Town Center
- Safety training for TVFR



- Tower height needed to accommodate for clearance above entertainment building roof



# Staff Notes Regarding Requests

- 10pm restriction for batting cages 
- Prohibition in '92 by City Council regarding outdoor music and paging  
- Potential need for trees to be planted for additional screening to the north 
- Staff recommends *approval* of the project with conditions.

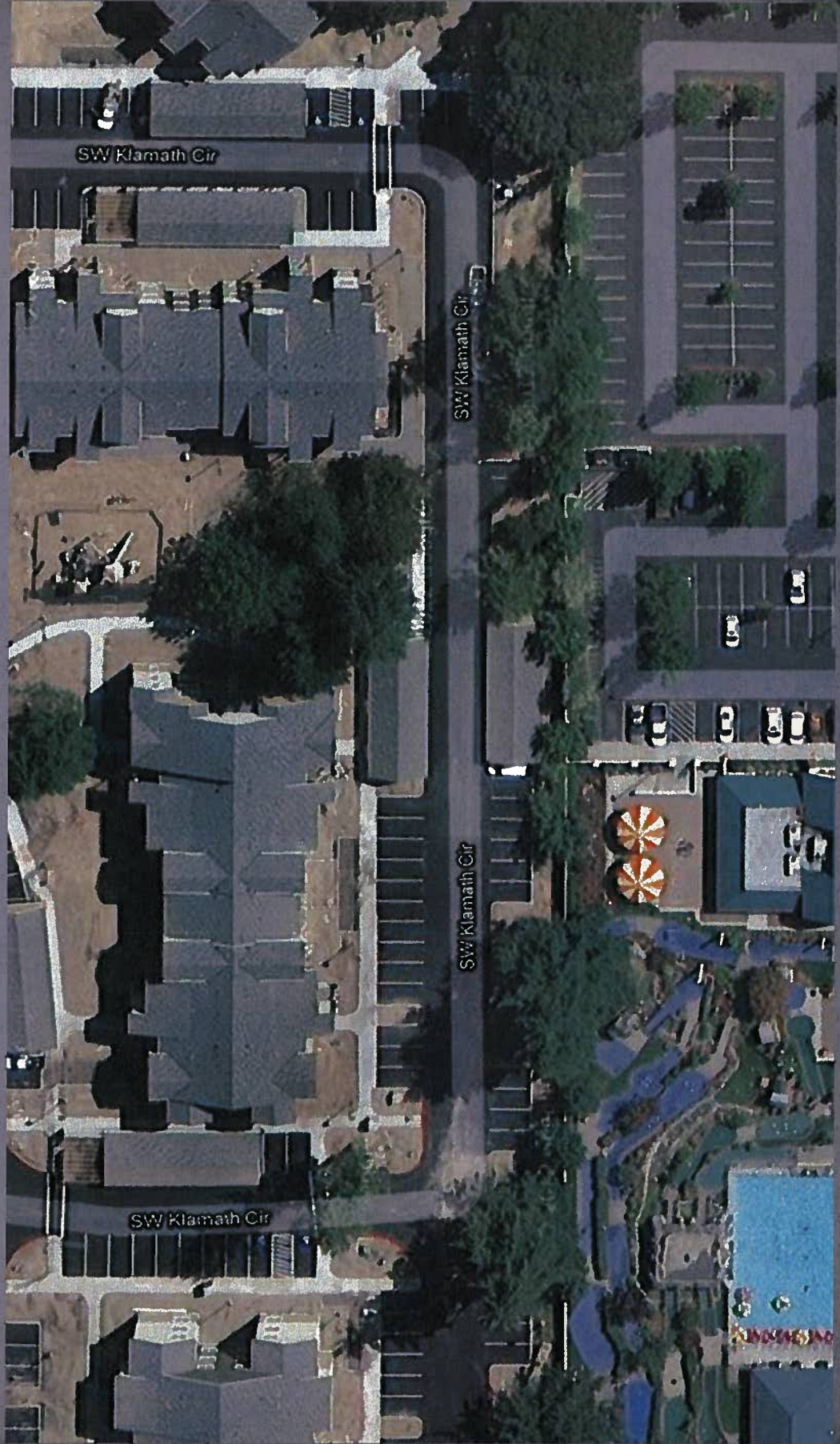


ADDITIONAL CONDITIONS OF APPROVAL ADOPTED BY THE  
WILSONVILLE CITY COUNCIL FOR 92 PC 05

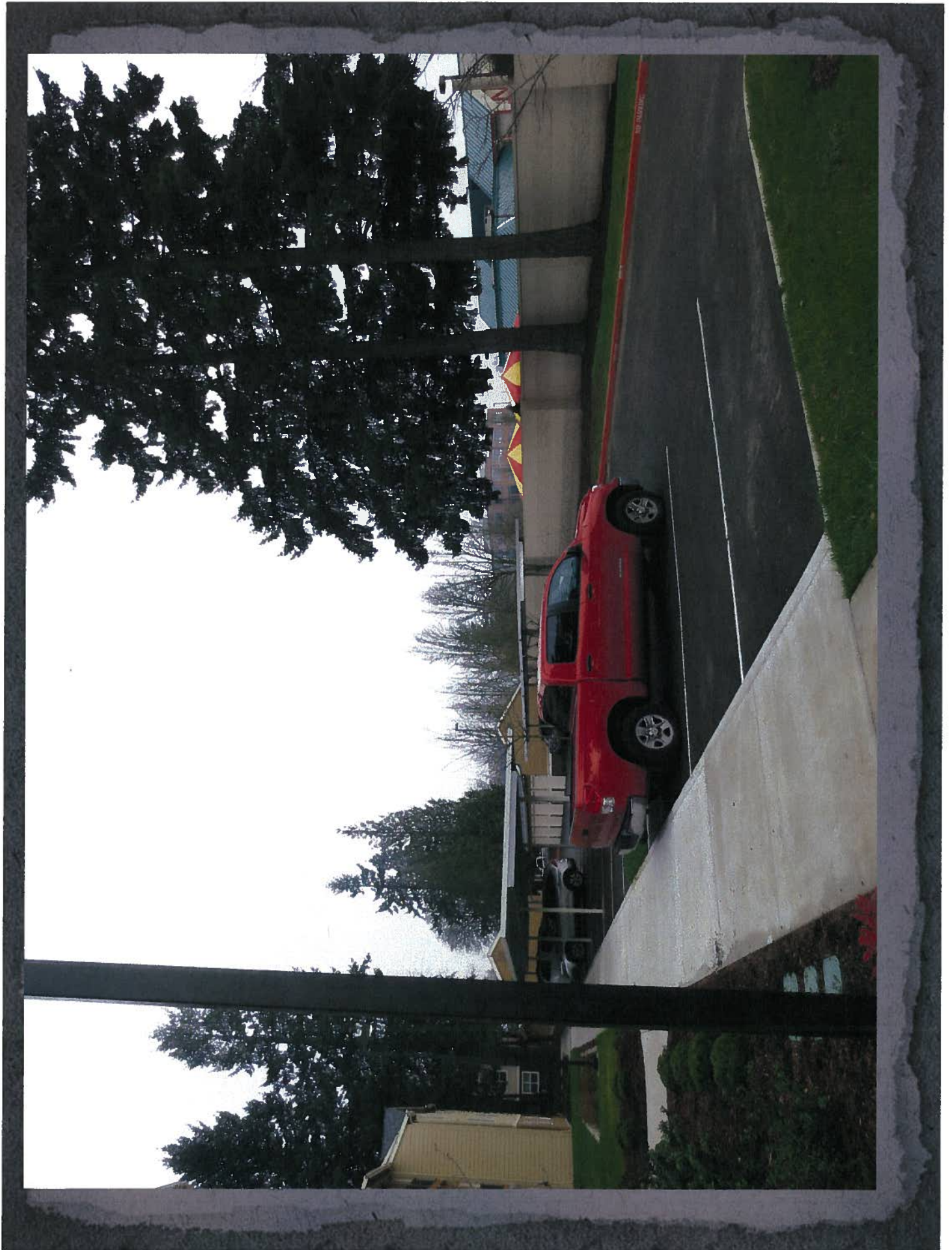
24. The proposed batting cage shall be located in the northwest portion of the subject property. Provided, however, it may be relocated on the property subject to Design Review Board approval and provided any such relocation does not increase the noise level to the adjacent Thunderbird Mobile Club properties from that of the location in the northwest portion of the subject property. The pitching machine shed roof and the shed front overhang shall be underlaid with a one (1) inch thick resilient foam or fiberglass and covered with fiberglass shingles.
25. Outdoor music reproduction and paging including outdoor telephone ringing devices, shall be prohibited.
26. A four (4) foot high Go-Cart barrier shall be constructed from the northwest corner of the track to the south edge of the Go-Cart driver loading zone. This noise barrier shall be approximately six (6) feet from the edge of the track and shall have a minimum thickness of eight (8) inches of solid wood or equivalent noise barrier material.
27. The Go-Cart Honda engines shall be of a type and kind as represented by the Environmental Noise Analysis and shall be fitted with the better exhaust muffler available from Honda. Nothing in this condition shall prevent replacement of the Honda engines by a quieter engine.
28. The applicant, at the City's request and direction, shall conduct noise measurements by an acoustical expert approved by the City at least two (2) times a year for the first three years of operation to demonstrate compliance with state standards regulating noise. Any change in operation during the first three years or any time thereafter which may increase noise may be subject to a noise measurement at the City's request. The applicant shall bear the expense of all noise measurements and, if not in compliance with state standards, shall take immediate measures to comply with state standards. Any portion of this facility found to violate a state noise standard shall not be operated until corrective measures are taken. The applicant shall notify the City immediately in writing of any complaints and/or violations and of what, if any, corrective measures taken.
29. The applicant shall construct a ten (10) foot high concrete block or masonry sound protection wall along the north property line. This wall shall be located inside of the property line and a landscape (planting) area shall be provided north of the wall. The actual location and design of the wall and planting area shall be subject to final approval of the Design Review Board. The Design Review Board may approve a reduction to at least 6 feet for a portion of the wall that extends 274 feet east of the northwest property corner if there is a written request and agreement to do so between the applicant and the Thunderbird Mobile Home Club Association. Said agreement, if any, shall be presented to the Design Review Board prior to or during a public hearing regarding this project.
30. Operation of the batting cage shall be prohibited between the hours of 10 p.m. and the regular opening time.



# Tree discussion









February 11<sup>th</sup>, 2013

Amanda Hoffman  
City of Wilsonville  
Planning Department  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

Re: Bullwinkle's Fun Center Zip-line

Amanda:

We received notification in the mail of Bullwinkle's Fun Center proposed zip-line. As the property owners of Jory Trail apartments to the north of Bullwinkle's, Holland Partner Group and our partners have concern over the visual and sound impact this could have on our property and residents. I had the opportunity to meet with Darren Harmon, general manager for Bullwinkle's Fun Center, to discuss the project and communicate our concerns in person.

Darren has always been a good neighbor and we believe if precautionary measures are taken we can be supportive of their proposal. We would like to recommend the following measures which we believe will mitigate the visual and sound impacts to our property and residents:

- Adding trees to areas of the fence line where there are currently gaps. This topic was brought up in our meeting and we would be happy to meet with Darren to discuss further and ensure adequate coverage.
- Stopping the zip-line rides by 10pm every night
- Darren and I discussed volume from exterior speakers playing music and complaints from residents at the south end of Jory Trail. If the volume of the exterior music is decreased, speakers on the fence line are removed and speakers that are currently facing north are repositioned to face south this should alleviate this issue. We also want to ensure the music is turned off at 10pm every night.

If you have any questions let me know.

Thank you,

Brenner Daniels  
Development Director



City of Wilsonville  
Exhibit D1 DB12-0070 et al



## **VII. Public Hearing:**

**B. Resolution 248. Old Town Single Family: Mark and Darla Britcliffe – owner/ applicant.** The applicant is requesting approval of a Site Design Review for two (2) single-family dwellings with attached accessory dwellings and a Type A Tree Removal Permit for two trees. The subject parcels are located at 9115 and 9185 SW 4<sup>th</sup> Street on Tax Lots 500 and 501, Section 23AC; T3S-R1W; Clackamas County; Wilsonville, Oregon. Staff: Amanda Hoffman

Case Files: DB13-0002 – Site Design Review  
TR13-0006 – Type A Tree Removal Permit

**DEVELOPMENT REVIEW BOARD  
RESOLUTION NO. 248**

**A RESOLUTION ADOPTING FINDINGS AND CONDITIONS APPROVING A SITE DESIGN REVIEW FOR TWO (2) SINGLE-FAMILY DWELLINGS WITH ATTACHED ACCESSORY DWELLINGS IN THE OLD TOWN OVERLAY DISTRICT. THE SUBJECT PARCELS ARE LOCATED ON TAX LOTS 500 AND 501, SECTION 23AC, T3S-R1W, CLACKAMAS COUNTY, OREGON, 9155 AND 9185 SW 4<sup>TH</sup> STREET. MARK AND DARLA BRITCLIFFE – OWNER/APPLICANT.**

WHEREAS, an application, together with planning exhibits for the above-captioned development, has been submitted in accordance with the procedures set forth in Section 4.008 of the Wilsonville Code, and

WHEREAS, the Planning Staff has prepared a staff report on the above-captioned subject dated March 4, 2013, and

WHEREAS, said planning exhibits and staff report were duly considered by the Development Review Board Panel A at a regularly scheduled meeting conducted on March 11, 2013, at which time exhibits, together with findings and public testimony were entered into the public record, and

WHEREAS, the Development Review Board considered the subject applications and the recommendations contained in the staff report, and

WHEREAS, interested parties, if any, have had an opportunity to be heard on the subject.

NOW, THEREFORE, BE IT RESOLVED that the Development Review Board of the City of Wilsonville does hereby adopt the staff report dated March 4, 2013, as amended, attached hereto as Exhibit A1, with findings and recommendations contained therein, and authorizes the Planning Director to issue permits consistent with said recommendations for:

DB13-0002 Site Design Review for (2) two single-family homes with attached accessory dwellings in the Old Town Overlay District.

ADOPTED by the Development Review Board of the City of Wilsonville at a regular meeting thereof this 11<sup>th</sup> day of March 2013 and filed with the Planning Administrative Assistant on \_\_\_\_\_. This resolution is final on the 15th calendar day after the postmarked date of the written notice of decision per *WC Sec 4.022(.09)* unless appealed per *WC Sec 4.022(.02)* or called up for review by the council in accordance with *WC Sec 4.022(.03)*.

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Mary Fierros Bower, Chair - Panel A  
Wilsonville Development Review Board

Attest:

---

Shelley White, Planning Administrative Assistant

**Exhibit A1**

**STAFF REPORT**  
**WILSONVILLE PLANNING DIVISION**  
*4<sup>th</sup> St. Single-Family Dwellings*  
**DEVELOPMENT REVIEW BOARD PANEL 'A'**  
**QUASI-JUDICIAL PUBLIC HEARING**

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**HEARING DATE:** March 11, 2013  
**DATE OF REPORT:** March 4, 2013

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**APPLICATION NO.:** DB13-0002: *Site Design Review*

**APPLICANT:** **Mark and Darla Britcliffe**

**OWNER:** **Mark and Darla Britcliffe**

**REQUEST:** The proposal includes Site Design Review for Tax Lots 500 and 501 in Section 23AC; T3S R1W; Clackamas County, Oregon. The applicant proposes to develop two single-family dwellings with attached accessory dwellings in the Old Town Overlay District.

**LOCATION:** 9155 & 9158 SW 4<sup>th</sup> Street, north of the unimproved spur of 4<sup>th</sup> Street (See Vicinity Map on Page 2).

**LEGAL DESCRIPTION:** Tax Lots 500 & 501 Township 3 South, Range 1 West, Section 23AC, Clackamas County, Oregon.

**LAND USE DESIGNATIONS:** Wilsonville Comprehensive Plan Map  
Designation: *Residential*

**ZONING DESIGNATIONS:** Wilsonville Zone Map Classification: *Planned Development Residential-4 (PDR-4)*.

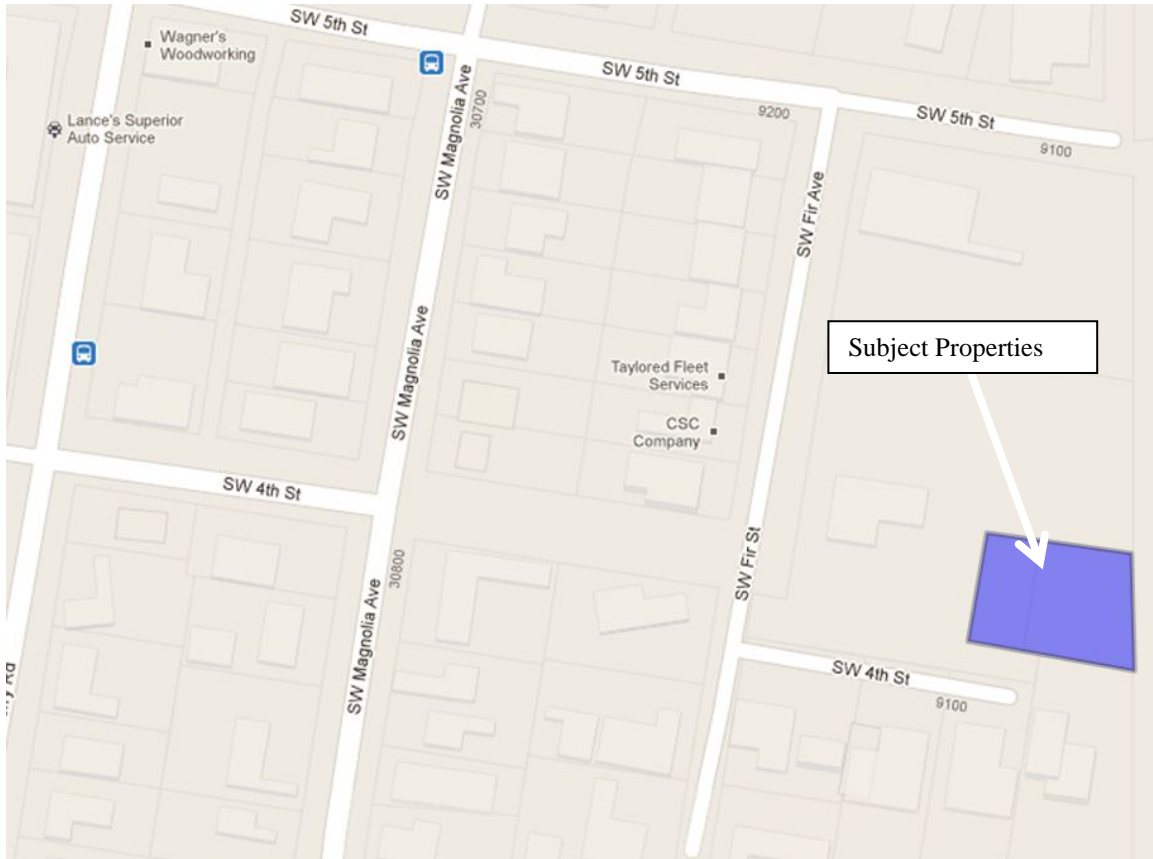
**STAFF REVIEWER:** Amanda Hoffman, Assistant Planner

**REQUESTED ACTIONS:**  
The Development Review Board is being asked to review:

**DB13-0002:** Site Design Review for 2 single-family dwellings with attached accessory dwellings in the Old Town Overlay District.

**STAFF RECOMMENDATION:** Approve the Site Design Review Plans, with recommended conditions of approval.

**VICINITY MAP:**



**APPLICABLE CRITERIA:**

**Planning and Land Development Ordinance:** Planning and Land Development Ordinance: Sections 4.001 (7); 4.008 – 4.015; 4.113; 4.118; 4.124.4; 4.138; 4.140; 4.155; 4.320; 4.400-4.450. **Other:** Boones Ferry Architectural Pattern Book

***SUMMARY:***

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- **Class 3 Site Design Review (DB13-0002):**

As demonstrated in findings 1 through 25, the proposed Site Design Review Plan (architecture and landscaping) with proposed conditions referenced therein **meets** the City criteria in Sections 4.400 – 4.450.

***CONCLUSIONS AND RECOMMENDATIONS:***

---

Staff has reviewed the applicant's analysis of compliance with the applicable criteria. Staff finds that the analysis satisfactorily demonstrates compliance with the City's Comprehensive Plan and the Planning and Land Development Ordinance. The Staff report adopts the applicant's responses as Findings of Fact, except as noted in the Conclusionary Findings, and modified by proposed Conditions of Approval. Based on the Findings of Fact and information included in this Staff Report, and information received from a duly advertised public hearing, Staff recommends that the Development Review Board approve the proposed application (DB13-0002), together with the following conditions:

***CONDITIONS OF APPROVAL:***

---

The application and supporting documents are hereby adopted for approval with the following conditions:

<b>PD = Planning Division Conditions</b>	<b>A. DB13-0002: Site Design Review</b>
--	---

**Planning Division Conditions:**

***DB13-0002: Class 3 Site Design Review***

**PDA 1.** The applicant/owner shall develop the houses in substantial compliance with the site design plans approved by the DRB, unless altered with Board approval, or minor revisions are approved by the Planning Director under a Class I administrative review process.

**PDA 2.** The applicant/owner shall provide the general contractor for the proposed project with a copy of the approved plans and conditions of approval adopted by the City.

**MASTER EXHIBITS LIST:**

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The following exhibits are hereby entered into the public record by the Development Review Board as confirmation of its consideration of the application as submitted. This is the master exhibits list that includes exhibits for Planning Case Files DB13-0002.

**A. Staff’s Written and Graphic Materials:**

**A1. Staff Report:**

- Findings of Fact
- Proposed Conditions of Approval
- Conclusionary Findings

**B. Applicant’s Written and Graphic Materials:**

**B1. Application Form; dated 1/23/2013**

**B2. Applicant’s Narrative; dated 1/23/2013**

**B3. Plan Set (Reduced size and full size):**

Page No.	Sheet Title
1	Site Plan
2	Elevations-9185 SW 4 <sup>th</sup> St
3	Elevations-9185 SW 4 <sup>th</sup> St
4	Tree Removal Plan
5	Elevations-9155 SW 4 <sup>th</sup> St
6	Elevations-9155 SW 4 <sup>th</sup> St

**B4. Color & Materials Board – *To be made available at Public Hearing***

**B5. Statutory Bargain and Sale Deed; dated 1/16/13**

**D. General Correspondence:**

- D1.** Letters (neither For nor Against): None submitted
- D2.** Letters (In Favor): None submitted
- D3.** Letters (Opposed): None submitted

***FINDINGS OF FACT:***

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**1. Statutory Timeline:**

The statutory 120-day time limit applies to this application. The application was received on January 24, 2013. On January 30, 2013, staff conducted a completeness review within the statutorily allowed 30-day review period. On January 31, 2013, the application was deemed complete. The City must render a final decision for the request, including any appeals, by May 31, 2013.

**2. Adjacent land uses:**

<b>Compass Direction</b>	<b>Zone:</b>	<b>Existing Use:</b>
North:	PDR-4	Vacant
East:	PDR-4	Single-Family home
South:	RAH-R	Single-Family homes
West:	RAH-P	ODOT

**3. Comprehensive Plan and Zone Maps:** The site has a Comprehensive Plan designation of Residential and is zoned Planned Development Residential (PDR-4).

**4. Natural Characteristics:** The subject premises has gently sloping terrain. Vegetation is limited to landscape and 2 large trees within the two single-family lots.

**5. Streets:** The subject property is located at the east of SW Fir Street and 4<sup>th</sup> Street. Access to the site is from an unimproved portion of 4th Street.

**6. Review Procedures:** The required public notices have been sent and all proper notification procedures have been satisfied.

**7. Previous land use approvals:** The subject property is part of the original Fir Street Development approval, however this approval has expired. DB06-0051-56, DB07-0020 Fir Street Development: Zone Map Amendment, Stage I Preliminary Plan, Tentative Partition Plat, Stage II Final Plan, Site Design Review Type C Tree Plan and Density Waiver.



## GENERAL INFORMATION

### ***Section 4.008. Application Procedures - In General.***

The applicant is requesting the necessary site development permit applications to develop Tax Lots 500 & 501 of Section 23AC; T3S R1W; Clackamas County, Oregon. The applicant proposes to develop two single-family homes on two lots in Old Town.

### ***Section 4.009. Who May Initiate Applications.***

The application has been submitted by Mark and Darla Britcliffe, the owners of the subject properties. This provision is met.

### ***Section 4.034. Application Requirements.***

Following is documentation, including the standards and procedures appropriate to Site Development Review.

**REQUEST DB13-0006: SITE DESIGN REVIEW  
CONCLUSIONARY FINDINGS**

The applicant is requesting approval of Site Design Review for two single-family dwellings on two lots with two accessory dwellings in the Old Town Overlay Zone.

***Section 4.124 PDR (Planned Development Residential) Zone***

***(.01) Examples of principal uses that are typically permitted:***

***B. Single-Family Dwelling Units.***

***(.02) Permitted accessory uses to single family dwellings:***

***C. Accessory Dwelling Units***

**A1.** The proposal is for two-single family dwellings with attached accessory dwellings on two lots, as permitted by the provisions of this section.

***(.03) Parking.***

**A2.** Per the requirements of Sections 4.155. beginning on page 11 of this report.

***Section 4.113 Standards Applying to Residential Developments in Any Zone.***

***(.11) Accessory Dwelling Units***

**A3.** The applicant is proposing 600 square foot accessory dwelling units (ADU's) in order to be mindful of the wishes of the Old Town residents, although the code requirement states 800 square foot maximum. The ADU's are proposed to be attached to each of the two main dwelling units and are proposed to meet all zone standards. The Accessory dwellings are subject to the Building Code requirements and will be reviewed as such with a building permit. The units are proposed to be integrated within the design of the proposed single-family dwellings with the same color, siding, windows, doors and roofing materials. The applicant is proposing an excess of one parking space per dwelling unit and therefore exceeds the requirement of one space per unit. Each ADU will be complete with separate living, sleeping, eating, cooking and bathing purposes and is proposed to have a secure separate entrance. They are both accessible by 4<sup>th</sup> Street via a driveway. Based on this finding, staff finds this criterion will be met.

***Section 4.138: Old Town (O) Overlay***

***(.01) Purpose***

***The Old Town Overlay is intended to capture the spirit of Wilsonville's past, and reflect it in new development or redevelopment. Period architecture, quality design, and relationship to surrounding uses is required by this overlay.***

*The applicant's proposal has achieved all of these purposes, using details from the early 1900's, and thoughtful consideration of the site's resources, and provided ample links to the surrounding neighborhood.*

***(.02) Applicable to new development, redevelopment***

**A4.** The applicant has applied the provisions of this overlay to the proposed two single-family dwellings with attached accessory dwellings.

***(.03) Development standards***

**A5.** The applicant proposes to be in compliance with the required setbacks, landscaping and building height requirements of the Old Town Overlay Zone. The applicant proposes a 15 foot rear setback, 5 foot side setbacks and a 20 foot front yard setback for 9185 and a 17 foot rear setback 10' side setbacks and a 43' front yard setback for 9155. The landscaping is proposed at over 20% for both properties and the building heights are proposed single-story at approximately 18 feet.

***(.04) Pedestrian environment***

**A6.** The applicant proposes the two single-family dwelling entrances to face 4<sup>th</sup> Street to give special attention to the primary building entrances, assuring that they are both attractive and functional. Staff finds the proposal to be in compliance with this requirement.

***(.05) Building compatibility***

**A7.** The proposal includes two-single family dwellings with attached accessory dwellings. The design of the residential dwellings are intended to reflect the size and shape of traditional dwellings from the period from 1880 to 1930 and both single-story. The applicant is proposing one Ranch-style home and one Craftsman-style home. The applicant has also proposed the entrance to the accessory dwellings on the side of the homes so that it gives the appearance of two smaller dwellings. The applicant has incorporated many of the Residential Design Standards suggested within the Boones Ferry Historic District Architectural Pattern Book. Therefore, staff finds that the building compatibility has been met.

***(.06) Building materials***

**A8.** The proposed house located at 9155 is a ranch style with a long and narrow efficient design, the applicant is proposing the use of 2 foot overhangs featuring a shed-style front porch. The proposed house will be clad with fiber cement horizontal plank with wood grain.

- A9.** The proposed house located at 9185 is a Craftsman style home with tapered pillars and wide slat pickets on the front porch. This house will also be clad with Hardiplank siding. The roof has multiple prominent corbels on the peak. Staff finds the proposal to be in compliance with this requirement.

***(.07) Roof materials, design***

- A10.** The applicant is proposing for the Ranch-style home, a 4/12 pitch with asphalt shingles CertainTeed Landmark color Heather Blend (shown on the materials board exhibit B4). The applicant is proposing for the Craftsman-style home a 5/12 pitch with asphalt shingles CertainTeed Landmark color Driftwood. Staff finds the proposal to be in compliance with this requirement.

***(.09) Building facades***

- A11.** The applicant has proposed covered porches for both single-family dwellings. 9185 has a gabled front porch supports and shake accents above the porches. 9155 has a shed roof and 2-foot eaves. Each entrance to the Accessory dwellings also have small porch overhangs. Staff finds the proposal to be in compliance with this requirement.

***(.11) Landscapes and streetscapes***

- A12.** The applicant proposes over 20% landscaping for each property.

***(.12) Lighting***

- A13.** The applicant is proposing to install a new street light to replace the existing pole that is now located in the middle of 4<sup>th</sup> Street directly in front of 9185. The applicant, as well as the neighbors believe that the new street light would enhance the livability and safety at the end of 4<sup>th</sup> Street. The new pole would be placed on the SW corner of 9185 and would have one 150 watt bulb and would be fitted with a flat lens to reduce glare. Other proposed lighting is limited to building lighting. Staff finds that the applicant's proposal complies with the provisions of Section 4.199.

***(.14) Storage of trash and recyclables***

- A14.** The trash and recyclables will be stored within the three-car garages.

***Subsection 4.140 Planned Development Regulations***

***Comprehensive Plan and Zoning: Planned Development Residential***

**A15.** The subject property contains one zoning district – PDR-4. The Comprehensive Plan identifies the subject property as Residential.

***Section 4.155 General Regulations – Parking, Loading and Bicycle Parking***

***(.03) Minimum and Maximum Off-Street Parking Requirements:***

***A. Access and Maneuvering***

**A16.** The subject properties have a limited frontage on SW 4<sup>th</sup> Street. However, the applicant proposes two driveways to access the proposed single-family dwellings. There is no proposed on-street parking, staff finds the applicant has proposed ample off-street parking.

***B.6. Off-street Parking for Multi-family***

**A17.** The proposal includes a 3-car garage in addition to ample off-street parking. Therefore there are sufficient areas established to provide for parking and storage of motorcycles, mopeds and bicycles.

***B.8. Parking Standards.***

TABLE 5: PARKING STANDARDS			
USE	PARKING MINIMUMS	PARKING MAXIMUMS	BICYCLE MINIMUMS
<b>a. Residential</b>			
1. Single and attached units and any apartments (9 or fewer units)	1 per D.U., except accessory dwelling units, which have no minimum.	No Limit	0

**A18.** Based upon the table above, the applicant is required to provide 4 parking spaces, one for each single-family dwelling and one for each accessory dwelling unit. The applicant is proposing 3 garage spaces and 3 off-street parking spaces for 9185. The applicant is proposing 3 garage spaces and 5 off-street parking spaces for 9155. The applicant states that because of the lack of on-street parking the proposal includes twice the recommendation for parking. Staff finds the proposal to be in compliance with this requirement.

***Section 4.320 Underground Utilities***

**A19.** The applicant is proposing all utilities to be installed underground.

***Section 4.421. Criteria and Application of Design Standards.***

**(.01) The following standards shall be utilized by the Board in reviewing the plans, drawings, sketches and other documents required for Site Design Review. These standards are intended to provide a frame of reference for the applicant in the development of site and building plans as well as a method of review for the Board. These standards shall not be regarded as inflexible requirements. They are not intended to discourage creativity, invention and innovation. The specifications of one or more particular architectural styles is not included in these standards. (Even in the Boones Ferry Overlay Zone, a range of architectural styles will be encouraged.)**

**A. Preservation of Landscape.**

**A20.** A landscape plan was provided as part of the applicant's submitted materials. The applicant does propose to remove two trees under a Type A Tree Removal permit in order to build the home located at 9185 SW 4<sup>th</sup> Street. This permit will be processed prior to building permit approval. This criterion is satisfied.

**B. Relation of Proposed Buildings to Environment.**

**A21.** Staff finds that the subject property does not contain steep slopes, is not within a Significant Resource Overlay Zone (SROZ), and has very little vegetation. This criterion is satisfied.

**C. Drives, Parking and Circulation.**

**A22.** Section 4.155, on page 10 above, provides a detailed discussion regarding drives, parking and circulation.

**D. Surface Water Drainage.**

**A23.** Surface water drainage will be reviewed and enforced with the building permit.

**E. Utility Service.**

**A24.** The applicant proposes to install all utilities underground.

***SUMMARY FINDINGS FOR DB13-0002 – Site Design Review:***

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**A25.** The applicant has borne the burden of proof in demonstrating that the proposed Site Design Review plans should be approved.

# CITY OF WILSONVILLE

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503.682.4960  
Fax: 503.682.7025  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)

## Planning Division Development Permit Application

Final action on development application or zone change is required within 120 days in accordance with provisions of ORS 227.175

A pre application conference is normally required prior to submittal of an application. Please visit the City's website for submittal requirements

Incomplete applications will not be scheduled for public hearing until all of the required materials are submitted.

Pre-Application meeting date: \_\_\_\_\_

### TO BE COMPLETED BY APPLICANT:

Please PRINT legibly

Legal Property Owner's Name:

mark and Darla Britcliffe

Address: 27485 SW Xanthus CT

Sherwood, or 97140

Phone: 503 516 7700

Fax: 925 8387

E-mail: markebrit@yahoo.com

Authorized Representative:

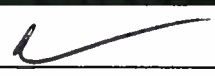
Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Fax: \_\_\_\_\_

E-mail: \_\_\_\_\_

Property Owner or

Authorized Signature: 

Name

Printed MARK Britcliffe Date 1/23/13

Title: owner

### Site Location and Description:

Project Address if Available: 9155-9185 SW 4th ST. Suite/Unit \_\_\_\_\_

Project Location: \_\_\_\_\_

Tax Map #(s): 31W23A000 Tax Lot #(s): 500-501 County:  Washington  Clackamas

### Request:

Build one home on each parcel with attached ADU's

Project Type: Class I  Class II  Class III

Residential  Commercial  Industrial  Other (describe below)

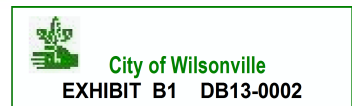
### Application Type:

- Annexation  Appeal  Comp Plan Map Amend  Conditional Use
- Final Plat  Major Partition  Minor Partition  Parks Plan Review
- Plan Amendment  Planned Development  Preliminary Plat  Request to Modify Condition
- Request for Special Meeting  Request for Time Extension  Signs  Site Design Review
- SROZ/SRIR Review  Staff Interpretation  Stage I Master Plan  Stage II Final Plan
- Type C Tree Removal Plan  Tree Removal Permit (B or C)  Temporary Use  Variance
- Villebois SAP  Villebois PDP  Villebois FDP  Waiver
- Zone Map Amendment  Other

### FOR STAFF USE ONLY:

Application Rec'd: \_\_\_\_\_ Fee: \_\_\_\_\_ Check #: \_\_\_\_\_ Application Complete: \_\_\_\_\_ By: \_\_\_\_\_

File No (s) \_\_\_\_\_



## NARRATIVE

This application is submitted by Mark and Darla Britcliffe 27485 SW Xanthus Ct Sherwood Oregon for building two single family homes. One each on two existing vacant building lots, 9185 and 9155 SW 4th street Wilsonville Oregon. On 9185 we propose a 2000 sq ft home included within a 600 sq ft Accessory Dwelling Unit plus attached 3 car garage. On 9155 we propose an 1800 sq ft home included within a 600 sq ft Accessory Dwelling Unit plus attached 3 car garage.

Formerly there was a mobile home located on 9155 but it was removed prior to our acquisition of the property.

We have reviewed the recommendations in the Old Town Neighborhood Plan and Boones Ferry Historic District and Architectural Pattern Book and developed our plan around it.

### SETBACKS

All of the planed building setbacks are within the recommended minimums: 5 ft side yard, 15 ft front and back yard with 20 allowed in front of the garages. The highest point on any building is 17 ft with the recommendation allowing for a maximum 35 ft. As shown on the attached plot and elevation plans.

### BUILDING COVERAGE

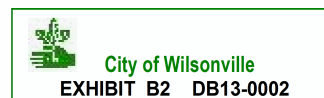
For building coverage, the current city code allows for 75% building coverage on each lot. The proposed coverage for 9155 is 29% with 2568 sq ft of covered area on a 9000 sq ft lot. The proposed coverage for 9185 is 54% with 2650 sq ft of covered area on a 5000 sq ft lot. The Pattern book shows a plot plan of a 5000 sq ft lot with 35% coverage as an example, however in that example there is no allotment for covered parking(garage/carport). This subdivision was completed long ago and the current configuration would certainly not pass today's standards. 4<sup>th</sup> Street dead ends leaving less that 14 ft street frontage for 9155 In the current configuration there is little to no allowance for on-street parking in the direct vicinity of the 2 lots therefore we thought it necessary to allow ample off-street parking which does affect our coverage percentage. The total proposed coverage for the 2 lots is 38% which is half the allowance under the current code. Under these conditions we believe we have offered a completely reasonable coverage plan

### ACCESSORY DWELLING UNIT

Each home will have an attached 600 sq ft Accessory Dwelling Unit (ADU) which is allowable under current guide lines. There is a recommendation to limit the amount of ADU's in the Overlay Area however these are the first to be proposed so that recommendation would not apply here. There is some discussion as to who should be allowed to occupy these spaces. The current city code allows occupancy of the ADU to be determined by the homeowner. However some would want to have only family members allowed to occupy these spaces. Restricting occupancy would be troublesome. There is substantial investment in developing these units and it would be a financial burden and waste to the homeowner who is waiting for a family member who needs to use the space or as it sits vacant after it is no longer needed for that purpose. There would be economic pressures on the homeowner that might cause them to violate any such rule or law, enforcement would have to put in place and neighbors would be doing genealogy studies on the people across the street. I certainly agree with the current code that allows the owner of the home to determine the homes occupants.

### PARKING

For parking the recommended amount of parking spots needed is 3 per dwelling, including 1 for the ADU. For 9185 we propose 6 spots, 3 covered and 3 uncovered which is twice the recommendation. Because of the lack of on-street parking due to the odd configuration of 9155 we have allotted for 7 spots, 3 covered and 5 uncovered, ample for guest parking. Parking is shown on attached plot plan.





## ARCHITECTUAL DESIGN AND MATERIALS

For architectural design we referred to the Pattern Book. For 9150 we chose the Ranch style. Long and narrow with efficient design. The roof has a 4/12 pitch with 2 ft overhangs featuring a Shed style front porch. The roof will be covered with Architectural style asphalt shingles CertainTeed Landmark color Hearther Blend. The house will be clad with fiber cement horizontal plank with wood grain.(Hardiplank).The body of the home will be painted with Sherman Williams color Buckram Binding and the trim with Wickerworks. For 9185 we chose the Arts and Crafts style with tapered pillars and wide slat pickets on the front porch. This house will also be clad with Hardiplank. The roof has a 5/12 pitch with multiple prominent corbels on the peak. The roof will be covered with CertainTeed Landmark color Driftwood. The body of the house will be painted with Sherman Williams color Belvedere Cream and the trim with White Hyacinth. See attached drawings and colorboard for details. On both houses we tried to limit the amount and size of the windows to reduce freeway noise and odor from the neighboring sewage treatment plant. Also most of the recommended home designs in the Pattern Book have no front-facing garages. These designs would be more appropriate for a corner lot or in a development with pre-planned ally's such as the Villebois Subdivision in west Wilsonville, not for a lot with front facing access and the need for off-street parking as we have here. We believe we have proposed a reasonable compromise considering the current lot configuration constraints.

## EXTERIOR LIGHTING

For exterior lighting we propose a standard 60 watt light fixture installed at all entrances to the homes. We also propose a street light typical of the 2 other street lights on 4<sup>th</sup> st. This light would be installed atop a new utility pole that will be placed on the sw corner of 9185 it would have one 150 watt bulb similar to the other 2 however it would be fitted with a flat lens to reduce glare. The new pole will replace the existing pole that is now located in the middle of 4<sup>th</sup> st directly in front of 9185 as seen on the attached plot to plan. The Old Town Plan recommends a specific light pole that would be a separate pole from the new utility pole. As these 2 building lots were developed over 35 years ago there is no requirement that we install any street lighting., however we believe, as do the neighbors who would be directly affected by a new streetlight, that the new light would enhance the livability and safety at the end of 4<sup>th</sup> st

## LANDSCAPING

There is a recommendation that each lot have at least 15% coverage by landscaping. As shown on the attached plot plan we propose to cover over 20%. On 9185 there will 1000 sq ft and 9155 over 2000sq ft. There will be a lawn in the front and back of both homes with scrubs along the perimeter with special attention to block the view of I-5 to the east.

## TREE REMOVAL

There are 2 large fir trees on 9185 we propose to remove. These trees are 40 inches in diameter and 75 ft tall. As shown on the attached Tree Removal Plan the canopy/drip line of these trees covers approximately 70% of the buildable area of this lot. It is widely regarded that there should be no excavation or vehicle traffic in this area if they are to be preserved. It has been recommended that effort should be given in planning around existing trees however in this instance due amount impact these trees pose it is simply not feasible to leave them

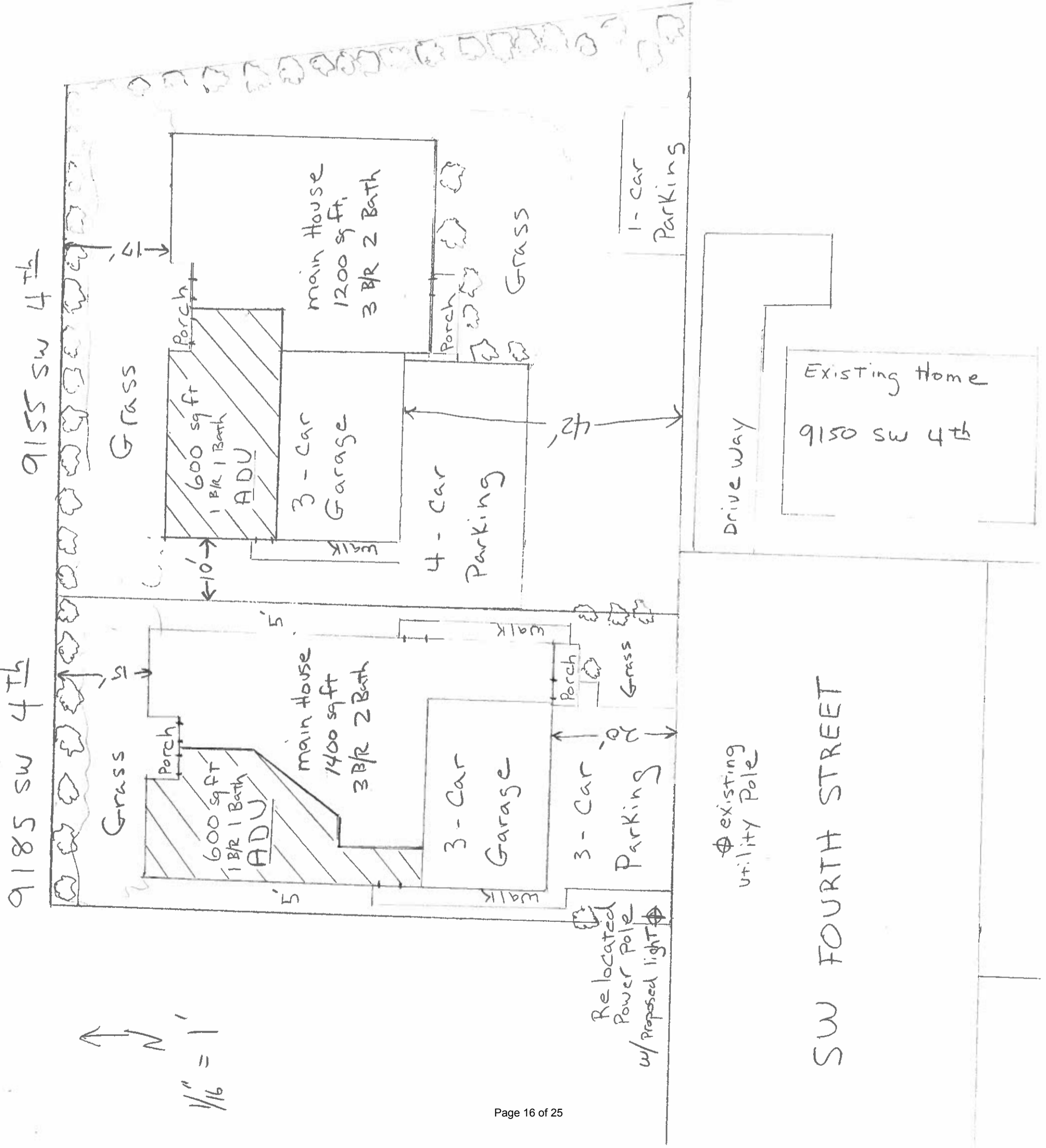
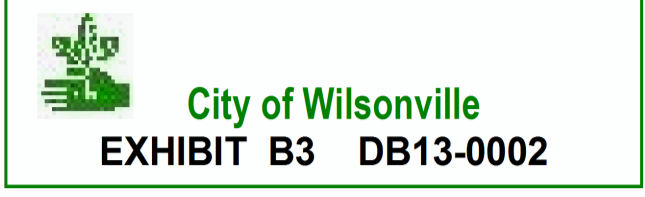
## SUMMARY

We believe we have submitted a plan for two houses that will enhance the existing neighborhood. This Plan meets all current city planning codes and restrictions and meets or exceeds most of the recommendation set forth in the Old Town Plan while balancing the economic issues of building on lots at the end of a non city owned or maintained gravel street that are flanked by a very visible and well heard I-5 a couple hundred feet to the east and an unseen but often smelled sewage treatment plant just a couple hundred feet to the south.

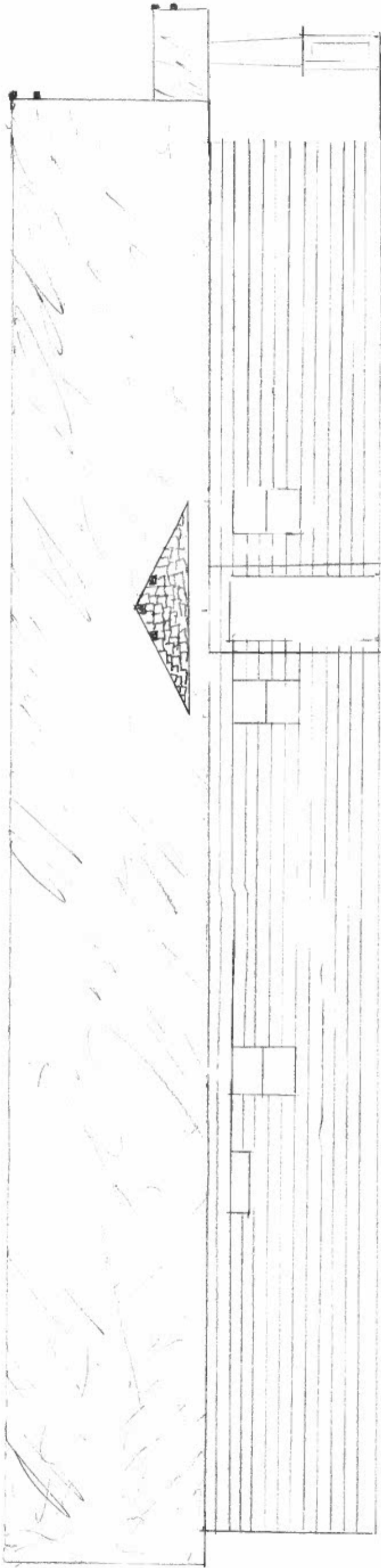
Mark and Darla Britcliffe  
27485 SW Xanthus Ct  
Sherwood, Or.

# SITE PLAN

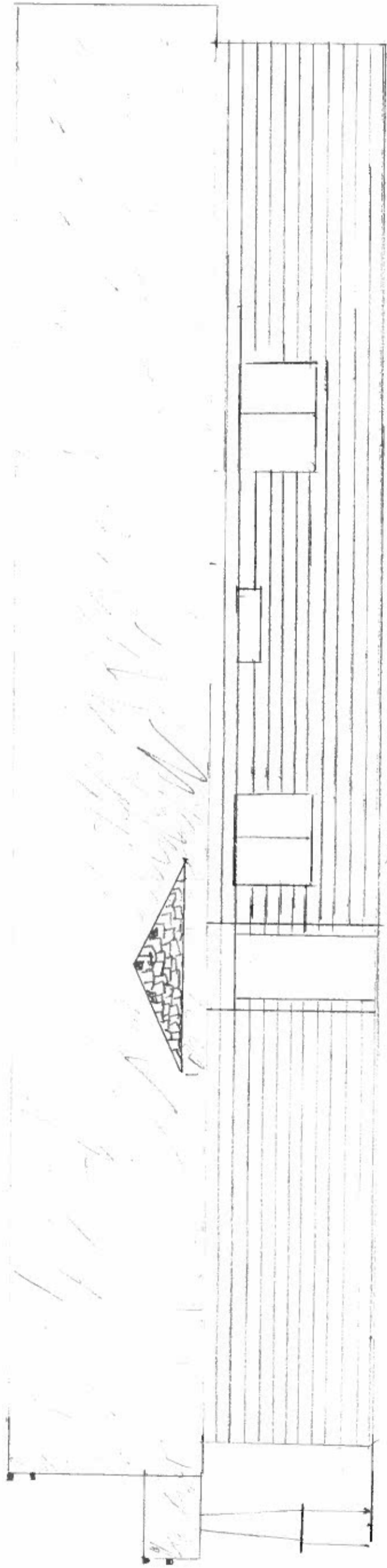
- landscape
- Parking
- lighting
- Set Backs



3/16" = 1'  
9185 SW 4<sup>th</sup>



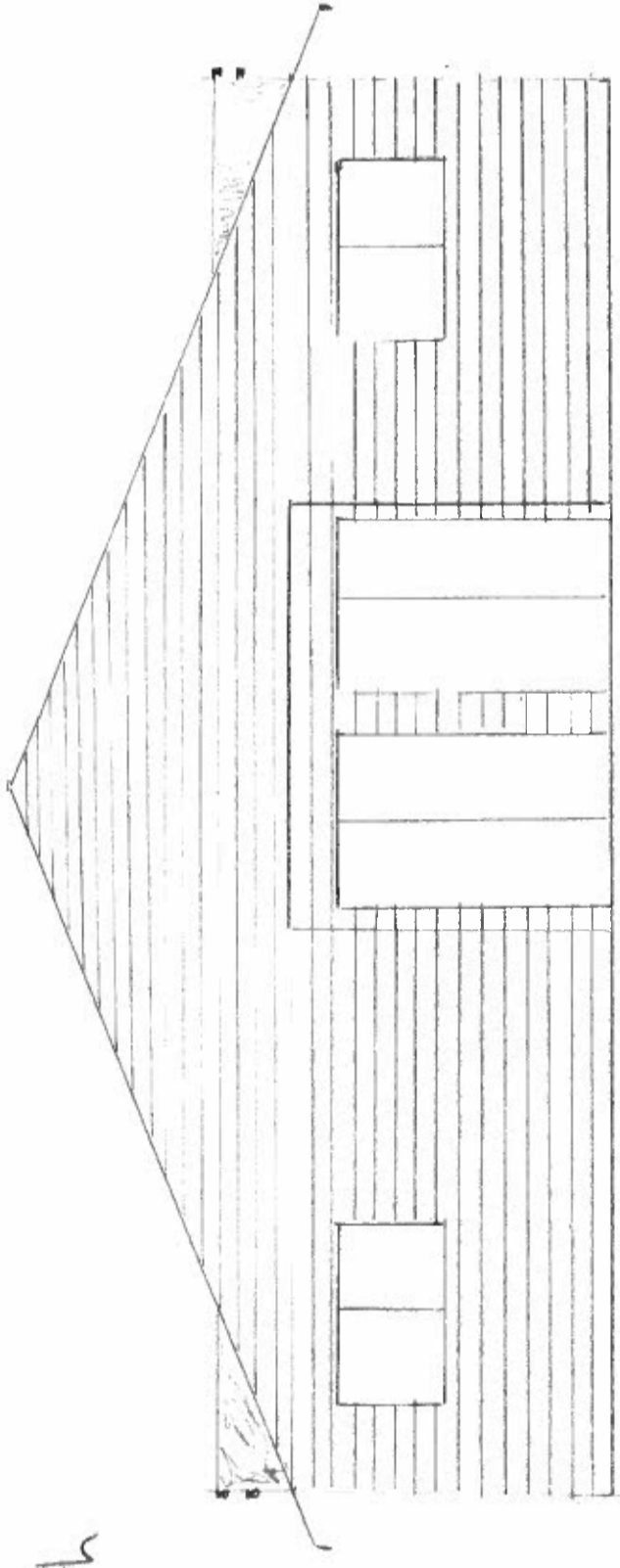
West Elevation



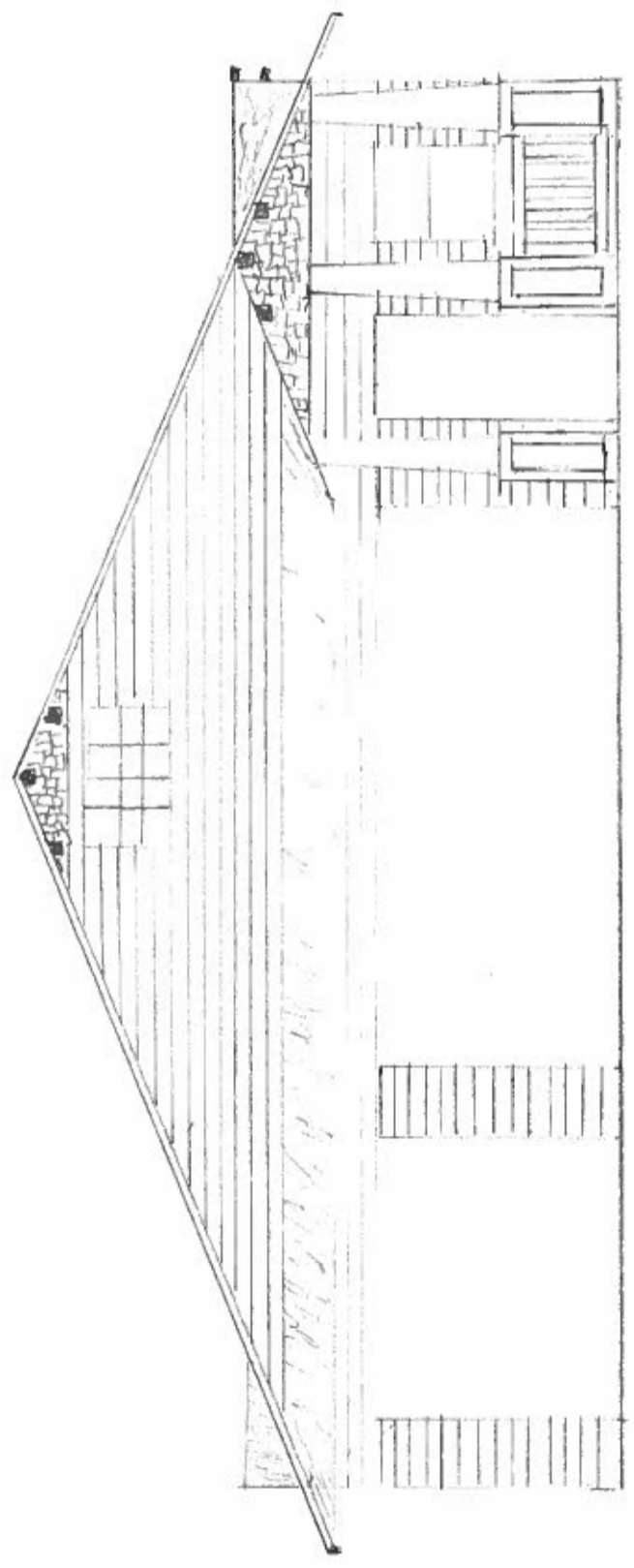
East Elevation

9185 SW 4th

3/16" = 1'



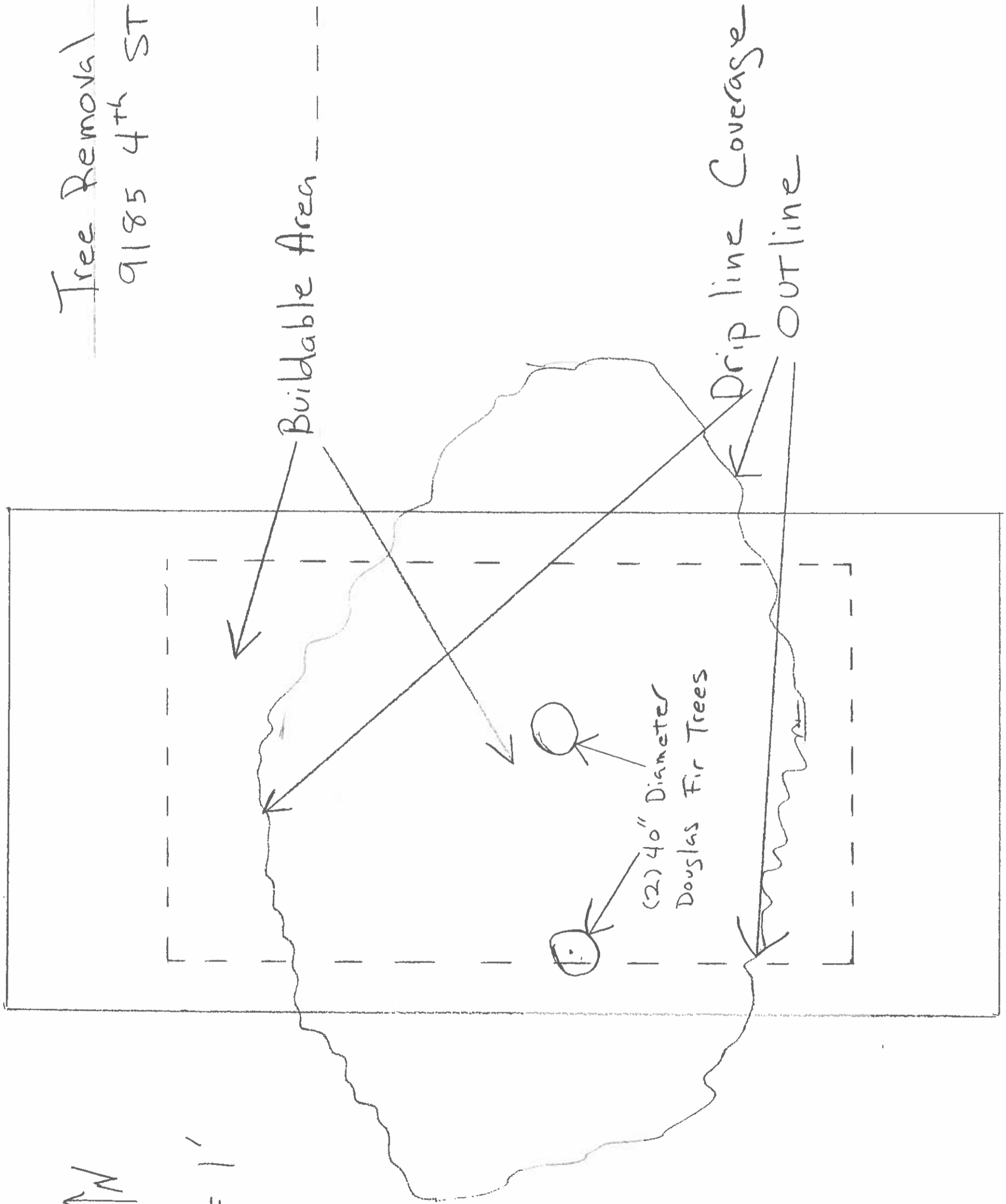
Rear Elevation



Front Elevation

# Tree Removal Plan

9185 4<sup>th</sup> ST



3/32" = 1'

Buildable Area

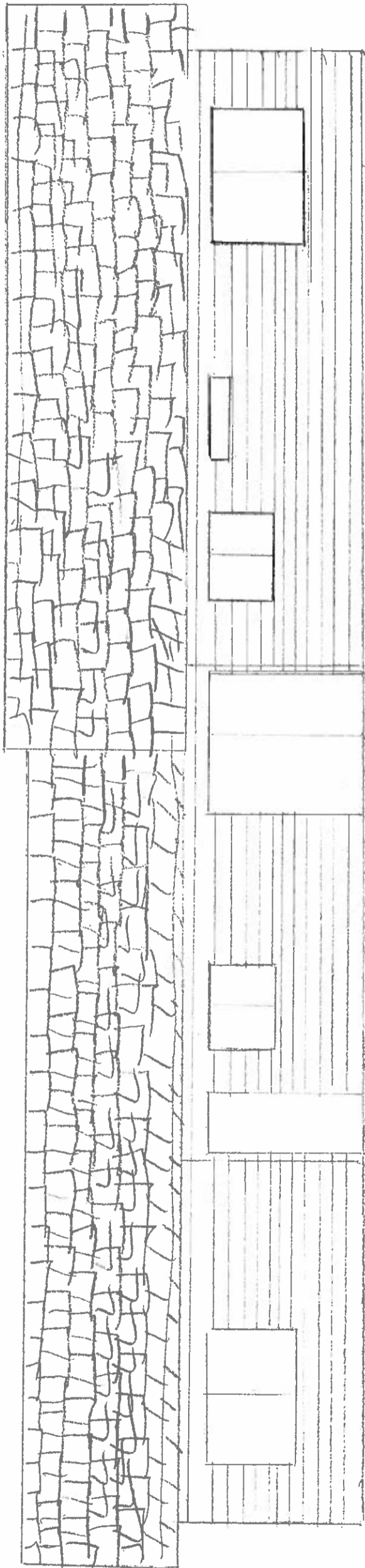
(2) 40" Diameter  
Douglas Fir Trees

Drip line Coverage  
Outline

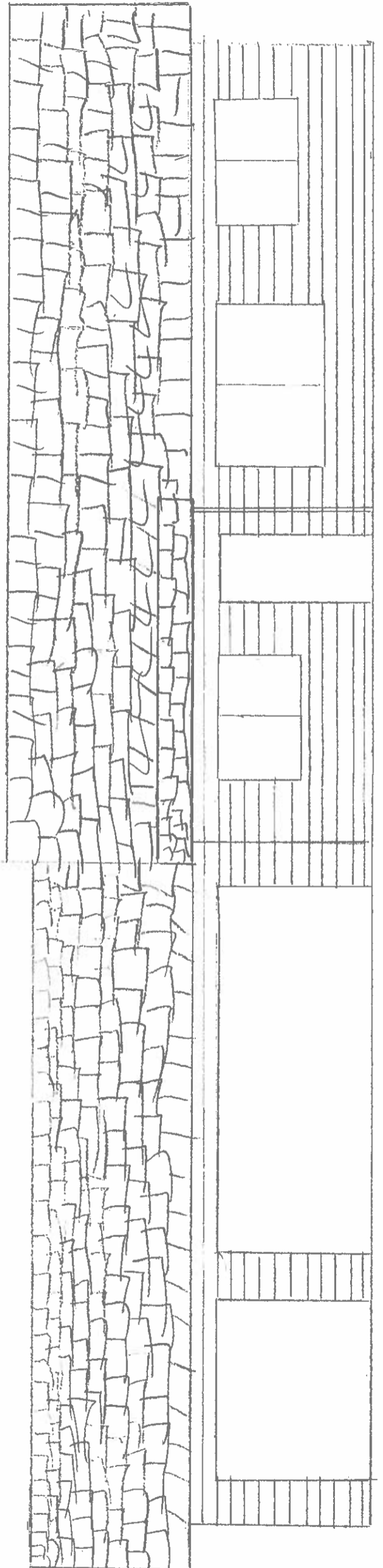
SW 4<sup>th</sup> Street

9155 sw 4<sup>th</sup>

3/16" = 1'



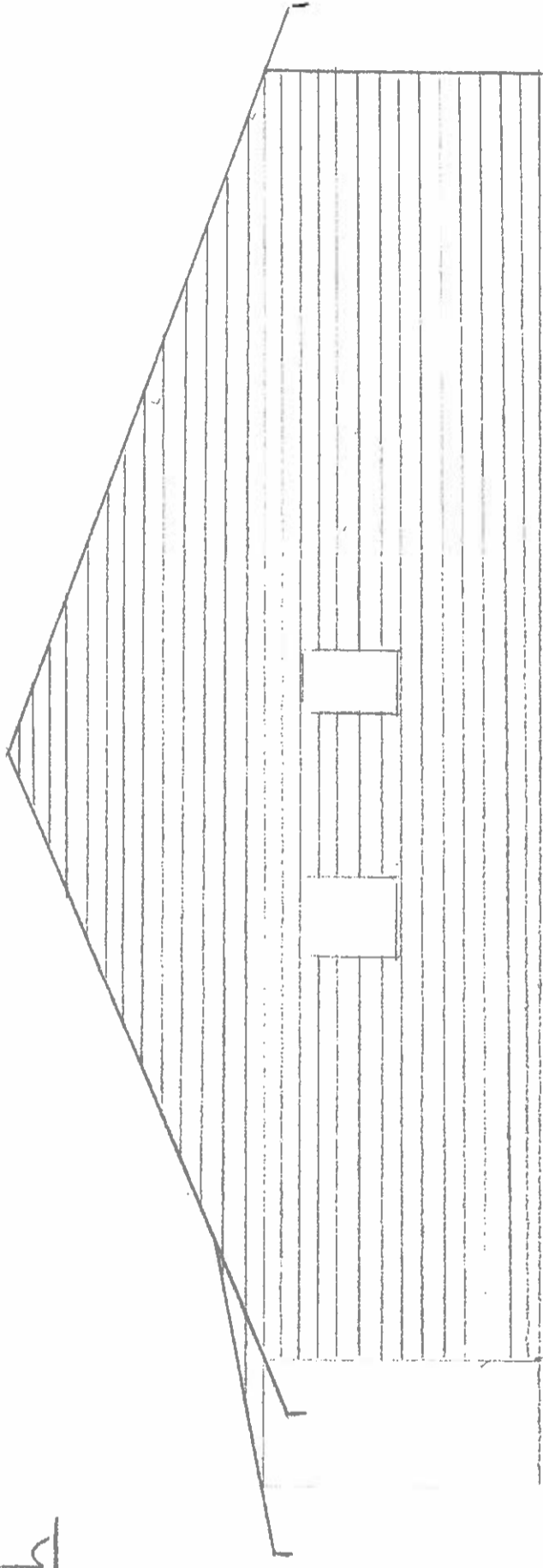
Rear Elevation



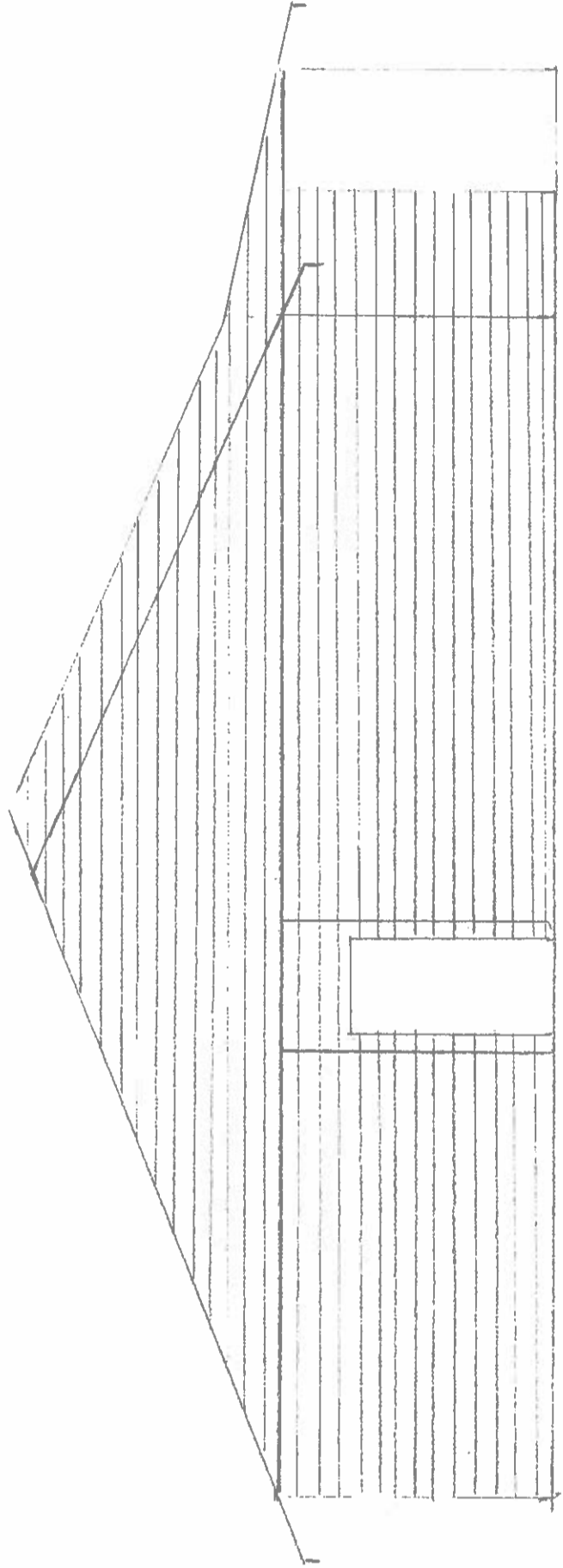
Front Elevation

9155 Sw 4<sup>th</sup>

3/16" = 1'



West Side Elevation



East Side Elevation

472512505765 - 45

CHICAGO TITLE

RECORDING REQUESTED BY:

Chicago Title Company of Oregon  
10135 SE Sunnyside Road Suite 130  
Clackamas, OR 97015

GRANTOR:  
Bernadine H. Becker

GRANTEE:  
Mark Britcliffe and Darla Britcliffe, husband and wife  
27485 SW Xanthus Ct.  
Sherwood, OR 97140

SEND TAX STATEMENTS TO:  
Mark Britcliffe and Darla Britcliffe  
27485 SW Xanthus Ct.  
Sherwood, OR 97140

AFTER RECORDING RETURN TO:  
Mark Britcliffe and Darla Britcliffe  
27485 SW Xanthus Ct.  
Sherwood, OR 97140

Escrow No: 472512505765TW-CT45

9185 & 9155 SW 4th St.  
Wilsonville, OR 97070

Clackamas County Official Records		<b>2013-003402</b>
Sherry Hall, County Clerk		01/16/2013 08:41:16 AM
D-D	Cnt=1 Str=5 CONNIEBRO	
\$15.00	\$16.00 \$10.00 \$17.00	\$58.00

SPACE ABOVE THIS LINE FOR RECORDER'S USE

**WARRANTY DEED – STATUTORY FORM**  
(INDIVIDUAL or CORPORATION)

Bernadine H. Becker

Grantor, conveys and warrants to

Mark Britcliffe and Darla Britcliffe, husband and wife

Grantee, the following described real property free of encumbrances except as specifically set forth herein:

SEE LEGAL DESCRIPTION ATTACHED HERETO

The true consideration for this conveyance is \$109,000.00.

ENCUMBRANCES: None

**BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010. THIS INSTRUMENT DOES NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY THAT THE UNIT OF LAND BEING TRANSFERRED IS A LAWFULLY ESTABLISHED LOT OR PARCEL, AS DEFINED IN ORS 92.010 OR 215.010, TO VERIFY THE APPROVED USES OF THE LOT OR PARCEL, TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES, AS DEFINED IN ORS 30.930, AND TO INQUIRE ABOUT THE RIGHTS OF NEIGHBORING PROPERTY OWNERS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7,**

472512505765TW-CT45  
Deed (Warranty – Statutory (Individual or Corporation))



RECORDING REQUESTED BY:

Chicago Title Company of Oregon  
10135 SE Sunnyside Road Suite 130  
Clackamas, OR 97015

GRANTOR:  
Bernadine H. Becker

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Sherwood, OR 97140

Escrow No: 472512505765TW-CT45

9185 & 9155 SW 4th St.  
Wilsonville, OR 97070

SPACE ABOVE THIS LINE FOR RECORDER'S USE

CHICAGO TITLE

472512505765 - 45

**WARRANTY DEED – STATUTORY FORM**  
(INDIVIDUAL or CORPORATION)

Bernadine H. Becker

Grantor, conveys and warrants to

Mark Britcliffe and Darla Britcliffe, husband and wife

Grantee, the following described real property free of encumbrances except as specifically set forth herein:

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ENCUMBRANCES: None

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472512505765TW-CT45  
Deed (Warranty – Statutory (Individual or Corporation))

CHAPTER 8, OREGON LAWS 2010.

Dated January 22, 2013; if a corporate grantor, it has caused its name to be signed by order of its board of directors.

Bernadine H. Becker  
Bernadine H. Becker

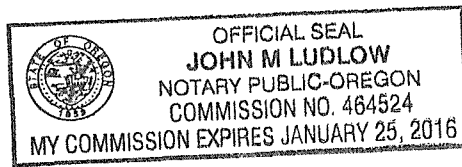
STATE OF OREGON  
County of Yamhill

This instrument was acknowledged before me on 01/ 22 /13 by Bernadine H. Becker.

[Signature]  
Notary Public for Oregon

My Commission Expires:

(SEAL)



## LEGAL DESCRIPTION

### Parcel I:

A tract of land in the Thomas Bailey Donation Land Claim, and in Section 23, Township 3 South, Range 1 West, of the Willamette Meridian, in the City of Wilsonville, County of Clackamas and State of Oregon, described as follows:

Beginning at the Southwesterly corner of that tract of land conveyed to Arthur L. Pack, et ux, by deed recorded June 12, 1968, under Recorder's Fee No. 68-10973, which beginning point bears South 7°42' West, 15 feet and South 82°18' East, 191 feet from the intersection of the center line of Fir Avenue, with the South line of 4th Street, in the PLAT OF THE TOWN OF WILSONVILLE; thence South 82°8' East along the Southerly line of said Pack tract, 50 feet; thence North 7°42' East, 100 feet to the Northerly line of said Pack tract; thence North 82°18' West along the Northerly line of said Pack tract, 50 feet to the Northwesterly corner thereof; thence South 7°42' West along the Westerly line of said Pack tract, 100 feet to the point of beginning.

### Parcel II:

A tract of land in the Thomas Bailey Donation Land Claim, and in Section 23, Township 3 South, Range 1 West, of the Willamette Meridian, in the City of Wilsonville, County of Clackamas and State of Oregon, described as follows:

Beginning at a point which is 15 feet South 7 degrees 42' West, and 241 feet South 82 degrees 18' East of the intersection of the center line of Fir Avenue, and the South side of 4th Street, of the platted TOWN OF WILSONVILLE, Clackamas County, Oregon, in the Thomas Bailey Donation Land Claim, in Section 23, Township 3 South, Range 1 West, of the Willamette Meridian; thence 97.67 feet South 82 degrees 18' East to an iron rod; thence 101.4 feet North 0 degrees 45' West to an iron rod; thence 82.66 feet North 82 degrees 18' West; thence 100 feet South 7 degrees 42' West and the point of beginning.

## **VII. Public Hearing:**

**C. Resolution No. 249. Boones Ferry Pointe - Carl's Jr Restaurant and Multi-Tenant Commercial Building: Ben Altman, SFA Design Group and CB Anderson Architects – Representatives for Josh Veentjer, Wilsonville Devco LLC - Applicant/Owner and Garry LaPoint, LaPoint Business Group - Owner.** The applicant is requesting approval of a Stage II Final Plan, Site Design Review and Master Sign Plan for development of a new 2,867 square foot drive-thru fast food restaurant and 3,150 square foot multi-tenant commercial building. The site is located on Tax Lots 300 and 302, Section 02DB; T3S-R1W; Washington County; Wilsonville, Oregon. Staff: Daniel Pauly

Case Files:       DB12-0074 – Stage II Final Plan  
                      DB12-0075 – Site Design Review  
                      DB12-0076 – Master Sign Plan

**DEVELOPMENT REVIEW BOARD  
RESOLUTION NO. 249**

**A RESOLUTION ADOPTING FINDINGS AND CONDITIONS APPROVING A STAGE II FINAL PLAN, SITE DESIGN REVIEW, AND MASTER SIGN PLAN FOR DEVELOPMENT OF A NEW 2,867 SQUARE FOOT DRIVE-THRU FAST FOOD RESTAURANT AND 3,150 SQUARE FOOT MULTI-TENANT COMMERCIAL BUILDING. THE SITE IS LOCATED ON TAX LOTS 300 AND 302, SECTION 02DB; T3S-R1W; WASHINGTON COUNTY; WILSONVILLE, OREGON. BEN ALTMAN, SFA DESIGN GROUP AND CRAIG ANDERSON, CB ANDERSON ARCHITECTS- REPRESENTATIVES FOR JOSH VEENTJER, WILSONVILLE DEVCO LLC - APPLICANT/OWNER AND GARRY LAPOINT, LAPOINT BUSINESS GROUP-OWNER.**

WHEREAS, an application, together with planning exhibits for the above-captioned development, has been submitted in accordance with the procedures set forth in Section 4.008 of the Wilsonville Code, and

WHEREAS, the Planning Staff has prepared staff report on the above-captioned subject dated March 4, 2013, and

WHEREAS, said planning exhibits and staff report were duly considered by the Development Review Board Panel A at a scheduled meeting conducted on March 11, 2013, at which time exhibits, together with findings and public testimony were entered into the public record, and

WHEREAS, the Development Review Board considered the subject and the recommendations contained in the staff report, and

WHEREAS, interested parties, if any, have had an opportunity to be heard on the subject.

NOW, THEREFORE, BE IT RESOLVED that the Development Review Board of the City of Wilsonville does hereby adopt the staff report dated March 4, 2013, attached hereto as Exhibit A1, with findings and recommendations contained therein, and authorizes the Planning Director to issue permits consistent with said recommendations for:

DB12-0074 through DB12-0076 Class 3 Stage II Final Plan, Site Design Review, and Master Sign Plan for Fast Food Restaurant and Multi-tenant Commercial Building.

ADOPTED by the Development Review Board of the City of Wilsonville at a regular meeting thereof this 11<sup>th</sup> day of March, 2013 and filed with the Planning Administrative Assistant on \_\_\_\_\_. This resolution is final on the 15th calendar day after the postmarked date of the written notice of decision per *WC Sec 4.022(.09)* unless appealed per *WC Sec 4.022(.02)* or called up for review by the council in accordance with *WC Sec 4.022(.03)*.

\_\_\_\_\_  
Mary Fierros Bower Chair, Panel A  
Wilsonville Development Review Board

Attest:

\_\_\_\_\_  
**Shelley White, Planning Administrative Assistant**

Exhibit A1  
**STAFF REPORT**  
**WILSONVILLE PLANNING DIVISION**

*Boones Ferry Pointe: Carl's Jr. Restaurant and Multi-tenant Commercial Building*

**DEVELOPMENT REVIEW BOARD PANEL 'A'**  
**QUASI-JUDICIAL PUBLIC HEARING**  
**STAFF REPORT**

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<b>HEARING DATE</b>	March 11, 2013
<b>DATE OF REPORT:</b>	March 4, 2013

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**APPLICATION NOS.:** DB12-0074 Stage II Final Plan  
DB12-0075 Site Design Review  
DB12-0076 Master Sign Plan for Boones Ferry Pointe

**REQUEST/SUMMARY:** The Development Review Board is being asked to review a Stage II Final Plan, Site Design Review, and Master Sign Plan for the development of a new 2,867 square drive-thru foot fast food restaurant and 3,150 square foot multi-tenant commercial building at the corner of 95<sup>th</sup> Avenue and Boones Ferry Road in North Wilsonville.

**LOCATION:** The proposed restaurant and commercial building are on the southeast corner of the 95th Avenue/ Boones Ferry Road intersection near the Elligsen Road/I-5 Interchange. The property is specifically known as Tax Lots 0300 and 0302, Section 2DB, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Washington County, Oregon.

**OWNERS:** Josh Veentjer  
Wilsonville Devco LLC  
(TL 302)

Garry LaPoint  
LaPoint Business Group  
(TL 300)

**APPLICANT:** Josh Veentjer  
Wilsonville Devco LLC

**APPLICANT'S REPS.:** Ben Altman  
SFA Design Group

Craig Anderson  
CB Anderson Architects

**COMPREHENSIVE PLAN MAP DESIGNATION:** Commercial

**ZONE MAP CLASSIFICATION:** PDC (Planned Development Commercial)

**STAFF REVIEWERS:** Daniel Pauly AICP, Associate Planner  
Mike Ward PE, Civil Engineer  
Kerry Rappold, Natural Resource Program Manager  
Don Walters, Building Plans Examiner

**STAFF RECOMMENDATION:** Approve with conditions the requested State II Final Plan, Site Design Review request, and Master Sign Plan.

**APPLICABLE REVIEW CRITERIA**

<b>Section 4.008</b>	<b>Application Procedures-In General</b>
<b>Section 4.009</b>	<b>Who May Initiate Application</b>
<b>Section 4.010</b>	<b>How to Apply</b>
<b>Section 4.011</b>	<b>How Applications are Processed</b>
<b>Section 4.014</b>	<b>Burden of Proof</b>
<b>Section 4.031</b>	<b>Authority of the Development Review Board</b>
<b>Subsection 4.035 (.04)</b>	<b>Site Development Permit Application</b>
<b>Subsection 4.035 (.05)</b>	<b>Complete Submittal Requirement</b>
<b>Section 4.110</b>	<b>Zones</b>
<b>Section 4.116</b>	<b>Standards Applying to Commercial Development in All Zones</b>
<b>Section 4.118</b>	<b>Standards Applying to Planned Development Zones</b>
<b>Section 4.131</b>	<b>Planned Development Commercial Zone (PDC)</b>
<b>Section 4.140</b>	<b>Planned Development Regulations</b>
<b>Section 4.155</b>	<b>Parking, Loading, and Bicycle Parking</b>
<b>Sections 4.156.01 through 4.156.11</b>	<b>Sign Regulations</b>
<b>Section 4.167</b>	<b>Access, Ingress, and Egress</b>
<b>Section 4.171</b>	<b>Protection of Natural Features and Other Resources</b>
<b>Section 4.175</b>	<b>Public Safety and Crime Prevention</b>
<b>Section 4.176</b>	<b>Landscaping, Screening, and Buffering</b>
<b>Section 4.177</b>	<b>Street Improvement Standards</b>
<b>Section 4.178</b>	<b>Sidewalk and Pathway Standards</b>
<b>Section 4.179</b>	<b>Mixed Solid Waste and Recyclables Storage</b>
<b>Sections 4.199.20 through 4.199.60</b>	<b>Outdoor Lighting</b>
<b>Sections 4.300 through 4.320</b>	<b>Underground Utilities</b>
<b>Sections 4.400 through 4.450 as applicable</b>	<b>Site Design Review</b>



## Vicinity Map



### **BACKGROUND/SUMMARY:**

#### **Approved Stage I Master Plan/Site History**

The subject property is part of the Edwards Business Center Industrial Master Plan. This master plan envisioned a variety of industrial and commercial uses. The Master Plan designated the currently vacant site as commercial, but did not specify the type of commercial use. Previously the City received an application for an office building on the site, which was never built. The current application to construct a fast-food restaurant and a multi-tenant commercial building is consistent with the designation of the property in the Master Plan.

#### **Stage II Final Plan (DB12-0074)**

The Stage II Final Plan looks at the function and overall aesthetics of the proposed development, including traffic, parking, and circulation.

The development includes two buildings, a 2,867 square foot drive-thru fast food restaurant and a 3,150 square foot multi-tenant commercial building, and associated site improvements including parking and circulation. Buildings are planned to be built in a single phase. The restaurant building sits towards the southeast area of the site along SW 95<sup>th</sup> Avenue while the multi-tenant building is at the north end of the site. The restaurant building has a flat roof with a parapet to screen view of mechanical equipment. Each side of the building has a wide tower with

a brick façade. The multi-tenant building has a gabled roof over the center tenant space. The end tenant spaces have flat roofs with parapets to screen mechanical equipment, similar to the fast food restaurant. A drive through lane wraps around the east and south side of the restaurant building. Parking is to the north and west. North of the multi-tenant building is a plaza that includes a pedestrian path, benches, flag pole, and a monument sign identifying the development.

Vehicle Access to the site is via an existing shared driveway with Holiday Inn and Chevron proposed to be improved as part of the development of this site.

The Stage II Final Plan proposes 16,139 square feet of landscaping, 48 parking spaces (42 required), maneuvering and circulations areas, and mixed solid waste and recyclables storage. The total gross area of the development site is 55,605 square feet or 1.28 acres.

### **Site Design Review (DB12-0075)**

#### ***Architectural Design***

The applicant provides a thorough explanation of the architectural design of the buildings on pages 57-58 of their compliance narrative in their submitted notebook, Exhibit B1. The narrative explains how the design goal was to identify with the general environment of commercial development at Argyle Square and along Wilsonville Road while also adding a unique personality to the development and proper identity to the planned tenants. The two smaller scale wood-frame structures use traditional exterior materials reinforcing their location in Wilsonville's small town setting. The buildings feature brick, horizontal lap siding, and board and batten materials. They are designed to complement one another, but to have separate unique identities.

#### **Carl's Jr. Building Rendering**



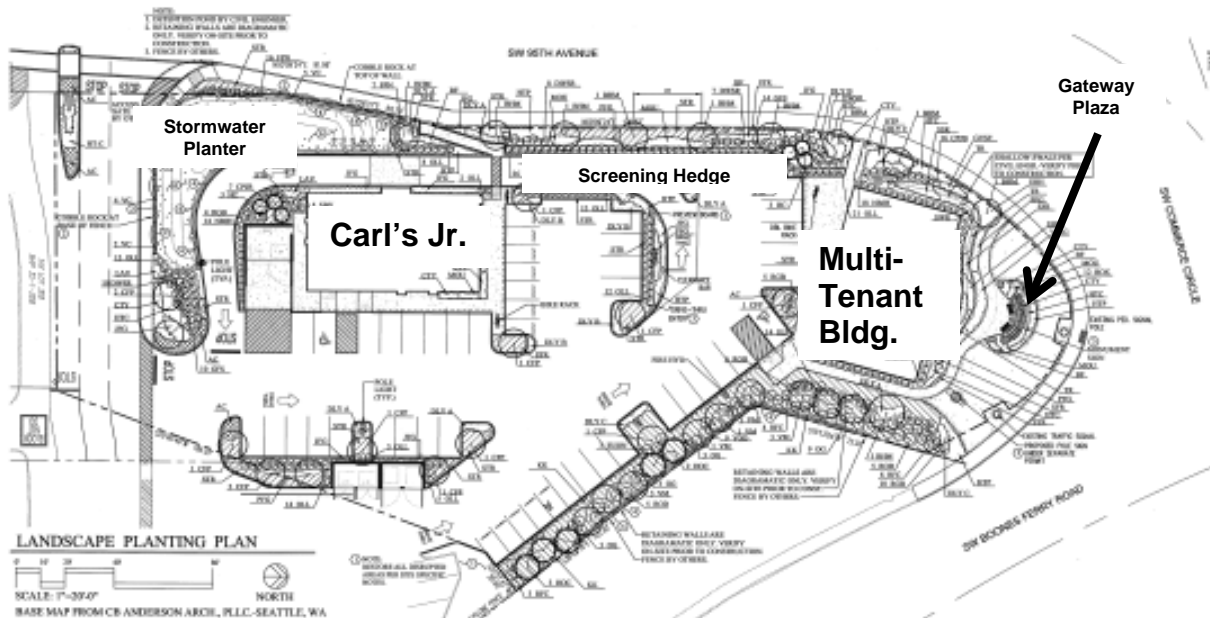
## Multi-Tenant Building Rendering



### *Landscape and Hardscape Design*

The applicant has designed a planter and plaza at the north of the site to acknowledge the gateway at a prominent intersection on the northern edge of the City. The remainder of the landscaping is typical of parking lots and commercial areas in Wilsonville. As part of the landscaping a six (6) foot tall evergreen hedge is proposed along a portion of 95<sup>th</sup> Avenue to screen the drive-thru signs from off-site view.

### **Proposed Landscape Plan (sheet L2.0 in applicant's plan set, Exhibit B2)**



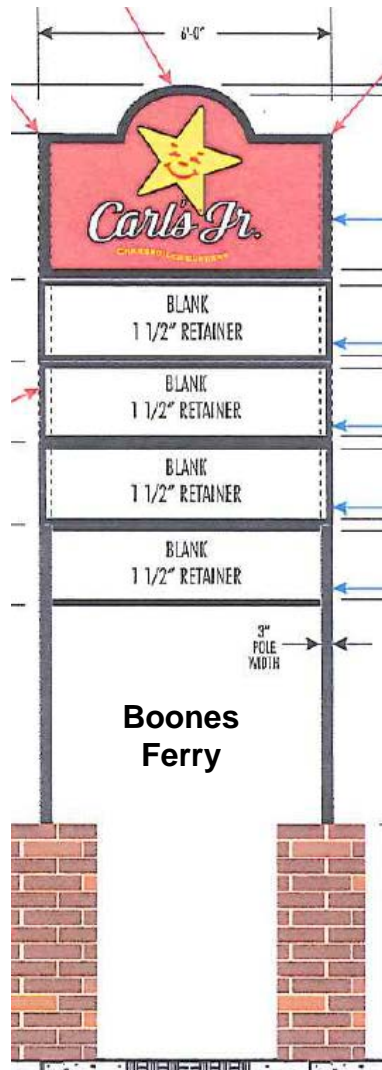
**Master Sign Plan (DB12-0076)**

***Freestanding Signs***

Due to the having significant street frontage on two (2) streets the lot with the proposed Carl's Jr. and multi-tenant building development is allowed two (2) freestanding signs. Each freestanding sign is allowed to be fifty-nine (59) square feet, based on the size of the buildings on the site and the number of tenants. A new twenty (20) foot tall sign is proposed along SW Boones Ferry Road north of the multi-tenant building. The sign features a brick treatment at the base of the support poles to match the buildings and otherwise has a minimal amount of visible support emphasizing the different sign panels.

A thirty-six (36) square foot sign panel is being collocated on an existing Chevron pylon sign at the driveway along SW 95<sup>th</sup> Avenue. The collocation will reduce clutter and provide for efficient placement of signs for multiple developments using a shared driveway.

**Proposed Freestanding Signs**





***Monument Sign***

Thirteen (13) square feet of the unused allowance for a second freestanding sign is being used for a planned development sign on the north end of the site in the plaza area, as allowed in Subsection 4.156 (.03) B. The monument is a wall incorporated into the plaza with the name of development in individual non- illuminated letters.

**Proposed Monument Sign**



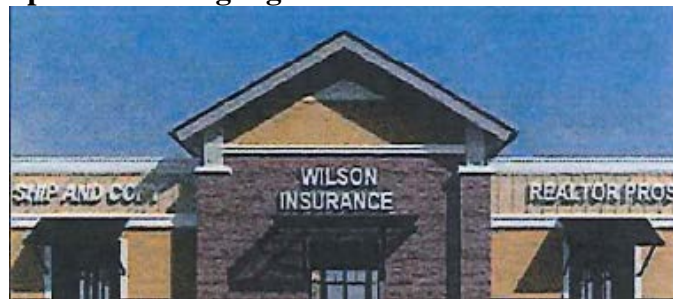
***Building Signs***

All four facades of both the restaurant and multi-tenant building are eligible for building signs, with the allowed area based on the length of the different facades. The building signs will be wall mounted internally illuminated logo cabinets, like Carl’s Jr., or individual internally illuminated channel letters. The signs will be appropriately placed on the buildings either centered in architectural features or centered above doors or windows. The sign design and placement is similar to other commercial retail developments in Wilsonville including Argyle Square and Old Town Square.

**Examples of Proposed Building Signs**



Logo Cabinet



Individual Channel Letters

**DISCUSSION TOPICS:**

***Shared Driveway and Development Agreement***

Prior to submittal of the subject applications, the applicant worked with the City and the owners of the Chevron Station and Holiday Inn to refine access, egress, and site circulation centered on the shared driveway from SW 95<sup>th</sup> Avenue. The driveway is the only public street access for Chevron and the proposed development, and a primary access point for Holiday Inn. A copy of the Development Agreement is included in the applicant’s notebook, Exhibit B1.

The Development Agreement provides for a new driveway configuration, which will provide two inbound lanes, with a fifty (50) foot north bound curb radius to accommodate trucks. The Development Agreement also provides for relocation of the existing freestanding Chevron Sign, required to be moved due to the new driveway configuration. The Development Agreement also allows for the collocation of a Carl's Jr. sign panel on the existing sign pylon as requested as part of the Master Sign Plan.

#### *Freestanding Sign Placement along Boones Ferry Road*

A number of easements exist on the northern portion of the site at the intersection of Boones Ferry Road and 95<sup>th</sup> Avenue, including in the area where the new freestanding sign is proposed. Though they have yet to, the applicant hopes to execute an agreement with relevant easement holders to place the sign within the easements subject to easement holder not being liable for the costs if the sign must be taken down or otherwise impacted for work in the easement. Condition of Approval PDC 6 makes approval of the sign contingent on approval of the easement holder. If agreement cannot be reached with a relevant easement holder or holders and no alternative location north of the multi-tenant building meeting code requirements can be found then the sign cannot be installed.

#### *Cover for Mixed Solid Waste and Recycling Enclosure for Chevron*

In addition to the mixed solid waste and recycling enclosures proposed on the site for the restaurant building and the multi-tenant commercial building, a third is proposed to serve the Chevron fuel station and convenience store. Exhibit B5 expresses concerns from the Chevron owner and Allied Waste about the cover required for the Chevron waste enclosure due to handling issues related to the type of large collection containers Chevron uses. The cover is required by Condition of Approval NR 8 pursuant to Subsection 8.210 (9) Wilsonville Code to help prevent contaminants from entering the public storm sewer system. The requirement is also mentioned in comments from the City's Public Works Department, Exhibit C5. The ability to waive or grant a variance from the requirement to cover the enclosure or remove/modify Condition of Approval NR 8 is not under the authority of the Development Review Board (see preface to Engineering, Building, Natural Resources, and TVF&R Conditions of Approval on Pages 11 and 12 of 60 of this report). Staff understands Chevron is pursuing an exemption or other relief from the requirement through the appropriate channels. For this reason staff recommends the Development Review Board approve both a covered and uncovered design for the Chevron enclosure so the either can be built depending on the final resolution of the applicant's request to not comply with the requirement.

## CONCLUSION AND CONDITIONS OF APPROVAL:

Staff has reviewed the applicant's analysis of compliance with the applicable criteria. The Staff report adopts the applicant's responses as Findings of Fact except as noted in the Findings. Based on the Findings of Fact and information included in this Staff Report, and information received from a duly advertised public hearing, staff recommends that the Development Review Board approve the proposed application (DB12-0074 through DB12-0076) with the following conditions:

### REQUEST A: DB12-0074 STAGE II FINAL PLAN

#### **Planning Division Conditions:**

- PDA 1.** All travel lanes shall be constructed to be capable of carrying a twenty-three (23) ton load. See Finding A47.
- PDA 2.** The approved final plan schedule shall control the issuance of all building permits and shall restrict the nature, location and design of all uses. Minor changes to the approved final development plan may be approved by the Planning Director through administrative review pursuant to Section 4.030 if such changes are consistent with the purposes and general character of the plan. All other modifications shall be processed in the same manner as the original application and shall be subject to the same procedural requirements.

### REQUEST B: DB12-0075 SITE DESIGN REVIEW

#### **Planning Division Conditions:**

- PDB 1.** Construction, site development, and landscaping shall be carried out in substantial accord with the Development Review Board approved plans, drawings, sketches, and other documents. Minor revisions may be approved by the Planning Director through administrative review pursuant to Section 4.030. See Findings B3. and A49.
- PDB 2.** The paint color for the mixed solid waste and recycling enclosures shall be of the same or complementary color palette as the restaurant and multi-tenant buildings. This includes the walls, gates, and other structural and trim elements. Support beams, covers, and roofing material for the enclosures shall be the same or complementary to the materials used on restaurant and multi-tenant buildings. It is understood a cover may not be required for the enclosure serving the Chevron property. Final color choice for the enclosure and along with cover and roofing material, shall be approved by the Planning Division through the Class I Administrative Review process prior to construction. See Finding B6.
- PDB 3.** All landscaping required and approved by the Board shall be installed prior to issuance of occupancy permits, unless security equal to one hundred and ten percent (110%) of the cost of the landscaping as determined by the Planning Director is filed with the City assuring such installation within six (6) months of occupancy. "Security" is cash, certified check, time certificates of deposit, assignment of a savings account or such other assurance of completion as shall meet with the approval of the City Attorney. In such cases the developer shall also provide written authorization, to the satisfaction of the City Attorney, for the City



	<p>or its designees to enter the property and complete the landscaping as approved. If the installation of the landscaping is not completed within the six-month period, or within an extension of time authorized by the Board, the security may be used by the City to complete the installation. Upon completion of the installation, any portion of the remaining security deposited with the City will be returned to the applicant. See Finding B9.</p>
<b>PDB 4.</b>	<p>The approved landscape plan is binding upon the applicant/owner. Substitution of plant materials, irrigation systems, or other aspects of an approved landscape plan shall not be made without official action of the Planning Director or Development Review Board, pursuant to the applicable sections of Wilsonville's Development Code. See Finding B10.</p>
<b>PDB 5.</b>	<p>All landscaping shall be continually maintained, including necessary watering, weeding, pruning, and replacing, in a substantially similar manner as originally approved by the Board, unless altered as allowed by Wilsonville's Development Code. See Findings B11 and B12.</p>
<b>PDB 6.</b>	<p>Occupancy shall not be granted for the Carl's Jr. restaurant building until the required screening planting along 95<sup>th</sup> Avenue is in place and approved by the City. A temporary occupancy permit may be issued upon a posting of a bond or other security equal to one hundred ten percent (110%) of the cost of such planting and its installation. See Findings B20 and B23.</p>
<b>PDB 7.</b>	<p>The following requirements for planting of shrubs and ground cover shall be met:</p> <ul style="list-style-type: none"> <li>• Non-horticultural plastic sheeting or other impermeable surface shall not be placed under landscaping mulch.</li> <li>• Native topsoil shall be preserved and reused to the extent feasible.</li> <li>• Surface mulch or bark dust shall be fully raked into soil of appropriate depth, sufficient to control erosion, and shall be confined to areas around plantings.</li> <li>• All shrubs shall be well branched and typical of their type as described in current AAN Standards and shall be equal to or better than 2-gallon containers and 10" to 12" spread.</li> <li>• Shrubs shall reach their designed size for screening within three (3) years of planting.</li> <li>• Ground cover shall be equal to or better than the following depending on the type of plant materials used: gallon containers spaced at 4 feet on center minimum, 4" pot spaced 2 feet on center minimum, 2-1/4" pots spaced at 18 inch on center minimum.</li> <li>• No bare root planting shall be permitted.</li> <li>• Ground cover shall be sufficient to cover at least 80% of the bare soil in required landscape areas within three (3) years of planting.</li> <li>• Appropriate plant materials shall be installed beneath the canopies of trees and large shrubs to avoid the appearance of bare ground in those locations.</li> <li>• Compost-amended topsoil shall be integrated in all areas to be landscaped, including lawns.</li> </ul> <p>See Finding B24.</p>
<b>PDB 8.</b>	<p>Plant materials shall be installed to current industry standards and be properly staked to ensure survival. Plants that die shall be replaced in kind, within one</p>

	growing season, unless appropriate substitute species are approved by the City. See Finding B29.
<b>PDB 9.</b>	Outdoor lighting associated with the multi-tenant commercial building shall be dimmed at 10:00 p.m. by an automatic system. See Finding B41.
<b>PDB 10.</b>	Luminaire U, as shown in the applicant's lighting plans, and all other luminaires shall be limited to down lighting. Luminaires, except luminaire DD, shall be mounted and aimed consistent with their fully shielded classification. See Finding B38 and B40.

**REQUEST C DB12-0076 MASTER SIGN PLAN FOR BOONES FERRY POINTE**

<b>PDC 1.</b>	Non-exempt signs, including the building and freestanding signs, shall be issued a Class I Sign Permit through the Planning Division prior to installation to ensure compliance with the approved Master Sign Plan.
<b>PDC 2.</b>	Non turf grass landscaping consistent with what is found around the foundation of other structures on the site, including shrubs and groundcover, meeting applicable Sections of Wilsonville Code shall be provided around the foundation of the freestanding sign along Boones Ferry Road, if such sign is installed. The landscaping shall be approved by the Planning Division as part of the Class I Sign Permit for the sign. See Finding C7.
<b>PDC 3.</b>	All wall mounted signs on the multi-tenant commercial building shall be similarly mounted. The applicant/owner shall indicate prior to or with the first sign permit for such signs whether exposed raceways are allowed, and such decision shall apply to all wall mounted signs for the building. If exposed raceways are allowed they shall be painted to match the building fascia on which they are mounted. See Finding C8.
<b>PDC 4.</b>	All wall mounted signs shall have the same color of returns. See Finding C8.
<b>PDC 5.</b>	The freestanding sign along Boones Ferry Road north of the multi-tenant retail building, if built, shall be no further than fifteen (15) feet from the property line and no closer than two (2) feet from a the public sidewalk or other hard surface in the public right-of-way. See Finding C22.
<b>PDC 6.</b>	No sign permit or building permit shall be issued for any sign without proof of placement approval by easement holders for easements on which it encroaches.
<b>PDC 7.</b>	The illuminated directional signs at internal circulation drive intersections shall be limited in height to four (4) feet. See Finding C31.
<b>PDC 8.</b>	The flags on the flag pole located in the plaza north of the multi-tenant commercial building shall be limited to thirty (30) feet in height. No more than two (2) flags can be displayed. See Finding C14.

**CONDITIONS OF APPROVAL FROM THE ENGINEERING DIVISION, NATURAL RESOURCES DIVISION, BUILDING DIVISION, AND TUALATIN VALLEY FIRE AND RESCUE FOR ALL REQUESTS**

The following Conditions of Approval are provided by the Engineering, Natural Resources, or Building Divisions of the City's Community Development Department or Tualatin Valley Fire and Rescue, all of which have authority over development approval. A number of these Conditions of Approval are not related to land use regulations under the authority of the Development Review Board or Planning Director. Only those Conditions of Approval related to

criteria in Chapter 4 of Wilsonville Code and the Comprehensive Plan, including but not limited to those related to traffic level of service, site vision clearance, recording of plats, and concurrency, are subject to the Land Use review and appeal process defined in Wilsonville Code and Oregon Revised Statutes and Administrative Rules. Other Conditions of Approval are based on City Code chapters other than Chapter 4, state law, federal law, or other agency rules and regulations. Questions or requests about the applicability, appeal, exemption or non-compliance related to these other Conditions of Approval should be directed to the City Department, Division, or non-City agency with authority over the relevant portion of the development approval.

<b>Engineering Division Conditions:</b>													
<b>Standard Comments:</b>													
<b>PF 1.</b>	All construction or improvements to public works facilities shall be in conformance to the City of Wilsonville Public Works Standards.												
<b>PF 2.</b>	<p>Applicant shall submit insurance requirements to the City of Wilsonville in the following amounts:</p> <table border="0"> <tr> <td>General Aggregate</td> <td>\$2,000,000</td> </tr> <tr> <td>Products-Completed Operations Aggregate</td> <td>\$2,000,000</td> </tr> <tr> <td>Each Occurrence</td> <td>\$2,000,000</td> </tr> <tr> <td>Automobile Insurance</td> <td>\$1,000,000</td> </tr> <tr> <td>Fire Damage (any one fire)</td> <td>\$ 50,000</td> </tr> <tr> <td>Medical Expense (any one person)</td> <td>\$ 10,000</td> </tr> </table>	General Aggregate	\$2,000,000	Products-Completed Operations Aggregate	\$2,000,000	Each Occurrence	\$2,000,000	Automobile Insurance	\$1,000,000	Fire Damage (any one fire)	\$ 50,000	Medical Expense (any one person)	\$ 10,000
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Medical Expense (any one person)	\$ 10,000												
<b>PF 3.</b>	No construction of, or connection to, any existing or proposed public utility/improvements will be permitted until all plans are approved by Staff, all fees have been paid, all necessary permits, right-of-way and easements have been obtained and Staff is notified a minimum of 24 hours in advance.												
<b>PF 4.</b>	All public utility/improvement plans submitted for review shall be based upon a 22"x 34" format and shall be prepared in accordance with the City of Wilsonville Public Work's Standards.												
<b>PF 5.</b>	<p>Plans submitted for review shall meet the following general criteria:</p> <ol style="list-style-type: none"> <li>a. Utility improvements that shall be maintained by the public and are not contained within a public right-of-way shall be provided a maintenance access acceptable to the City. The public utility improvements shall be centered in a minimum 15-ft. wide public easement for single utilities and a minimum 20-ft wide public easement for two parallel utilities and shall be conveyed to the City on its dedication forms.</li> <li>b. Design of any public utility improvements shall be approved at the time of the issuance of a Public Works Permit. Private utility improvements are subject to review and approval by the City Building Department.</li> <li>c. In the plan set for the PW Permit, existing utilities and features, and proposed new private utilities shall be shown in a lighter, grey print. Proposed public improvements shall be shown in bolder, black print.</li> <li>d. All elevations on design plans and record drawings shall be based on NAVD 88 Datum.</li> <li>e. All proposed on and off-site public/private utility improvements shall comply</li> </ol>												

with the State of Oregon and the City of Wilsonville requirements and any other applicable codes.

- f. Design plans shall identify locations for street lighting, gas service, power lines, telephone poles, cable television, mailboxes and any other public or private utility within the general construction area.
- g. As per City of Wilsonville Ordinance No. 615, all new gas, telephone, cable, fiber-optic and electric improvements etc. shall be installed underground. Existing overhead utilities shall be undergrounded wherever reasonably possible.
- h. Any final site landscaping and signing shall not impede any proposed or existing driveway or interior maneuvering sight distance.
- i. Erosion Control Plan that conforms to City of Wilsonville Ordinance No. 482.
- j. Existing/proposed right-of-way, easements and adjacent driveways shall be identified.
- k. All engineering plans shall be stamped by a Professional Engineer registered in the State of Oregon.

**PF 6.** Submit plans in the following general format and order for all public works construction to be maintained by the City:

- a. Cover sheet
- b. City of Wilsonville construction note sheet
- c. General construction note sheet
- d. Existing conditions plan.
- e. Erosion control and tree protection plan.
- f. Site plan. Include property line boundaries, water quality pond boundaries, sidewalk improvements, right-of-way (existing/proposed), easements (existing/proposed), and sidewalk and road connections to adjoining properties.
- g. Grading plan, with 1-foot contours.
- h. Composite utility plan; identify storm, sanitary, and water lines; identify storm and sanitary manholes.
- i. Detailed plans; show plan view and either profile view or provide i.e.'s at all utility crossings; include laterals in profile view or provide table with i.e.'s at crossings; vertical scale 1"= 5', horizontal scale 1"= 20' or 1"= 30'.
- j. Street plans.
- k. Storm sewer/drainage plans; number all lines, manholes, catch basins, and cleanouts for easier reference
- l. Water and sanitary sewer plans; plan; number all lines, manholes, and cleanouts for easier reference.
- m. Detailed plan for storm water detention facility (both plan and profile views), including water quality orifice diameter and manhole rim elevations. Provide detail of inlet structure and energy dissipation device. Provide details of drain inlets, structures, and piping for outfall structure. Note that although storm water detention facilities are typically privately maintained they will be inspected by engineering, and the plans must be part of the Public Works Permit set.
- n. Detailed plan for water quality facility (both plan and profile views). Note that

	<p>although storm water quality facilities are typically privately maintained they will be inspected by Natural Resources, and the plans must be part of the Public Works Permit set.</p> <ul style="list-style-type: none"> <li>o. Composite franchise utility plan.</li> <li>p. City of Wilsonville detail drawings.</li> <li>q. Illumination plan.</li> <li>r. Striping and signage plan.</li> <li>s. Landscape plan.</li> </ul>
<b>PF 7.</b>	Prior to manhole and sewer line testing, design engineer shall coordinate with the City and update the sanitary and stormwater sewer systems to reflect the City's numbering system. Video testing and sanitary manhole testing will refer to the updated numbering system. Design engineer shall also show the updated numbering system on As-Built drawings submitted to the City.
<b>PF 8.</b>	The applicant shall install, operate and maintain adequate erosion control measures in conformance with the standards adopted by the City of Wilsonville Ordinance No. 482 during the construction of any public/private utility and building improvements until such time as approved permanent vegetative materials have been installed.
<b>PF 9.</b>	Applicant shall work with City's Natural Resources office before disturbing any soil on the respective site. If 5 or more acres of the site will be disturbed applicant shall obtain a 1200-C permit from the Oregon Department of Environmental Quality. If 1 to less than 5 acres of the site will be disturbed a 1200-CN permit from the City of Wilsonville is required.
<b>PF 10.</b>	To lessen the impact of the proposed project on the downstream storm drain system, and adjacent properties, project run-off from the site shall be detained and limited to the difference between a developed 25-year storm and an undeveloped 25-year storm. The detention and outfall facilities shall be designed and constructed in conformance with the Public Works Standards.
<b>PF 11.</b>	A storm water analysis prepared by a Professional Engineer registered in the State of Oregon shall be submitted for review and approval by the City to address appropriate pipe and detention facility sizing.
<b>PF 12.</b>	The applicant shall be in conformance with all water quality requirements for the proposed development per the Public Works Standards. If a mechanical water quality system is used, prior to City acceptance of the project the applicant shall provide a letter from the system manufacturer stating that the system was installed per specifications and is functioning as designed.
<b>PF 13.</b>	Storm water quality facilities shall have approved landscape planted and/or some other erosion control method installed and approved by the City of Wilsonville prior to streets and/or alleys being paved.
<b>PF 14.</b>	The applicant shall provide the City with a Stormwater Maintenance and Access Easement (on City approved forms) for City inspection of those portions of the storm system to be privately maintained. Applicant shall maintain all LID storm water components and private conventional storm water facilities located within medians and from the back of curb onto and including the project site.
<b>PF 15.</b>	Fire hydrants shall be located in compliance with TVF&R fire prevention ordinance and approval of TVF&R.

<b>PF 16.</b>	All survey monuments on the subject site, or that may be subject to disturbance within the construction area, or the construction of any off-site improvements shall be adequately referenced and protected prior to commencement of any construction activity. If the survey monuments are disturbed, moved, relocated or destroyed as a result of any construction, the project shall, at its cost, retain the services of a registered professional land surveyor in the State of Oregon to restore the monument to its original condition and file the necessary surveys as required by Oregon State law. A copy of any recorded survey shall be submitted to Staff.
<b>PF 17.</b>	Sidewalks, crosswalks and pedestrian linkages in the public right-of-way shall be in compliance with the requirements of the U.S. Access Board.
<b>PF 18.</b>	No surcharging of sanitary or storm water manholes is allowed.
<b>PF 19.</b>	The project shall connect to an existing manhole or install a manhole at each connection point to the public storm system and sanitary sewer system.
<b>PF 20.</b>	A City approved energy dissipation device shall be installed at all proposed storm system outfalls. Storm outfall facilities shall be designed and constructed in conformance with the Public Works Standards.
<b>PF 21.</b>	All required pavement markings, in conformance with the Transportation Systems Plan and the Bike and Pedestrian Master Plan, shall be completed in conjunction with any conditioned street improvements.
<b>PF 22.</b>	Street and traffic signs shall have a hi-intensity prismatic finish meeting ASTM 4956 Spec Type 4 standards.
<b>PF 23.</b>	The applicant shall provide adequate sight distance at all project driveways by driveway placement or vegetation control. Specific designs to be submitted and approved by the City Engineer. Coordinate and align proposed driveways with driveways on the opposite side of the proposed project site.
<b>PF 24.</b>	Access requirements, including sight distance, shall conform to the City's Transportation Systems Plan (TSP) or as approved by the City Engineer. Landscaping plantings shall be low enough to provide adequate sight distance at all street intersections and alley/street intersections.
<b>PF 25.</b>	Applicant shall prepare an Ownership and Maintenance agreement between the City and the Owner. Stormwater or rainwater facilities may be located within the public right-of-way upon approval of the City Engineer. The Ownership and Maintenance agreement shall specify that the rainwater and stormwater facilities shall be privately maintained by the Applicant; maintenance shall transfer to the respective homeowners association when it is formed.
<b>PF 26.</b>	All water lines that are to be temporary dead-end lines due to the phasing of construction shall have a valved tee with fire-hydrant assembly installed at the end of the line.
<b>PF 27.</b>	Applicant shall provide a minimum 6-foot Public Utility Easement on lot frontages to all public right-of-ways. An 8-foot PUE shall be provided along Minor and Major Collectors. A 10-ft PUE shall be provided along Minor and Major Arterials.
<b>PF 28.</b>	Mylar Record Drawings: At the completion of the installation of any required public improvements, and before a 'punch list' inspection is scheduled, the Engineer shall perform a record survey. Said survey shall be the basis for the preparation of 'record drawings' which

<p>will serve as the physical record of those changes made to the plans and/or specifications, originally approved by Staff, that occurred during construction. Using the record survey as a guide, the appropriate changes will be made to the construction plans and/or specifications and a complete revised 'set' shall be submitted. The 'set' shall consist of drawings on 3 mil. Mylar, an electronic copy in AutoCAD, current version, and a digitally signed PDF.</p>	
<p><b>Specific Comments:</b></p>	
<p><b>PF 29.</b></p>	<p>At the request of Staff, DKS Associates completed a Trip Generation and Access Analysis dated March 20, 2012. The project is hereby limited to no more than the following impacts.</p> <p style="text-align: center;">Estimated New PM Peak Hour Trips <span style="float: right;">60</span></p>
<p><b>PF 30.</b></p>	<p>The applicant shall pay the City for required ROW and easements (\$250 per tax lot) for future MP street and signal improvements, in addition to the legal descriptions and exhibits. If quitclaim and reconveyed, applicant shall pay an additional \$500 per tax lot for recording fees and insurance.</p>
<p><b>PF 31.</b></p>	<p>This development is conditioned on the execution of work agreed to by the applicant in the Development Agreement filed with Washington County on August 17, 2012.</p>
<p><b>PF 32.</b></p>	<p>The existing 24 inch storm pipe shall be either be removed or abandoned in place and filled with compact density fill (CDF), in accordance with the Public Works Standards.</p>
<p><b>PF 33.</b></p>	<p>All work performed on property belonging to the Oregon Department of Transportation shall have the appropriate approval from ODOT prior to work taking place.</p>
<p><b>PF 34.</b></p>	<p>The proposed 24 inch storm line shall be a public utility and constructed in accordance with the Public Works Standards.</p>

<p><b>Natural Resources Division Conditions:</b></p>	
<p>This memorandum includes staff conditions of approval. The conditions of approval are based on the submitted Stage II Final Plan and Site Design Review. The conditions of approval apply to the applicant's submittal of construction documents (i.e. engineering drawings).</p>	
<p>The following conditions of approval are based on the material submitted by the applicant. Any subsequent revisions to the submitted plans may require conditions of approval to be modified by staff.</p>	
<p><b>Stormwater Management:</b></p>	
<p><b>NR 1.</b></p>	<p>Pursuant to the policies and implementation measures of the 2012 Stormwater Master Plan, the applicant shall prioritize the use of Low Impact Development in the design and implementation of the stormwater management system. Low Impact Development entails managing rainfall at the source, using decentralized, small scale controls that provide infiltration, filtration, vegetative uptake, and the creation of extended flow paths.</p>
<p><b>NR 2.</b></p>	<p>Submit a final drainage report and drainage plans. The report and plans shall demonstrate proposed stormwater facilities satisfy the policies and standards of the City of Wilsonville's Stormwater Master Plan and Public Works Standards.</p>
<p><b>NR 3.</b></p>	<p>Provide profiles, plan views and specifications for proposed stormwater facilities</p>



	consistent with the requirements of the Public Works Standards.
<b>NR 4.</b>	Pursuant to the Public Works Standards, the applicant shall submit a maintenance plan (including the City’s stormwater maintenance and access easement) for proposed stormwater facilities prior to approval for occupancy of the associated development.
<b>NR 5.</b>	Pursuant to the Public Works Standards, access shall be provided to all areas of proposed stormwater facilities. At a minimum, at least one access shall be provided for maintenance and inspection.
<b>Other:</b>	
<b>NR 6.</b>	Pursuant to the City of Wilsonville’s Ordinance No. 482, the applicant shall submit an erosion and sedimentation control plan. The following techniques and methods shall be incorporated, where necessary: <ul style="list-style-type: none"> <li>a. Gravel construction entrance;</li> <li>b. Stockpiles and plastic sheeting;</li> <li>c. Sediment fence;</li> <li>d. Inlet protection (Silt sacks are recommended);</li> <li>e. Dust control;</li> <li>f. Temporary/permanent seeding or wet weather measures (e.g. mulch);</li> <li>g. Limits of construction; and</li> <li>h. Other appropriate erosion and sedimentation control methods.</li> </ul>
<b>NR 7.</b>	The applicant shall comply with all applicable state and federal requirements for the proposed construction activities and proposed facilities (e.g. DEQ NPDES #1200-C permit).
<b>NR 8.</b>	Pursuant to the Wilsonville City Code, the applicant is required to provide covered waste and recycling enclosures. A drain is not allowed within the enclosure, and the floor in the enclosure shall be raised to prevent stormwater runoff from entering. The enclosure shall contain adequate area for proper use of all receptacles. These measures minimize the risk of pollutants entering the public stormwater system.

<b>Building Division Conditions:</b>	
<b>BD 1.</b>	ACCESSIBLE PARKING cannot be fully reviewed at this time. Accessible parking will be fully reviewed as part of the plan review of the building permit. The additional information available at plan review may require changes to the number and location of accessible parking spaces shown on these preliminary plans.
<b>BD 2.</b>	A GREASE INTERCEPTOR will be required for Carl’s Junior. If there is a possibility that a business requiring a grease interceptor will occupy the new retail building, a sewer line shall be run from the retail building to the grease interceptor. The grease interceptor shall be sized as directed in the Plumbing Code.
<b>BD 3.</b>	TRASH ENCLOSURE. If a drain is installed in the covered trash enclosure it shall be plumbed to the grease interceptor.

<b>TVF&amp;R Conditions:</b>	
<b>FD 1.</b>	<b><u>SURFACE AND LOAD CAPACITIES:</u></b> Fire apparatus access roads shall be of an all-weather surface that is easily distinguishable from the surrounding area and

	is capable of supporting not less than 12,500 pounds point load (wheel load) and 60,000 pounds live load (gross vehicle weight). You may need to provide documentation from a registered engineer that the design will be capable of supporting such loading. (OFC D102.1) <i>Applicable to the parking lot.</i>
<b>FD 2.</b>	<b><u>PAINTED CURBS:</u></b> Where required, fire apparatus access roadway curbs shall be painted red and marked “NO PARKING FIRE LANE” at approved intervals. Lettering shall have a stroke of not less than one inch wide by six inches high. Lettering shall be white on red background. (OFC 503.3) <i>Provide curb lane striping along the face of the curb at the landscape island housing the new fire hydrant.</i>
<b>FD 3.</b>	<b><u>COMMERCIAL BUILDINGS - REQUIRED FIRE FLOW:</u></b> The required fire flow for the building shall not exceed 3,000 gallons per minute (GPM) or the available GPM in the water delivery system at 20 psi, whichever is less as calculated using IFC, Appendix B. A worksheet for calculating the required fire flow is available from the Fire Marshal’s Office. (OFC B105.3) <i>Please provide a current fire flow test of the nearest fire hydrant demonstrating available flow at 20 psi residual pressure as well as fire flow calculation worksheets. Please forward copies to both TVF&amp;R as well as local building department. Fire flow calculation worksheets as well as instructions are available on our web site at <a href="http://www.tvfr.com">www.tvfr.com</a>.</i>
<b>FD 4.</b>	<b><u>FIRE HYDRANTS – COMMERCIAL BUILDINGS:</u></b> Where a portion of the building is more than 400 feet from a hydrant on a fire apparatus access road, as measured in an approved route around the exterior of the building, on-site fire hydrants and mains shall be provided. This distance may be increased to 600 feet for buildings equipped throughout with an approved automatic sprinkler system. (OFC 507.5.1) <i>The proposed fire hydrant is obstructed by passenger vehicle parking stalls. Please relocate the new fire hydrant from its proposed location to the landscape island to the south.</i>
<b>FD 5.</b>	<b><u>REFLECTIVE HYDRANT MARKERS:</u></b> Fire hydrant locations shall be identified by the installation of reflective markers. The markers shall be blue. They shall be located adjacent and to the side of the centerline of the access road way that the fire hydrant is located on. In case that there is no center line, then assume a centerline, and place the reflectors accordingly. (OFC 510.1)
<b>FD 6.</b>	<b><u>PHYSICAL PROTECTION:</u></b> Where fire hydrants are subject to impact by a motor vehicle, guard posts, bollards or other approved means of protection shall be provided. (OFC 507.5.6) <i>Please provide bollards at the new fire hydrant.</i>
<b>FD 7.</b>	<b><u>CLEAR SPACE AROUND FIRE HYDRANTS:</u></b> A 3 foot clear space shall be provided around the circumference of fire hydrants. (OFC 507.5.5)
<b>FD 8.</b>	<b><u>ACCESS AND FIRE FIGHTING WATER SUPPLY DURING CONSTRUCTION:</u></b> Approved fire apparatus access roadways and fire fighting water supplies shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. (OFC 1410.1 & 1412.1)
<b>FD 9.</b>	<b><u>KNOX BOX:</u></b> A Knox Box for building access is required for this building. Please contact the Fire Marshal’s Office for an order form and instructions regarding installation and placement. (OFC 506.1) <i>Provide a Knox box at each</i>

	<i>new building.</i>
<b>FD 10.</b>	<b><u>PREMISES IDENTIFICATION:</u></b> Buildings shall have approved address numbers, building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Address numbers shall be Arabic numerals or alphabet numbers. Numbers shall be a minimum of 4 inches high with a ½ inch stroke. (OFC 505.1) <i>Please provide a physical address for each new building visible from the approaching roadway.</i>
<b>FD 11.</b>	<b><u>FIRE DEPARTMENT ACCESS TO EQUIPMENT:</u></b> Fire protection equipment shall be identified in an approved manner. Rooms containing controls for HVAC, fire sprinklers risers and valves or other fire detection, suppression or control features shall be identified with approved signs. (OFC 509.1)

**MASTER EXHIBIT LIST:**

The following exhibits are hereby entered into the public record by the Development Review Board as confirmation of its consideration of the application as submitted. This is the exhibit list that includes exhibits for Planning Case File DB12-0074 through DB12-0076.

- A1.** Staff report and findings (this document)
- A2.** Staff’s public hearing presentation slides (not available until public hearing)
- B1.** Applicant’s Notebook (under separate cover):
  - Response to Second Request for Additional Information and Revisions
  - Response to Incomplete Application
  - Outdoor Lighting Photometric Plan
  - Title Report
  - Application
  - Compliance Narrative
  - Prior Approvals
  - Development Agreement Recorded August 17, 2012
  - Earnest Agreement Including Note about Future Application Fees
  - Traffic Impact Report
  - Storm Drainage Report
    - Appendix A: Vicinity Map/Existing Conditions Plan
    - Appendix B: Soil Maps & Classification/Runoff Curve Number Designation
    - Appendix C: Prelim. Storm Design and Prelim. Plans
    - Appendix D: On-site Basin Maps
    - Appendix E: Geotechnical Report
  - Notice Mailing labels provided by applicant
  - Tax Map
  - Reduced Plans (smaller version of Exhibit B2)
- B2.** Plan Sets and Architectural Drawings (under separate cover):
  - Color Perspectives of Carl’s Jr. Building
  - Color Perspectives of Multi-tenant Commercial Building
  - A1.0 Architectural Site Plan
  - A3.1 Plywood Backing Plan (Carl’s Jr. Building)

- A3.0 Floor Plan & Details (Carl’s Jr. Building)
- A4.0 Exterior Elevations and Details (Carl’s Jr. Building)
- A5.0 Exterior Elevations (Carl’s Jr. Building)
- A10.2 Trash Enclosure
- A13.0 Fixture, Materials, Finishes Schedule
- T2.0 Occupancy & Exiting Diagram (Carl’s Jr. Building)
- AC1.0 Floor Plan (Multi-tenant Commercial Building)
- AC2.0 Elevations (North and South) (Multi-tenant Commercial Building)
- AC2.1 Retail Building: Elevations (East and West) (Multi-tenant Commercial Building)
- DD1 Existing Conditions Plan
- DD2 Preliminary Utility Plan
- DD3 Preliminary Grading Plan
- DD4 Shared Driveway Plan and Profile and Holiday Inn Parking Improvements
- DD5 Proposed Truck Turning Movements
- L 1.0 Landscape Irrigation Plan
- L 2.0 Landscape Planting Plan
- L 3.0 Specifications: Irrigation and Planting Plans
- SE1.0 Photometric Site Plan
- SE1.1 Site Lighting Specifications
- SE1.2 Site Lighting Specifications
- B3.** Materials Boards (available at public hearing or for inspection at Planning office)
- B4.** Updated Photometric Plan SE1.4 (under separate cover)
- B5.** Correspondence and Related Drawings Received by the City February 25, 2013 Regarding Concerns about Trash Enclosure Design
- C1.** Natural Resource Division Comments and Conditions
- C2.** Engineering Division Comments and Conditions
- C3.** Building Division Comments and Conditions
- C4.** Tualatin Valley Fire & Rescue Comments and Conditions
- C5.** Comments from Wilsonville Public Works Department
- C6.** Comments from Portland General Electric

**FINDINGS OF FACT:**

1. The statutory 120-day time limit applies to this application. The application was received on November 30, 2012. On December 12, 2012, staff conducted a completeness review within the statutorily allowed 30-day review period, and, on January 22, 2013, the Applicant submitted new materials. Additional materials were submitted on February 7, 2013. On February 8, 2013 the application was deemed complete. The City must render a final decision for the request, including any appeals, by June 8, 2013.
2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	PDI	95 <sup>th</sup> /Boones Ferry Intersection/ Riverwood Industrial Campus

East:	PDC	Chevron/Boones Ferry Rd.
South:	PDC	Holiday Inn
West:	PDC	95 <sup>th</sup> Avenue/AGC Center

3. Prior land use actions include:

Edwards Business Center Industrial Park Plat-Stage I  
 97DB28 Stage II, Site Design Review, LaPoint Center  
 DB06-0041, DB06-0043, DB06-0057, DB06-0042 Stage II Final Plan, Site Design Review,  
 Waiver to Building Height, Master Sign Plan for Brice Office Building (Expired)

4. The applicant has complied with Sections 4.013-4.031 of the Wilsonville Code, said sections pertaining to review procedures and submittal requirements. The required public notices have been sent and all proper notification procedures have been satisfied.

**CONCLUSIONARY FINDINGS:**

NOTE: Pursuant to Section 4.014 the burden of proving that the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

**GENERAL INFORMATION**

***Section 4.008 Application Procedures-In General***

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville’s development review process.

**Finding:** These criteria are met.

**Explanation of Finding:** The application is being processed in accordance with the applicable general procedures of this Section.

***Section 4.009 and Subsection 4.140 (.03) Who May Initiate Application and Ownership***

**Review Criterion:** “Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply.” “The tract or tracts of land included in a proposed Planned Development must be in one (1) ownership or control or the subject of a joint application by the owners of all the property included.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The application has been submitted on behalf of the property owner, Wilsonville Devco LLC. The application form is signed by Josh Veentjer, Managing Member. A small portion of the landscape island improvement are on property owned by LaPoint Business Group LLC. Mr. Garry LaPoint has signed an application to include the property in the application.

***Subsection 4.010 (.02) Pre-Application Conference***

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Explanation of Finding:** A pre-application conference was held on February 16, 2012 in accordance with this subsection.

***Subsection 4.011 (.02) B. Lien Payment before Application Approval***

**Review Criterion:** “City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

***Subsection 4.035 (.04) A. General Site Development Permit Submission Requirements***

**Review Criteria:** “An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code.” Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

***Section 4.110 Zoning-Generally***

**Review Criteria:** “The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192.” “The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

**REQUEST A: DB12-0074 STAGE II FINAL PLAN**

**Planned Development Regulations**

***Subsection 4.140 (.01) Purpose of Planned Development Regulations***

A1. **Review Criterion:** The proposed Stage II Final Plan shall be consistent with the Planned Development Regulations purpose statement.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Based on the information provided by the applicant in their narrative, staff is of the professional opinion that the purpose of the planned development regulations is met by the proposed Stage II Final Plan.

***Subsections 4.140 (.02) and (.05) Planned Development Lot Size and Permit Process***

A2. **Review Criteria:** “Planned Development may be established on lots which are suitable for and of a size to be planned and developed in a manner consistent with the purposes and objectives of Section 4.140.” “Any site designated for development in the Comprehensive Plan may be developed as a Planned Development, provided that it is zoned ‘PD.’ All sites which are greater than two (2) acres in size, and designated in the Comprehensive Plan for commercial, residential, or industrial use shall be developed as Planned Developments, unless approved for other uses permitted by the Development Code.”

“All parcels of land exceeding two (2) acres in size that are to be used for residential, commercial or industrial development, shall, prior to the issuance of any building permit:

1. Be zoned for planned development;
2. Obtain a planned development permit; and
3. Obtain Development Review Board, or, on appeal, City Council approval.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The development site is less than two (2) acres. However, it is previously been zoned for Planned Development. The property is designated for commercial development in the Comprehensive Plan, and is zoned Planned Development Commercial. The property is of sufficient size and will be developed as a planned development in accordance with this subsection.

***Subsection 4.140 (.04) Professional Design Team Required for Planned Developments***

A3. **Review Criteria:** “The applicant for all proposed Planned Developments shall certify that the professional services of the appropriate professionals have been utilized in the planning process for development. One of the professional consultants chosen by the applicant shall be designated to be responsible for conferring with the planning staff with respect to the concept and Explanation of the plan.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant’s compliance narrative lists the appropriate professionals involved in the planning and permitting process. Ben Altman of SFA Design Group has been designated the coordinator for the planning portion of the project.



## **Stage II Final Plan Submission Requirements and Process**

### ***Subsection 4.140 (.09) A. Timing of Submission***

- A4. **Review Criterion:** “Unless an extension has been granted by the Development Review Board, within two (2) years after the approval or modified approval of a preliminary development plan (Stage I), the applicant shall file with the City Planning Department a final plan for the entire development or when submission in stages has been authorized pursuant to Section 4.035 for the first unit of the development”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** A previous Stage I approval identified the subject property as a future commercial stage. A Stage II Final Plan is now being submitted consistent with the previous Stage I Master Plan.

### ***Subsection 4.140 (.09) C. Conformance with Stage I and Additional Submission Requirements***

- A5. **Review Criteria:** “The final plan shall conform in all major respects with the approved preliminary development plan, and shall include all information included in the preliminary plan plus the following:” listed 1. through 6.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant states, and staff concurs, that the Stage II plans substantially conforms to the proposed revised Stage I Master plan. The applicant has provided the required drawings and other documents showing all the additional information required by this subsection.

### ***Subsection 4.140 (.09) D. Stage II Final Plan Detail***

- A6. **Review Criterion:** “The final plan shall be sufficiently detailed to indicate fully the ultimate operation and appearance of the development or phase of development.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant has provided sufficiently detailed information to indicate fully the ultimate operation and appearance of the development, including a detailed site plan, landscape plans, floor plans, elevation drawings, and material information.

### ***Subsection 4.140 (.09) E. Submission of Legal Documents***

- A7. **Review Criterion:** “Copies of legal documents required by the Development Review Board for dedication or reservation of public facilities, or for the creation of a non-profit homeowner’s association, shall also be submitted.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No additional legal documentation is required for dedication or reservation of public facilities.

### ***Subsection 4.140 (.09) J. Planned Development Permit Requirements***

- A8. **Review Criteria:** “A planned development permit may be granted by the Development Review Board only if it is found that the development conforms to all the following criteria, as well as to

the Planned Development Regulations in Section 4.140:” listed J. 1. through 3. Includes traffic level of service requirements.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposal is a commercial use in an area designated for commercial in the Comprehensive Plan. The proposed uses are at a corner and clustered with commercial uses similarly serving the travelling public, thus being part of a commercial center rather than strip commercial development. As demonstrated in the Traffic Impact Report in the applicant’s notebook, Exhibit B1, specifically Table 2 on page 4 of the report, the required traffic level of service is being maintained. All utilities and services are available to serve the development.

### **Commercial Development in Any Zone**

#### ***Subsection 4.116 (.01) Commercial Development to be in Centers and Complexes***

A9. **Review Criteria:** “Commercial developments shall be planned in the form of centers or complexes as provided in the City’s Comprehensive Plan. As noted in the Comprehensive Plan, Wilsonville’s focus on centers or complexes is intended to limit strip commercial development.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The proposed commercial development is in the form of a center at clustered at an intersection with other commercial development.

#### ***Subsection 4.116 (.05) All Commercial Activity to be Completely Enclosed***

A10. **Review Criteria:** “All businesses, service or processing, shall be conducted wholly within a completely enclosed building; except for:” Listed A. through G.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All commercial activity other than exempt activities will be within in the proposed buildings. The only exceptions from the list given noted by the applicant are off-street parking for customers and employees. Staff notes, there is the possibility of outdoor seating, as well as temporary outside sales.

#### ***Subsection 4.116 (.07) Uses Limited to those Meeting Industrial Performance Standards***

A11. **Review Criteria:** “Uses shall be limited to those which will meet the performance standards specified in Section 4.135(.05), with the exception of 4.135(.05)(M).(3).”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed development facilitates commercial uses meeting these performance standards. It is understood that all uses will need to continue to meet these standards over time.

#### ***Subsection 4.116 (.08) Vision Clearance Standards for Corner Lots***

A12. **Review Criteria:** “Corner lots shall conform to the vision clearance standards set forth in Section 4.177.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Vision clearance has been reviewed by the City’s Engineering Division and the City’s Public Works standards for vision clearance are met.

### ***Subsection 4.116 (.10) Commercial Development Generally***

A13. **Review Criteria:** This subsection lists a number of requirements for commercial development such as setback, lot size, lot coverage, and street frontage requirements.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All the applicable standards listed in this subsection are met.

### ***Subsection 4.116 (.14) B. Prohibited Uses***

A14. **Review Criteria:** “Any use that violates the performance standards of Section 4.135(.05), other than 4.135(.05)(M).(3.) is prohibited within commercial developments.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No uses prohibited by this subsection are proposed.

### **Standards Applying in All Planned Development Zones**

#### ***Subsection 4.118 (.01) Additional Height Guidelines***

A15. **Review Criterion:** “In cases that are subject to review by the Development Review Board, the Board may further regulate heights as follows:

A. Restrict or regulate the height or building design consistent with adequate provision of fire protection and fire-fighting apparatus height limitations.

B. To provide buffering of low density developments by requiring the placement of three or more story buildings away from the property lines abutting a low density zone.

C. To regulate building height or design to protect scenic vistas of Mt. Hood or the Willamette River.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Staff does not recommend the Development Review Board require a height less than the applicant proposes as the proposed height provides for fire protection access, does not abut a low density zone, and does not impact scenic views of Mt. Hood or the Willamette River.

#### ***Subsection 4.118 (.03) Waivers***

A16. **Review Criteria:** “Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may” waive a number of standards as listed in A. through E.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No planned development waivers have been requested by the applicant or are necessary to approve the application as proposed.

#### ***Subsection 4.118 (.03) E. Other Requirements or Restrictions***

A17. **Review Criteria:** “Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may adopt other requirements or restrictions, inclusive of, but not limited to, the following:” Listed 1. through 12.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No additional requirements or restrictions are recommended pursuant to this subsection.

***Subsection 4.118 (.04) Effect of Determination of Compliance and Conditions of Approval on Development Cost***

A18. **Review Criteria:** “The Planning Director and Development Review Board shall, in making their determination of compliance in attaching conditions, consider the effects of this action on availability and cost. The provisions of this section shall not be used in such a manner that additional conditions, either singularly or cumulatively, have the effect of unnecessarily increasing the cost of development. However, consideration of these factors shall not prevent the Board from imposing conditions of approval necessary to meet the minimum requirements of the Comprehensive Plan and Code.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** It is staff’s professional opinion that the determination of compliance or attached conditions do not unnecessarily increase the cost of development, and no evidence has been submitted to the contrary.

***Subsection 4.118 (.05) Requirements to Set Aside Tracts for Certain Purposes***

A19. **Review Criteria:** “The Planning Director, Development Review Board, or on appeal, the City Council, may as a condition of approval for any development for which an application is submitted, require that portions of the tract or tracts under consideration be set aside, improved, conveyed or dedicated for the following uses:” Recreational Facilities, Open Space Area, Easements.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No additional tracts are being required for the purposes given.

***Subsection 4.118 (.09) Habitat Friendly Development Practices***

A20. **Review Criteria:** “To the extent practicable, development and construction activities of any lot shall consider the use of habitat-friendly development practices, which include:

A. Minimizing grading, removal of native vegetation, disturbance and removal of native soils, and impervious area;

B. Minimizing adverse hydrological impacts on water resources, such as using the practices described in Part (a) of Table NR-2 in Section 4.139.03, unless their use is prohibited by an applicable and required state or federal permit, such as a permit required under the federal Clean Water Act, 33 U.S.C. §§1251 et seq., or the federal Safe Drinking Water Act, 42 U.S.C. §§300f et seq., and including conditions or plans required by such permit;

C. Minimizing impacts on wildlife corridors and fish passage, such as by using the practices described in Part (b) of Table NR-2 in Section 4.139.03; and

D. Using the practices described in Part (c) of Table NR-2 in Section 4.139.03.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As stated by the applicant, “The site has previously been rough graded and there is no significant native vegetation. The site does not contain any SROZ and no fish or wildlife habitats are associated with this property. The site has been designed consistent with the Habitat-Friendly practices. The storm system design provides for on-site water quality and volume control which protects the downstream wetland area south of the AGC building.”

## Planned Development Commercial Zone

### *Subsection 4.131 (.01) A. 1. Uses Typically Permitted*

A21. **Review Criteria:** This subsection lists the uses that are typically permitted in the PDC Zone.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed uses include a fast food restaurant (service establishment) and a commercial building for tenants yet to be determined. The remaining tenants can be a variety of commercial uses listed in this subsection as long as parking and other requirements continue to be met.

### *Subsection 4.131 (.02) Prohibited Uses*

A22. **Review Criteria:** This subsection lists the prohibited uses in the PDC Zone.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has not proposed any prohibited uses for the site.

### *Subsection 4.131 (.03) 1. Block and Access Standards: Connectivity for Different Modes*

A23. **Review Criteria:** “The Development Review Board shall determine appropriate conditions of approval to assure that adequate connectivity results for pedestrians, bicyclists, and motor vehicle drivers. Consideration shall be given to the use of public transit as a means of meeting access needs.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No new blocks are proposed, and the proposed development proposes to use the existing shared private driveway on 95<sup>th</sup> Avenue partially on the subject property. A development agreement has been agreed upon between the owner of the subject property, neighboring properties, and the City ensuring appropriate access from the shared driveway.

## Parking and Loading

### *Subsection 4.155 (.02) General Parking Provisions*

A24. **Review Criteria:** This subsection lists a number of general provisions for parking.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has provided sufficient information demonstrating compliance with the provisions in this subsection applicable to State II Final Plan review. Among the information provided parking calculations on sheet A1.0. of Exhibit B2. Staff specifically points out the following:

- In relation to provision B. all parking areas are accessible and usable for parking
- In relation to provisions D. the provided parking exceeds to the sum of the minimum parking for the fast food restaurant and the multi-tenant commercial building.
- In relation to provision J. a note on sheet A1.0 of Exhibit B2 states this requirement will be met.
- In relation to provision K. the parking area is paved and provided with adequate drainage.

- In relation to provision L. the parking lot lighting is fully shielded as to not shine into adjoining structures or the eyes of passerby's.
- In relation to provision N. 12 compact parking spaces are proposed, which is less than forty (40) percent of the proposed parking spaces. They are shown appropriately marked on Sheet A1.0 of Exhibit B2.

***Subsection 4.155 (.03) A. Functional Design of Parking, Loading, and Delivery Areas***

A25. **Review Criteria:** “Parking and loading or delivery areas shall be designed with access and maneuvering area adequate to serve the functional needs of the site and shall:

1. Separate loading and delivery areas and circulation from customer and/or employee parking and pedestrian areas. Circulation patterns shall be clearly marked.
2. To the greatest extent possible, separate vehicle and pedestrian traffic.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Sheet DD5 “Proposed Truck Turning Movements” of Exhibit B2 demonstrates sufficient access and maneuvering areas for delivery trucks, both for the Chevron fuel and Carl’s Jr. and the other commercial tenants. Staff notes fuel off-loading, and restaurant other commercial delivery parking are in the same area of the site separating these operations from the general employee and customer parking and pedestrian areas. The access and maneuvering areas for passenger vehicle parking areas appears sufficient providing adequate space for two-way travel. The applicant states on page 33 of the compliance narrative in their notebook, Exhibit B1, that “care has been given to the extent practicable to separate vehicle and pedestrian traffic.” Staff has reviewed the site plan and found no code supported site changes to further separate pedestrian and vehicle traffic.

***Subsection 4.155 (.03) B. 1.-3. Parking Area Landscaping***

A26. **Review Criteria:** “Parking and loading or delivery areas shall be landscaped to minimize the visual dominance of the parking or loading area, as follows:” Listed 1. through 3.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As shown in the planting plans (applicant’s sheet L1.0), the required amount of landscaping and trees are provided.

***Subsection 4.155 (.03) B. 4. Parking and Loading Areas-Safe and Convenient Access***

A27. **Review Criterion:** “Be designed for safe and convenient access that meets ADA and ODOT standards. All parking areas which contain ten (10) or more parking spaces, shall for every fifty (50) standard spaces., provide one ADA-accessible parking space that is constructed to building code standards, Wilsonville Code 9.000.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Two ADA parking spaces are proposed.

***Subsection 4.155 (.03) B. 5. Parking Connectivity and Efficient On-site Circulation***

A28. **Review Criteria:** “Where possible, parking areas shall be designed to connect with parking areas on adjacent sites so as to eliminate the necessity of utilizing the public street for multiple accesses or cross movements. In addition, on-site parking shall be designed for efficient on-site circulation and parking.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed development adds to an existing commercial center that includes a fuel station, convenience market, sit down restaurant, convention center, and hotel. The proposed uses as well as the existing Chevron and Holiday Inn share a common driveway off 95<sup>th</sup> Avenue and their access and parking areas are interconnected. Joint use of many the access and maneuvering areas is covered in the Development Agreement, a copy of which is in the applicant’s notebook, Exhibit B1. No specific standards are listed in the code to determine efficiency of on-site circulation and parking. Two factors commonly considered to determine such efficiency include proximity of parking to likely destinations, and direct vehicle and pedestrian paths between destinations with limited choke points. Parking is provided close to both the Carl’s Jr restaurant and multi-tenant building allowing for short, efficient pedestrian trips after parking. Multiple pedestrian accesses from the public sidewalk are provided, including ones providing the most direct path from the sidewalk to business entrances. All vehicles enter the site through a shared driveway with Holiday Inn and Chevron. While this could become a choke point, care has been taken to design the driveway for optimal performance to minimize traffic delays, as reflected in the Development Agreement. Straight drive aisles and multiple access points allow for direct vehicle travel within the site. In addition, the drive-thru lane is designed to allow for quick and efficient exiting use of the site.

**Subsection 4.155 (.03) B. 8. Parking Minimum and Maximum**

A29. **Review Criteria:** “Tables 5, below, shall be used to determine the minimum and maximum parking standards for various land uses. The minimum number of required parking spaces shown on Tables 5 shall be determined by rounding to the nearest whole parking space.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As shown in the table below, the proposed parking is consistent with Table 5: Parking Standards.

Use	Floor Area	Min	Max	Min	Max	Provided
Fast food (with drive-thru)	2,867	9.9 per 1,000 SF	14.9 per 1000 SF	28	43	
Retail	3150	4.1 per 1,000 SF	6.2 per 1000 SF	13	20	
Standard Spaces						34
Compact Spaces (40% Max)				--	18	12
Total Non-ADA Spaces				41	60	46
ADA Spaces				1	--	2
<b>Total Parking Spaces</b>						<b>48</b>

**Subsection 4.155 (.04) A. Required Number of Loading Berths**



- A30. **Review Criteria:** This subsection defines the requirements for loading berths including when loading berths are required and size requirements.  
**Finding:** These criteria are not applicable.  
**Explanation of Finding:** No loading berths are required for commercial uses of the proposed floor area.

#### ***Section 4.167 Access, Ingress, and Egress***

- A31. **Review Criterion:** “Each access onto streets or private drives shall be at defined points as approved by the City and shall be consistent with the public's health, safety and general welfare. Such defined points of access shall be approved at the time of issuance of a building permit if not previously determined in the development permit.”  
**Finding:** This criterion is satisfied.  
**Explanation of Finding:** The access points for the development site are existing and approved by the City. No change in access is proposed.

#### **Natural Features**

##### ***Section 4.171 Protection of Natural Features and Other Resources***

- A32. **Review Criteria:** This section provides for the protection of a number of natural features and other resources including: general terrain preparation, hillsides, trees and wooded areas, high voltage powerline easements and rights of way and petroleum pipeline easements, earth movement hazard areas, soil hazard areas, historic resources, and cultural resources.  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** None of the resources listed in this section exist on the site or will be foreseeably negatively impacted by the development.

#### **Public Safety and Crime Prevention**

##### ***Subsection 4.175 (.01) Design to Deter Crime and Ensure Public Safety***

- A33. **Review Criterion:** “All developments shall be designed to deter crime and insure public safety.”  
**Finding:** This criterion is satisfied.  
**Explanation of Finding:** The applicant asserts, and staff concurs, that attention has been given to site design to deter crime and allow natural surveillance. Staff has no evidence that the proposed development would otherwise negatively impact public safety.

##### ***Subsection 4.175 (.02) Addressing and Directional Signing***

- A34. **Review Criteria:** “Addressing and directional signing shall be designed to assure identification of all buildings and structures by emergency response personnel, as well as the general public.”  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** The design of the site provides for appropriate addressing and directional signage to assure easy identification. Addressing is on freestanding sign and buildings.

##### ***Subsection 4.175 (.03) Surveillance and Police Access***

- A35. **Review Criterion:** “Areas vulnerable to crime shall be designed to allow surveillance. Parking and loading areas shall be designed for access by police in the course of routine patrol duties.”  
**Finding:** This criterion is satisfied.  
**Explanation of Finding:** The parking and loading areas are easily assessable to law enforcement.

*Subsection 4.175 (.04) Lighting to Discourage Crime*

- A36. **Review Criterion:** “Exterior lighting shall be designed and oriented to discourage crime.”  
**Finding:** This criterion is satisfied.  
**Explanation of Finding:** While exterior lighting has been minimized that applicant states that it has been designed and oriented to discourage crime.

**Landscaping Standards**

*Subsection 4.176 (.01) Purpose of Landscape, Screening, and Buffering*

- A37. **Review Criteria:** “This Section consists of landscaping and screening standards and regulations for use throughout the City. The regulations address materials, placement, layout, and timing of installation. The City recognizes the ecological and economic value of landscaping and requires the use of landscaping and other screening or buffering to:” Listed A. through K.  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** In complying with the various landscape standards in Section 4.176 the applicant has demonstrated the proposed Stage II Final Plan is in compliance with the landscape purpose statement.

*Subsection 4.176 (.02) B. Landscaping Standards and Code Compliance*

- A38. **Review Criteria:** “All landscaping and screening required by this Code must comply with all of the provisions of this Section, unless specifically waived or granted a Variance as otherwise provided in the Code. The landscaping standards are minimum requirements; higher standards can be substituted as long as fence and vegetation-height limitations are met. Where the standards set a minimum based on square footage or linear footage, they shall be interpreted as applying to each complete or partial increment of area or length”  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** No waivers or variances to landscape standards have been requested. Thus all landscaping and screening must comply with standards of this section.

*Subsection 4.176 (.02) C. 1. General Landscape Standards-Intent*

- A39. **Review Criteria:** “The General Landscaping Standard is a landscape treatment for areas that are generally open. It is intended to be applied in situations where distance is used as the principal means of separating uses or developments and landscaping is required to enhance the intervening space. Landscaping may include a mixture of ground cover, evergreen and deciduous shrubs, and coniferous and deciduous trees.”  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** The applicant’s submitted landscape plans (applicant’s sheets L 1.0 through L3.0) show a variety of plant materials and placement consistent with the

general landscape standard, specifically along the frontage with SW 95<sup>th</sup> Avenue and SW Boones Ferry Road.

***Subsection 4.176 (.02) C. 2. General Landscape Standards-Required Materials***

A40. **Review Criteria:** “Shrubs and trees, other than street trees, may be grouped. Ground cover plants must fully cover the remainder of the landscaped area (see Figure 21: General Landscaping). The General Landscaping Standard has two different requirements for trees and shrubs:

a. Where the landscaped area is less than 30 feet deep, one tree is required for every 30 linear feet.

b. Where the landscaped area is 30 feet deep or greater, one tree is required for every 800 square feet and two high shrubs or three low shrubs are required for every 400 square feet.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The planting plan (applicant’s sheet L2.0) shows landscaping meeting the functional requirements of this subsection.

***Subsection 4.176 (.02) E. 1. High Screen Landscape Standard-Intent***

A41. **Review Criterion:** “The High Screen Landscaping Standard is a landscape treatment that relies primarily on screening to separate uses or developments. It is intended to be applied in situations where visual separation is required.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant’s submitted landscape plans, sheet L2.0 of Exhibit B2, shows plantings consistent with high screen landscape standard to provide visual screening of the drive-thru signage so as to make the signage not visible from off-site. consistent with the requirements of Subsection 4.176 (.04) C.

***Subsection 4.176 (.02) E. 2. High Screen Landscape Standard-Required Materials***

A42. **Review Criteria:** “The High Screen Landscaping Standard requires sufficient high shrubs to form a continuous screen at least six (6) feet high and 95% opaque, year-round. In addition, one tree is required for every 30 linear feet of landscaped area, or as otherwise required to provide a tree canopy over the landscaped area. Ground cover plants must fully cover the remainder of the landscaped area. A six (6) foot high masonry wall or a berm may be substituted for the shrubs, but the trees and ground cover plants are still required. When applied along street lot lines, the screen or wall is to be placed along the interior side of the landscaped area.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The high screen standard is being used to block off-site visibility of the drive-thru signage so as to allow the signs to be exempt from sign permit requirements pursuant to Subsection 4.156.05 (.02) E. The proposed landscape material is sufficient to provide a six (6) foot high screen, 95% opaque year around, for a sufficient length to block off-site view of the drive-thru signs. Details of plant materials are reviewed as part of Request B, Site Design Review.

***Subsection 4.176 (.03) Landscape Area and Locations***

A43. **Review Criteria:** “Not less than fifteen percent (15%) of the total lot area, shall be landscaped with vegetative plant materials. The ten percent (10%) parking area landscaping required by

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Exhibit A1

Boones Ferry Pointe: Carl’s Jr. Restaurant and Multi-tenant Commercial Building

DB12-0074 through DB12-0076

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section 4.155.03(B)(1) is included in the fifteen percent (15%) total lot landscaping requirement. Landscaping shall be located in at least three separate and distinct areas of the lot, one of which must be in the contiguous frontage area. Planting areas shall be encouraged adjacent to structures. Landscaping shall be used to define, soften or screen the appearance of buildings and off-street parking areas. Materials to be installed shall achieve a balance between various plant forms, textures, and heights. The installation of native plant materials shall be used whenever practicable.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** According to the applicant’s sheet L 2.0 16,139 square feet of the 55,605 square foot site, or twenty-nine percent (29%), is proposed to be in landscaping. This will be reduced by twenty-four (24) square feet to 16,115 square feet to allow a wider trash enclosure for Chevron as described in Exhibit B5. The landscaping is in a variety of areas throughout the site, including the street frontage areas. Landscaping is placed along the streets to soften the look of off-street parking areas. As shown on the applicant’s sheet L 2.0 a variety of landscape materials are being used.

#### ***Subsection 4.176 (.04) Buffering and Screening***

A44. **Review Criteria:** “Additional to the standards of this subsection, the requirements of the Section 4.137.5 (Screening and Buffering Overlay Zone) shall also be applied, where applicable.

C. All exterior, roof and ground mounted, mechanical and utility equipment shall be screened from ground level off-site view from adjacent streets or properties.

D. All outdoor storage areas shall be screened from public view, unless visible storage has been approved for the site by the Development Review Board or Planning Director acting on a development permit.

E. In all cases other than for industrial uses in industrial zones, landscaping shall be designed to screen loading areas and docks, and truck parking.

F. In any zone any fence over six (6) feet high measured from soil surface at the outside of fenceline shall require Development Review Board approval.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The buildings are designed so architectural parapets screen roof mounted equipment. Mixed-solid waste and recycling storage areas are within screening enclosures. No additional outdoor storage areas are proposed.

#### ***Subsection 4.176 (.09) Landscape Plans***

A45. **Review Criteria:** “Landscape plans shall be submitted showing all existing and proposed landscape areas. Plans must be drawn to scale and show the type, installation size, number and placement of materials. Plans shall include a plant material list. Plants are to be identified by both their scientific and common names. The condition of any existing plants and the proposed method of irrigation are also to be indicated.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Applicant’s sheets L 1.0, L 2.0, L 3.0 provide the required information.

#### ***Subsection 4.176 (.12) Mitigation Standards***

A46. **Review Criterion:** “A mitigation plan is to be approved by the City’s Development Review Board before the destruction, damage, or removal of any existing native plants.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No existing native plans are being removed requiring a mitigation plan pursuant to this subsection.

### **Other Standards**

#### ***Section 4.177 Street Improvement Standards***

A47. **Review Criteria:** This section establishes improvement standards for public streets, along with private access drives and travel lanes.

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDA 1.

**Explanation of Finding:**

- Access is provided to the proposed development clear of any obstructions.
- The travel lanes are proposed to be asphalt. The condition of approval will ensure the travel lane is constructed so as to be capable of carrying a 23-ton load.
- All access lanes are a minimum of 12 feet.
- The development will comply with requirements of the Fire District.
- No construction is proposed in the public right-of-way

#### ***Section 4.178 Sidewalk and Pathway Standards***

A48. **Review Criteria:** This section establishes standards for sidewalks and pathways.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** No new sidewalks, as defined by Wilsonville Code 4.001, or bicycle paths are required or proposed in relation to the proposed development. The pedestrian pathways provided on the site provide reasonably direct connections between likely destinations. See also Finding A28.

#### ***Section 4.179 Mixed Solid Waste and Recyclables Storage***

A49. **Review Criteria:** This section establishes standards for mixed solid waste and recyclables storage in new multi-family residential and non-residential buildings.

**Finding:** These criteria are satisfied.

**Explanation of Finding:**

- As shown on page 51 of the compliance report in the applicant's notebook, Exhibit B1, the minimum square footage of mixed solid waste and recyclables storage is well exceeded.
- Allied Waste, the City's franchise waste hauler, has approved the waste enclosures. See letter following page 15 of Response to Incomplete Application in the applicant's notebook, Exhibit B1. However, in Exhibit B5, Allied Waste expresses concern about the cover for the Chevron enclosure based on additional information on the type of containers being used. However, this concern is not under the authority of the DRB to review. With or without a cover, the enclosure will meet requirements of this Section.
- The proposed increase of the Chevron enclosure by approximately 2' 2", as requested by Exhibit B5, and mentioned as necessary in Allied Waste's letter in Exhibit B5, continues to meet the requirements of this Section and is being approved as part of this request.

***Sections 4.199.20 Outdoor Lighting***

A50. **Review Criteria:** This section states that the outdoor lighting ordinance is applicable to “Installation of new exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas” and “Major additions or modifications (as defined in this Section) to existing exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas.” In addition the exempt luminaires and lighting systems are listed.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All the outdoor lighting for the new development on the site is being required to comply with the outdoor lighting ordinance. A photometric site plan has been provided, sheet SE1.4 (Exhibit B4), showing the functional effect of the proposed lighting on the site. Detailed requirements for site lighting are being reviewed as a component of Request B, Site Design Review, of this application. See Findings B35 through B40.

***Sections 4.300-4.320 and Subsection 4.118 (.02) Underground Installation of Utilities***

A51. **Review Criteria:** These sections list requirements regarding the underground installation of utilities.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** There are no existing overhead facilities that require undergrounding as part of this development. All new utilities associated with the development are proposed to be installed underground.

## REQUEST B: DB12-0075 SITE DESIGN REVIEW

### Site Design Review

#### ***Subsection 4.400 (.01) and Subsection 4.421 (.03) Excessive Uniformity, Inappropriateness of Design, Etc.***

- B1. **Review Criteria:** “The Board shall also be guided by the purpose of Section 4.400, and such objectives shall serve as additional criteria and standards.” “Excessive uniformity, inappropriateness or poor design of the exterior appearance of structures and signs and the lack of proper attention to site development and landscaping in the business, commercial, industrial and certain residential areas of the City hinders the harmonious development of the City, impairs the desirability of residence, investment or occupation in the City, limits the opportunity to attain the optimum use in value and improvements, adversely affects the stability and value of property, produces degeneration of property in such areas and with attendant deterioration of conditions affecting the peace, health and welfare, and destroys a proper relationship between the taxable value of property and the cost of municipal services therefor.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant provides a response to this subsection on pages 57-59 of the compliance narrative in their notebook, Exhibit B1. Staff summarizes the compliance with this subsection as follows:

*Excessive Uniformity:* The design of the buildings are different from each other, including different roof types, and have an architectural character unique from the surrounding development preventing uniformity.

*Inappropriate or Poor Design of the Exterior Appearance of Structures:* Besides signs, structures on the site include the two buildings and solid waste and recycling enclosures. The buildings have been professionally designed with a unique historic “small-town” theme indicative of other commercial development in Wilsonville including Old Town Square (Fred Meyer development). The result is a professional design appropriate for Wilsonville. The waste and recycling enclosures are of a construction and design typical for their intended use.

*Inappropriate or Poor Design of Signs:* Signs are typical of the type of development proposed and meet applicable City standards. See Request C, Master Sign Plan.

*Lack of Proper Attention to Site Development:* The appropriate professional services have been used to design the site incorporating unique features of the site including site size and shape and available access, demonstrating appropriate attention being given to site development.

*Lack of Proper Attention to Landscaping:* Landscaping is provided exceeding the area requirements, has been professionally designed by a landscape architect, and includes a variety of plant materials, all demonstrating appropriate attention being given to landscaping.

#### ***Subsection 4.400 (.02) and Subsection 4.421 (.03) Purposes of Objectives of Site Design Review***

- B2. **Review Criteria:** “The Board shall also be guided by the purpose of Section 4.400, and such objectives shall serve as additional criteria and standards.” “The City Council declares that the



purposes and objectives of site development requirements and the site design review procedure are to:” Listed A through J.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant provides a response to this subsection on pages 57-59 of the compliance narrative in their notebook, Exhibit B1, demonstrating compliance with the listed purposes and objectives. In short, the proposal provides a high quality design appropriate for the site and its location in Wilsonville.

#### ***Section 4.420 Development in Accordance with Plans***

B3. **Review Criteria:** The section states that development is required in accord with plans approved by the Development Review Board.

**Finding:** These criteria will be satisfied by Condition of Approval PDB 1.

**Explanation of Finding:** A condition of approval has been included to ensure construction, site development, and landscaping are carried out in substantial accord with the Development Review Board approved plans, drawings, sketches, and other documents. No building permits will be granted prior to development review board approval.

#### ***Subsection 4.421 (.01) and (.02) Site Design Review-Design Standards***

B4. **Review Criteria:** This subsection lists the design standards for Site Design Review. Listed A through G. Pursuant to subsection (.02) ““The standards of review outlined in Sections (a) through (g) above shall also apply to all accessory buildings, structures, exterior signs and other site features, however related to the major buildings or structures.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has provided sufficient information demonstrating compliance with the standards of this subsection. Among the information provided is a written response to these standards on page 61 of the compliance narrative in the applicant’s notebook, Exhibit B1.

#### ***Subsection 4.421 (.05) Site Design Review-Conditions of Approval***

B5. **Review Criterion:** “The Board may attach certain development or use conditions in granting an approval that are determined necessary to insure the proper and efficient functioning of the development, consistent with the intent of the Comprehensive Plan, allowed densities and the requirements of this Code.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No additional conditions of approval are recommended to ensure the proper and efficient functioning of the development.

#### ***Subsection 4.421 (.06) Color or Materials Requirements***

B6. **Review Criterion:** “The Board or Planning Director may require that certain paints or colors of materials be used in approving applications. Such requirements shall only be applied when site development or other land use applications are being reviewed by the City.”

**Finding:** This criterion is satisfied will be satisfied by a Condition of Approval PDB 2.

**Explanation of Finding:** It is not clear what paint colors and roofing material, if a cover is constructed, is being used for the trash enclosures. The condition of approval requires the

colors of any roof of trash enclosures mirror the color themes and roofing materials used on the buildings.

#### ***Section 4.430 Design of Trash and Recycling Enclosures***

- B7. **Review Criteria:** “The following locations, design and access standards for mixed solid waste and recycling storage areas shall be applicable to the requirements of Section 4.179 of the Wilsonville City Code.” Listed (.02) A. through (.04) C.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has provided sufficient information demonstrating the planned mixed solid waste and recyclables enclosure and other waste management features on the site comply with the standards of this section. Among the information provided is a written response to the standards of this section on pages 63 of the compliance narrative and pages 13-14 of the Response to Incomplete Application, both in the applicant’s notebook, Exhibit B1 and additional information in Exhibit B5.

#### ***Section 4.440 Site Design Review-Submittal Requirements***

- B8. **Review Criteria:** This section lists additional submittal requirements for Site Design Review in addition to those listed in Section 4.035.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has submitted the required additional materials, as applicable.

#### ***Subsection 4.450 (.01) Landscape Installation or Bonding***

- B9. **Review Criterion:** “All landscaping required by this section and approved by the Board shall be installed prior to issuance of occupancy permits, unless security equal to one hundred and ten percent (110%) of the cost of the landscaping as determined by the Planning Director is filed with the City assuring such installation within six (6) months of occupancy. "Security" is cash, certified check, time certificates of deposit, assignment of a savings account or such other assurance of completion as shall meet with the approval of the City Attorney. In such cases the developer shall also provide written authorization, to the satisfaction of the City Attorney, for the City or its designees to enter the property and complete the landscaping as approved. If the installation of the landscaping is not completed within the six-month period, or within an extension of time authorized by the Board, the security may be used by the City to complete the installation. Upon completion of the installation, any portion of the remaining security deposited with the City shall be returned to the applicant.”

**Finding:** This criterion will be satisfied by Condition of Approval PDB 3.

**Explanation of Finding:** The condition of approval will assure installation or appropriate security at the time occupancy is requested.

#### ***Subsection 4.450 (.02) Approved Landscape Plan Binding***

- B10. **Review Criterion:** “Action by the City approving a proposed landscape plan shall be binding upon the applicant. Substitution of plant materials, irrigation systems, or other aspects of an approved landscape plan shall not be made without official action of the Planning Director or Development Review Board, as specified in this Code.”

**Finding:** This criterion will be satisfied by Condition of Approval PDB 4.

**Explanation of Finding:** The condition of approval shall provide ongoing assurance this criterion is met.

***Subsection 4.450 (.03) Landscape Maintenance and Watering***

B11. **Review Criterion:** “All landscaping shall be continually maintained, including necessary watering, weeding, pruning, and replacing, in a substantially similar manner as originally approved by the Board, unless altered with Board approval.”

**Finding:** This criterion will be satisfied by Condition of Approval PDB 5.

**Explanation of Finding:** The condition of approval will ensure landscaping is continually maintained in accordance with this subsection.

***Subsection 4.450 (.04) Addition and Modifications of Landscaping***

B12. **Review Criterion:** “If a property owner wishes to add landscaping for an existing development, in an effort to beautify the property, the Landscape Standards set forth in Section 4.176 shall not apply and no Plan approval or permit shall be required. If the owner wishes to modify or remove landscaping that has been accepted or approved through the City’s development review process, that removal or modification must first be approved through the procedures of Section 4.010.”

**Finding:** This criterion will be satisfied by Condition of Approval PDB 5.

**Explanation of Finding:** The condition of approval shall provide ongoing assurance that this criterion is met by preventing modification or removal without the appropriate City review. The landscape plan approval in this DRB action includes the slight reduction in landscaping, including groundcover and one (1) Otto Luyken Laurel, necessary to facilitate the 2’ 2’ widening of the Chevron solid waste and recycling enclosure.

**Parking**

***Subsection 4.155 (.02) Provision and Maintenance of Off-Street Parking***

B13. **Review Criteria:** This subsection lists general provisions for parking, A. through O.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The design of the parking described and illustrated in the applicant’s submitted narrative and plans in relation to these provisions are consistent with the purpose of site design review and the proposed Stage II Final Plan for the site. See Finding A24 under Request A.

***Subsection 4.155 (.03) B. 1.-3. Landscaping of Parking Areas***

B14. **Review Criteria:** “Parking and loading or delivery areas shall be landscaped to minimize the visual dominance of the parking or loading area, as follows:” Listed 1. through 3.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As shown in the planting plans, sheet L2.0 of Exhibit B2, landscape screening is provided between the proposed parking and the public right-of-way. Trees are provided for the twenty-four (24) proposed parking spaces as required by this subsection. Tree planting areas meet the minimum size requirements.

### ***Section 4.171 Protection of Natural Features and Other Resources***

B15. **Review Criterion:** This section provides for the protection of a number of natural features and other resources including: general terrain preparation, hillsides, trees and wooded areas, high voltage powerline easements and rights of way and petroleum pipeline easements, earth movement hazard areas, soil hazard areas, historic resources, and cultural resources.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The proposed design of the site provides for protection of natural features and other resources consistent with the proposed Stage II Final Plan for the site. See Finding A32 under Request A of this application.

### **Landscaping**

#### ***Subsection 4.176 (.02) B. Landscape Standards and Compliance with Code***

B16. **Review Criterion:** “All landscaping and screening required by this Code must comply with all of the provisions of this Section, unless specifically waived or granted a Variance as otherwise provided in the Code. The landscaping standards are minimum requirements; higher standards can be substituted as long as fence and vegetation-height limitations are met. Where the standards set a minimum based on square footage or linear footage, they shall be interpreted as applying to each complete or partial increment of area or length”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No waivers or variances to landscape standards have been requested. Thus all landscaping and screening must comply with standards of this section.

#### ***Subsection 4.176 (.02) C. 1. General Landscape Standards-Intent***

B17. **Review Criteria:** “The General Landscaping Standard is a landscape treatment for areas that are generally open. It is intended to be applied in situations where distance is used as the principal means of separating uses or developments and landscaping is required to enhance the intervening space. Landscaping may include a mixture of ground cover, evergreen and deciduous shrubs, and coniferous and deciduous trees.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant’s sheet L2.0 of Exhibit B2 shows a variety of plant materials and placement consistent with the general landscape standard.

#### ***Subsection 4.176 (.02) C. 2. General Landscape Standards-Required Materials***

B18. **Review Criteria:** “Shrubs and trees, other than street trees, may be grouped. Ground cover plants must fully cover the remainder of the landscaped area (see Figure 21: General Landscaping). The General Landscaping Standard has two different requirements for trees and shrubs:

a. Where the landscaped area is less than 30 feet deep, one tree is required for every 30 linear feet.

b. Where the landscaped area is 30 feet deep or greater, one tree is required for every 800 square feet and two high shrubs or three low shrubs are required for every 400 square feet.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The planting plan, sheet L2.0 of Exhibit B2, shows landscaping meeting the requirements of this subsection.

***Subsection 4.176 (.02) E. 1. High Screen Standard-Intent***

B19. **Review Criterion:** “The High Screen Landscaping Standard is a landscape treatment that relies primarily on screening to separate uses or developments. It is intended to be applied in situations where visual separation is required.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant’s submitted landscape plans, sheet L2.0 of Exhibit B2, shows plantings consistent with high screen landscape standard to provide visual screening of the drive-thru signage so as to make the signage not visible from off-site. consistent with the requirements of Subsection 4.176 (.04) C.

***Subsection 4.176 (.02) E. 2. High Screen Standard-Required Materials***

B20. **Review Criterion:** “The High Screen Landscaping Standard requires sufficient high shrubs to form a continuous screen at least six (6) feet high and 95% opaque, year-round. In addition, one tree is required for every 30 linear feet of landscaped area, or as otherwise required to provide a tree canopy over the landscaped area. Ground cover plants must fully cover the remainder of the landscaped area. A six (6) foot high masonry wall or a berm may be substituted for the shrubs, but the trees and ground cover plants are still required. When applied along street lot lines, the screen or wall is to be placed along the interior side of the landscaped area.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant’s submitted landscape plans, sheet L2.0 of Exhibit B2, shows plantings consistent with high screen landscape standard to provide visual screening of the drive-thru signage so as to make the signage not visible from off-site. consistent with the requirements of Subsection 4.176 (.04) C. The planting plans show a line of twenty-six (26) Emerald Green Arborvitae, six feet (6’) tall spaced three feet (3’) on center. This will provide the required screening. Along this high screen area Bowhall Maple trees are planted between the arborvitae screen and sidewalk twenty-eight feet (28’) on center. Ground cover is shown covering the remainder of the landscape area along this high screen area.

***Subsection 4.176 (.03) Landscape Area and Locations***

B21. **Review Criteria:** “Not less than fifteen percent (15%) of the total lot area, shall be landscaped with vegetative plant materials. The ten percent (10%) parking area landscaping required by section 4.155.03(B)(1) is included in the fifteen percent (15%) total lot landscaping requirement. Landscaping shall be located in at least three separate and distinct areas of the lot, one of which must be in the contiguous frontage area. Planting areas shall be encouraged adjacent to structures. Landscaping shall be used to define, soften or screen the appearance of buildings and off-street parking areas. Materials to be installed shall achieve a balance between various plant forms, textures, and heights. The installation of native plant materials shall be used whenever practicable.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Consistent with the proposed Stage II Final Plan for the site, the proposed design of the site provides for more than the required amount of landscaping and landscaping in at least three separate and distinct areas, including the area along SW 95<sup>th</sup> Avenue and SW Boones Ferry Road. See Finding A43 of Request A. The planting plans,

sheet L2.0 of Exhibit B2, show landscape placed in areas that will define, soften, and screen the appearance of buildings and off-street parking areas.

***Subsection 4.176 (.04) Buffering and Screening***

B22. **Review Criteria:** “Additional to the standards of this subsection, the requirements of the Section 4.137.5 (Screening and Buffering Overlay Zone) shall also be applied, where applicable.

C. All exterior, roof and ground mounted, mechanical and utility equipment shall be screened from ground level off-site view from adjacent streets or properties.

D. All outdoor storage areas shall be screened from public view, unless visible storage has been approved for the site by the Development Review Board or Planning Director acting on a development permit.

E. In all cases other than for industrial uses in industrial zones, landscaping shall be designed to screen loading areas and docks, and truck parking.

F. In any zone any fence over six (6) feet high measured from soil surface at the outside of fenceline shall require Development Review Board approval.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The buildings are designed so architectural parapets screen roof mounted equipment. Mixed-solid waste and recycling storage areas are within screening enclosures. No additional outdoor storage areas are proposed.

***Subsection 4.176 (.05) Site Obscuring Fence or Planting***

B23. **Review Criterion:** “The use for which a sight-obscuring fence or planting is required shall not begin operation until the fence or planting is erected or in place and approved by the City. A temporary occupancy permit may be issued upon a posting of a bond or other security equal to one hundred ten percent (110%) of the cost of such fence or planting and its installation.”

**Finding:** This criterion will be satisfied by Condition of Approval PDB 6.

**Explanation of Finding:** The condition of approval will assure installation or that appropriate security is posted.

***Subsection 4.176 (.06) A. Plant Materials-Shrubs and Groundcover***

B24. **Review Criteria:** This subsection establishes plant material and planting requirements for shrubs and ground cover.

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDB 7.

**Explanation of Finding:** The condition of approval requires that the detailed requirements of this subsection are met.

***Subsection 4.176 (.06) B. Plant Materials-Trees***

B25. **Review Criteria:** This subsection establishes plant material requirements for trees.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The plants material requirements for trees will be met as follows:

- The applicant’s planting plan, sheet L2.0 of Exhibit B2, shows all trees as B&B (Balled and Burlapped)
- Sheet L3.0 of Exhibit B2 requires landscape materials to meet ANSI standards.
- The applicant’s planting plan lists tree sizes required by code.

***Subsection 4.176 (.06) D. Plant Materials-Street Trees***

B26. **Review Criteria:** This subsection establishes plant material requirements for street trees.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As shown in their planting plan, sheet L2.0 of Exhibit B2, the applicant proposes Bowhall Maple street trees (*Acer rubrum* “Bowhall”). The proposed trees are a cultivar of *Acer rubrum*, which is listed as a satisfactory street tree in this subsection. The trees are proposed to be planted at 3” caliper, the required size for arterial streets.

***Subsection 4.176 (.06) E. Types of Plant Species***

B27. **Review Criteria:** This subsection discusses use of existing landscaping or native vegetation, selection of plant materials, and prohibited plant materials.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has provided sufficient information showing the proposed landscape design meets the standards of this subsection. See sheet L2.0 of Exhibit B2.

***Subsection 4.176 (.06) G. Exceeding Plant Material Standards***

B28. **Review Criterion:** “Landscape materials that exceed the minimum standards of this Section are encouraged, provided that height and vision clearance requirements are met.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The selected landscape materials do not violate any height or vision clearance requirements.

***Subsection 4.176 (.07) Installation and Maintenance of Landscaping***

B29. **Review Criteria:** This subsection establishes installation and maintenance standards for landscaping.

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDB 8.

**Explanation of Finding:** The installation and maintenance standards are or will be met as follows:

- Plant materials are required to be installed to current industry standards and be properly staked to ensure survival
- Plants that die are required to be replaced in kind, within one growing season, unless appropriate substitute species are approved by the City.
- Sheet L1.0 of Exhibit B2 shows a permanent built-in irrigation system with an automatic controller satisfying the related standards of this subsection.

***Subsection 4.176 (.09) Landscape Plans***

B30. **Review Criterion:** “Landscape plans shall be submitted showing all existing and proposed landscape areas. Plans must be drawn to scale and show the type, installation size, number and placement of materials. Plans shall include a plant material list. Plants are to be identified by both

their scientific and common names. The condition of any existing plants and the proposed method of irrigation are also to be indicated.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Sheets L1.0, L2.0, and L3.0 of Exhibit B2 provide the required information.

#### ***Subsection 4.176 (.10) Completion of Landscaping***

B31. **Review Criterion:** “The installation of plant materials may be deferred for a period of time specified by the Board or Planning Director acting on an application, in order to avoid hot summer or cold winter periods, or in response to water shortages. In these cases, a temporary permit shall be issued, following the same procedures specified in subsection (.07)(C)(3), above, regarding temporary irrigation systems. No final Certificate of Occupancy shall be granted until an adequate bond or other security is posted for the completion of the landscaping, and the City is given written authorization to enter the property and install the required landscaping, in the event that the required landscaping has not been installed. The form of such written authorization shall be submitted to the City Attorney for review.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant has not requested to defer installation of plant materials.

#### ***Subsection 4.176 (.12) Mitigation and Restoration Plantings***

B32. **Review Criterion:** “A mitigation plan is to be approved by the City’s Development Review Board before the destruction, damage, or removal of any existing native plants.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Consistent with the proposed Stage II Final Plan, the proposed landscape design involves no removal of existing native plants requiring a mitigation plan pursuant to this subsection.

#### ***Other Standards***

##### ***Section 4.178 Sidewalk and Pathway Standards***

B33. **Review Criterion:** This section establishes standards for sidewalks and pathways.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The proposed design of the site provides for pedestrian pathways consistent with the proposed Stage II Final Plan and purpose of site design review. See Findings A28 and B2. The proposed landscape design includes trees that will grow or can be pruned to provide the necessary overhead clearance.

##### ***Section 4.179 Mixed Solid Waste and Recyclables Storage***

B34. **Review Criterion:** This section establishes standards for mixed solid waste and recyclables storage in new multi-family residential and non-residential buildings.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The design of the proposed solid waste and recyclables enclosures are consistent with the proposed Stage II Final Plan in relation to this section



and the location, design, and access standards for mixed solid waste and recycling areas. See Findings A49 and B7.

### **Outdoor Lighting**

#### ***Section 4.199.20 Applicability of Outdoor Lighting Standards***

B35. **Review Criterion:** This section states that the outdoor lighting ordinance is applicable to “Installation of new exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas” and “Major additions or modifications (as defined in this Section) to existing exterior lighting systems in public facility, commercial, industrial and multi-family housing projects with common areas.” In addition the exempt luminaires and lighting systems are listed.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Non-exempt new outdoor lighting proposed for the development site is being required to comply with the outdoor lighting ordinance.

#### ***Section 4.199.30 Outdoor Lighting Zones***

B36. **Review Criterion:** “The designated Lighting Zone as indicated on the Lighting Overlay Zone Map for a commercial, industrial, multi-family or public facility parcel or project shall determine the limitations for lighting systems and fixtures as specified in this Ordinance.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The development site is within LZ 2 and the proposed outdoor lighting systems are being reviewed under the standards of this lighting zone.

#### ***Subsection 4.199.40 (.01) A. Alternative Methods of Outdoor Lighting Compliance***

B37. **Review Criterion:** “All outdoor lighting shall comply with either the Prescriptive Option or the Performance Option below.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant has submitted information to comply with the performance option.

#### ***Subsection 4.199.40 (.01) C. Performance Option for Outdoor Lighting Compliance***

***“If the lighting is to comply with the Performance Option, the proposed lighting design shall be submitted by the applicant for approval by the City meeting all of the following:” Listed 1. through 3.***

#### ***Subsection 4.199.40 (.01) C. 1. Weighted Average of Direct Uplight Lumens Standard***

B38. **Review Criteria:** “The weighted average percentage of direct uplight lumens shall be less than the allowed amount per Table 9.”

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDB 10.

**Explanation of Finding:** As shown in the revised sheet SE1.0 provided with the applicants Response to Second Request for Additional Information and Revisions in the applicant’s notebook, Exhibit B1, the only luminaires that are not fully shielded are the landscape bollards. The luminaires are such that the weighted average percentage of direct

uplight lumens will be less than five percent (5%). Staff notes that Fixture U can be configured to emit both up and down lighting. A condition of approval limits these fixtures to down lighting.

***Subsection 4.199.40 (.01) C. 2. Maximum Light Level at Property Lines***

B39. **Review Criteria:** “The maximum light level at any property line shall be less than the values in Table 9, as evidenced by a complete photometric analysis including horizontal illuminance of the site and vertical illuminance on the plane facing the site up to the mounting height of the luminaire mounted highest above grade.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant’s sheet SE1.4 in Exhibit B4 show the information required by this subsection demonstrating compliance with Table 9. Compliance was checked across 95<sup>th</sup> Avenue and Boones Ferry Road as allowed by Exception 1 of this subsection.

***Subsection 4.199.40 (.01) C. 2. Maximum Light Level at Property Lines***

B40. **Review Criteria:** “Luminaires shall not be mounted so as to permit aiming or use in any way other than the manner maintaining the shielding classification required herein.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The mountings will be in a downward position. Condition of Approval PDB 10 helps ensure this

***Subsection 4.199.40 (.01) D. Outdoor Lighting Curfew***

B41. **Review Criterion:** “All prescriptive or performance based exterior lighting systems shall be controlled by automatic device(s) or system(s) that:” Listed 1. through 3.

**Finding:** This criterion is satisfied or will be satisfied by Condition of Approval PDB 9.

**Explanation of Finding:** As stated on page 53 of the applicant’s compliance narrative in their notebook, Exhibit B1, Carl’s Jr. is exempt from lighting curfew as a 24/7 operation. However, the other retail has not been identified for 24/7 operation or eligible for another exemption. A condition of approval requires lighting associated with this building and supporting parking shall be dimmed at 10:00 p.m. pursuant to Table 10.

***Subsection 4.199.50 Submittal Requirements***

B42. **Review Criteria:** “Applicants shall submit the following information as part of DRB review or administrative review of new commercial, industrial, multi-family or public facility projects:” Listed A. through F. “In addition to the above submittal requirements, Applicants using the Prescriptive Method shall submit the following information as part of the permit set plan review:

A. A site lighting plan (items 1 A - F, above) which indicates for each luminaire the 3 mounting height line to demonstrate compliance with the setback requirements. For luminaires mounted within 3 mounting heights of the property line the compliance exception or special shielding requirements shall be clearly indicated.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has submitted sufficient information to review the application.

**REQUEST C: DB12-0076 MASTER SIGN PLAN FOR BOONES FERRY POINTE**

*Subsection 4.031 (.01) M. and Subsection 4.156.02 (.03) Review Process*

C1. **Review Criteria:** These subsections establish that Master Sign Plans are reviewed by the Development Review Board.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The application qualifies as a Master Sign Plan and is being reviewed by the Development Review Board.

*Subsection 4.156.02 (.07) Master Sign Plans Generally*

C2. **Review Criteria:** “A Master Sign Plan is required for non-residential developments with three (3) or more tenants. In creating a Master Sign Plan thought should be given to needs of initial tenants as well as the potential needs of future tenants.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposal involves potentially three (3) or more tenants and therefore a Master Sign Plan is required. As demonstrated in the Finding C10. needs of initial tenants as well as the potential needs of future tenants have been considered.

*Subsection 4.156.02 (.07) A. Master Sign Plan Submission Requirements*

C3. **Review Criteria:** This subsection identifies submission requirements for Master Sign Plans, which includes the submission requirements for Class II and Class III sign permits plus additional requirements specific to Master Sign Plans.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As indicated in the table below the Applicant has either satisfied the submission requirements, or has been granted a waiver under Subsection 4.156.02 (.10).

Requirement	Submitted	Waiver Granted		Condition of Approval	Not Applicable	Additional findings/notes
		Info Already Available to City	Info Not Necessary for Review			
Completed Application Form	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Sign Drawings or Descriptions	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Documentation of	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

<b>Building/Tenant Space Lengths</b>						
<b>Drawings of Sign Placement of Building Facades</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Project Narrative</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Information on Any Requested Waivers or Variances</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Written Explanation of the Flexibility of the Master Sign Plan for Different Potential Tenant Space Configurations</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Written Explanation of the Extent to which Different Sign Designs are Allowed</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Written Explanation of How Sign Plan Provides for Consistent and Compatible Sign Design Throughout the Development</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

**Subsection 4.156.02 (.07) B. Master Sign Plan Review Criteria**

***Subsections 4.156.02 (.07) B. and (.05) E.- Class II Sign Permit Review Criteria also Applicable to Master Sign Plans***

***Subsection 4.156.02 (.05) E. Class II Sign Permit Review Criteria: Generally and Site Design Review***

C4. **Review Criteria:** “Class II Sign Permits shall satisfy the sign regulations for the applicable zoning district and the Site Design Review Criteria in Sections 4.400 through 4.421,”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As indicated in Findings C20 through C44 these criteria are met.

**Subsection 4.156.02 (.05) E. 1. Class II Sign Permit Review Criteria: Compatibility with Zone**

C5. **Review Criteria:** “The proposed signage is compatible with developments or uses permitted in the zone in terms of design, materials used, color schemes, proportionality, and location, so that it does not interfere with or detract from the visual appearance of surrounding development;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed signage is typical of and compatible with development within the PDC zones. This includes a design and colors reflecting corporate identity, illuminated channel letters and logo on a raceway, freestanding cabinet signs, and individual non-illuminated letters on an architectural wall. The placement of signs on buildings is in recognizable sign bands, and proportional to the building facades. No

evidence exists nor has testimony been received that the subject signs would detract from the visual appearance of the surrounding development.

***Subsection 4.156.02 (.05) E. 2. Class II Sign Permit Review Criteria: Nuisance and Impact on Surrounding Properties***

- C6. **Review Criteria:** “The proposed signage will not create a nuisance or result in a significant reduction in the value or usefulness of surrounding development;”  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** There is no evidence and no testimony has been received that the subject signs would create a nuisance or negatively impact the value of surrounding properties.

***Subsection 4.156.02 (.05) E. 3. Class II Sign Permit Review Criteria: Items for Special Attention***

- C7. **Review Criteria:** “Special attention is paid to the interface between signs and other site elements including building architecture and landscaping, including trees.”  
**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDC 2.  
**Explanation of Finding:** The building signs are within an architectural feature identifiable as a sign band with a buffer within the sign band around the sign, which demonstrates consideration of the interface between the signs and building architecture. A condition of approval requires specific landscape treatment typical of landscaping next to structure for the area around the foundation of the freestanding sign along Boones Ferry Rd. No sign-tree conflicts have been noted.

***Subsection 4.156.02 (.07) B. 1. Consistent and Compatible Design***

- C8. **Review Criteria:** “The Master Sign Plan provides for consistent and compatible design of signs throughout the development;”  
**Finding:** These criteria are satisfied or will be satisfied by Conditions of Approval PDC 3, and PDC 4.  
**Explanation of Finding:** Building signs are limited to internally illuminated logo cabinet signs or internally illuminated individual channel letters. Logo cabinets and channel letters are generally compatible on the same building and are currently mixed on a variety of commercial buildings in the City, including Argyle Square. The Carl’s Jr. sign drawings in the Master Sign Plan section of the applicant’s notebook, Exhibit B1., shows the sign mounted without an exposed raceway. Use of raceways is not defined for the multi-tenant building. If exposed raceways are used a condition of approval requires they be painted to match the building. The applicant must indicate whether exposed raceways will be used on the multi-tenant building, and if so, all illuminated wall signs on the building will similarly be mounted on raceways. Also, there is no indication of the color of returns for the backlit signs. Master sign plans often require a consistent color of return to be used. The use of a consistent color of returns, as required by a condition of approval, will help ensure a consistent and compatible design between signs.

***Subsection 4.156.02 (.07) B. 2. Future Needs***

C9. **Review Criteria:** “The Master Sign Plan considers future needs, including potential different configurations of tenant spaces and different sign designs, if allowed.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The master sign plan allows for sufficient flexibility to provide different design needs and preferences of individual tenants.

**Section 4.156.03 Sign Measurement**

***Subsection 4.156.03 (.01) A. Measurement of Cabinet Signs and Similar***

C10. **Review Criteria:** “The area for signs enclosed by cabinet, frame, or other background (including lighted surface) not otherwise part of the architecture of a building or structure shall be the area of a shape drawn around the outer dimension of the cabinet, frame, or background.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed signs have been measured consistent with this subsection.

***Subsection 4.156.03 (.01) B. Measurement of Individual Element Signs***

C11. **Review Criteria:** “The area for signs constructed of individual elements (letters, figures, etc.) attached to a building wall or similar surface or structure shall be the summed area of up to three squares, rectangles, circles, or triangles drawn around all sign elements.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed signs have been measured consistent with this subsection.

***Subsection 4.156.03 (.02) A. Measurement of Sign Height Above Ground***

C12. **Review Criteria:** “The height above ground of a freestanding or ground-mounted sign is measured from the average grade directly below the sign to the highest point of the sign or sign structure except as follows:” Listed 1.-2.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed signs have been measured consistent with this subsection.

***Subsection 4.156.03 (.03) A.-B. Measurement of Sign Height and Length***

C13. **Review Criteria:** “Height of a sign is the vertical distance between the lowest and highest points of the sign.”

Length of a sign is the horizontal distance between the furthest left and right points of the sign.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed signs have been measured consistent with this subsection.

***Subsection 4.156.05 (.01) C. Signs Exempt From Sign Permit Requirements: Flags***

C14. **Review Criteria:** “Flags displayed from permanently-located freestanding on wall-mounted flagpoles that are designed to allow raising and lowering of flags. One site may have up to two (2) exempt flags; no exempt flag may be more than thirty (30) feet in height”

**Finding:** These criteria will be satisfied by Condition of Approval PDC 8.

**Explanation of Finding:** The condition ensures the flag pole shown on the site plan, sheet A1.0 of the applicant’s plan set, Exhibit B2, does not exceed the allowed height or number of signs exempted from sign permit requirements by this subsection.

**Subsection 4.156.08 (.01) Freestanding and Ground Mounted Signs in the PDC, PDI, and PF Zones**

***Subsection 4.156.08 (.01) A. General Allowance***

C15. **Review Criteria:** “One freestanding or ground mounted sign is allowed for the first two-hundred (200) linear feet of site frontage. One additional freestanding or ground mounted sign may be added for through and corner lots having at least two-hundred (200) feet of frontage on one street or right-of-way and one-hundred (100) feet on the other street or right-of-way.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The Carl’s Jr. site has not been included in a previous Master Sign Plan, and is considered a separate site for the purpose of this allowance. With at least 200 feet of frontage on 95<sup>th</sup> Avenue and greater than 100 feet of frontage on Boones Ferry Road the site is allowed two signs. Two are proposed, one on Boones Ferry Road, and one collocated with the existing Chevron sign on 95<sup>th</sup> Avenue.

***Subsection 4.156.08 (.01) B. Allowed Height***

C16. **Review Criteria:** “The allowed height above ground of a freestanding or ground mounted sign is twenty (20) feet except as noted in 1-2 below.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The sign along Boones Ferry north of the multi-tenant retail is proposed to be twenty feet (20’), which is the allowed height. The 95<sup>th</sup> avenue sign will also be mounted to be twenty feet (20’) high. The “Boones Ferry Pointe” monument sign is much less than the allowed twenty feet (20’).

***Subsection 4.156.08 (.01) C. Allowed Area***

C17. **Review Criteria:** This subsection identifies the allowed area for freestanding signs.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The following table shows the allowed area for each of the freestanding signs.

Building (# of Tenants)	Building Square Feet	Allowed Sign Area (sf)
Base Area		32
Carl's Jr. (1)	2867 (rounded down to 2000)	9
Retail Building (3)	3150 (rounded down to 3000)	18
Total		59

Note: the applicant's calculations of allowed area in the response to incomplete application and project narrative are not accurate.

*Boones Ferry Road Sign North of Multi-tenant Commercial:* Staff notes that the area for this sign is incorrectly represented in all tables listing its area in the applicant's submitted material. The correct calculation is as follows based on the revised sign drawing in the applicant's Response to Second Request for Additional Information and Revisions in the applicant's notebook, Exhibit B1:

Carl's Jr. cabinet: 22.48 square feet  
 4 additional cabinets at 9 sf each: 36 square feet  
 Total Area: 58.48 square feet

The proposed 58.48 square feet is less than the 59 square feet allowed.

*95<sup>th</sup> Avenue "Collocated" Sign and Monument Sign:* A thirty-six (36) square feet sign panel is being collocated on an existing pylon sign opposite the existing Chevron sign. The collocation will reduce clutter and provide for efficient placement of signs for multiple developments using a shared driveway. Thirteen (13) square feet of the allowance for a second sign is being used for a planned development sign on the north end of the site pursuant to Subsection 4.156 (.03) B. This leaves ten (10) square feet of the allowance unused.

***Subsection 4.156.08 (.01) D. Pole or Sign Support Placement***

C18. **Review Criterion:** "Pole or sign support placement shall be installed in a full vertical position."

**Finding:** This criterion is satisfied.

**Explanation of Finding:** All poles and sign supports are proposed to be in a full vertical position.

***Subsection 4.156.08 (.01) E. Extending Over Right-of-Way, Parking, and Maneuvering Areas***

C19. **Review Criterion:** "Freestanding and ground mounted signs shall not extend into or above public rights-of-way, parking areas, or vehicle maneuvering areas."

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Freestanding signs are not proposed to extend into or above the listed areas.



***Subsection 4.156.08 (.01) G. Design of Freestanding Signs to Match or Complement Design of Buildings***

C20. **Review Criterion:** “Freestanding and ground mounted signs shall be designed to match or complement the architectural design of buildings on the site.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The freestanding sign along Boones Ferry Road north of the multi-tenant commercial building has a base of brick matching the brick on the buildings. Otherwise it is of a basic cabinet nature reflective of the nature of the proposed businesses on the site and providing a clean look. The sign along 95<sup>th</sup> Avenue is a simple monopole support with attached cabinet signs. The sign also creates a clean look typical of commercial areas. Rather than designing sign structures to match the buildings, the applicant elected to have minimal structure to highlight the businesses and not distract for the architectural design of the site.

***Subsection 4.156.08 (.01) H. Width vs. Height of Signs Over 8 Feet***

C21. **Review Criterion:** “For freestanding and ground mounted signs greater than eight (8) feet in height, the width of the sign shall not exceed the height.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Both freestanding signs are greater than eight (8) feet in height, but both are much less in width than they are in height.

***Subsection 4.156.08 (.01) J. Sign Setback***

C22. **Review Criteria:** “Freestanding and ground mounted signs shall be no further than fifteen (15) feet from the property line and no closer than two (2) feet from a sidewalk or other hard surface in the public right-of-way.”

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDC 5.

**Explanation of Finding:** The planned development monument sign is approximately 13.5 feet from the property line and is well back from the public sidewalk. The final location of the freestanding sign along Boones Ferry has not yet been determined. The condition of approval requires the placement to meet the sign setback requirements defined in this subsection. The 95<sup>th</sup> Avenue sign is setback approximately 12.5 feet from the sidewalk and is within fifteen (15) feet of the property line. The existing Chevron sign collocated on the same pylon is approximately 6.5 feet from the sidewalk.

***Subsection 4.156.08 (.01) K. Address Requirement***

C23. **Review Criteria:** “Except for those signs fronting Interstate 5, freestanding and ground mounted signs shall include the address number of associated buildings unless otherwise approved in writing by the City and the Fire District.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Addresses are provided on the freestanding sign adjacent to the shared driveway. As there is no associated vehicle access and the addresses are otherwise visible, the freestanding sign on Boones Ferry Road and monument sign are not being required to have addresses by the City or the Fire District.

***Subsection 4.156.08 (.01) L. Design of Sign Based on Initial Tenant Configuration and Size***

C24. **Review Criteria:** “When a sign is designed based on the number of planned tenant spaces it shall remain a legal, conforming sign regardless of the change in the number of tenants or configuration of tenant spaces.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The master sign plan is proposed based on the number of planned tenants, and it is understood the sign plan will be valid regardless on the number of future tenants.

**Subsection 4.156.08 (.02) Building Signs in the PDC, PDI, and PF Zones**

***Subsection 4.156.08 (.02) A. Sign Eligible Facades***

C25. **Review Criteria:** “Building signs are allowed on a facade of a tenant space or single tenant building when one or more of the following criteria are met:

1. The facade has one or more entrances open to the general public;
2. The facade faces a lot line with frontage on a street or private drive with a cross section similar to a public street, and no other buildings on the same lot obstruct the view of the building facade from the street or private drive; or
3. The facade is adjacent to the primary parking area for the building or tenant.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All facades of the two proposed buildings are sign eligible for the reasons listed below:

*Carl’s Jr. Building:* North: public entrance, primary parking area  
 East: public entrance, primary parking area  
 South: faces private drive  
 West: faces public street

*Multi-tenant Commercial Building:* North: faces public street  
 East: faces public street  
 South: public entrance, primary parking area  
 West: faces public street

***Subsection 4.156.08 (.02) B. Building Sign Area Allowed***

C26. **Review Criteria:** This subsection includes a table identifying the sign area allowed for facades based on the linear length of the façade. Exception are listed 2. through 5.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The allowed area for each facade is as follows:

<b>Carl’s Jr. Building</b>			
<b>Façade</b>	<b>Linear Length</b>	<b>Sign Area Allowed</b>	<b>Proposed Max</b>
North	41 feet	36 sf	36 sf
East	72 feet	36 sf	36 sf
South	41 feet	36 sf	36 sf
West	72 feet	36 sf	36 sf

The applicant proposes a maximum of thirty-six (36) square feet on each façade. However, smaller signs can be installed at the tenant’s request. Sheet A4.0 and A5.0 in the applicant’s plan set, Exhibit B2, show nine foot by 4 foot areas on each façade as the identified sign bands. These sheets say these sign locations are conceptual. However, page 9 of the Response to Incomplete Application in the applicant’s notebook, Exhibit B1, identifies these as the proposed sign locations. For the purposes of the Master Sign Plan these boxes identify the sign band, or allowed sign placement area, for each façade. Signs may be of a different shape that extend beyond the boxes, but should be centered at the center of the box and maintain proportional and recognizable spacing from the edges of the architectural feature.

<b>Multi-tenant Commercial Building</b>			
<b>Façade</b>	<b>Linear Length</b>	<b>Sign Area Allowed</b>	<b>Proposed Max</b>
<b>North</b>			
Tenant Space A (western most)	22.5 feet	24 sf	24 sf
Tenant Space B (middle)	25 feet	32 sf	24 sf
Tenant Space C (eastern most)	22.5 feet	24 sf	24 sf
Space B combined with A or C as single tenant space	47.5 feet	36 sf	32 sf
All spaces combined as single tenant space	70 feet	36 sf	36 sf
<b>East</b>	48 feet	36 sf	36 sf
<b>South</b>			
Tenant Space A (western most)	22.5 feet	24 sf	24 sf
Tenant Space B (middle)	25 feet	32 sf	24 sf
Tenant Space C (eastern most)	22.5 feet	24 sf	24 sf
Space B combined with A or C as single tenant space	47.5 feet	36 sf	32 sf
All spaces combined as single tenant space	70 feet	36 sf	36 sf
<b>West</b>	48 feet	36 sf	36 sf

The applicant proposes a maximum of thirty-six (36) square feet on the east and west façade. On the north and south façade the applicant proposes each of the three tenant spaces are allowed a twenty-four (24) square sign. However, if the center tenant space is combined with either of the end tenant spaces, then the center sign area can be increased to thirty-two (32) square feet. If all three tenant spaces are occupied by the same tenant they can elect to put a single thirty-six (36) square foot sign in the center sign area. See page 10 of the Response to Incomplete Application in the applicant’s notebook, Exhibit B1. Sheets AC2.0 and AC2.1 in the applicant’s plan set, Exhibit B2, show sign bands for each of the signs. These sheets say they are conceptual and typical. However, for the purposes of the Master Sign Plan these boxes identify the sign band, or allowed sign placement area, for each façade. The height is understood to be the maximum due to the requirement that a recognizable space is maintained between upper and lower edges of the sign band. However, signs could be longer as long as they remain similarly centered of the window or entry. If the center sign areas are increased to 32 or 36 square feet due to combing of tenant

spaces is it understood the signs will continue to be centered above the canopies in the brick area of the façade.

***Subsection 4.156.08 (.02) B. 6. Calculating Linear Length to Determine Sign Area Allowed.***

C27. **Review Criteria:** “For facades of a single tenant building the length the facade measured at the building line, except as noted in a. and b. below. For multi-tenant buildings the width of the façade of the tenant space shall be measured from the centerline of the party walls or the outer extent of the exterior wall at the building line, as applicable, except as noted in a. and b. below. Applicants shall provide the dimensions needed to calculate the length. Each tenant space or single occupant building shall not be considered to have more than five (5) total facades.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has supplied the required measurements used to determine linear lengths according to this subsection.

***Subsection 4.156.08 (.02) C. Building Sign Length Allowed***

C28. **Review Criterion:** “The length of individual tenant signs shall not exceed seventy-five (75) percent of the length of the facade of the tenant space.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** None of the proposed sign bands shown in sheets A4.0, A5.0, AC2.0, and AC2.1 of the applicant’s plan set, Exhibit B2, exceed seventy-five (75) percent of the length of the façade.

***Subsection 4.156.08 (.02) D. Building Sign Height Allowed***

C29. **Review Criteria:** “The height of building signs shall be within a definable sign band, fascia, or architectural feature and allow a definable space between the sign and the top and bottom of the sign band, fascia, or architectural feature.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All of the proposed sign bands shown in sheets A4.0, A5.0, AC2.0, and AC2.1 of the applicant’s plan set are within a definable architectural feature and have a definable space between the sign and the top and bottom of the architectural feature.

***Subsection 4.156.08 (.02) E. Building Sign Types Allowed***

C30. **Review Criterion:** “Types of signs permitted on buildings include wall flat, fascia, projecting, blade, marquee and awning signs. Roof-top signs are prohibited.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** All the proposed buildings signs are wall flat, which is an allowable type.

***Subsection 4.156.08 (.03) A. Additional Signs: Directional Signs***

C31. **Review Criteria:** “Notwithstanding the signs allowed based on the site in (.01) and (.02) above, the following signs may be permitted, subject to standards and conditions in this Code:” “In addition to exempt directional signs allowed under Subsection 4.156.05 (.02) C. freestanding or

ground mounted directional signs six (6) square feet or less in area and four (4) feet or less in height:

1. The signs shall be designed to match or complement the architectural design of buildings on the site;
2. The signs shall only be placed at the intersection of internal circulation drives; and
3. No more than one (1) sign shall be placed per intersection corner with no more than two (2) signs per intersection.”

**Finding:** These criteria are satisfied or will be satisfied by Condition of Approval PDC 7.

**Explanation of Finding:** Two (2) illuminated double faced directional signs are proposed as part of the Master Sign Plan. The signs are identified as Signs L and M in the Master Sign Plan and can be found on page 12 of the Master Sign Plan section of the applicant’s notebook, Exhibit B1. Each sign is six (6) square feet. The signs are shown at 4’1” tall. Condition of Approval PDC 8 limits the height to the allowed four (4) feet. The signs match the design of other signs on the property and complement the architecture of the building similarly. The signs are placed at the intersection of internal circulation drives, and only one sign is placed per intersection.

#### ***Subsection 4.156.08 (.03) A. Additional Signs: Planned Development Signs***

- C32. **Review Criteria:** “Notwithstanding the signs allowed based on the site in (.01) and (.02) above, the following signs may be permitted, subject to standards and conditions in this Code:” “Up to thirty (32) square feet of the allowed sign area for freestanding signs in a planned development may be used for a separate on-site monument sign or off-site monument sign on an adjacent parcel identifying the Planned Development project.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Thirteen (13) square feet of the allowed area for freestanding signs is being used for an on-site monument sign identifying the “Boones Ferry Pointe” development. See also Finding C16 above.

#### **Site Design Review**

#### ***Subsections 4.400 (.01) and 4.421 (.03) Excessive Uniformity, Inappropriateness of Design, Etc.***

- C33. **Review Criteria:** “The Board shall also be guided by the purpose of Section 4.400, and such objectives shall serve as additional criteria and standards.” “Excessive uniformity, inappropriateness or poor design of the exterior appearance of structures and signs and the lack of proper attention to site development and landscaping in the business, commercial, industrial and certain residential areas of the City hinders the harmonious development of the City, impairs the desirability of residence, investment or occupation in the City, limits the opportunity to attain the optimum use in value and improvements, adversely affects the stability and value of property, produces degeneration of property in such areas and with attendant deterioration of conditions affecting the peace, health and welfare, and destroys a proper relationship between the taxable value of property and the cost of municipal services therefor.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:**

*Excessive Uniformity:* The sign plan allows for a variety of sign shapes, fonts, and colors chosen by different tenants so as to avoid excessive uniformity.

*Inappropriate or Poor Design of Signs:* Signs are typical of the type of development proposed found to be appropriate throughout the City. As issuance of the Class I Sign Permits consistent with the Master Sign Plan the City will ensure quality design of signs.

*Lack of Proper Attention to Site Development:* The appropriate professional services have been used to design the site incorporating unique features of the site including site size and shape, and available access, demonstrating appropriate attention being given to site development and sign placement.

*Lack of Proper Attention to Landscaping:* No specific landscaping is proposed around the proposed freestanding sign. Condition of Approval PDC 2 ensures proper attention is paid . See Finding C7.

#### ***Subsections 4.400 (.02) and 4.421 (.03) Purposes of Objectives of Site Design Review***

- C34. **Review Criteria:** “The Board shall also be guided by the purpose of Section 4.400, and such objectives shall serve as additional criteria and standards.” “The City Council declares that the purposes and objectives of site development requirements and the site design review procedure are to:” Listed A through J. including D. which reads “Conserve the City's natural beauty and visual character and charm by assuring that structures, signs and other improvements are properly related to their sites, and to surrounding sites and structures, with due regard to the aesthetic qualities of the natural terrain and landscaping, and that proper attention is given to exterior appearances of structures, signs and other improvements;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** It is staff’s professional opinion that the signs comply with the purposes and objectives of site design review, especially objective D. which specifically mentions signs. The proposed signs are of a scale and design appropriately related to the subject site and the appropriate amount of attention has been given to visual appearance.

#### ***Subsection 4.421 (.01) Site Design Review-Design Standards***

- C35. **Review Criteria:** This subsection lists the design standards for Site Design Review. Listed A through G. Only F. is applicable to this application, which reads, “Advertising Features. In addition to the requirements of the City's sign regulations, the following criteria should be included: the size, location, design, color, texture, lighting and materials of all exterior signs and outdoor advertising structures or features shall not detract from the design of proposed buildings and structures and the surrounding properties.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** There is no indication that the size, location, design, color, texture, lighting or material of the proposed signs would detract from the design of the building and the surrounding properties.

#### ***Subsection 4.421 (.02) Applicability of Design Standards to Signs***

- C36. **Review Criteria:** “The standards of review outlined in Sections (a) through (g) above shall also apply to all accessory buildings, structures, exterior signs and other site features, however related to the major buildings or structures.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Design standards have been applied to exterior signs, as applicable, see Finding C34 above.

***Subsection 4.421 (.05) Site Design Review-Conditions of Approval***

C37. **Review Criterion:** “The Board may attach certain development or use conditions in granting an approval that are determined necessary to insure the proper and efficient functioning of the development, consistent with the intent of the Comprehensive Plan, allowed densities and the requirements of this Code.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** No additional conditions of approval are recommended to ensure the proper and efficient functioning of the development.

***Subsection 4.421 (.06) Color or Materials Requirements***

C38. **Review Criterion:** “The Board or Planning Director may require that certain paints or colors of materials be used in approving applications. Such requirements shall only be applied when site development or other land use applications are being reviewed by the City.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Staff does not recommend any additional requirements for materials or colors for the proposed signs.

***Section 4.440 Site Design Review-Procedures***

C39. **Review Criteria:** “A prospective applicant for a building or other permit who is subject to site design review shall submit to the Planning Department, in addition to the requirements of Section 4.035, the following:” Listed A through F.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has submitted a sign plan as required by this section.



RECEIVED  
FEB 25 2013

February 25 2013

Daniel Pauley, Associate Planner and  
Kerry Rappold, Natural Resources Manager  
City of Wilsonville  
29799 SW Town Center Loop E.  
Wilsonville, OR 97070

RE: Concern of Trash Enclosure Design, Boones Ferry Pointe, DB12-0074-DB12-0076.

Dear Daniel & Kerry:

Garry LaPoint, one of the applicants, has raised a concern regarding the design of his trash enclosure (relocated) as shown in the submitted plans, see attached. He has two concerns:

1. The enclosure need to be wider to accommodate the 8 yard box he is currently using. The minimum width (inside dimension) between the gate posts needs to be 20 feet.

The Chevron site currently uses two 8 yard containers for their waste and recycling.

- The trash container is 8' wide x 6' deep, and 7.5' tall;
- The recycling container is 8' wide x 6.5' deep, and 6.5' tall; and
- Both containers have covers

While Allied/Republic Services provided a letter approving the revised trash enclosure designs, they did not actually check their records to confirm the size of container currently in use at the Chevron Station. As submitted, and approved by the hauler, the design anticipated two 4 yard boxes. The outside wall dimension is 19' 8" wide, but the inside measurement is only 17' 5". The depth is shown as 10' 11", but inside is only 9' 10".

The dimensions of the enclosure submitted are shown as outside to outside, rather than inside dimensions. Consequently the layout will not allow for any access behind the box, should items be spilled. In addition, the use of the 8 yard box limits net space for the recycling materials.



City of Wilsonville  
EXHIBIT B5 DB12-0074 et al



The 8 yard box is necessary because of the high volume of waste materials from the C-Store. Even with the 8 yard box, their volume requires pick-up, at least, twice a week.

Therefore the enclosure needs to be widened by 2' 2". The depth is ok.

Current landscape coverage is 29%, so the minor reduction in landscaping required to accommodate the added enclosure width is not a problem. Only about 24 square feet of landscaping will be lost.

It is our understanding this width correction is simple and will be covered in the staff report, including a Condition of Approval for staff to approve the final design.

2. Given the size and height of the height of the boxes, which have covers, the roof structure is too low. As submitted the roof structure has 9' 1-1/2" vertical clearance under cross beam, see also attached letter from Republic Services.

With the roof structure, the cover on the box cannot be opened fully when inside the enclosure. In addition, the structure will hamper the hauler's truck access making it difficult, if not impossible, to left the box in and out of the enclosure. It is important to recognize that the 8 yard box does not have wheels.

The roof structure creates the following problems:

- Limits hauler truck access, particularly the ability to left the box in and out of the enclosure. As noted, the box used for the Chevron Station does not have wheels, so it must be lifted or dragged in and out. Obviously dragging is not desirable because of pavement damage.
- - In order to provide adequate vertical clearance the roof structure will need to be, at least, 14 feet under the cross beam. This would result in the high point of the roof at 16' 3/4".
- Forces hauler truck to make multiple backing maneuvers, which are the most dangerous move the truck makes. The safety concern for the truck at this location is amplified by the high level of customer vehicle and pedestrian activity.

The trash enclosure is at a point where site activity, both vehicle and pedestrian movements are very high, with a lot of congestion and vehicles moving is different directions. Thus this location is very dangerous for any backing maneuvers required by the hauler truck.

- With this development, the existing Chevron enclosure is being relocated from its current location north of the pump islands. Allied/Republic has been serving the Chevron site for many years and has had problems accessing the structure without conflicting with pump island traffic. Therefore the existing facility will be replaced, thereby improving access.
- In developing the site plan for the New Development (Carl's Jr & Retail) it was difficult to find an appropriate location for the combined Chevron/Retail trash enclosures. While not ideal, given high circulation volumes, the proposed location west of the fuel islands was found to be the most accessible for the hauler. There simply is no other safe and functional location available.


We understand that the roof is not required under the Planning (DRB) Chapter 4 standards, but rather is under Chapter 8. For this reason it would appear that any adjustment to the standard would be an Administrative Decision, rather than a DRB issue.

1. Pursuant to Subsection 8.210 (9) Wilsonville Code and Subsections 4.430 (.04) B. information on required covers for mixed solid waste and recycling storage areas. The City's stormwater regulations require the waste and recycling enclosures be covered. Wilsonville Code Subsection 8.210 (9) reads, "Outside storage areas for grease, oil, waste products, recycling, garbage, and other sources of contaminants shall be a covered enclosure adequately sized to allow all containers to be accessible. No drainage is allowed to enter the storm sewer system." Subsection 4.430 (.04) B. requires vertical clearance to be at least eight feet (8').

Mr. LaPoint is requesting a Waiver to the roof requirement for the reasons stated herein.

We look forward to working with you on this project. Thank you for your cooperation and assistance. If you have any questions or need additional information, please contact me immediately, so we can respond quickly.

Sincerely,  
SFA Design Group

  
Ben Altman  
Senior Planner/Project Manager

cc: Josh Veentjer  
Garry LaPoint





February 25, 2013

Dan Pauly, AICP  
Associate Planner  
City of Wilsonville Planning Department  
29799 SW Town Center Loop East  
Wilsonville, OR 97070

Dear Dan:

RE: Boones Ferry Pointe – Trash Enclosure

I have been asked to share with you my concern regarding constructing a trash enclosure with a roof at the Chevron Station at Boones Ferry Pointe. It is my understanding a City Code provision requires a roof on trash enclosures. This code provision is administered by the city's Natural Resources and Public Works group. *Republic Services of Clackamas/Washington County* is the waste collection service provider for Chevron under an exclusive franchise issued by the City of Wilsonville.

Chevron has two 8-yard containers at this site for waste and recycling disposal. These containers have lids to prevent rain water entering the container. The container used for waste disposal is 8-feet wide, 7.5-feet tall and 6-feet deep. The container used for recycling is 8-feet wide, 6.5-feet tall and 6.5-feet deep. A trash enclosure for these two containers needs to be at least 20-feet wide and 10-feet deep.

The containers are too heavy to manually push-out of the enclosure to empty into the collection vehicle. As a result, our collection vehicle operator must stab and lift a container off the ground inside the enclosure by two-to-five feet. The collection vehicle then backs 10 to 15 feet away from the front of the enclosure, lifts the container overhead to empty the container into the collection vehicle and then lowers the container and moves it back inside the enclosure. Given the height of our collection vehicle plus the height of the containers, our vehicle operator needs at least 20-feet of unobstructed air space above the collection vehicle to safely empty each container.

My biggest concern regarding the lifting of containers to dispose of materials generated by commercial customers are overhead obstacles. Low hanging wires, tree branches, poles and banners are just a few obstacles that we need to avoid. Our company considers roofs over trash enclosure an "obstacle" we want to avoid which is the reason we have lids on the containers we use with commercial customers, such as Chevron. It is our opinion roofs over trash enclosures provide little or no margin for human error when our collection vehicle operator is lifting a container. A small error as our operator lifts a container could significantly damage an enclosure's roof and cost my company several thousands in roof, and possibly collection vehicle, repair expenses

I try to work closely with architects and planners on all new developments so trash enclosures are appropriately designed and placed in safe locations for service. The design of a trash enclosure should allow room for movement around all sides of the containers for loading, leveling and clean-up of the ground. Enclosed are the specifications our company uses for trash enclosures.

Sincerely,

Frank Lonergan, Operations Manager  
503-404-2135

cc: Ben Altman

---

10295 SW Ridder Road  
Wilsonville, OR 97070  
503-570-0626 • Fax 503-570-0523  
republicservices.com



**REGISTERED ARCHITECT**  
 CRAIG B. ANDERSON  
 407 S. 4TH ST.  
 SEATTLE, WA 98104  
 PHONE: 206-468-9800  
 FAX: 206-468-9801

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DATE: 10-2-2013  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 DATE: 10-2-2013

NO DATE  
 DATE: 10-2-2013

ARCH. NO. 1198  
 SCALE: AS SHOWN  
 SHEET

**ARCHITECTURAL SITE PLAN**

BOONES FERRY POINTE  
 5W 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

PROJECT

CLIENT

Wilsonville Devco LLC  
 c/o Integrated Investment Partners  
 PO Box 916  
 Portland, OR 97207  
 (503) 688-9600

CB ANDERSON ARCHITECTS P.C.  
 7209 Greenwood Avenue N  
 Seattle, Washington 98103  
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 Fax: 206-762-2624

**ARCHITECTS**

**CB ANDERSON ARCHITECTS P.C.**

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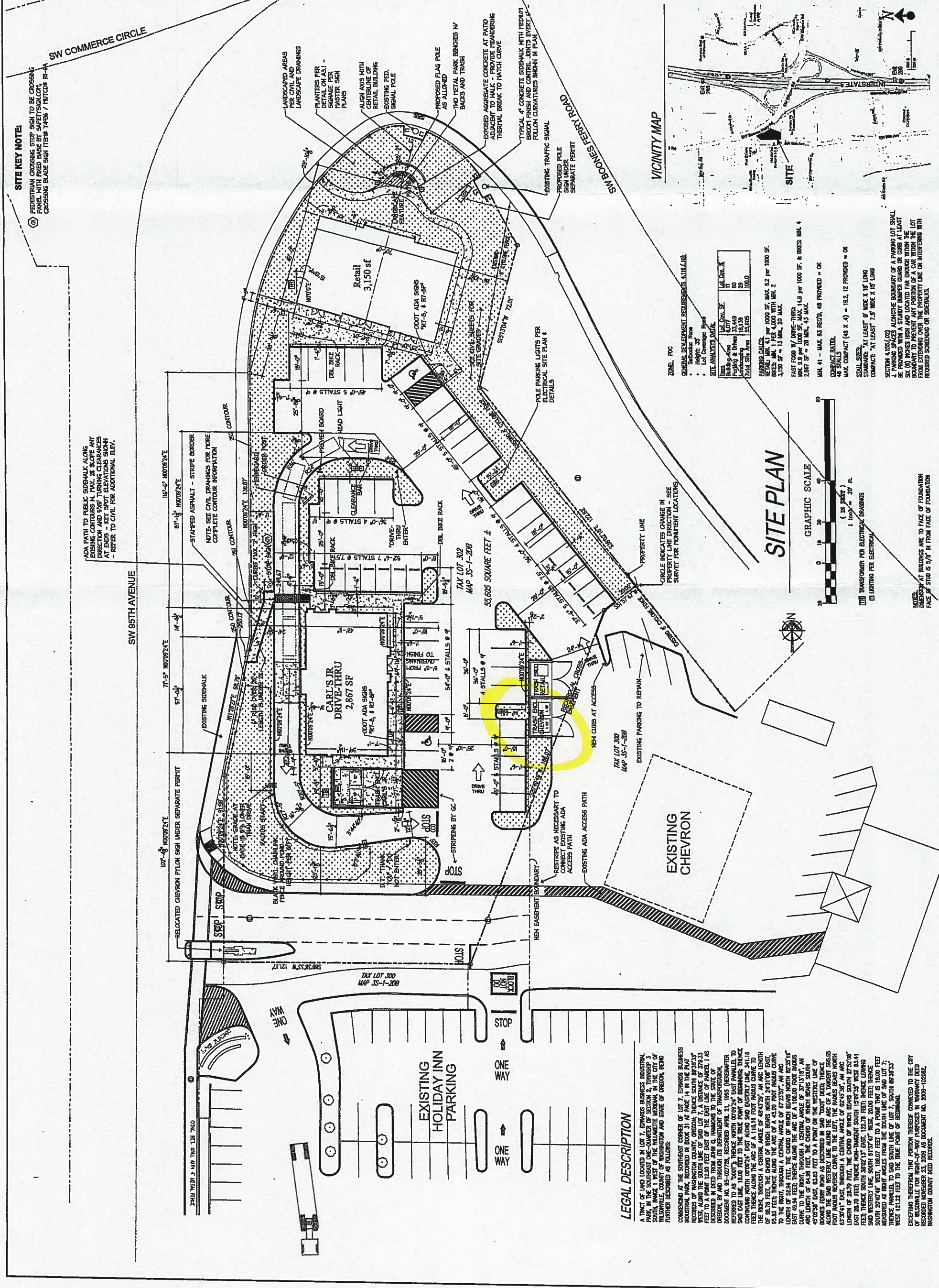
BOONES FERRY POINTE  
 5W 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

PROJECT

CLIENT

Wilsonville Devco LLC  
 c/o Integrated Investment Partners  
 PO Box 916  
 Portland, OR 97207  
 (503) 688-9600

KGK Foods Conversion Based on KPE Prototype Version 22-14



**SITE KEY NOTE**

PEDESTRIAN CROSSING STOP SIGN TO BE CROSSING PANEL WITH FROD BASE BY SAFETY SIGN, CROSSING BLADE SIGN ITEM# 14766 / MUTCD# R1-4

ADA PATH TO PUBLIC SIDEWALK ALONG EXISTING CONTOUR. MAX. 2% SLOPE ANY DIRECTION AND 5% TURNING CLEARANCES AT ENDS. SET POINT ELEVATION AND FINISH ELEVATION - REFER TO CIVIL FOR ADDITIONAL ELEV.

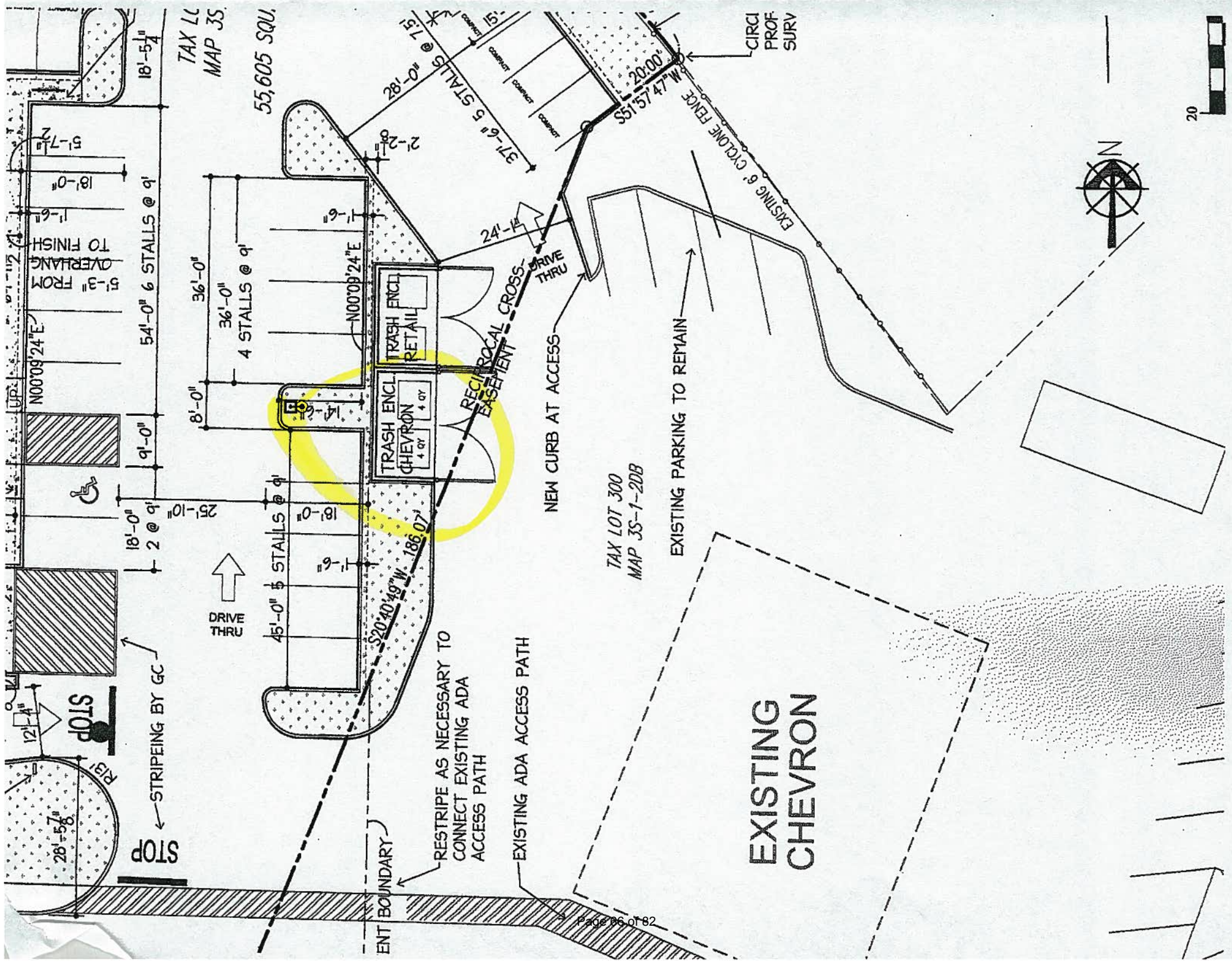
RELOCATED CHEVRON Pylon Sign UNDER SEPARATE PERMIT

EXISTING CHEVRON

LEGAL DESCRIPTION

EXCEPTING THEREFROM THAT PORTION THEREOF CONNECTED TO THE CITY OF WILSONVILLE FOR RIGHT-OF-WAY PURPOSES IN WARRIANT DEED RECORDED NUMBER 251,200 AS DOCUMENT NO. 2008-102062, WILSONVILLE COUNTY DEED RECORDS.





TAX LC  
MAP 3S

55,605 SQU.

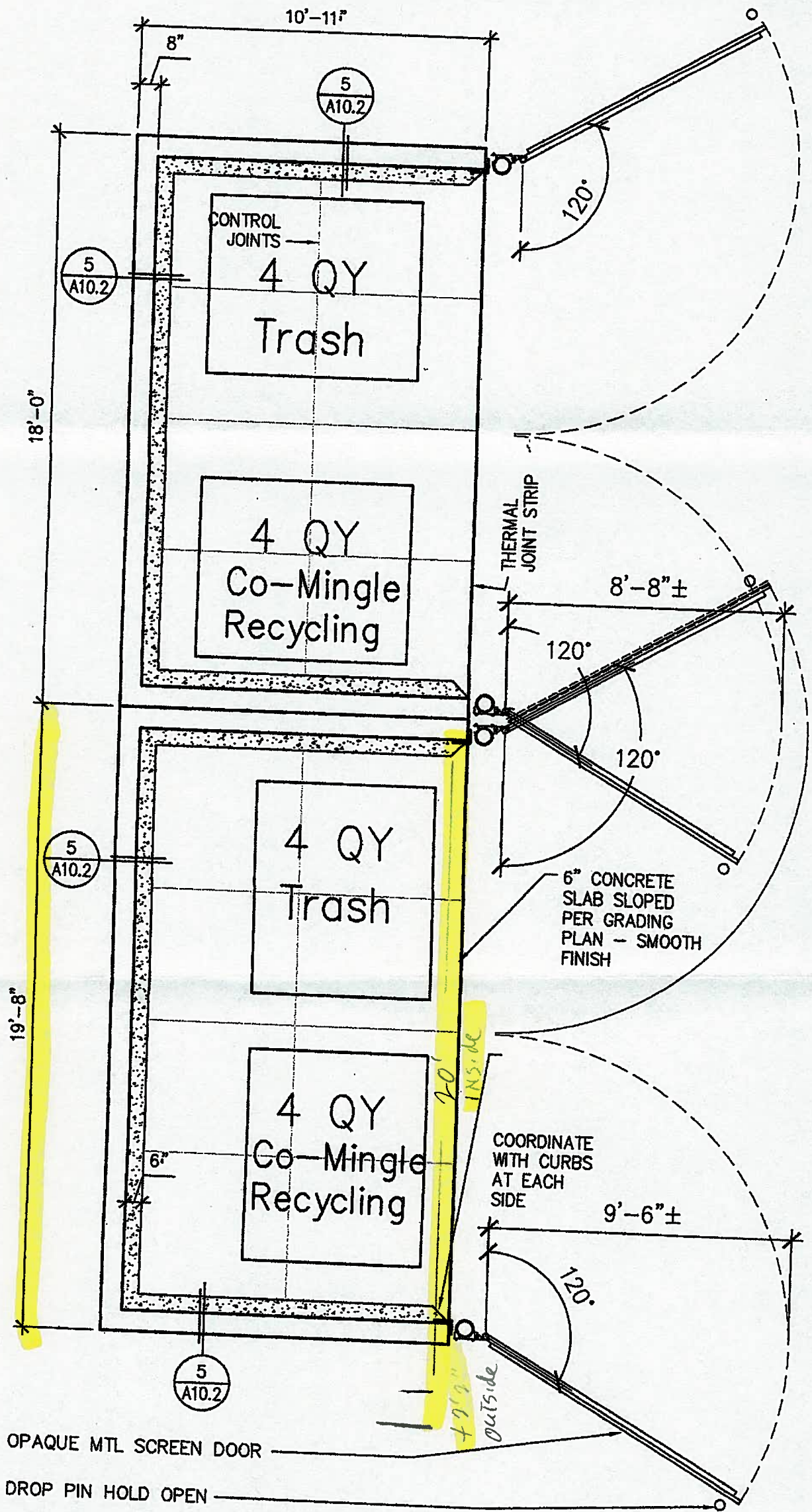
TAX LOT 300  
MAP 3S-1-2DB

EXISTING  
CHEVRON



20

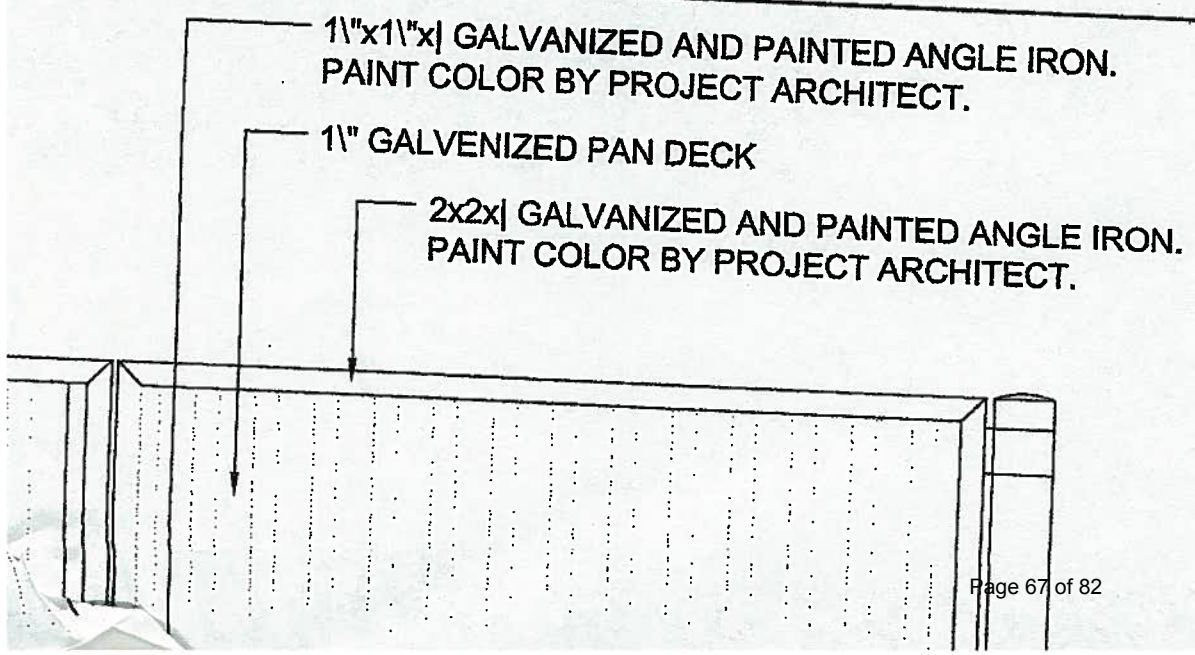




OPAQUE MTL SCREEN DOOR  
 DROP PIN HOLD OPEN

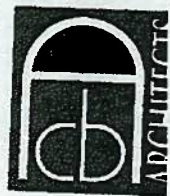
**ENCLOSURE AT PARKING - PLAN**

SCALE:  
 1/4" = 1'-0"      1



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**VERSION 22-14**



**CB ANDERSON ARCHITECTS PLLC**

7209 Greenwood Avenue  
 Seattle, Washington 9810  
 206-782-291  
 Fax 782-562

CLIENT

**Wilsonville Devco LLC**  
 c/o Integrated Investment Partners

PO Box 916  
 Portland, OR 97207  
 (503) 688-9600

PROJECT



**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE

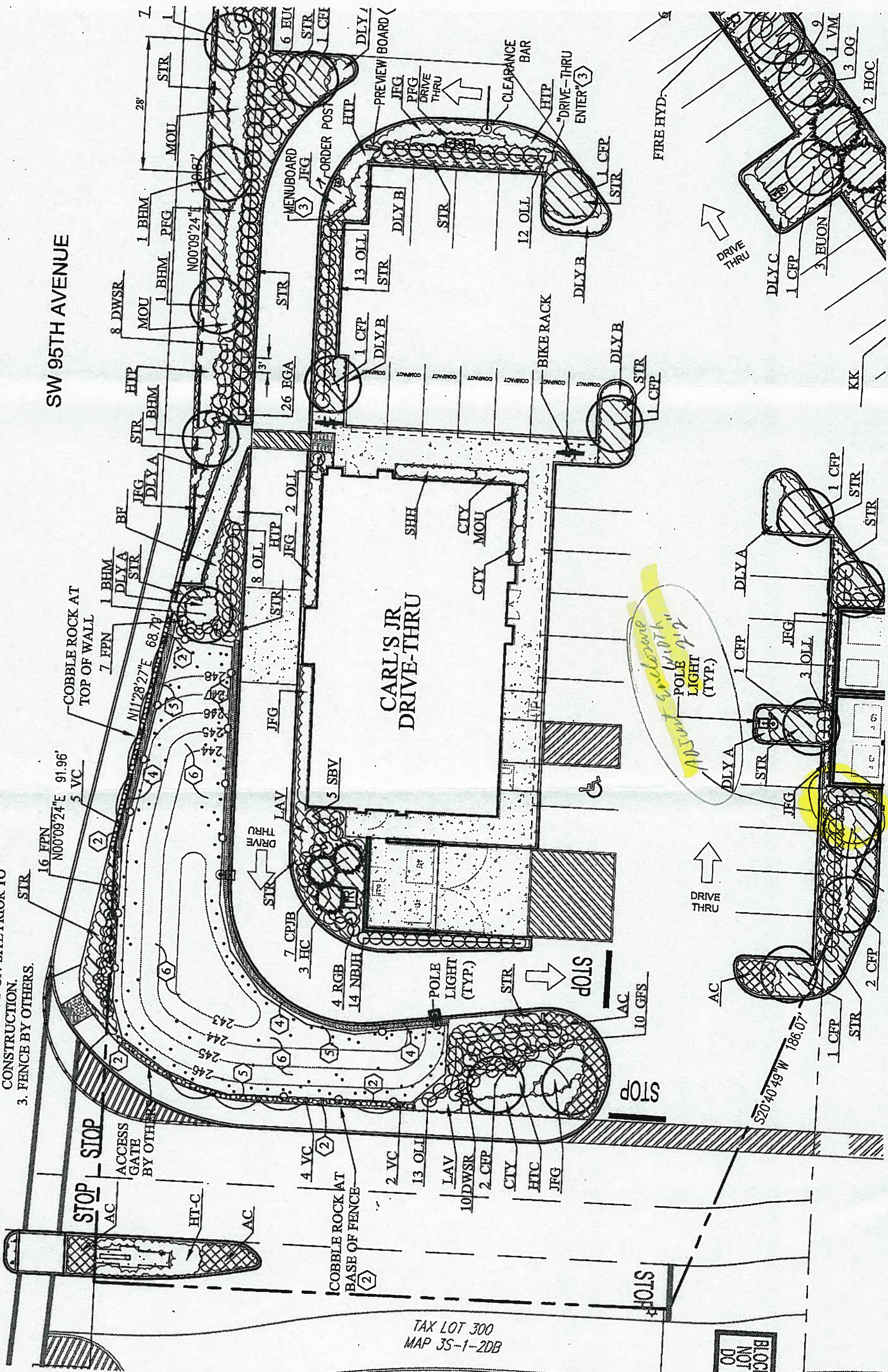
**TRASH ENCLOSURE**

REVISIONS




SW 95TH AVENUE

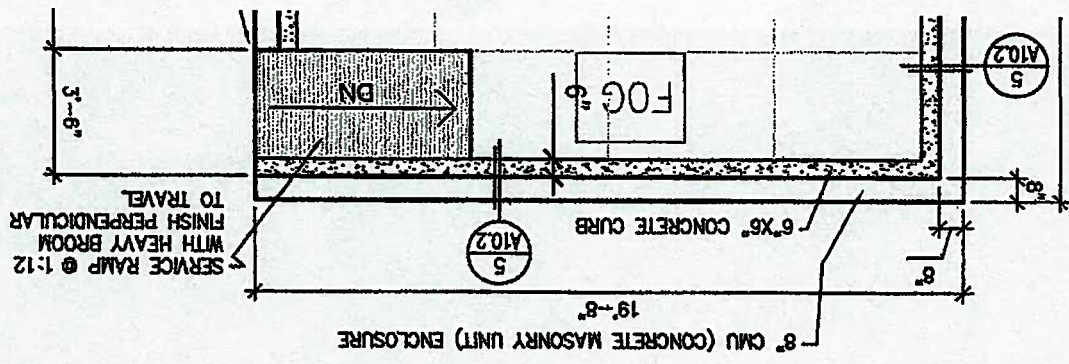
- NOTE:
1. DETENTION POND BY CIVIL ENGINEER.
  2. RETAINING WALLS ARE DIAGRAMATIC ONLY. VERIFY ON-SITE PRIOR TO CONSTRUCTION.
  3. FENCE BY OTHERS.



TAX LOT 300  
MAP 35-1-2DB

BLOCK NOT DO



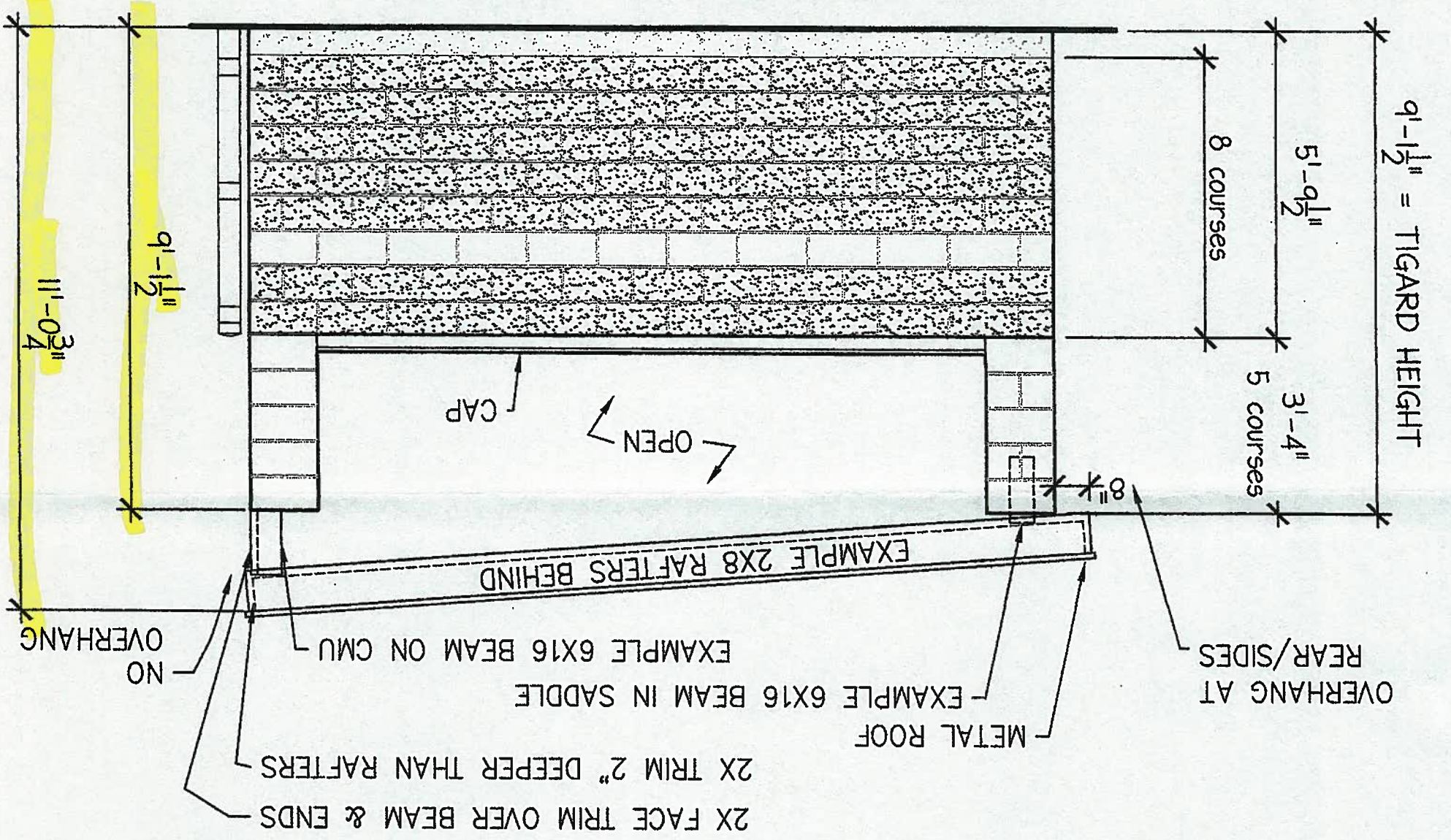


ELEVATION

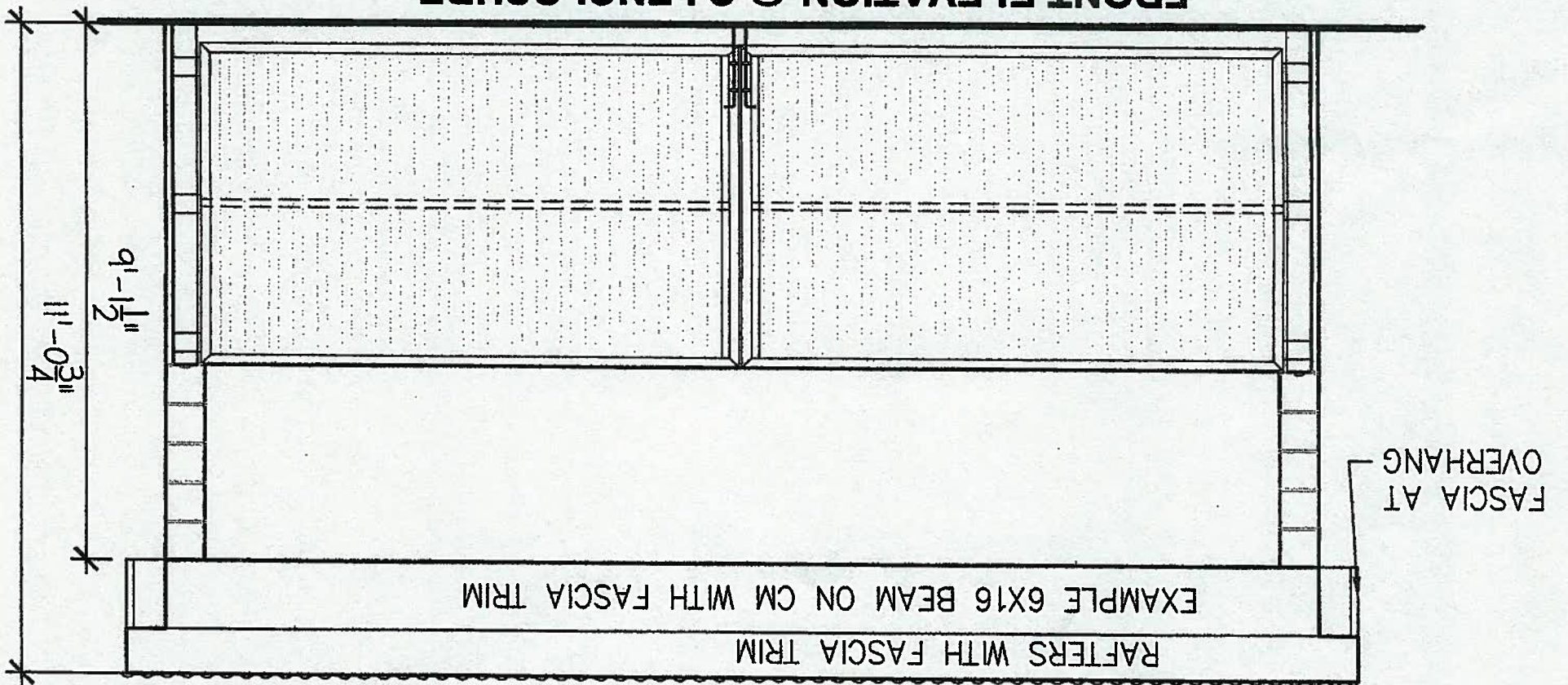
SCALE: 3/8"=1'-0"

3

**SIDE ELEVATION @ CJ ENCLOSURE**  
 (OTHER ENCLOSURES SIMILAR)



**FRONT ELEVATION @ CJ ENCLOSURE**  
 (OTHER ENCLOSURES SIMILAR)





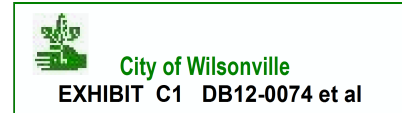
## COMMUNITY DEVELOPMENT MEMORANDUM

To: Daniel Pauly, Associate Planner

From: Kerry Rappold, Natural Resources Program Manager

Date: February 25, 2013

**RE: Commercial Development (DB12-0074 – Carl's Jr.)**



This memorandum includes staff conditions of approval. The conditions of approval are based on the submitted Stage II Final Plan and Site Design Review. The conditions of approval apply to the applicant's submittal of construction documents (i.e. engineering drawings).

### CONDITIONS OF APPROVAL:

The following conditions of approval are based on the material submitted by the applicant. Any subsequent revisions to the submitted plans may require conditions of approval to be modified by staff.

#### Stormwater Management

1. Pursuant to the policies and implementation measures of the 2012 Stormwater Master Plan, the applicant shall prioritize the use of Low Impact Development in the design and implementation of the stormwater management system. Low Impact Development entails managing rainfall at the source, using decentralized, small scale controls that provide infiltration, filtration, vegetative uptake, and the creation of extended flow paths.
2. Submit a final drainage report and drainage plans. The report and plans shall demonstrate proposed stormwater facilities satisfy the policies and standards of the City of Wilsonville's Stormwater Master Plan and Public Works Standards.
3. Provide profiles, plan views and specifications for proposed stormwater facilities consistent with the requirements of the Public Works Standards.
4. Pursuant to the Public Works Standards, the applicant shall submit a maintenance plan (including the City's stormwater maintenance and access easement) for proposed stormwater facilities prior to approval for occupancy of the associated development.
5. Pursuant to the Public Works Standards, access shall be provided to all areas of proposed stormwater facilities. At a minimum, at least one access shall be provided for maintenance and inspection.

## Other

6. Pursuant to the City of Wilsonville's Ordinance No. 482, the applicant shall submit an erosion and sedimentation control plan. The following techniques and methods shall be incorporated, where necessary:
  - a. Gravel construction entrance;
  - b. Stockpiles and plastic sheeting;
  - c. Sediment fence;
  - d. Inlet protection (Silt sacks are recommended);
  - e. Dust control;
  - f. Temporary/permanent seeding or wet weather measures (e.g. mulch);
  - g. Limits of construction; and
  - h. Other appropriate erosion and sedimentation control methods.
7. The applicant shall comply with all applicable state and federal requirements for the proposed construction activities and proposed facilities (e.g. DEQ NPDES #1200-C permit).
8. Pursuant to the Wilsonville City Code, the applicant is required to provide covered waste and recycling enclosures. A drain is not allowed within the enclosure, and the floor in the enclosure shall be raised to prevent stormwater runoff from entering. The enclosure shall contain adequate area for proper use of all receptacles. These measures minimize the risk of pollutants entering the public stormwater system.

**EXHIBIT A  
PLANNING DIVISION  
STAFF REPORT**

**PROJECT NAME**

**DEVELOPMENT REVIEW BOARD PANEL ‘ \_\_\_ ’  
QUASI JUDICIAL HEARING**

---

**Public Hearing Date:**

**Date of Report:**

**Application Numbers:**

**Request A: DB12-0074 Stage II Final Plan  
Request B: DB12-0075 Site Design Review  
Request C: DB12-0076 Master Sign Plan for Boones  
Ferry Pointe**

**Property**

**Owners/Applicants:**

**PD = Planning Division conditions**

**BD – Building Division Conditions**

**PF = Engineering Conditions.**

**NR = Natural Resources Conditions**

**TR = SMART/Transit Conditions**

**FD = Tualatin Valley Fire and Rescue Conditions**

<b>Standard Comments:</b>													
<b>PFB 1.</b>	All construction or improvements to public works facilities shall be in conformance to the City of Wilsonville Public Works Standards.												
<b>PFB 2.</b>	Applicant shall submit insurance requirements to the City of Wilsonville in the following amounts: <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="padding-left: 20px;">General Aggregate</td> <td style="text-align: right;">\$2,000,000</td> </tr> <tr> <td style="padding-left: 20px;">Products-Completed Operations Aggregate</td> <td style="text-align: right;">\$2,000,000</td> </tr> <tr> <td style="padding-left: 20px;">Each Occurrence</td> <td style="text-align: right;">\$2,000,000</td> </tr> <tr> <td style="padding-left: 20px;">Automobile Insurance</td> <td style="text-align: right;">\$1,000,000</td> </tr> <tr> <td style="padding-left: 20px;">Fire Damage (any one fire)</td> <td style="text-align: right;">\$ 50,000</td> </tr> <tr> <td style="padding-left: 20px;">Medical Expense (any one person)</td> <td style="text-align: right;">\$ 10,000</td> </tr> </table>	General Aggregate	\$2,000,000	Products-Completed Operations Aggregate	\$2,000,000	Each Occurrence	\$2,000,000	Automobile Insurance	\$1,000,000	Fire Damage (any one fire)	\$ 50,000	Medical Expense (any one person)	\$ 10,000
General Aggregate	\$2,000,000												
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Each Occurrence	\$2,000,000												
Automobile Insurance	\$1,000,000												
Fire Damage (any one fire)	\$ 50,000												
Medical Expense (any one person)	\$ 10,000												
<b>PFB 3.</b>	No construction of, or connection to, any existing or proposed public utility/improvements will be permitted until all plans are approved by Staff, all fees have been paid, all necessary permits, right-of-way and easements have been obtained and Staff is notified a minimum of 24 hours in advance.												
<b>PFB 4.</b>	All public utility/improvement plans submitted for review shall be based upon a 22”x 34” format and shall be prepared in accordance with the City of Wilsonville Public Work’s Standards.												
<b>PFB 5.</b>	Plans submitted for review shall meet the following general criteria:												

- a. Utility improvements that shall be maintained by the public and are not contained within a public right-of-way shall be provided a maintenance access acceptable to the City. The public utility improvements shall be centered in a minimum 15-ft. wide public easement for single utilities and a minimum 20-ft wide public easement for two parallel utilities and shall be conveyed to the City on its dedication forms.
- b. Design of any public utility improvements shall be approved at the time of the issuance of a Public Works Permit. Private utility improvements are subject to review and approval by the City Building Department.
- c. In the plan set for the PW Permit, existing utilities and features, and proposed new private utilities shall be shown in a lighter, grey print. Proposed public improvements shall be shown in bolder, black print.
- d. All elevations on design plans and record drawings shall be based on NAVD 88 Datum.
- e. All proposed on and off-site public/private utility improvements shall comply with the State of Oregon and the City of Wilsonville requirements and any other applicable codes.
- f. Design plans shall identify locations for street lighting, gas service, power lines, telephone poles, cable television, mailboxes and any other public or private utility within the general construction area.
- g. As per City of Wilsonville Ordinance No. 615, all new gas, telephone, cable, fiber-optic and electric improvements etc. shall be installed underground. Existing overhead utilities shall be undergrounded wherever reasonably possible.
- h. Any final site landscaping and signing shall not impede any proposed or existing driveway or interior maneuvering sight distance.
- i. Erosion Control Plan that conforms to City of Wilsonville Ordinance No. 482.
- j. Existing/proposed right-of-way, easements and adjacent driveways shall be identified.
- k. All engineering plans shall be stamped by a Professional Engineer registered in the State of Oregon.

**PFB 6.** Submit plans in the following general format and order for all public works construction to be maintained by the City:

- a. Cover sheet
- b. City of Wilsonville construction note sheet
- c. General construction note sheet
- d. Existing conditions plan.
- e. Erosion control and tree protection plan.
- f. Site plan. Include property line boundaries, water quality pond boundaries, sidewalk improvements, right-of-way (existing/proposed), easements (existing/proposed), and sidewalk and road connections to adjoining properties.
- g. Grading plan, with 1-foot contours.
- h. Composite utility plan; identify storm, sanitary, and water lines; identify storm and sanitary manholes.
- i. Detailed plans; show plan view and either profile view or provide i.e.'s at all

	<p>utility crossings; include laterals in profile view or provide table with i.e.'s at crossings; vertical scale 1"= 5', horizontal scale 1"= 20' or 1"= 30'.</p> <ul style="list-style-type: none"> <li>j. Street plans.</li> <li>k. Storm sewer/drainage plans; number all lines, manholes, catch basins, and cleanouts for easier reference</li> <li>l. Water and sanitary sewer plans; plan; number all lines, manholes, and cleanouts for easier reference.</li> <li>m. Detailed plan for storm water detention facility (both plan and profile views), including water quality orifice diameter and manhole rim elevations. Provide detail of inlet structure and energy dissipation device. Provide details of drain inlets, structures, and piping for outfall structure. Note that although storm water detention facilities are typically privately maintained they will be inspected by engineering, and the plans must be part of the Public Works Permit set.</li> <li>n. Detailed plan for water quality facility (both plan and profile views). Note that although storm water quality facilities are typically privately maintained they will be inspected by Natural Resources, and the plans must be part of the Public Works Permit set.</li> <li>o. Composite franchise utility plan.</li> <li>p. City of Wilsonville detail drawings.</li> <li>q. Illumination plan.</li> <li>r. Striping and signage plan.</li> <li>s. Landscape plan.</li> </ul>
<b>PFB 7.</b>	Prior to manhole and sewer line testing, design engineer shall coordinate with the City and update the sanitary and stormwater sewer systems to reflect the City's numbering system. Video testing and sanitary manhole testing will refer to the updated numbering system. Design engineer shall also show the updated numbering system on As-Built drawings submitted to the City.
<b>PFB 8.</b>	The applicant shall install, operate and maintain adequate erosion control measures in conformance with the standards adopted by the City of Wilsonville Ordinance No. 482 during the construction of any public/private utility and building improvements until such time as approved permanent vegetative materials have been installed.
<b>PFB 9.</b>	Applicant shall work with City's Natural Resources office before disturbing any soil on the respective site. If 5 or more acres of the site will be disturbed applicant shall obtain a 1200-C permit from the Oregon Department of Environmental Quality. If 1 to less than 5 acres of the site will be disturbed a 1200-CN permit from the City of Wilsonville is required.
<b>PFB 10.</b>	To lessen the impact of the proposed project on the downstream storm drain system, and adjacent properties, project run-off from the site shall be detained and limited to the difference between a developed 25-year storm and an undeveloped 25-year storm. The detention and outfall facilities shall be designed and constructed in conformance with the Public Works Standards.
<b>PFB 11.</b>	A storm water analysis prepared by a Professional Engineer registered in the State of Oregon shall be submitted for review and approval by the City to address appropriate pipe and detention facility sizing.

<b>PFB 12.</b>	The applicant shall be in conformance with all water quality requirements for the proposed development per the Public Works Standards. If a mechanical water quality system is used, prior to City acceptance of the project the applicant shall provide a letter from the system manufacturer stating that the system was installed per specifications and is functioning as designed.
<b>PFB 13.</b>	Storm water quality facilities shall have approved landscape planted and/or some other erosion control method installed and approved by the City of Wilsonville prior to streets and/or alleys being paved.
<b>PFB 14.</b>	The applicant shall provide the City with a Stormwater Maintenance and Access Easement (on City approved forms) for City inspection of those portions of the storm system to be privately maintained. Applicant shall maintain all LID storm water components and private conventional storm water facilities located within medians and from the back of curb onto and including the project site.
<b>PFB 15.</b>	Fire hydrants shall be located in compliance with TVF&R fire prevention ordinance and approval of TVF&R.
<b>PFB 16.</b>	All survey monuments on the subject site, or that may be subject to disturbance within the construction area, or the construction of any off-site improvements shall be adequately referenced and protected prior to commencement of any construction activity. If the survey monuments are disturbed, moved, relocated or destroyed as a result of any construction, the project shall, at its cost, retain the services of a registered professional land surveyor in the State of Oregon to restore the monument to its original condition and file the necessary surveys as required by Oregon State law. A copy of any recorded survey shall be submitted to Staff.
<b>PFB 17.</b>	Sidewalks, crosswalks and pedestrian linkages in the public right-of-way shall be in compliance with the requirements of the U.S. Access Board.
<b>PFB 18.</b>	No surcharging of sanitary or storm water manholes is allowed.
<b>PFB 19.</b>	The project shall connect to an existing manhole or install a manhole at each connection point to the public storm system and sanitary sewer system.
<b>PFB 20.</b>	A City approved energy dissipation device shall be installed at all proposed storm system outfalls. Storm outfall facilities shall be designed and constructed in conformance with the Public Works Standards.
<b>PFB 21.</b>	All required pavement markings, in conformance with the Transportation Systems Plan and the Bike and Pedestrian Master Plan, shall be completed in conjunction with any conditioned street improvements.
<b>PFB 22.</b>	Street and traffic signs shall have a hi-intensity prismatic finish meeting ASTM 4956 Spec Type 4 standards.
<b>PFB 23.</b>	The applicant shall provide adequate sight distance at all project driveways by driveway placement or vegetation control. Specific designs to be submitted and approved by the City Engineer. Coordinate and align proposed driveways with driveways on the opposite side of the proposed project site.
<b>PFB 24.</b>	Access requirements, including sight distance, shall conform to the City's Transportation Systems Plan (TSP) or as approved by the City Engineer. Landscaping plantings shall be low enough to provide adequate sight distance at all street intersections and alley/street intersections.
<b>PFB 25.</b>	Applicant shall prepare an Ownership and Maintenance agreement between the City and the Owner. Stormwater or rainwater facilities may be located within the



# Development Review Template

**DATE:** 2/27/13

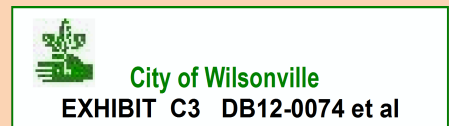
**TO:** DAN PAULY AICP, ASSOCIATE PLANNER  
**FROM:** DON WALTERS  
**SUBJECT:** DEVELOPMENT REVIEW # DB12-0074-76

**WORK DESCRIPTION: CARL'S JUNIOR AND ADJACENT RETAIL BUILDING**

\*\*\*\*\*

**Building Division Conditions:**

- |       |  |
|-------|--|
| BD 1. | ACCESSIBLE PARKING cannot be fully reviewed at this time. Accessible parking will be fully reviewed as part of the plan review of the building permit. The additional information available at plan review may require changes to the number and location of accessible parking spaces shown on these preliminary plans.     |
| BD 2. | A GREASE INTERCEPTOR will be required for Carl's Junior. If there is a possibility that a business requiring a grease interceptor will occupy the new retail building, a sewer line shall be run from the retail building to the grease interceptor. The grease interceptor shall be sized as directed in the Plumbing Code. |
| BD 3. | TRASH ENCLOSURE. If a drain is installed in the covered trash enclosure it shall be plumbed to the grease interceptor.   |

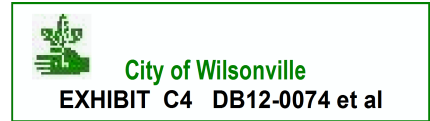






February 26, 2013

Daniel Pauly  
Associate Planner  
City of Wilsonville  
Wilsonville, Oregon  
97070



**Re: Case File DB 12-0074 thru 12-0076, New Fast Food Restaurant and Retail Building**

Dear Mr. Pauly,

Thank you for the opportunity to review the proposed site plan surrounding the above named development project. Tualatin Valley Fire & Rescue endorses this proposal predicated on the following criteria and conditions of approval:

- 1) **SURFACE AND LOAD CAPACITIES:** Fire apparatus access roads shall be of an all-weather surface that is easily distinguishable from the surrounding area and is capable of supporting not less than 12,500 pounds point load (wheel load) and 60,000 pounds live load (gross vehicle weight). You may need to provide documentation from a registered engineer that the design will be capable of supporting such loading. (OFC D102.1) ***Applicable to the parking lot.***
- 2) **PAINTED CURBS:** Where required, fire apparatus access roadway curbs shall be painted red and marked "NO PARKING FIRE LANE" at approved intervals. Lettering shall have a stroke of not less than one inch wide by six inches high. Lettering shall be white on red background. (OFC 503.3) ***Provide curb lane striping along the face of the curb at the landscape island housing the new fire hydrant.***
- 3) **COMMERCIAL BUILDINGS - REQUIRED FIRE FLOW:** The required fire flow for the building shall not exceed 3,000 gallons per minute (GPM) or the available GPM in the water delivery system at 20 psi, whichever is less as calculated using IFC, Appendix B. A worksheet for calculating the required fire flow is available from the Fire Marshal's Office. (OFC B105.3) ***Please provide a current fire flow test of the nearest fire hydrant demonstrating available flow at 20 psi residual pressure as well as fire flow calculation worksheets. Please forward copies to both TVF&R as well as local building department. Fire flow calculation worksheets as well as instructions are available on our web site at [www.tvfr.com](http://www.tvfr.com).***
- 4) **FIRE HYDRANTS – COMMERCIAL BUILDINGS:** Where a portion of the building is more than 400 feet from a hydrant on a fire apparatus access road, as measured in an approved route around the exterior of the building, on-site fire hydrants and mains shall be provided. This distance may be increased to 600 feet for buildings equipped throughout with an approved automatic sprinkler system. (OFC 507.5.1) ***The proposed fire hydrant is obstructed by passenger vehicle parking stalls. Please relocate the new fire hydrant from its proposed location to the landscape island to the south.***
- 5) **REFLECTIVE HYDRANT MARKERS:** Fire hydrant locations shall be identified by the installation of reflective markers. The markers shall be blue. They shall be located adjacent and to the side of the centerline of the access road way that the fire hydrant is located on. In case that there is no center line, then assume a centerline, and place the reflectors accordingly. (OFC 510.1)
- 6) **PHYSICAL PROTECTION:** Where fire hydrants are subject to impact by a motor vehicle, guard posts, bollards or other approved means of protection shall be provided. (OFC 507.5.6) ***Please provide bollards at the new fire hydrant.***
- 7) **CLEAR SPACE AROUND FIRE HYDRANTS:** A 3 foot clear space shall be provided around the circumference of fire hydrants. (OFC 507.5.5)

- 8) **ACCESS AND FIRE FIGHTING WATER SUPPLY DURING CONSTRUCTION:** Approved fire apparatus access roadways and fire fighting water supplies shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. (OFC 1410.1 & 1412.1)
- 9) **KNOX BOX:** A Knox Box for building access is required for this building. Please contact the Fire Marshal's Office for an order form and instructions regarding installation and placement. (OFC 506.1)  
***Provide a Knox box at each new building.***
- 10) **PREMISES IDENTIFICATION:** Buildings shall have approved address numbers, building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Address numbers shall be Arabic numerals or alphabet numbers. Numbers shall be a minimum of 4 inches high with a ½ inch stroke. (OFC 505.1) ***Please provide a physical address for each new building visible from the approaching roadway.***
- 11) **FIRE DEPARTMENT ACCESS TO EQUIPMENT:** Fire protection equipment shall be identified in an approved manner. Rooms containing controls for HVAC, fire sprinklers risers and valves or other fire detection, suppression or control features shall be identified with approved signs. (OFC 509.1)

If you have questions or need further clarification, please feel free to contact me at 503-259-1404.

Sincerely,

*Drew S. DeBois*

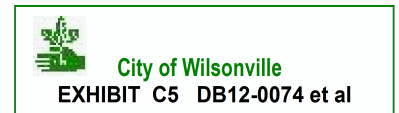
Drew DeBois  
Deputy Fire Marshal II/CFI

Copy: D. Walters, COW, File

Public Works Plan Review Comment Form

**Plans for Review:** Boones Ferry Pointe – Carl's, Jr  
**Return All Comments To:** Daniel Pauly  
**Due Date:** February 25, 2013

Name	Sec No.	Comments	Engineering's Response
Randy Watson		<p>Carl's Jr shall comply with the new approved Oregon Plumbing Specialty Code which requires all plumbing fixtures, garbage disposals, dishwashers, floor drains, and cooking equipment with drain connections in food and/or beverage preparation areas be connected to the approved GRD.</p> <p>The trash rack area shall be covered per Wilsonville Code 8.210(9) and if a drain is provided, it too shall be connected to a grease removal device.</p>	
Steve Munsterman		No Comments	
Dan House			
Steve Gering	DD2	There appears to be an existing waterline located under the ESE corner of the proposed 3150 sf retail building. Is the line being relocated?	
Matt Baker			
Arnie Gray			
Mark Folz			
Paul Havens			





**Pauly, Daniel**

**From:** White, Shelley  
**Sent:** Monday, February 11, 2013 2:10 PM  
**To:** Pauly, Daniel  
**Subject:** FW: Development Review Team Mailing (DB12-0074 et al - Boones Ferry Pointe/Carl's Jr)

Dan – a response to the Boones Ferry Pointe/Carl's Jr. DRT notice below...

*Shelley White*

Administrative Assistant  
City of Wilsonville  
Ph: 503 570-1575  
[swhite@ci.wilsonville.or.us](mailto:swhite@ci.wilsonville.or.us)

DISCLOSURE NOTICE: Messages to and from this E-mail address may be subject to the Oregon Public Records Law.

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**From:** Raymond Lambert [<mailto:Raymond.Lambert@pgn.com>]  
**Sent:** Monday, February 11, 2013 1:12 PM  
**To:** White, Shelley  
**Subject:** RE: Development Review Team Mailing (DB12-0074 et al - Boones Ferry Pointe/Carl's Jr)

Power is not readily available to this parcel. Developer should contact PGE to develop a plan as soon as possible. Call the PGE service coordinator at 503-736-5450.

Thanks,  
Ray Lambert  
S&D Project Manager  
PGE – Wilsonville

---

**From:** White, Shelley [<mailto:swhite@ci.wilsonville.or.us>]  
**Sent:** Monday, February 11, 2013 8:39 AM  
**To:** Gray, Arnie; Jacobson, Barbara; Ben Baldwin ([DevelopmentReview@trimet.org](mailto:DevelopmentReview@trimet.org)); Bill Rhoades ([rhoadesw@wlwv.k12.or.us](mailto:rhoadesw@wlwv.k12.or.us)); Edmonds, Blaise; Brian Harper ([Brian.Harper@oregonmetro.gov](mailto:Brian.Harper@oregonmetro.gov)); Stevenson, Brian; Cosgrove, Bryan; Neamtzu, Chris; Christine Jeibmann ([Christine.Jeibmann@nwnatural.com](mailto:Christine.Jeibmann@nwnatural.com)); Knoll, Dan; Stark, Dan; Pauly, Daniel; Kerber, Delora; Walters, Don; Drew DeBois ([Drew.Debois@tvfr.com](mailto:Drew.Debois@tvfr.com)); Parent, Gail; Miller, Holly; Massa Smith, Jen; ([Karen.mohling@tvfr.com](mailto:Karen.mohling@tvfr.com)); Kenneth Parris ([kenneth\\_parris@cable.comcast.com](mailto:kenneth_parris@cable.comcast.com)); Rappold, Kerry; Lance Cheeley ([Lance.Cheeley@nwnatural.com](mailto:Lance.Cheeley@nwnatural.com)); Lorraine Katz; Bushman, Luke; Marah Danielson ([marah.b.danielson@odot.state.or.us](mailto:marah.b.danielson@odot.state.or.us)); Brown, Martin; Baker, Matt; Ward, Mike; Wheeler, Mike; Kraushaar, Nancy; Watt, Nick; Duke, Pat; Raymond Lambert; Region 1 Development Review Applications ([Region1DEVREVAApplications@odot.state.or.us](mailto:Region1DEVREVAApplications@odot.state.or.us)); Robert Buck; Ryan Truair ([rmk@nwnatural.com](mailto:rmk@nwnatural.com)); Sandra Larsen ([sandra.larsen@aviation.state.or.us](mailto:sandra.larsen@aviation.state.or.us)); Simonton, Scott; Lashbrook, Stephan; Adams, Steve; Allen, Steve; Munsterman, Steve; Steven Schalk ([Steven.B.Schalk@ODOT.state.or.us](mailto:Steven.B.Schalk@ODOT.state.or.us)); Tiffany Ritchey; Tom Maier ([Thomas.Maier@awin.com](mailto:Thomas.Maier@awin.com))  
**Subject:** Development Review Team Mailing (DB12-0074 et al - Boones Ferry Pointe/Carl's Jr)

Development Review Team members,

Please find the attached Development Review Team mailing for your review:

DB12-0074 et al – Boones Ferry Pointe: Carl's Jr. Restaurant and Multi-Tenant Commercial Building

**Comments are due to Daniel Pauly, Associate Planner, by 4 pm on February 25, 2013 for the March 11, 2013 DRB public hearing.**

Thank you.

*Shelley White*

Administrative Assistant  
City of Wilsonville  
Ph: 503 570-1575  
[swhite@ci.wilsonville.or.us](mailto:swhite@ci.wilsonville.or.us)

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**Land Use Application  
City of Wilsonville**



BY:.....

**Boones Ferry Pointe  
Carl's Jr. and Retail Center  
includes Shared Access, Egress and Signage  
with Chevron Site**

**Stage II Final Development Plans;  
Master Sign Plan; and  
Site Design Review**

**November 30, 2012  
Revised for Completeness  
January 21, 2013**

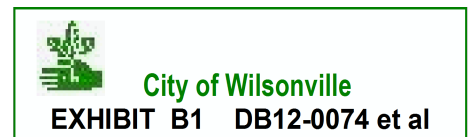
**APPLICANT/OWNER:  
Wilsonville Devco, LLC  
PO Box 916  
Portland, OR 97207  
Contact: Josh Veentjer  
josh@iipcorp.com**

**APPLICANT/OWNER:  
LaPoint Business Group  
10618 Crosby Road, NE  
Woodburn, OR 97071  
Garry LaPoint  
gl@eoni.com**

**APPLICANT'S REPRESENTATIVES:**

**CB Anderson, Project Architects  
7209 Greenwood Ave. N.  
Seattle, WA 98103  
Contact: Craig Anderson  
206-782-2911  
craig@cba-arch.com**

**SFA Design Group, Project Planning  
9020 SW Washington Square Dr., Ste 505  
Portland, OR 97223  
Contact: Ben Altman  
503-641-8311  
baltman@sfadg.com**





## SFA Design Group, LLC

STRUCTURAL | CIVIL | LAND USE PLANNING | SURVEYING

February 7, 2013

Daniel Pauley, Associate Planner  
City of Wilsonville  
29799 SW Town Center Loop E.  
Wilsonville, OR 97070

RE: Response to Second Request for Additional Information and Revisions -  
Incomplete Application, Boones Ferry Pointe (Carl's Jr.).

Dear Daniel:

This letter is in response to your request for clarifications and revisions related to the Master Sign Plan portion of the Application for the Boones Ferry Pointe, DB12-0074-DB12-0076.

Based on our discussions about the best way to address the Free Standing Signage, here is what we are proposing:

1. We are not modifying the Chevron Master Sign Plan. It will remain as a separate approval.
2. For the Boones Ferry Pointe Free Standing Sign, we are allowed 2 Free Standing Signs, each allowed a maximum of 59 square feet, based on the area of the two buildings, see Revised Table 5.
  - A. The 95<sup>th</sup> Avenue sign will be considered as a new separate sign, which happens to be "co-located" on the same pole as the Chevron Sign.
  - B. The north sign will provide for signage for Carl's Jr., 3 tenants in the Retail Building, and the Coke Fountain Mart (Chevron C-Store).
  - C. We are also proposing the Boones Ferry Pointe Monument Sign, which will obtain its sign area from the net allowed area of the 95<sup>th</sup> Avenue sign.

Under this scenario we are not combining the two Master Sign Plans. The Proposed Master Sign Plan only applies to the new development site. The Chevron Master Sign Plan will remain as approved.

In addition, with this approach a waiver is not necessary. However, since the waiver findings are already in the narrative, I believe they still serve to add historical perspective on the phased development of this property over time.



Building addressing will be added to the 95<sup>th</sup> Avenue freestanding sign, consistent with Section 505.1 of the 2010 Oregon Fire Code. There will be three address plates hung under the Carl's Jr. sign.

**TABLE 5  
Allowed and Proposed  
Freestanding Signs**

Tenants (# tenants)	Building Area Sq. Ft.	Additional Sign Area Allowed over Base Square Footage.  Two Free Standing Signs Allowed	Proposed 95 <sup>th</sup> Avenue Sign	Proposed Boones Ferry Sign
			West	North
Base Area		32		
Carl's Jr. (1)	2,867  Rounded to 2,000	9	36	22.48
Retail Building (3)	3,150  Rounded to 3,000	18		18
BFP Monument		0	13	
C-Store (1)				9
Total		59 each sign	49	49.48
Combined Total		x 2 = 118		98.48

As demonstrated in the summary table above, the three proposed free standing signs comply with the new standards, relative to the allowed area

**Outdoor Lighting**

***Section 4.199.50. Submittal Requirements.***

*(.01) Applicants shall submit the following information as part of DRB review or administrative review of new commercial, industrial, multi-family or public facility projects:*

- A. A statement regarding which of the lighting methods will be utilized, prescriptive or performance, and a map depicting the lighting zone(s) for the property.*
- B. A site lighting plan that clearly indicates intended lighting by type and location. For adjustable luminaires, the aiming angles or coordinates shall be shown.*
- C. For each luminaire type, drawings, cut sheets or other documents containing specifications for the intended lighting including but not limited to, luminaire description, mounting, mounting height, lamp type and manufacturer, lamp watts,*



*ballast, optical system/distribution, and accessories such as shields.*

*D. Calculations demonstrating compliance with Oregon Energy Efficiency Specialty Code, Exterior Lighting, as modified by Section 4.199.40(.01)(B.) (2.) [Amended by Ord. 688, 11/15/10]*

*E. Lighting plans shall be coordinated with landscaping plans so that pole lights and trees are not placed in conflict with one another. The location of lights shall be shown on the landscape plan. Generally, pole lights should not be placed within one pole length of landscape and parking lot trees.*

*F. Applicants shall identify the hours of lighting curfew.*

*(.03) In addition to the above submittal requirements, Applicants using the Performance Method shall submit the following information as part of the permit set plan review:*

*A. Site plan showing horizontal isocandle lines, or the output of a point-by-point computer calculation of the horizontal illumination of the site, showing property lines and light levels immediately off of the subject property.*

*B. For each side of the property, the output of a point-by-point vertical footcandle calculation showing illumination in the vertical plane at the property line from grade to at least 10 feet higher than the height of the tallest pole.*

*C. Lighting plans shall be prepared by a qualified licensed engineer.*

## **RESPONSE**

A revised Photometric Plan (Sheet SE1.0) has been provided, prepared by a qualified licensed engineer. The revised site plan shows horizontal isocandle lines, (point-by-point computer calculation of the horizontal illumination), showing property lines and light levels immediately off of the subject property. The highest perimeter levels are shown across the top of the page.

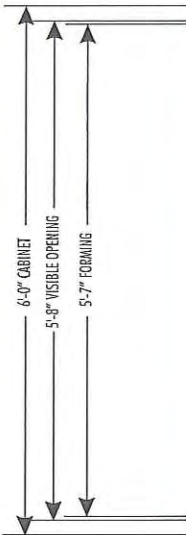
For each side of the property, the output of a point-by-point vertical footcandle calculation also shows illumination in the vertical plane at the property line from grade to at least 10 feet higher than the height of the tallest pole. A 3D image has also been provided to show the lighting levels at the perimeter of the site.

Thank you for your cooperation and extensive assistance. If you have any questions or need additional information, please contact me immediately, so we can respond quickly.

Sincerely,  
SFA Design Group



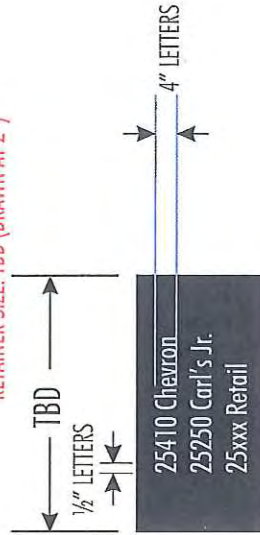
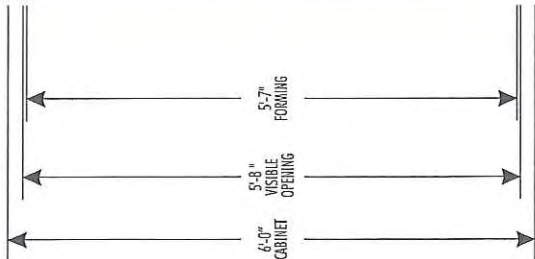
Ben Altman  
Senior Planner/Project Manager



FRONT VIEW

O.D. 6'-0" H X 6'-0" W

RETAINER SIZE: TBD (DRAWN AT 2")



25410 Chevron  
25250 Carl's Jr.  
25xxxx Retail

ADDRESS PANEL  
NON ILLUMINATED  
FONT TBD

ILLUMINATED PYLON SIGN

ORDER PART # CKEZ0042

DEPTH TO BE DETERMINED



SIDE VIEW



Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	E





REVISION HISTORY:	
NO.	DESCRIPTION
1	INITIAL DRAWING RELEASE
2	ADDED POLE & FOOTING DETAIL
3	ADDED SO. FT. & REVISED PIPE SIZE
4	ADDED ADDITIONAL PANEL

**DO NOT MANUFACTURE  
UNLESS APPROVED BY SIGN RESOURCE**

**PARTS LIST:**

ITEM	DESCRIPTION
1	BLACK
2	RED VINYL 3/4" X 3/4" X 3/4"
3	YELLOW VINYL 3/4" X 3/4" X 3/4"
4	WHITE ACRYLIC
5	BRICK
6	CONCRETE
7	STEEL
8	PIPE
9	FLANGE
10	WASHER
11	WASHER
12	WASHER
13	WASHER
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99	WASHER
100	WASHER

**FIRST SURFACE DECORATION  
FLUORESCENT ILLUMINATION**  
THIS SIGN IS DESIGNED TO BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE (NEC) AND THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA) 70B. THIS INCLUDES PROPER GROUNDING AND BONDING OF THE SIGN.

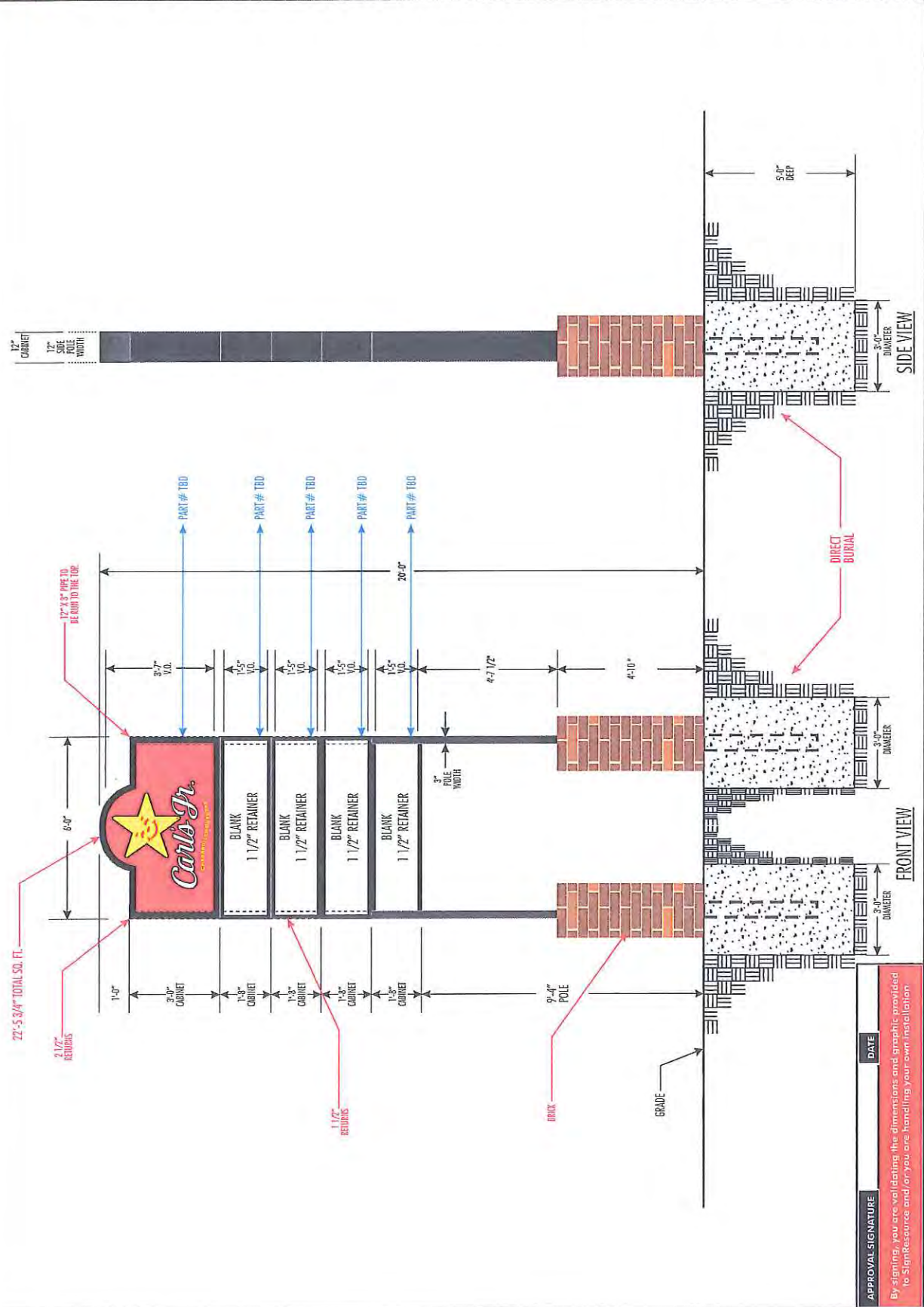
**GENERAL NOTES**  
1. TOLERANCE (UNLESS NOTED)  
• GRAPHIC ± 1/16" • FACE SIZE ± 1/16" ± 1/16"  
• DESIGN ± 1/16" • VENT OVERLAP ± 1/8" ± 1/16"  
2. FINISHES: UNLESS OTHERWISE SPECIFIED, ALL FINISHES ARE TO BE PERFORMED BY THE CONTRACTOR.  
3. FINISH COLOR: UNLESS OTHERWISE SPECIFIED, ALL FINISHES ARE TO BE PERFORMED BY THE CONTRACTOR.  
4. NO DEVIATION OR MATERIAL SUBSTITUTION WITHOUT E.O.I.  
5. ALL ELECTRICAL SHALL COMPLY WITH ILL. 46.

**DESCRIPTION:**  
CARL'S JR. CUSTOM ILL  
20 FT TP POLE PYLON

**VEHICLE:** CREDIT      **RESUBMITTED:**  
**DATE:** 11/29/12      **DATE:**  
**BY:** D. DAWSON      **BY:**  
**FOR:** CARL'S JR.      **FOR:**  
**LOCATION:** WILSONVILLE

**NO.:** 189700      **DATE:** 11/29/12  
**PROJECT NO.:** CKE0048      **SHEET #:** 1 OF 1

**APPROVAL SIGNATURE** \_\_\_\_\_ **DATE** \_\_\_\_\_  
By signing, you are validating the dimensions and graphic provided to SignResource and/or you are handling your own installation.





**504.2 Maintenance of exterior doors and openings.** Exterior doors and their function shall not be eliminated without prior approval. Exterior doors that have been rendered nonfunctional and that retain a functional door exterior appearance shall have a sign affixed to the exterior side of the door with the words THIS DOOR BLOCKED. The sign shall consist of letters having a principal stroke of not less than  $\frac{3}{4}$  inch (19.1 mm) wide and at least 6 inches (152 mm) high on a contrasting background. Required fire department access doors shall not be obstructed or eliminated. *Exit* and *exit access* doors shall comply with Chapter 10. Access doors for *high-piled combustible storage* shall comply with Section 2306.6.1.

**504.3 Stairway access to roof.** New buildings four or more stories above grade plane, except those with a roof slope greater than four units vertical in 12 units horizontal (33.3-percent slope), shall be provided with a *stairway* to the roof. *Stairway* access to the roof shall be in accordance with Section 1009.12. Such *stairway* shall be marked at street and floor levels with a sign indicating that the *stairway* continues to the roof. Where roofs are used for roof gardens or for other purposes, *stairways* shall be provided as required for such occupancy classification.

#### SECTION 505 PREMISES IDENTIFICATION

**505.1 Address identification.** New and existing buildings shall have *approved* address numbers, building numbers or *approved* building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Address numbers shall be Arabic numbers or alphabetical letters. Numbers shall be a minimum of 4 inches (101.6 mm) high with a minimum stroke width of 0.5 inch (12.7 mm). Where access is by means of a private road and the building cannot be viewed from the *public way*, a monument, pole or other sign or means shall be used to identify the structure.

**505.2 Street or road signs.** Streets and roads shall be identified with *approved* signs. Temporary signs shall be installed at each street intersection when construction of new roadways allows passage by vehicles. Signs shall be of an *approved* size, weather resistant and be maintained until replaced by permanent signs.

#### SECTION 506 KEY BOXES

**506.1 Where required.** Where access to or within a structure or an area is restricted because of secured openings or where immediate access is necessary for life-saving or fire-fighting purposes, the *fire code official* is authorized to require a key box to be installed in an *approved* location. The key box shall be of an *approved* type and shall contain keys to gain necessary access as required by the *fire code official*.

**Exception:** Pharmacies in accordance with OAR 855-041-0035.

**506.1.1 Locks.** An *approved* lock shall be installed on gates or similar barriers when required by the *fire code official*.

**506.2 Key box maintenance.** The operator of the building shall immediately notify the *fire code official* and provide the new key when a lock is changed or rekeyed. The key to such lock shall be secured in the key box.

#### SECTION 507 FIRE PROTECTION WATER SUPPLIES

**507.1 Required water supply.** An *approved* water supply capable of supplying the required fire flow for fire protection shall be provided to premises upon which facilities, buildings or portions of buildings are hereafter constructed or moved into or within the jurisdiction.

**507.2 Type of water supply.** A water supply shall consist of reservoirs, pressure tanks, elevated tanks, water mains or other fixed systems capable of providing the required fire flow.

**507.2.1 Private fire service mains.** Private fire service mains and appurtenances shall be installed in accordance with NFPA 24.

**507.2.2 Water tanks.** Water tanks for private fire protection shall be installed in accordance with NFPA 22.

**507.3 Fire flow.** Fire flow requirements for buildings or portions of buildings and facilities shall be determined by an *approved* method. See Appendix B.

**507.4 Water supply test.** The *fire code official* shall be notified prior to the water supply test. Water supply tests shall be witnessed by the *fire code official* or *approved* documentation of the test shall be provided to the *fire code official* prior to final approval of the water supply system.

**507.5 Fire hydrant systems.** Fire hydrant systems shall comply with Sections 507.5.1 through 507.5.6. See Appendix C.

**507.5.1 Where required.** Where a portion of the facility or building hereafter constructed or moved into or within the jurisdiction is more than 400 feet (122 m) from a hydrant on a fire apparatus access road, as measured by an *approved* route around the exterior of the facility or building, on-site fire hydrants and mains shall be provided where required by the *fire code official*.

##### Exceptions:

1. For Group R-3 and Group U occupancies, the distance requirement shall be 600 feet (183 m).
2. For buildings equipped throughout with an *approved automatic sprinkler system* installed in accordance with Section 903.3.1.1 or 903.3.1.2, the distance requirement shall be 600 feet (183 m).

**507.5.2 Inspection, testing and maintenance.** Fire hydrant systems shall be subject to periodic tests as required by the *fire code official*. Fire hydrant systems shall be maintained in an operative condition at all times and shall be repaired where defective. Additions, repairs, *alterations* and servicing shall comply with *approved* standards.





COMcheck Software Version 3.9.1

# Exterior Lighting Compliance Certificate

## 2010 Oregon Energy Efficiency Specialty Code

### Section 1: Project Information

Project Type: **New Construction**

Project Title : Carl's Jr Wilsonville Oregon

Exterior Lighting Zone: **2 (Neighborhood business district)**

Construction Site:

Intersection of SW 95th Ave  
and Commerce Circle  
Wilsonville, OR 97070

Owner/Agent:

Josh Ventjeer  
Wilsonville Devco LLC  
PO bOX 916  
Portland, OR 97207  
503-688-9600  
josh@ijpcorp.com

Designer/Contractor:

Craig Anderson  
CB Anderson Architects PLLC  
7209 Greenwood Ave N  
Seattle, WA 98103  
206-782-2911  
craig@cba-arch.com

### Section 2: Exterior Lighting Area/Surface Power Calculation

A Exterior Area/Surface	B Quantity	C Allowed Watts / Unit	D Tradable Wattage	E Allowed Watts (B x C)	F Proposed Watts
2 Windows/Doors (Drive-up windows/doors)	2 windows or doors	400	No	800	128
south entrance (Entry canopy)	48 ft2	0.25	Yes	12	170
east entrance (Entry canopy)	35 ft2	0.25	Yes	9	128
Illuminated length of facade wall or surface	24 ft	2.5	No	60	1100
W-W-E Walkway (Walkway < 10 feet wide)	75 ft of walkway length	0.7	Yes	53	1536
E-N-W Walkway (Walkway < 10 feet wide)	115 ft of walkway length	0.7	Yes	81	294
parking area (Parking area)	13999 ft2	0.06	Yes	840	1375
Total Tradable Watts* =				994	3503
Total Allowed Watts =				1854	
Total Allowed Supplemental Watts** =				600	

\* Wattage tradeoffs are only allowed between tradable areas/surfaces.

\*\* A supplemental allowance equal to 600 watts may be applied toward compliance of both non-tradable and tradable areas/surfaces.

### Section 3: Exterior Lighting Fixture Schedule

A Fixture ID : Description / Lamp / Wattage Per Lamp / Ballast	B Lamps/ Fixture	C # of Fixtures	D Fixture Watt.	E (C X D)
<b>2 Windows/Doors ( Drive-up windows/doors 2 windows or doors): Non-tradable Wattage</b>				
Linear Fluorescent 1: S: 4 ft Strip fixture / 48" T8 32W (Super T8) / Electronic	2	2	64	128
<b>south entrance ( Entry canopy 48 ft2): Tradable Wattage</b>				
Linear Fluorescent 2: T: 4 ft Strip wet location / 48" T8 32W (Super T8) / Electronic	2	2	64	128
Compact Fluorescent 1: N: Entrance Recessed Downlight / Triple 4-pin 42W / Electronic	1	1	42	42
<b>east entrance ( Entry canopy 35 ft2): Tradable Wattage</b>				
Linear Fluorescent 3: 48" T8 32W (Super T8) / Electronic	2	2	64	128
Compact Fluorescent 2: Triple 4-pin 42W / Electronic	1	0	42	0
<b>Illuminated length of facade wall or surface (24 ft): Non-tradable Wattage</b>				
HID 1: Metal Halide 250W / Standard	1	4	275	1100
<b>W-W-E Walkway ( Walkway &lt; 10 feet wide 75 ft of walkway length): Tradable Wattage</b>				
Linear Fluorescent 4: 48" T8 32W (Super T8) / Electronic	2	24	64	1536

E-N-W Walkway ( Walkway < 10 feet wide 115 ft of walkway length): Tradable Wattage				
Compact Fluorescent 3: U2: Compact Fluorescent / Triple 4-pin 42W / Electronic	1	7	42	294
parking area ( Parking area 13999 ft2): Tradable Wattage				
HID 2: P1: Single Shoe box / Metal Halide 250W / Standard	1	4	275	1100
HID 3: P2: Douvble Shoe box / Metal Halide 250W / Standard	2	1	275	275
Total Tradable Proposed Watts =				3503

## Section 4: Requirements Checklist

In the following requirements, the relevant code section reference is shown in [ ]. '+' denotes that more details exist in the specified code section. Checkboxes identify requirements that the applicant has not acknowledged as being met. Check marked requirements identify those the applicant acknowledges are met or excepted from compliance. 'Plans reference page/section' identifies where in the plans/specs the requirement can be verified as being satisfied.

### Lighting Wattage:

1. [505.6.2 +] Within each non-tradable area/surface, total proposed watts must be less than or equal to total allowed watts. Across all tradable areas/surfaces, total proposed watts must be less than or equal to total allowed watts.

Complies: Invalid number of fixtures

### Controls, Switching, and Wiring:

2. [505.6.2 +] All exemption claims are associated with fixtures that have a control device independent of the control of the nonexempt lighting.

Plans reference page/section: \_\_\_\_\_

3. [505.2.4 +] Lighting not designated for dusk-to-dawn operation is controlled by either a a photosensor (with time switch), or an astronomical time switch.

Plans reference page/section: \_\_\_\_\_

4. [505.2.4 +] Lighting designated for dusk-to-dawn operation is controlled by an astronomical time switch or photosensor.

Plans reference page/section: \_\_\_\_\_

5. [505.2.4 +] All time switches retain programming and the time setting during loss of power for a period of at least 10 hours.

Plans reference page/section: \_\_\_\_\_

6. [505.2.4 +] Lighting designated to operate more than 2000 hours per year for Uncovered Parking Areas shall be equipped with motion sensors that will reduce the luminaire power by thirty-three percent or turn off one-third the luminaires when no activity is detected.

Plans reference page/section: \_\_\_\_\_

### Exterior Lighting Restrictions and Exceptions:

7. [505.6.1 +] Mercury vapor and incandescent lighting is not permitted for use as exterior lighting.

Exception(s):

Incandescent lighting controlled by motion sensors and having total power less than 150 watts.

Incandescent lighting used in or around swimming pools, water features, or other locations subject to the requirements of Article 680 of the National Electric Code.

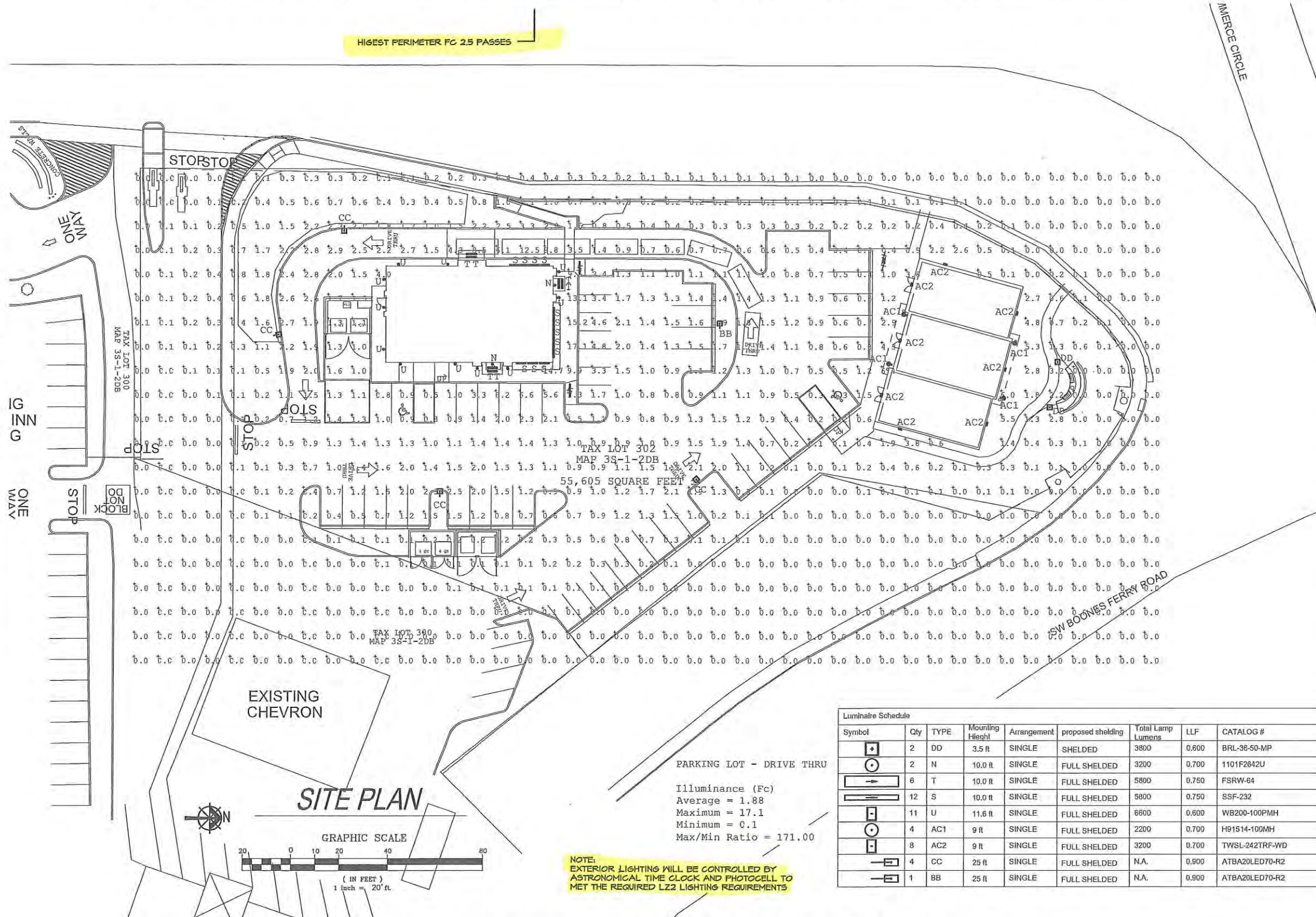
8. [505.6.1 +] Exempt lighting fixtures are equipped with a control device independent of the control of the nonexempt lighting and are identified in Section 3 table above.

Plans reference page/section: \_\_\_\_\_





FILE NAME:



Luminaire Schedule										
Symbol	Qty	TYPE	Mounting Height	Arrangement	proposed shielding	Total Lamp Lumens	LLF	CATALOG #	Proposed Wallage	Max. wattage per light zone L22
	2	DD	3.5 ft	SINGLE	SHELD	3800	0.600	BRL-36-50-MP	32	35
	2	N	10.0 ft	SINGLE	FULL SHELD	3200	0.700	1101F2642U	42	100
	6	T	10.0 ft	SINGLE	FULL SHELD	5800	0.750	FSRW-64	(2) 42	100
	12	S	10.0 ft	SINGLE	FULL SHELD	5800	0.750	SSF-232	(2) 42	100
	11	U	11.6 ft	SINGLE	FULL SHELD	6600	0.600	WB200-100PMH	100	100
	4	AC1	9 ft	SINGLE	FULL SHELD	2200	0.700	H91514-100MH	100	100
	8	AC2	9 ft	SINGLE	FULL SHELD	3200	0.700	TWSL-242TRF-WD	(2) 42	100
	4	CC	25 ft	SINGLE	FULL SHELD	N.A.	0.900	ATBA20LED70-R2	N.A.	100
	1	BB	25 ft	SINGLE	FULL SHELD	N.A.	0.900	ATBA20LED70-R2	N.A.	100

PHOTOMETRIC SITE PLAN  
SCALE: 3/32"=1'-0"

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

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**ARCHITECTS**  
CB ANDERSON ARCHITECTS LLC  
7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2911  
Fax 782-5624

**CLIENT**  
Wilsonville Devco LLC  
c/o Integrated Investment Partners  
PO Box 916  
Portland, OR 97207  
(503) 688-9620

**PROJECT**  
Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**DRAWING TITLE**  
PHOTOMETRIC SITE PLAN

**REVISIONS**

**STAMP**

DATE: December 5, 2012  
BLOC SUBMITTAL DATE:   
DATE:   
DATE:   
DATE:

BO DATE:  
CKE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

**SE1.0**





## **SFA Design Group, LLC**

STRUCTURAL | CIVIL | LAND USE PLANNING | SURVEYING

January 21, 2013

Daniel Pauley, Associate Planner  
City of Wilsonville  
29799 SW Town Center Loop E.  
Wilsonville, OR 97070

RE: Response to Letter of Incomplete Application, Boones Ferry Pointe (Carl's Jr.).

Dear Daniel:

The following narrative and attached revised documents represent our response to your letter of Incomplete Application for the Boones Ferry Pointe, DB12-0074-DB12-0076.

1. *Pursuant to Section 4.011 documentation that no liens exist on the property.*

### **RESPONSE**

We have provided an updated Title Report, which reflects that no liens (City or other) were identified.

2. *Pursuant to Subsection 4.140 (.03) A. signature of the owner of all tracts of land included in the application. A portion the landscape area west of the Carl's Jr. restaurant is on the property owned by Lapoint Business Group LLC. Mr. Lapoint needs to be a joint applicant to include development on his land.*

### **RESPONSE**

Garry LaPoint, LaPoint Business Group, is the owner of the adjacent Chevron- C-Store (Tax Lot 300). He has signed the application as a Co-Applicant, which covers land he owns within the access/egress area, and also provides for a Coordinated Master Sign Plan.

3. *Pursuant to Subsection 4.035 (.04) A. 6 sheet A1.0 "Architectural Site Plan" at a standardized scale. The scale on the full sized sheet indicated it is 1 inch equals 20 feet. However, the sheet was reproduced in a manner that is no longer 1:20 scale.*

### **RESPONSE**

Drawing A1.0 has been reprinted to the 24" x 36" paper size to correctly reflect the 1:20 scale.

## **Parking and Circulation**

4. Pursuant to Subsection 4.140 (.09) D., 4.155 (.03) B. 5., 4.177 (.01) E. 3., 4.421(.01) C. and 4.430 (.02) G., drawings to scale indicating circulation, parking, and turning movements, as applicable, for the following as well as the related access easements:

- Passenger vehicles
- Fire and rescue apparatus
- Fuel delivery trucks
- Delivery trucks for restaurant and multi-tenant commercial building
- Busses, RV's, trailers, and other large format vehicles
- Pedestrians (circulation internally, and from Holiday Inn for guests and potentially drivers/passengers of larger format vehicles)

*Note: Mitigation and Recommendations of the DKS traffic report also suggests the applicant provide turning templates. Specifically it asks the turning templates show how passengers from drive-thru can make 180 degree right turn without extending into the oncoming traffic lanes and show how trucks can stay in north bound lanes turning out onto 95<sup>th</sup> Avenue as well as likely on-site truck maneuvering.*

## **RESPONSE**

A new Civil Drawing, (DD5) has been provided that reflects the various vehicle turn radii.

5. Pursuant to Subsection 4.421 (.01) C. documentation regarding the recommendation on page 20 of the DKS Traffic Report to move back the stop bars at various locations to limit pedestrian/vehicle conflicts.
6. Pursuant to Subsection 4.421 (.01) C. demonstrating special attention given to pedestrian/vehicle conflicts in the drive-thru lane. The pedestrian crossing of the drive-thru lane is a high risk location for pedestrian/vehicle conflicts. Striping can fade over time, so staff recommends articulating the crossing by making it concrete or otherwise a different material from the drive lane to create a very durable and visible demarcation of the pedestrian crossing.

## **RESPONSE**

The details of the site plan, including access circulation and stop bar locations have been designed consistent with the executed Development Agreement. The Development Agreement was executed after the DKS report was prepared and the negotiated design details were prepared in response to the DKS comments, as well as in response to the concerns of the other signers.

Staff raised a concern that the pedestrian access that crosses the drive-up lane was a high risk location, and that typical painted striping tends to fade over time making it less visible.

We disagree that this is a high risk crossing. This crossing is expected to have very low usage, given the typical amount of foot traffic in the area. In addition it is crossing a drive-up lane where vehicles are moving very slowly and often stopped in a queue line.

Never-the-less, to ensure pedestrian safety the cross-walk has been modified to enhance its visibility and to provide more long lasting and durable markings. The cross-walk will be stamped asphalt to create a clearly visible pattern. The stamped pattern will also extend the durability of the painted stripping. In addition vertical cross-walk signs (flexible posts) will be installed to emphasize the crossing's visibility.

### Signs

7. *Pursuant to Subsections 4.156.02 (.05) D. 2. and 5., (.06) B., (.07) A. and C., 4.156.08 (.01) C., C. 1., and C. 1. b. materials supporting the collocation of the Carl's Jr. sign on the relocated freestanding Chevron sign. Information is not provided demonstrating how the proposed sign satisfies the current sign regulations. The sign, as proposed, likely requires modification to the Chevron Master Sign Plan and a waiver to the allowed sign area. Pursuant to the development agreement also include information on the sign, access, and maintenance agreement and approval from Chevron corporate for the Carl's Jr. sign to be collocated. The collocation of the Carl's Jr. sign may require a building permit to review the engineering of the existing pole sign structure with the additional load.*
8. *Pursuant to Subsection 4.156.02 (.07) and (.07) A. 1. definition of flexibility, including placement, for signage in the scenario of future rebranding or tenant change for Carl's Jr. restaurant.*
9. *Pursuant to Subsections 4.156.02 (.07) A. 3., B. 1. and B. 2. more definition and guidance on materials, design, and placement of building signs. The goal is appropriate guidelines to ensure consistent and compatible sign design while allowing the "right" amount of flexibility. Also, consideration needs to be given to sign placement for different tenant configurations in the multi-tenant building (scenarios where building is occupied by 1, 2, or 3 tenants as stated on page 8 of the submitted narrative). Subsection 4.440 (.01) D. requires, the location, size, design, material, color and methods of illumination of all exterior signs, including those on retail building be identified.*

### **RESPONSE**

The owner of the Chevron station has signed the application as a Co-Applicant. The Master Sign Plan has been revised to incorporate the existing freestanding Chevron signs. However, with this revision it is now necessary to request a Waiver from the Code for total sign area allowed, as addressed below.

The revised Master Plan provides more details relative to the allowed tenant signage including maximum area, locations illumination, etc., as addressed in more detail below.

10. Pursuant to Subsection 4.156.03 (.01) B. define shapes used to measure area of odd shaped signs consistent with this subsection, specifically the Carl's Jr. sign on top of the proposed new freestanding sign and Carl's Jr. wall signs.

## RESPONSE

The area of the north freestanding sign has been calculated using rectangles and circles.

11. Pursuant to 4.156.02 (.05) D. 2. information demonstrating the proposed monument and freestanding signs on the northern part of the site can be constructed within the easements in which they are shown. Alternatively, the signs can be relocated outside the easements.

## RESPONSE

The applicant determined that the original sign location encroachment effected two easements:

1. The private utility easement for LaPoint's Chevron station; and
2. ODOT's slope and utilities easement, which as of February 28, 2011 was transferred to the City of Wilsonville.

Mr. LaPoint is a joint applicant and benefits from the proposed sign, so he is cooperative in allowing the sign encroachment. However, because the LaPoint easement contains active utilities, we have relocated the sign, so that it now only encroaches in the City's slope easement, which has no active utilities within it.

The applicant is prepared to enter into an agreement with the City to determine the ultimate location of the sign, and further to hold the City harmless relative to any potential additional costs that might be associated with normal and necessary maintenance within the easement, due to the sign's location. Therefore we request that a Condition of Approval be added, requiring such resolution or relocation of the sign by agreement with the City.

12. Pursuant to Subsections 4.156.02 (.05) D. 2. and 5., and 4.156.05 (.02) E. information addressing sign code compliance of the drive-thru menu board. In order to be exempt from sign permitting requirements the drive-thru menu board needs to be screened from off-site view. Landscaping or fencing meeting the high screen standard (Subsection 4.176 (.02) F.) would need to be used for a sufficient distance along 95<sup>th</sup> Avenue to block off-site view of the menu board. The landscaping is required to be 6 feet tall and 95% opaque year-round within three (3) years of planting. Alternatively, the sign could be included in the Master Sign Plan request as a non-exempt sign, which would require waivers to the sign regulations.

## RESPONSE

The Landscaping Plan has been revised to provide screening consistent with Section 4.176(.02) F., thereby exempting the menu board from the sign area calculations.

13. Pursuant to Subsection 4.156.08 (.01) K. information on building addresses for freestanding signs. All freestanding signs need to include addresses of related buildings unless otherwise approved in writing by the City and Fire District.

## **RESPONSE**

Building addressing will be added to the freestanding signs, as well as on the buildings.

14. Pursuant to Subsection 4.156.08 (.03) B. information reflecting the area of the monument sign being subtracted from the allowed area for freestanding signs. Please show this in sign tables and calculations for the Master Sign Plan.

## **RESPONSE**

The Site Monument Sign (Boones Ferry Pointe) has been factored into the sign area for the freestanding signs, as addressed in the Table 1 below.

### Revised Master Sign Plan

The application Compliance Narrative and Master Sign Plan Exhibits have been modified to provide for a Coordinated Master Sign Plan including, Carl's Jr., the new Retail Building (3-tenants), the Chevron Station, and the Coca Cola Fountain Mart (C-Store).

The Master Sign Plan incorporates the two existing freestanding Chevron Signs, with the addition of the new signage for Carl's Jr. and the Retail Building. The package includes the following:

- Two existing Chevron freestanding signs, with LED price panels.
  - One faces Boones Ferry Road northeast of the C-Store.
  - One faces 95<sup>th</sup> Avenue just north of the driveway.
    - Relocated to refuge island in modified driveway
    - Carl's Jr. sign box added
- One new freestanding sign for the complex located north of the Retail Building.
  - Includes Carl's Jr.; 3 –Retail Tenants; and Coca Cola Fountain Mart (C-Store)
- A complex Monument Sign (Boones Ferry Pointe) located on the curved plaza wall.
- Addressing on the freestanding signs and buildings.
- Building signs on each elevation.

The Plan specifies the location for each sign, together with the maximum allowed area, cabinet style, and back lighting requirements.

## Historical Perspective

In order to understand the proposed coordinated Master Sign Plan it is important to provide some historical perspective. At the time the Chevron property was originally planned (Exxon) and developed it was combined under the same ownership with the Carl's Jr. site. The original approved plans envisioned a second phase on the northern section of the property. The existing Chevron sign on 95<sup>th</sup> Avenue was placed for best visibility, with combined signage for Phase 2 envisioned.

Subsequent to the initial site development, as an Exxon station, the northern portion of the property was partitioned off and sold. The partitioning and sale included an agreement for the existing sign to remain (now off-site), still anticipating combined signage for the future development.

Then prior to the submittal for the currently proposed development (Carl's Jr & Retail Building) there was considerable negotiations between the adjoining property owners and the City, which resulted in a Development Agreement. The Agreement included combined use of the existing freestanding sign, as well as relocating it to accommodate the modifications to the ingress/egress configuration.

In addition, subsequent to the approval of the Chevron Master Sign Plan (DB07-0056) the City amended the Sign Code. The amended Code changed the amount and method of calculating allowed signage. It also separated the allowed signage providing different standards for freestanding versus building signage, and modified Master Sign Plan provisions.

The Code revisions overall eliminated many identified problems with the old standards, and improved general administration. However, unfortunately, the new freestanding sign provisions make it a little more difficult to provide for combined signage for the two properties. Therefore it has been determined that a Waiver is necessary to accommodate the proposed coordinated freestanding signs, including the Site Monument.

First, the new Code allows one freestanding sign, with the first 200 feet of street frontage. Then it allows a second sign, if the property has at least 100 feet of frontage on a second street.

The Carl's Jr. site has approximately 460 linear feet of frontage along 95<sup>th</sup> Avenue and another 180 feet of frontage along Boones Ferry Road. Therefore this property qualifies for two freestanding signs under the new Code.

The Chevron site only has 11 feet of frontage on 95<sup>th</sup> Avenue, but also has approximately 230 feet of frontage on Boones Ferry Road. It technically only qualified for one freestanding sign under the old Code, but was allowed the second sign based on the original frontage, prior to the Carl's Jr. site being partitioned.

However, under the new Code, if the two sites were combined for master plan purposes, they would still only qualify for two freestanding signs. Recognizing the Chevron's existing two signs the Development Agreement included a provision to add a Carl's Jr. sign box to the existing 95<sup>th</sup> Avenue sign. As a reciprocal action, the applicant agreed to allow Mr. LaPoint to add a sign panel on the proposed new north sign to identify the Coca Cola Fountain Mart C-Store.

It is noted that with the original Exxon approval, the two signs included the Fountain Mart. But when the station was converted to Chevron and the new LED price signs were approved the Fountain Mart panels were dropped.

Second, the new Code sets new standards for the sign area allowed for freestanding signs, based on building square footage as follows:

Allowed Freestanding Sign Area: 32 sq ft, Plus 3 sq ft, + 3/1000, rounded to nearest 1,000 sq. ft.  
Total 6 Tenants, with Chevron and C-Store

Freestanding Signs

**TABLE 1  
Allowed and Proposed  
Freestanding Signs**

<b>Tenants (# tenants)</b>	<b>Building Area Sq. Ft.</b>	<b>Additional Sign Area Allowed over Base Sq. Ft.</b>	<b>Proposed 95<sup>th</sup> Avenue West</b>	<b>Proposed Boones Ferry North</b>
Base Area		32		
Carl's Jr. (1)	2,867	12	<b>32</b>	<b>18</b>
Retail Building (3)	3,150	12	<b>0</b>	<b>18</b>
BFP Monument		0	<b>0</b>	<b>13</b>
Chevron Canopy (1)	3,536	15	<b>82 Existing</b>	
C-Store (1)	2,267	9	<b>0</b>	<b>12</b>
Total		80 65 less Chevron	<b>114</b>	<b>61</b>

1. The Chevron sign has 3 exempt fuel price panels = 33 sq. ft.
2. A separate freestanding sign for Carl's Jr. could be 44 sq. ft.

Based on the summary table, the proposed new north sign complies with the new standards, including the Boones Ferry Pointe monument sign and accounting for 5 tenants, Carl's, 3-Retail and the Fountain Mart C-Store.

However, the 95<sup>th</sup> Avenue sign is 2 square feet over the allowed area just with the existing Chevron sign. Adding the Carl's Jr. sign puts this sign 34 square feet of the allowed area.

If no consideration for joint signage were considered the Carl's Jr. site would be eligible for a 44 square foot sign just by itself, or up to an 56 square foot sign for the 4 tenants anticipated in the 2 building complex.

The applicants believe it is better to combine the signage than to add an additional sign. Therefore we are requesting a Waiver to the allowed area for the 95<sup>th</sup> Avenue sign, thus allowing for the addition of the 32 square foot Carl's Jr. sign box on the east side of the post. The additional application fee of \$160 dollars for the Waiver is covered by available credits.

### Waiver Criteria

*(.08) Waivers and Variances. Waivers and variances are similar in that they allow deviation from requirements such as area, and height from ground. They differ in that waivers are granted by the DRB as part of a comprehensive review of the design and function of an entire site to bring about an improved design and variances are granted by either the Planning Director or DRB to relieve a specific hardship caused by the regulations.*

*A. Waivers. The DRB may grant waivers for sign area, sign height from ground (no waiver shall be granted to allow signs to exceed thirty-five (35) feet in height), number of signs, or use of electronic changeable copy signs in order to better implement the purpose and objectives of the sign regulations as determined by making findings that all of the following criteria are met:*

- 1. The waiver will result in improved sign design, in regards to both aesthetics and functionality.*
- 2. The waiver will result in a sign or signs more compatible with and complementary to the overall design and architecture of a site, along with adjoining properties, surrounding areas, and the zoning district than signs allowed without the waiver.*
- 3. The waiver will result in a sign or signs that improve, or at least do not negatively impact, public safety, especially traffic safety.*
- 4. Sign content is not being considered when determining whether or not to grant a waiver.*

### **REPSONSE**

The waiver is necessary to account for the existing approved Chevron signs, while adding a Carl's Jr. sign to the 95<sup>th</sup> Avenue sign and a Coca Cola Fountain Mart sign to the new north freestanding sign. The Waiver is further required to allow for factoring in the proposed Complex Monument sign. The combined signage results in improved coordinated signage for the combined uses, while maintaining aesthetics and functionality.



The waiver will result in a more coordinated sign program for the 6 tenants occupying the two properties. The coordinated design provided for complementary design and a reduced number of independent signs. The coordinated design is complementary to the surrounding architecture adjoining properties, surrounding areas, and the zoning district than signs that would be allowed without the waiver.

The waiver will result in a coordinated sign package that provides good identity for all on-site tenants while not negatively impacting public safety, especially traffic safety.

The Waiver allows for a Coordinate Master Plan, which defines allowable signage for a variety of existing or future tenants, without limiting content as a basis for granting the waiver.

Therefore all Waiver Approval Criteria are met.

### Building Signs

The applicant has provided specific sign details for the Carl's Jr. building and generic sign areas designated on the Retail Building, anticipating up to 3 tenants. After reviewing signage with the planning staff, it was determined more appropriate for the Master Sign Plan to define generic allowed signage for both buildings. This framework would allow for consideration of different tenants or franchises without need to amend the Sign Plan.

The revised Building Sign Plan provides for the following sign areas, as reflected on the Building Elevation drawings.

The Carl's Jr. Building has two facades that are 40 feet wide and two that are 70 feet long. The building is designed to accommodate a single tenant. The east and west elevations are allowed 36 square feet, while the north and south elevations are also allowed 36 square feet.

The proposed sign locations for this building are shown on Elevation Drawings A4.0 & A5.0, which allocates a maximum of 36 square feet to each of the four elevations for potential future tenants.

However, the actual proposed Carl's Jr. signage is shown in the Master Sign Plan set, which allocates 30 square feet to three of the elevations, and 6.25 square feet to the south elevation.

The Retail Building has two facades that are 45 feet wide and two that are 70 feet long. The building is designed to accommodate up to three tenants, with anticipated tenant facades of 25 and 2 @ 22.5 feet. The two end tenants would each have either the east or west facade available for another sign.

With these building dimensions the new Code allows:

1. Up to 36 square feet of signage on each elevation for the Carl's Jr. building.
2. The Retail building is allowed:
  - a. 24 square feet per tenant, on each elevation, assuming 3;
  - b. 32 square feet if two tenants; or
  - c. 36 square feet per elevation if a single tenant.

**TABLE 2  
Allowed and Proposed Building Signage**

<b>Building</b>	<b>Façade</b>	<b>Sign Location</b>	<b>Maximum Sign Area</b>	<b>Proposed Sign Area</b>
Carl's Jr	North	Over door on Tower	36	36
	West	Over Drive-up Window on Tower	36	36
	South	Over door on Tower	36	26
	East	Over door on Tower	36	36
<b>Building</b>	<b>Façade</b>	<b>Sign Location</b>	<b>Maximum Sign Area</b>	<b>Proposed Sign Area</b>
Retail	North	Over Windows	24-36	24
	West	Over Windows	36	36
	South	Over Doors	24-36	24
	East	Over Windows	36	36

**Landscaping**

15. *Pursuant to Subsection 4.155 (.03) B. 2. information demonstrating all landscape islands meet the eight foot by eight foot (8' x 8') dimensional standard. One (1) landscape island immediately northeast of the Carl's Jr. restaurant does not meet the dimensional standard.*

**RESPONSE**

Only one planter island, which is located at the northeast corner of Carl's Jr. building was undersized. This planter has been increased to provide the minimum 8'x 8' planter area.

16. *Pursuant to Subsection 4.176 (.06) A. 3. information demonstrating the maximum area of turf or lawn is not exceeded. Turf or lawn cannot be used to cover more than ten percent (10%) of the landscaped area. The proposed landscape plan shows well in excess of ten percent (10%) of the landscaped area in turf grass.*

## RESPONSE

The Landscaping Plan includes the water quality pond, and provides a total of 16,139 square feet of planted area. This amount of landscaping allows for a maximum (@ 10%) of 1,614 square feet of lawn area.

However, in conversations with Planning Staff it has been determined that, given the corner lot and large area of street frontage, it would be appropriate to allow more than just the minimum lawn area.

Based on staff comments, the Landscaping Plan has been modified to increase ground cover and storm swale plantings. This reduces the lawn area to 2,080 square feet or 13% of the total landscaped area.

17. Pursuant to Subsection 4.176 (.02) D. accurate information regarding street tree requirements along 95<sup>th</sup> Avenue. The section of 95<sup>th</sup> Avenue along the property as well as Boones Ferry Road are arterials. The submitted narrative identifies 95<sup>th</sup> as a collector. Street trees need to be 3" caliper along these streets rather than 2" caliper shown in the landscape plans.

## RESPONSE

The Landscaping Plan has been modified to increase the street trees to 3 inch caliper.

### Outdoor Lighting

18. Pursuant to Subsections 4.199.40 (.01) A. and 4.199.50 (.01) C. information on all outdoor luminaires. Cut sheets need to be provided for, and lighting plans and other compliance documents need to show all outdoor luminaires including building mounted lighting such as those shown on Sheets A5.0 and AC2.1
19. Pursuant to Subsections 4.199.40 (.01) B. 2. and 4.199.50 (.01) D. plan check sheet demonstrating compliance with the *Oregon Energy Efficiency Specialty Code, Exterior Lighting* for the proposed outdoor lighting.
20. Pursuant to Subsection 4.199.40 (.01) B. 3. information on mounting height for all building mounted luminaires.
21. Pursuant to Subsection 4.199.40 (.01) B. 1. information demonstrating that all proposed outdoor luminaires comply with Table 7 of the Outdoor Lighting Ordinance. This includes the specific wattage and shielding of each proposed outdoor luminaire. Staff requests this be in a table format similar to the table below. If other design considerations conflict with this requirement, you may want to explore demonstrating compliance with the Outdoor Lighting Ordinance using the performance method.

*Example of Table Demonstrating Compliance with Table 7 of the Outdoor Lighting Ordinance*

<i>Luminaire</i>	<i>Proposed Shielding</i>	<i>Proposed Wattage</i>	<i>Maximum Wattage Based on Shielding: Lighting Zone LZ2</i>
<i>Luminaire A</i>	<i>Fully Shielded</i>	<i>100</i>	<i>100</i>
<i>Luminaire B</i>	<i>Fully Shielded</i>	<i>100</i>	<i>100</i>
<i>Luminaire C</i>	<i>Shielded</i>	<i>25</i>	<i>35</i>
<i>Luminaire D</i>	<i>Partly Shielded</i>	<i>35</i>	<i>39</i>
<i>Luminaire E</i>	<i>Shielded</i>	<i>25</i>	<i>35</i>
<i>Luminaire F</i>	<i>Fully Shielded</i>	<i>75</i>	<i>39</i>

**RESPONSE**

Additional details and revised exhibit drawings have been provided regarding proposed outdoor lighting, including building mounted lights. The applicant proposes to meet the requirements of this Section through the Performance Option. Parking lot lighting has been changed to LED, and Auto-Dimmers will be installed to reduce lighting levels after 10:00 PM to 50%.

***Section 4.199.50. Submittal Requirements.***

*(.01) Applicants shall submit the following information as part of DRB review or administrative review of new commercial, industrial, multi-family or public facility projects:*

- A. A statement regarding which of the lighting methods will be utilized, prescriptive or performance, and a map depicting the lighting zone(s) for the property.*
- B. A site lighting plan that clearly indicates intended lighting by type and location. For adjustable luminaires, the aiming angles or coordinates shall be shown.*
- C. For each luminaire type, drawings, cut sheets or other documents containing specifications for the intended lighting including but not limited to, luminaire description, mounting, mounting height, lamp type and manufacturer, lamp watts, ballast, optical system/distribution, and accessories such as shields.*
- D. Calculations demonstrating compliance with Oregon Energy Efficiency Specialty Code, Exterior Lighting, as modified by Section 4.199.40(.01)(B.)(2.) [Amended by Ord. 688, 11/15/10]*
- E. Lighting plans shall be coordinated with landscaping plans so that pole lights and trees are not placed in conflict with one another. The location of lights shall be shown on the landscape plan. Generally, pole lights should not be placed within one pole length of landscape and parking lot trees.*
- F. Applicants shall identify the hours of lighting curfew.*

*(.03) In addition to the above submittal requirements, Applicants using the Performance Method shall submit the following information as part of the permit set plan review:*

- A. Site plan showing horizontal isocandle lines, or the output of a point-by-point computer calculation of the horizontal illumination of the site, showing property lines and light levels immediately off of the subject property.*
- B. For each side of the property, the output of a point-by-point vertical footcandle calculation showing illumination in the vertical plane at the property line from grade to at least 10 feet higher than the height of the tallest pole.*
- C. Lighting plans shall be prepared by a qualified licensed engineer.*

## **RESPONSE**

Additional Photometric Plans have been provided. The revised site plan shows horizontal isocandle lines, (point-by-point computer calculation of the horizontal illumination), showing property lines and light levels immediately off of the subject property.

For each side of the property, the output of a point-by-point vertical footcandle calculation also shows illumination in the vertical plane at the property line from grade to at least 10 feet higher than the height of the tallest pole.

Lighting plans have been prepared by a qualified licensed engineer.

### **Mixed Solid Waste and Recycling Storage Area**

22. Pursuant to Subsection 4.430 (.04) A. a letter from allied waste approving mixed solid waste and recycling areas and access.

## **RESPONSE**

The applicant has made minor revisions to the trash enclosures based on comments from Allied Waste, and has obtained a letter from Allied Waste approving the revised trash enclosure designs, see attached.

- a. The hinges have been designed as 180 degree hinges regardless of the door swing shown. We are showing some stops at 120 degrees right now at the Chevron and the retail trash enclosures, but they can be moved to a larger degree if desired.
  - b. The minimum width for access to the trash enclosure is 12'. So if the trash enclosure is 5'+ feet wider, it seems to me that 90 degrees is not a problem.
  - c. The CJ trash enclosure is 17'-6 wide at the opening and used 90 degree stops due to the side walk, building and landscaping. However, it is unrealistic to expect the doors to open at 180 degrees in this location since the building prevents one door and the landscaping requirements the other.
23. Pursuant to Subsection 8.210 (9) Wilsonville Code and Subsections 4.430 (.04) B. information on required covers for mixed solid waste and recycling storage areas. The City's stormwater regulations require the waste and recycling enclosures be covered. Wilsonville Code Subsection 8.210 (9) reads, "Outside storage areas for grease, oil, waste products, recycling, garbage, and other sources of contaminants shall be a covered enclosure adequately sized to allow all containers to be accessible. No drainage is allowed to enter the storm sewer system." Subsection 4.430 (.04) B. requires vertical clearance to be at least eight feet (8').

## **RESPONSE**

Two solid waste and recycling storage areas have been carefully designed to encourage proper use. One enclosure, located at the southeast corner of the Carl's Jr. Building is for that use only. The other enclosure is a double unit, one side for the New Retail Building, and the other side is a replacement (relocated) for the Chevron and C-Store.

The design has been coordinated with Allied Waste Services to ensure convenient access for their collection trucks. It is noted that Allied has been serving the Chevron site for many years and has had problems accessing the structure without conflicting with pump island traffic. Therefore the existing facility will be removed and replaced, thereby improving access.

The design of the two enclosures has been modified to provide the required roof covers to protect the storm water system from contaminants, per Sections 4.430(.04) and 8.210(9).

### **Site Design Review**

24. Pursuant to Subsection 4.440 (.01) D. information on color, texture, and material for glazing on both buildings, detention pond fencing, wall for placement of monument sign, exterior site furnishings such as benches and waste baskets for the public, and covers and related supports for mixed solid waste and recycling enclosures.

## **RESPONSE**

Additional materials details (examples) have been provided. The glazing details for both buildings will be the same, a smoky tinted glass.

The retaining wall in the Pond is to be constructed of Lock & Load block, but the wall is all below grade, Civil Plan DD3. The pond fencing is 4 foot tall black chain link, with no slats, but with climbing vines. The vines will only be on the interior drive-isle side to soften the appearance of the fence from the drive-up lane.

The monument sign is mounted on a screen wall, which is constructed of CMU block. Details for the wall have been added to the Architectural Plans. Additional exhibits have been provided to clearly identify the style for the bench and trash cans at the north plaza. And, details have been provided to show the modified trash enclosures, including roofing materials.

25. Findings for Site Design Review, Subsections 4.400 through 4.440, refer to another development project. Please update these findings to specifically address the subject application.



## RESPONSE

The Site Design Review section has been edited to eliminate any references to the other project. The text and drawings have also been revised to address the roof cover requirement for the enclosures, per Section 8.210(9).

We have also provided an updated Storm Drainage Report, dated 12-31-12, which has been inserted at the Storm Drainage Index Tab.

With these edits, additions and corrections the applicant should be complete.

We are providing ten complete copies of the revised compliance findings and all drawings, included those revised or added, in both reduced and full-sized drawings. We are also providing ten compact digital disks (CD-ROMs) of the submitted findings and all project drawings.

We look forward to working with you on this project. Thank you for your cooperation and assistance. If you have any questions or need additional information, please contact me immediately, so we can respond quickly.

Sincerely,  
SFA Design Group



Ben Altman  
Senior Planner/Project Manager



December 28, 2012

Ben Altman  
SFA Design Group, LLC  
9020 SW Washington Square Dr. Ste. 350  
Portland, OR 97223

Re: Waste & Recycling enclosures for Boones Ferry Pointe

Dear Ben,

Thank you, for sending me the site plans for the new Carl Jr's and Chevron station.

My company; Allied Waste Services Inc. can provide complete waste removal, and recycling services for this location. I still would suggest an opening on the side for some roll carts for glass recycling if it is required. The location for the enclosure will be adequate for my trucks to safely service the containers.

I appreciate your willingness to consider our service issues. Good luck with this project.

Sincerely;

A handwritten signature in cursive script that reads 'Frank J. Lonergan'.

Frank J. Lonergan  
Operations Manager  
Allied Waste of Clackamas &  
Washington Counties

## Ben Altman

---

**From:** Cam Fultz [cam@cba-arch.com]  
**Sent:** Wednesday, December 26, 2012 2:02 PM  
**To:** Ben Altman  
**Cc:** josh@iipcorp.com; 'Chris Kittredge'; Craig B Anderson  
**Subject:** Re: FW: Review of Trash Enclosures - Boones Ferry Pointe  
**Attachments:** 1199 A10.2 Trash Enclosure 2012-12-26.pdf; cj trash plan on site.pdf; other trash on site.pdf

Ben,  
I've attached the full trash enclosure sheet - and updated it.

Please note:

1) that the hinges have always been designed as 180 degree hinges regardless of the door swing shown. We are showing some stops at 120 degrees right now at the Chevron and the retail trash enclosures, but they can be moved to a larger degree if desired.

2) The minimum width for access to the trash enclosure is 12'. So if the trash enclosure is 5'+ feet wider, it seems to me that 90 degrees is not a problem.

3) The CJ trash enclosure is 17'-6 wide at the opening and used 90 degree stops due to the side walk, building and landscaping. It is unrealistic to expect the doors to open at 180 degrees in this location since the building prevents one door and the landscaping requirements the other.

Please ask Frank to revisit the trash enclosure situation.

Best Regards,  
Cameron Fultz



7209 Greenwood Avenue N      206-782-2911  
Seattle Washington 98103      Fax-782-5624

[cam@cba-arch.com](mailto:cam@cba-arch.com)

[www.cba-arch.com](http://www.cba-arch.com)

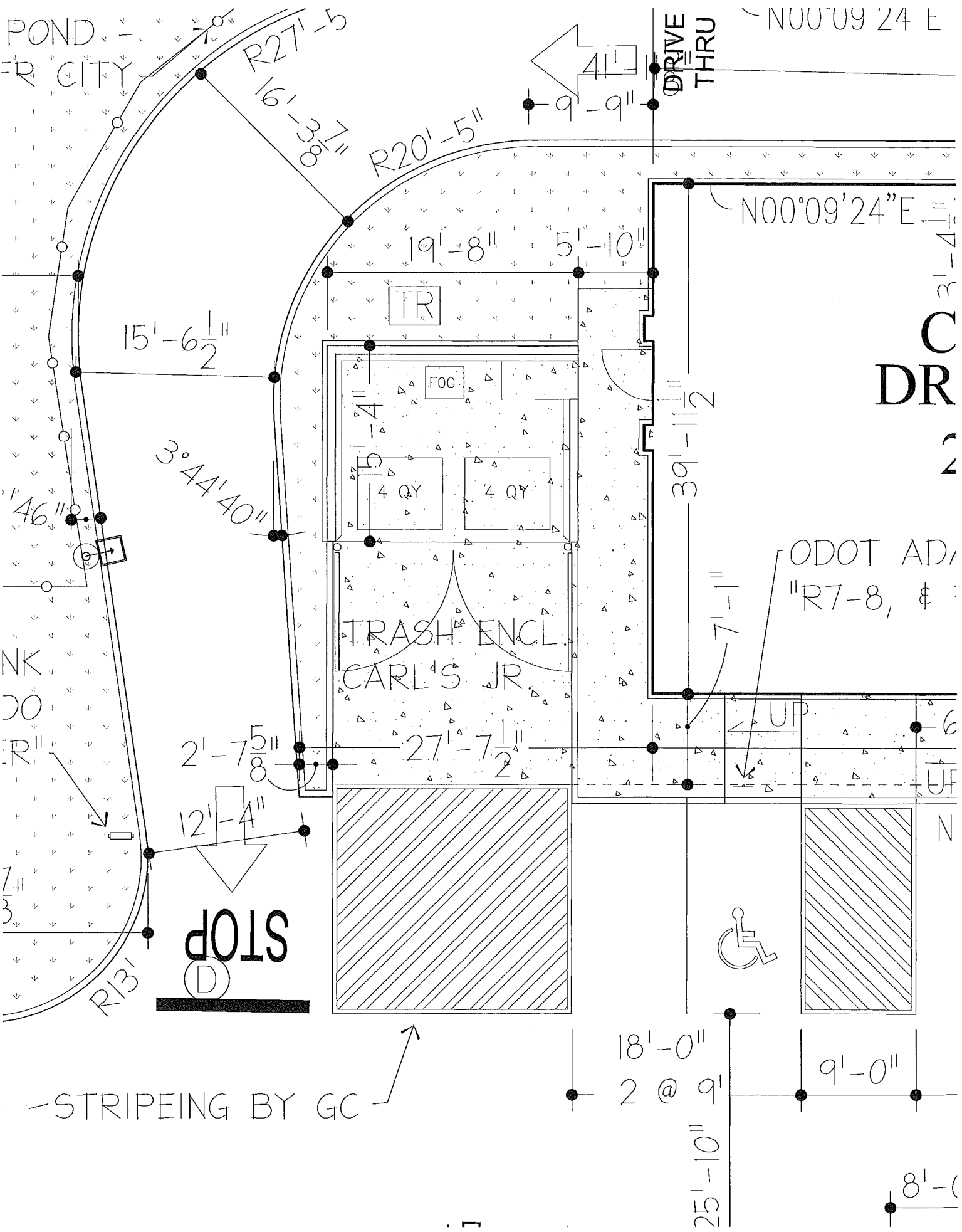
On 12/19/2012 2:29 PM, Ben Altman wrote:

Frank says the roof design will work. Or at least they will make it work... But, not his comment regarding to door swing.

Thanks,

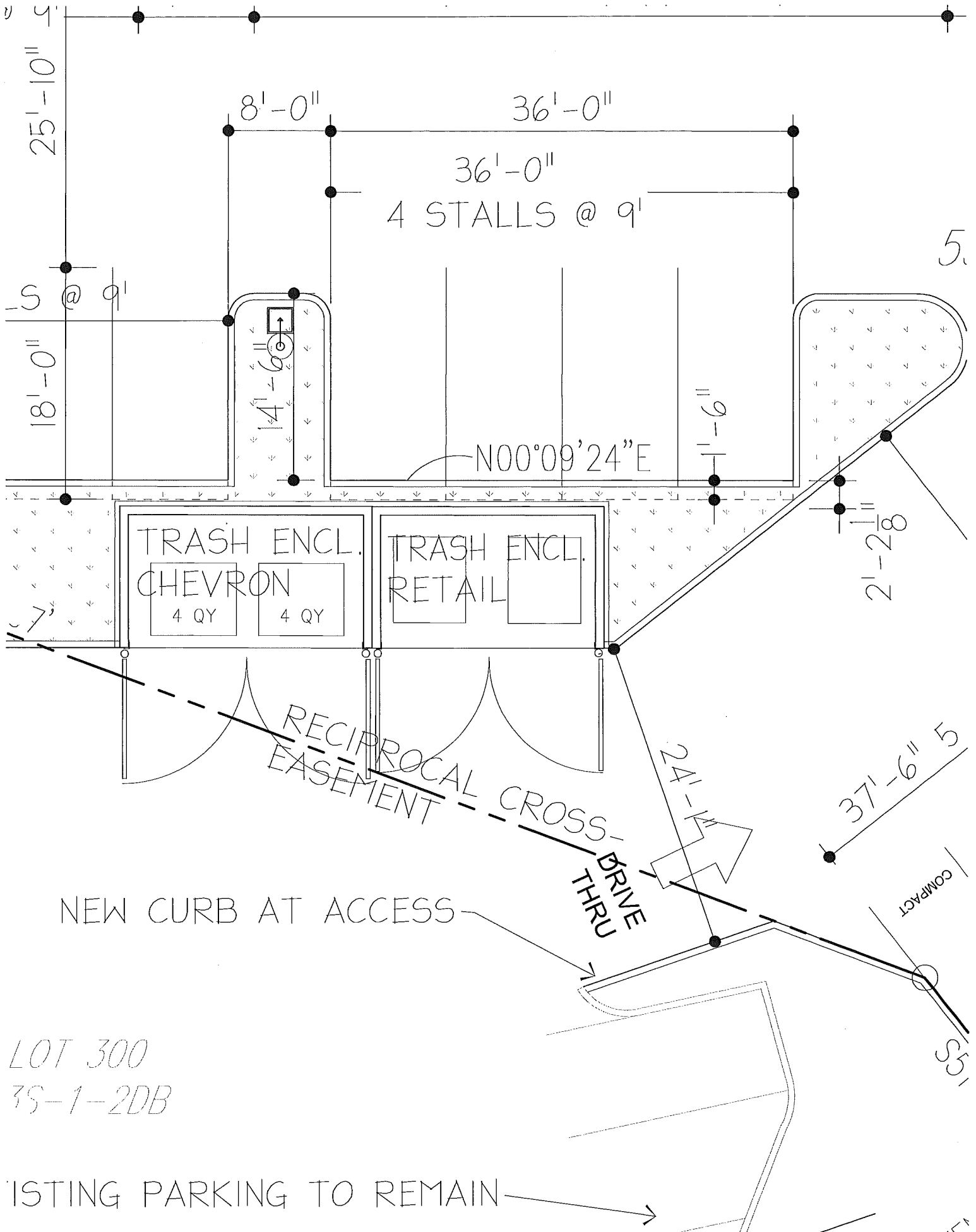
**Ben Altman**  
Senior Planner/Project Manager

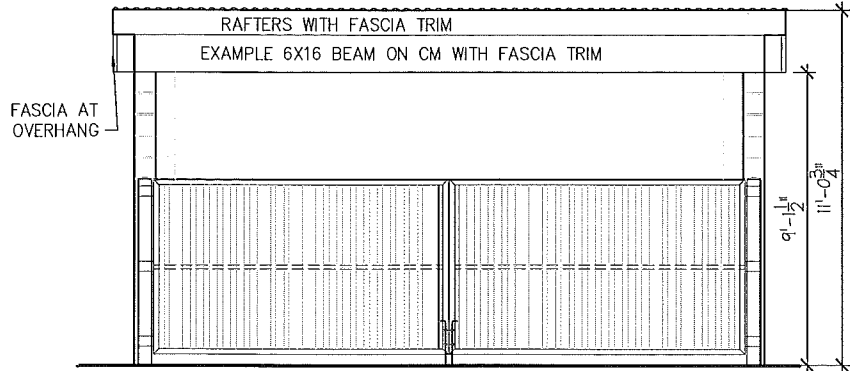
12/26/2012



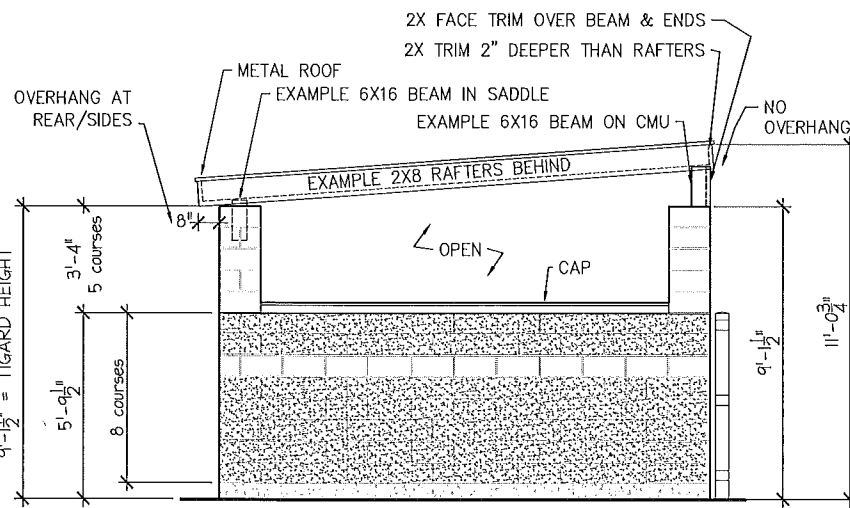
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DR  
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-STRIPEING BY GC

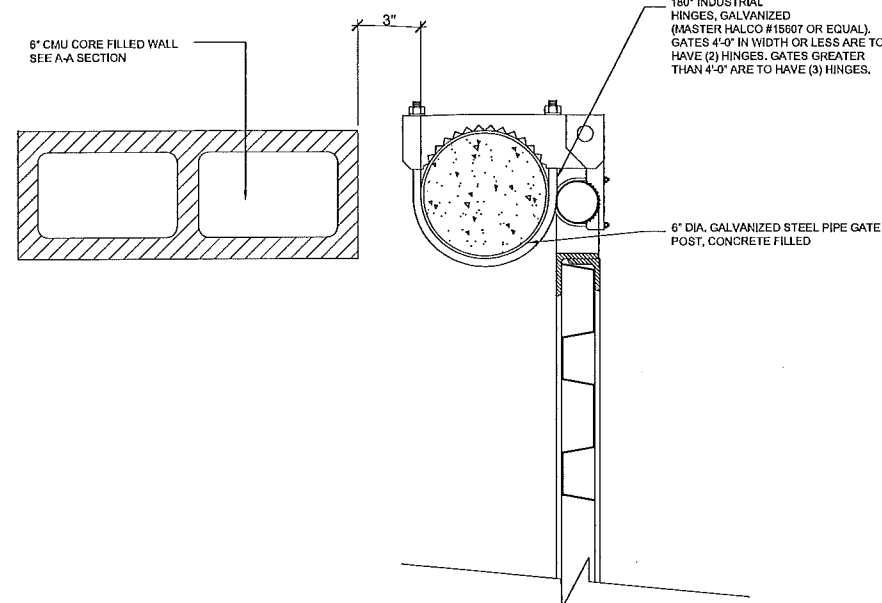




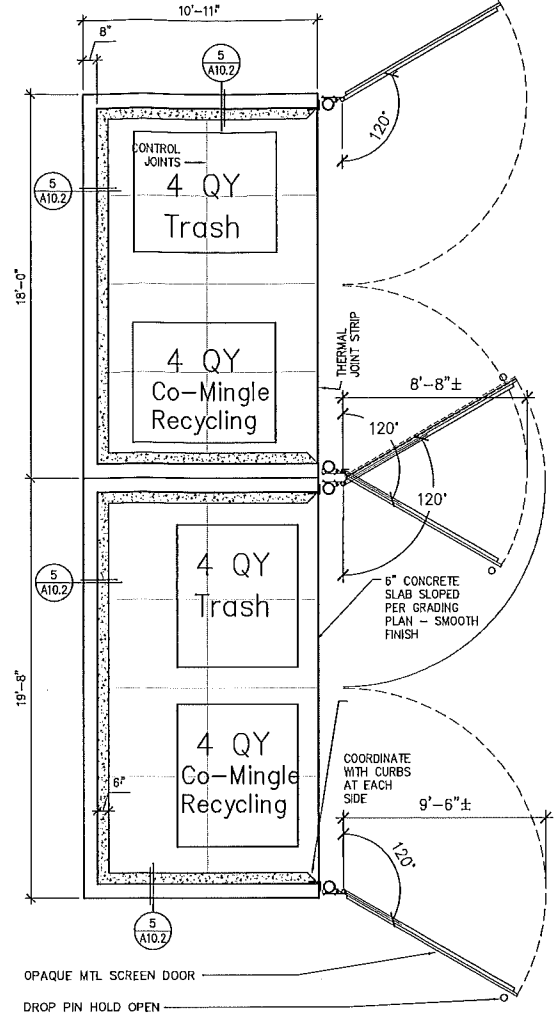
**FRONT ELEVATION @ CJ ENCLOSURE**  
(OTHER ENCLOSURES SIMILAR)



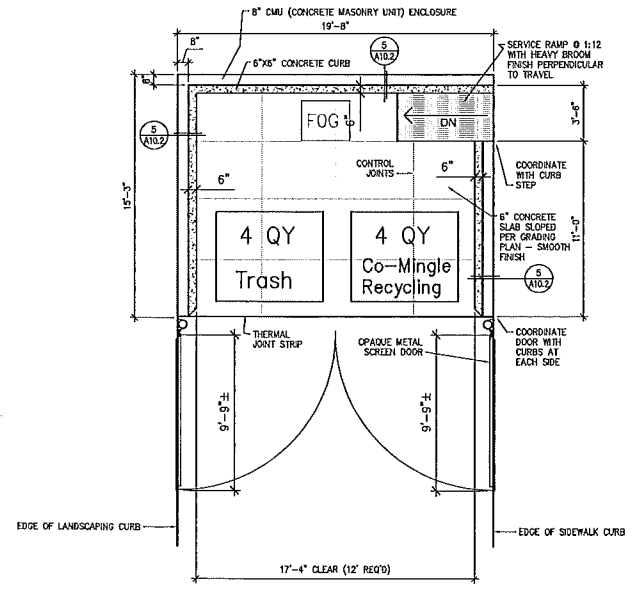
**SIDE ELEVATION @ CJ ENCLOSURE**  
(OTHER ENCLOSURES SIMILAR)



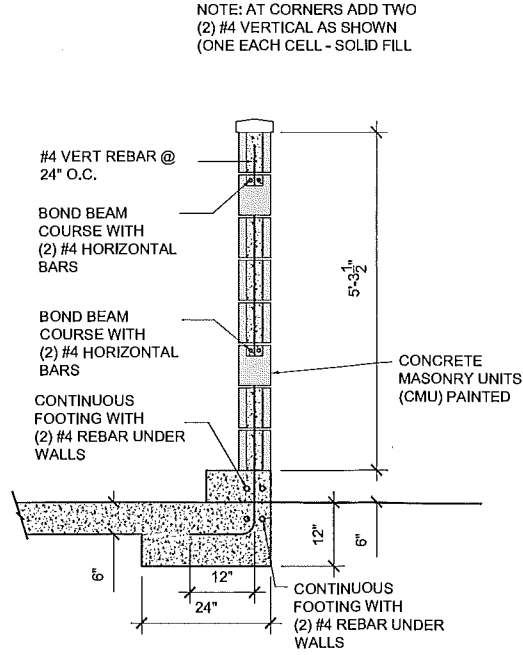
**INDUSTRIAL HINGE DETAIL**



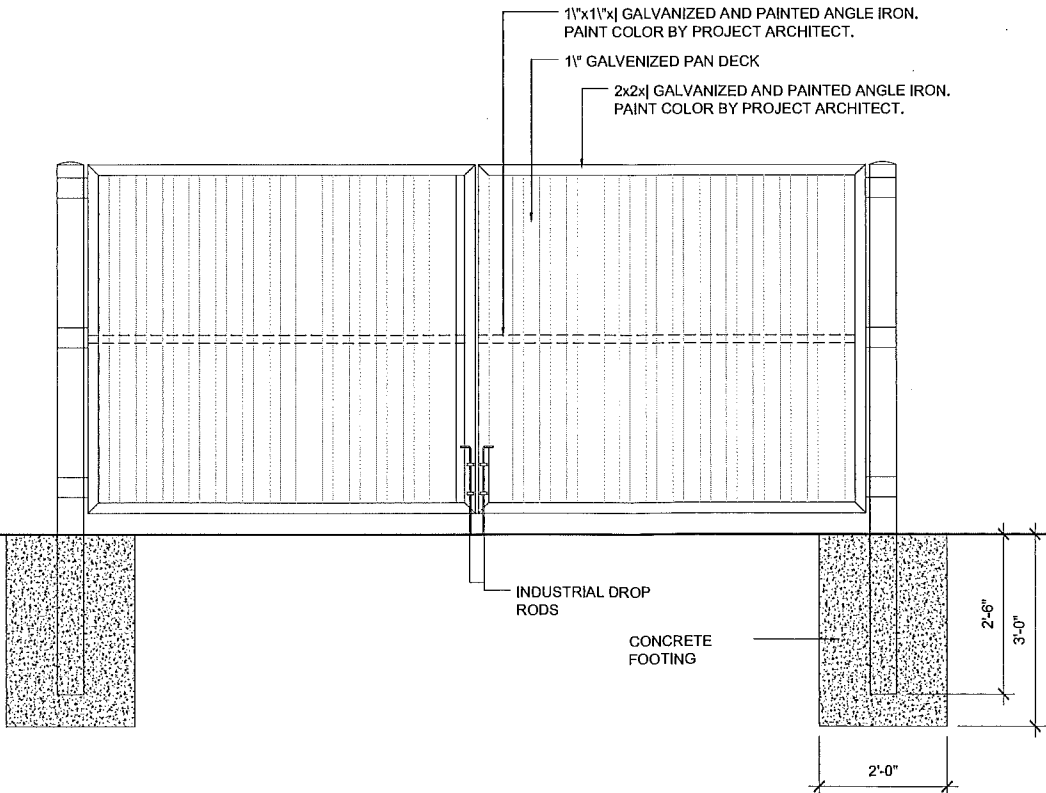
**TRASH ENCLOSURE AT PARKING - PLAN**



**TRASH ENCLOSURE AT CJr - PLAN**



**TRASH ENCLOSURE - A-A**



**TRASH ENCLOSURE - ELEVATION**

ELEVATION

SCALE: 3/8"=1'-0" 3

SCALE: 3/8"=1'-0" 2

SCALE: 1/4"=1'-0" 1

SCALE: 1/4"=1'-0" 6

SCALE: 3/4"=1'-0" 5

SCALE: 3/4"=1'-0" 4

FILE NAME:

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**CB ANDERSON ARCHITECTS PLLC**

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Seattle, Washington 98103  
206-728-2911  
Fax 762-5624

CLIENT

**Wilsonville Devco LLC**  
C/O Integrated Investment Partners  
90 Box 914  
Portland, OR 97207  
(503) 688-9600

PROJECT

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

BRAWING TITLE

**TRASH ENCLOSURE**

REVISIONS

NO.	DATE	DESCRIPTION
1	Dec-28-2012	
2		
3		
4		

STAMP

REGISTERED ARCHITECT

**CRAIG B. ANDERSON**

4677

Exp 6-30-13

STATE OF OREGON

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATORY AGENCIES

DATE: Dec-5-2012

BLDG. SUBMITTAL DATE: ①

DATE: ②

BID DATE: ③

CKE NO.:

ARCH. NO.: 1199

SCALE: AS SHOWN

SHEET

**A10.2**

CKG Foods Conversion Based on CKE Prototype Version 22-14





**CB ANDERSON ARCHITECTS LLC**

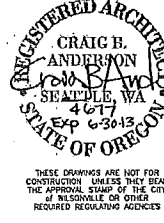
7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-782-2011  
Fax 782-5624

**CLIENT**  
Wilsonville Devco LLC  
c/o Integrated Investment Partners  
PO Box 716  
Portland, OR 97207  
(503) 668-9600

**PROJECT**  
Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**ARCHITECTURAL SITE PLAN**

**REVISIONS**

**STAMP**  
DATE: Jan-2-2013  
BLOC. SUBMITTAL: ①  
DATE: ②  
BID DATE: ③  
CKE NO.:  
ARCH. NO.: 1199  
SCALE: AS SHOWN  
SHEET

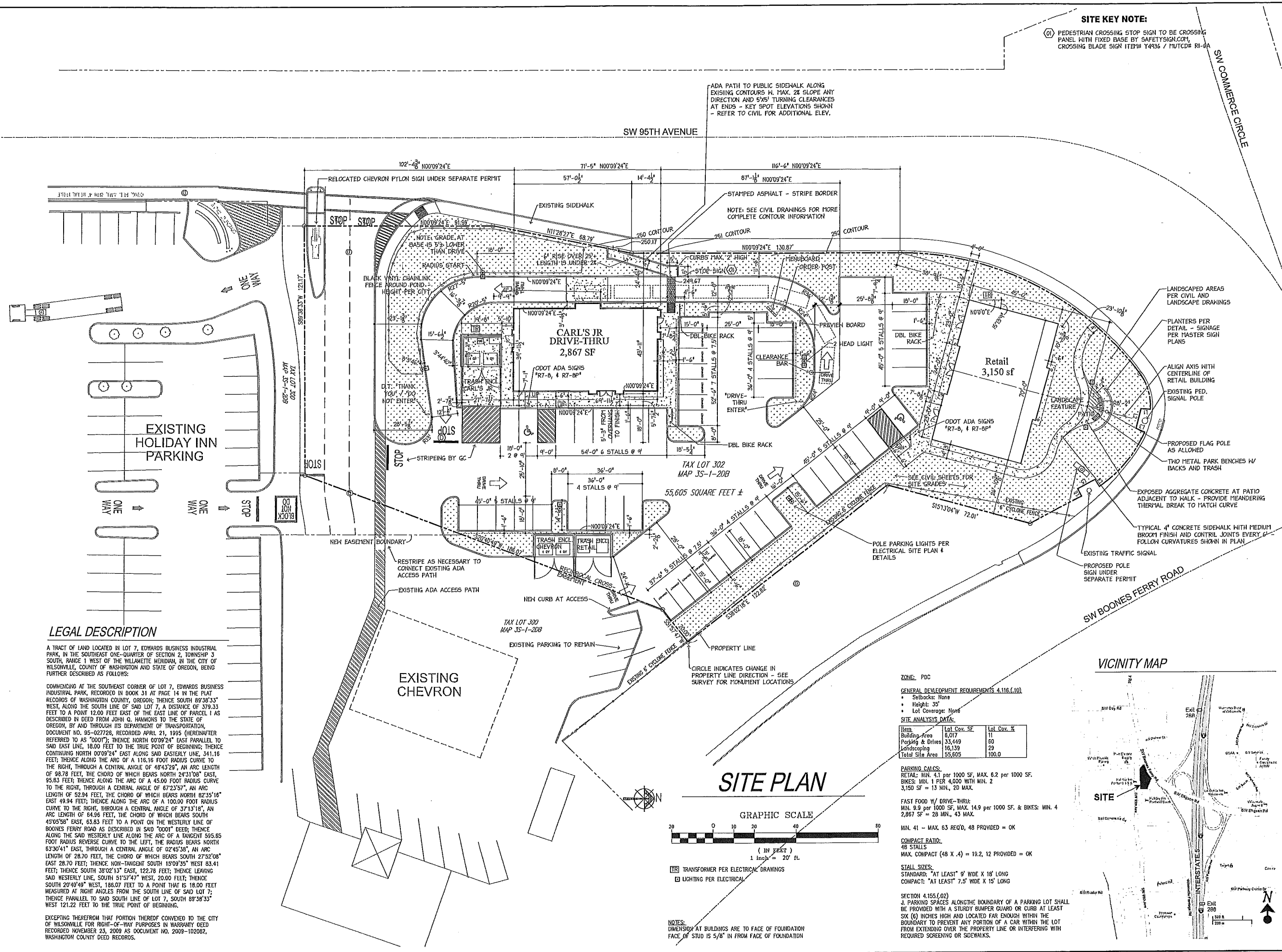
**A1.0**

KGK Foods Conversion Based on CKE Prototype Version 22-14  
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**SITE KEY NOTE:**

① PEDESTRIAN CROSSING STOP SIGN TO BE CROSSING PANEL WITH FIXED BASE BY SAFETY SIGN CO., CROSSING BLADE SIGN ITEM# Y4936 / MUTCD# R1-6A

ADA PATH TO PUBLIC SIDEWALK ALONG EXISTING CONTOURS H. MAX. 2% SLOPE ANY DIRECTION AND 5'x5' TURNING CLEARANCES AT ENDS - KEY SPOT ELEVATIONS SHOWN - REFER TO CIVIL FOR ADDITIONAL ELEV.



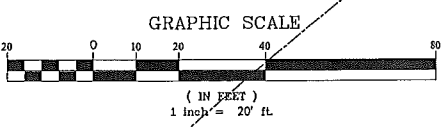
**LEGAL DESCRIPTION**

A TRACT OF LAND LOCATED IN LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, IN THE SOUTHEAST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, IN THE CITY OF WILSONVILLE, COUNTY OF WASHINGTON AND STATE OF OREGON, BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, RECORDED IN BOOK 31 AT PAGE 14 IN THE PLAT RECORDS OF WASHINGTON COUNTY, OREGON; THENCE SOUTH 89°38'33" WEST, ALONG THE SOUTH LINE OF SAID LOT 7, A DISTANCE OF 379.33 FEET TO A POINT 12.00 FEET EAST OF THE EAST LINE OF PARCEL 1 AS DESCRIBED IN DEED FROM JOHN Q. HAMMONS TO THE STATE OF OREGON, BY AND THROUGH ITS DEPARTMENT OF TRANSPORTATION, DOCUMENT NO. 95-027726, RECORDED APRIL 21, 1995 (HEREINAFTER REFERRED TO AS "2001"); THENCE NORTH 0°09'24" EAST PARALLEL TO SAID EAST LINE, 18.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 0°09'24" EAST ALONG SAID EASTERLY LINE, 341.16 FEET; THENCE ALONG THE ARC OF A 116.15 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 45°43'29", AN ARC LENGTH OF 98.78 FEET, THE CHORD OF WHICH BEARS NORTH 24°31'08" EAST, 95.83 FEET; THENCE ALONG THE ARC OF A 45.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 6°23'57", AN ARC LENGTH OF 52.94 FEET, THE CHORD OF WHICH BEARS NORTH 82°35'16" EAST 49.94 FEET; THENCE ALONG THE ARC OF A 100.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 37°13'18", AN ARC LENGTH OF 84.96 FEET, THE CHORD OF WHICH BEARS SOUTH 45°05'50" EAST, 63.63 FEET TO A POINT ON THE WESTERLY LINE OF BOONES FERRY ROAD AS DESCRIBED IN SAID "2001" DEED; THENCE ALONG THE SAID WESTERLY LINE ALONG THE ARC OF A TANGENT 595.65 FOOT RADIUS REVERSE CURVE TO THE LEFT, THE RADIUS BEARS NORTH 63°30'41" EAST, THROUGH A CENTRAL ANGLE OF 02°45'38", AN ARC LENGTH OF 28.70 FEET, THE CHORD OF WHICH BEARS SOUTH 27°52'08" EAST 28.70 FEET; THENCE NON-TANGENT SOUTH 15°09'35" WEST 83.41 FEET; THENCE SOUTH 38°02'13" EAST, 122.76 FEET; THENCE LEAVING SAID WESTERLY LINE, SOUTH 51°57'41" WEST, 20.00 FEET; THENCE SOUTH 20°40'49" WEST, 186.07 FEET TO A POINT THAT IS 18.00 FEET MEASURED AT RIGHT ANGLES FROM THE SOUTH LINE OF SAID LOT 7; THENCE PARALLEL TO SAID SOUTH LINE OF LOT 7, SOUTH 89°38'33" WEST 121.22 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION THEREOF CONVEYED TO THE CITY OF WILSONVILLE FOR RIGHT-OF-WAY PURPOSES IN WASHINGTON DEED RECORDED NOVEMBER 23, 2009 AS DOCUMENT NO. 2009-102082, WASHINGTON COUNTY DEED RECORDS.

**SITE PLAN**



**ZONE: PDC**  
GENERAL DEVELOPMENT REQUIREMENTS 4.116(.10)

- Setbacks: None
- Height: 35'
- Lot Coverage: None

**SITE ANALYSIS DATA:**

Item	Lot Cov. SF	Lot Cov. %
Building Area	6,017	11
Parking & Drives	35,449	60
Landscaping	16,139	29
Total Site Area	55,605	100.0

**PARKING CALCS:**  
RETAIL: MIN. 4.1 per 1000 SF, MAX. 6.2 per 1000 SF.  
BIKES: MIN. 1 PER 4,000 WITH MIN. 2  
3,150 SF = 13 MIN., 20 MAX.

**FAST FOOD 1/2 DRIVE-THRU:**  
MIN. 9.3 per 1000 SF, MAX. 14.9 per 1000 SF. & BIKES: MIN. 4  
2,867 SF = 28 MIN., 43 MAX.

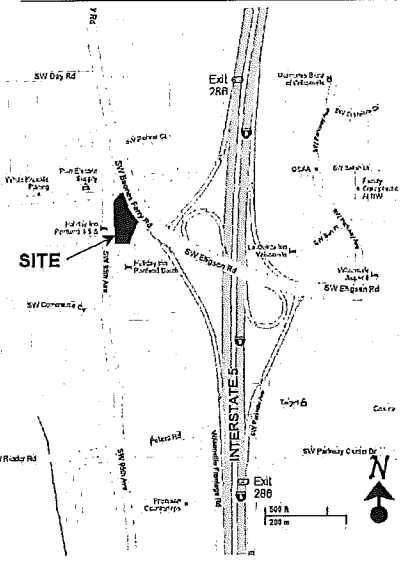
MIN. 41 - MAX. 63 REQ'D, 48 PROVIDED = OK

**COMPACT RATIO:**  
48 STALLS  
MAX. COMPACT (48 X 4) = 19.2, 12 PROVIDED = OK

**STALL SIZES:**  
STANDARD: "AT LEAST" 9' WIDE X 18' LONG  
COMPACT: "AT LEAST" 7.5' WIDE X 15' LONG

**SECTION 4.155(.02)**  
J. PARKING SPACES ALONG THE BOUNDARY OF A PARKING LOT SHALL BE PROVIDED WITH A STURDY BUMPER GUARD OR CURB AT LEAST SIX (6) INCHES HIGH AND LOCATED FAR ENOUGH WITHIN THE BOUNDARY TO PREVENT ANY PORTION OF A CAR WITHIN THE LOT FROM EXTENDING OVER THE PROPERTY LINE OR INTERFERING WITH REQUIRED SCREENING OR SIDEWALKS.

**VICINITY MAP**



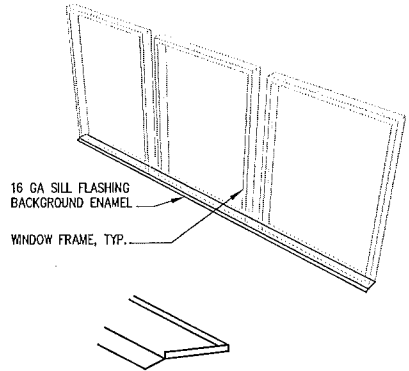
**NOTES:**  
DIMENSIONS AT BUILDINGS ARE TO FACE OF FOUNDATION  
FACE OF STUD IS 5/8" IN FROM FACE OF FOUNDATION

FILE NAME:

1. ALL EXTERIOR NAILING SHALL BE GALVANIZED FINISH NAILS DRIVEN FLUSH WITH SURFACE ON ALL WOOD TRIM & SIDING WHERE OCCURS.
2. ALL EXTERIOR TRIM SHALL BE AS CALLED OUT IN SPECIFICATIONS.
3. ADDRESS NUMBERS TO BE INSTALLED BY G.C. ADDRESS NUMBERS SHALL BE ARABIC NUMERALS OR ALPHABET LETTERS. NUMBERS SHALL BE A MINIMUM OF 4 INCHES HIGH WITH A MINIMUM STROKE WIDTH OF 0.5 INCH. CONTACT FIRE DEPARTMENT TO VERIFY LOCATION.
4. RE: A13.0 (EXTERIOR FINISH SCHEDULE) FOR ALL COLORS & FINISH MATERIALS. (X)

SCALE: 6  
N.T.S.

SCALE: 3  
N.T.S.



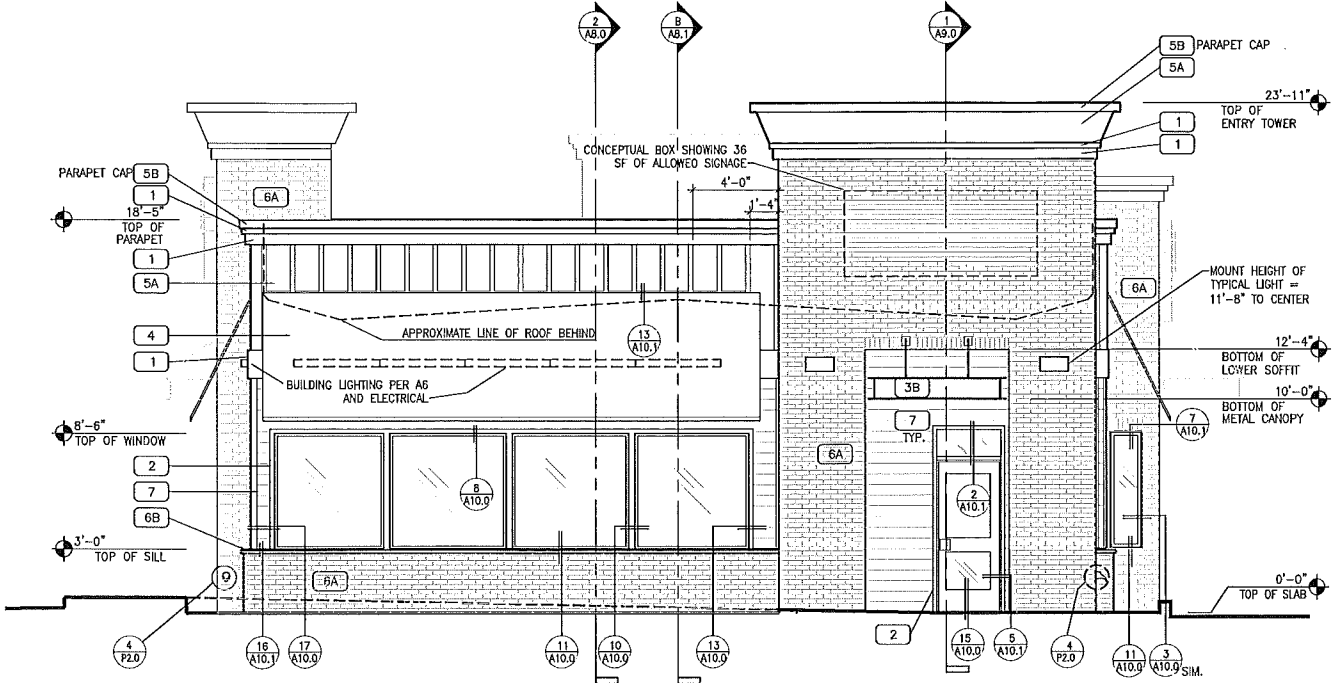
SCALE: 7  
N.T.S.

WINDOW FLASHING DETAIL

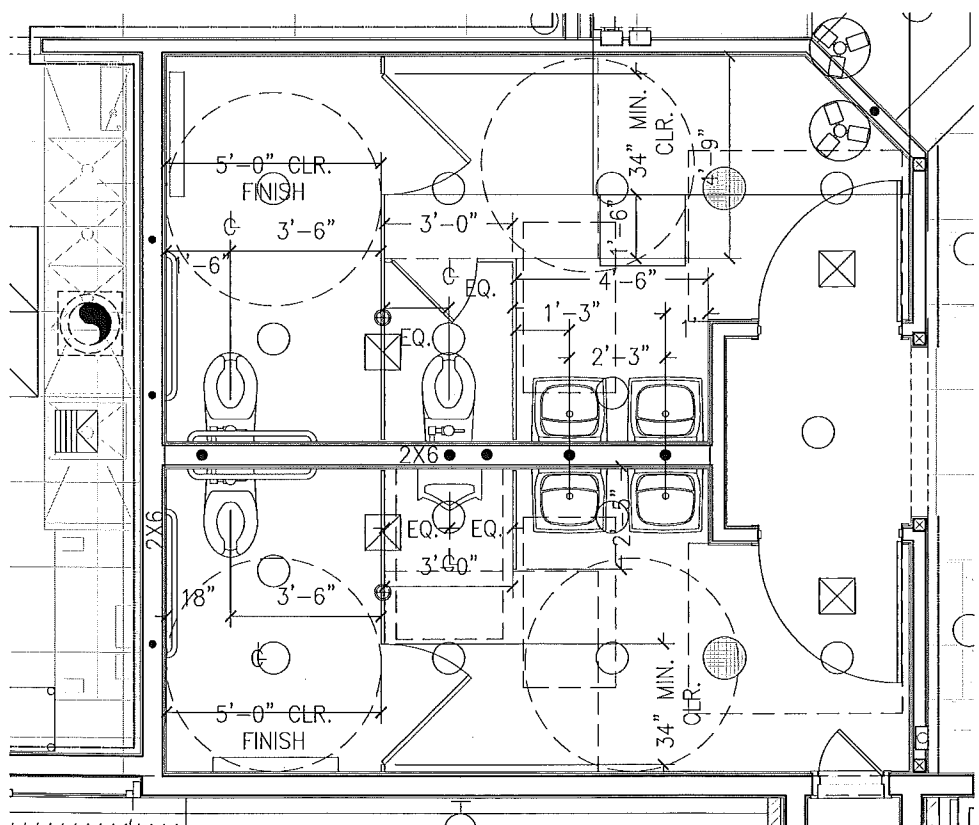
SCALE: 4  
N.T.S.

NORTH ELEVATION

SCALE: 1  
1/4"=1'-0"



SEE A-11 FOR LOCATION OF RESTROOM ACCESSORIES



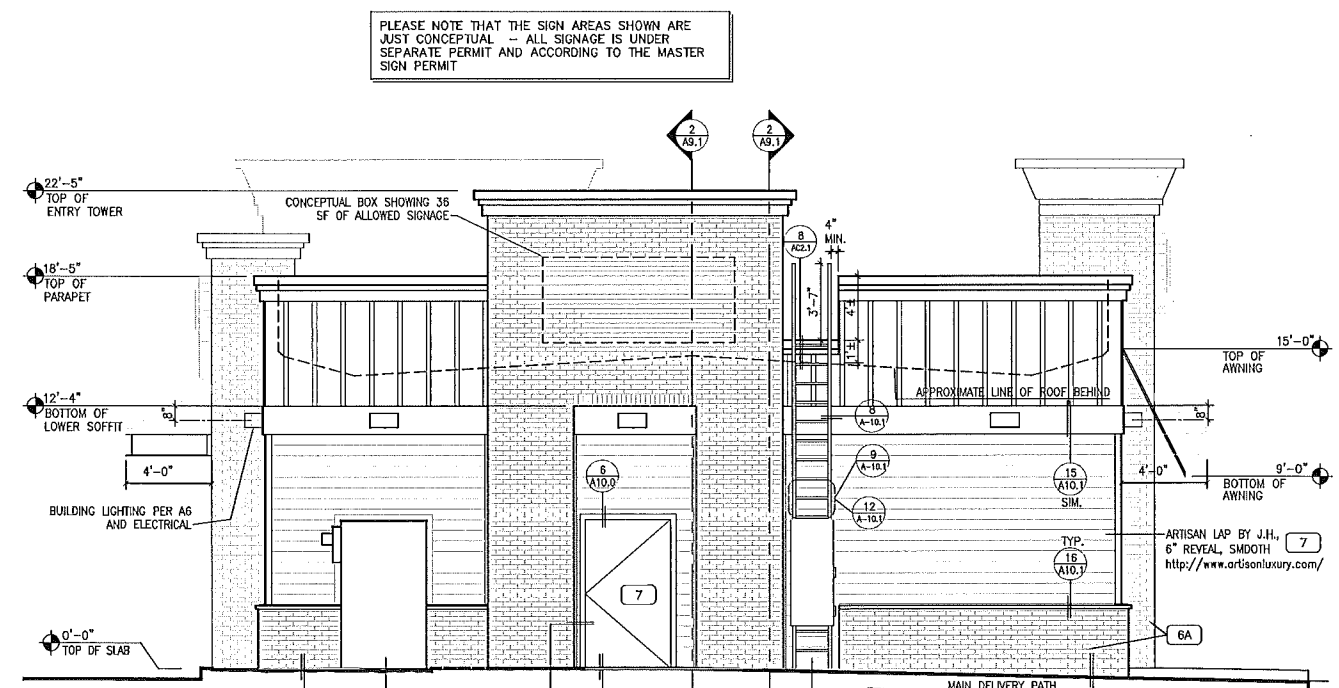
SCALE: 5  
1/2"=1'-0"

MENS #105 / WOMENS #106 RESTROOMS

SCALE: 5  
1/2"=1'-0"

SOUTH ELEVATION

SCALE: 2  
1/4"=1'-0"



PLEASE NOTE THAT THE SIGN AREAS SHOWN ARE JUST CONCEPTUAL - ALL SIGNAGE IS UNDER SEPARATE PERMIT AND ACCORDING TO THE MASTER SIGN PERMIT

REVISIONS

△	Dec-26-2012
△	
△	
△	
△	

STAMP  
**REGISTERED ARCHITECT**  
CRAIG B. ANDERSON  
No. B-1102  
SEATTLE, WA  
4617  
EXP. 6-30-13  
STATE OF OREGON

DATE: Dec-5-2012  
BLDG. SUBMITTAL DATE: ①  
DATE: ②  
BID DATE: ③  
CNE NO.:  
ARCH. NO.: 1199  
SCALE: AS SHOWN  
SHEET

CGK Foods CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14  
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FILE NAME



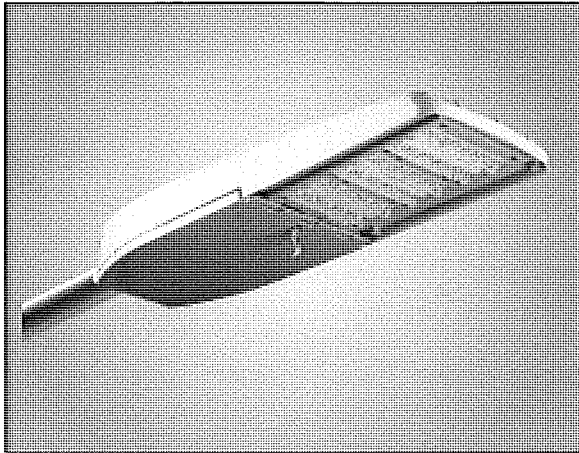


Consistent with LEED® goals & Green Globes™ criteria for light pollution reduction

# Autobahn Series ATB0

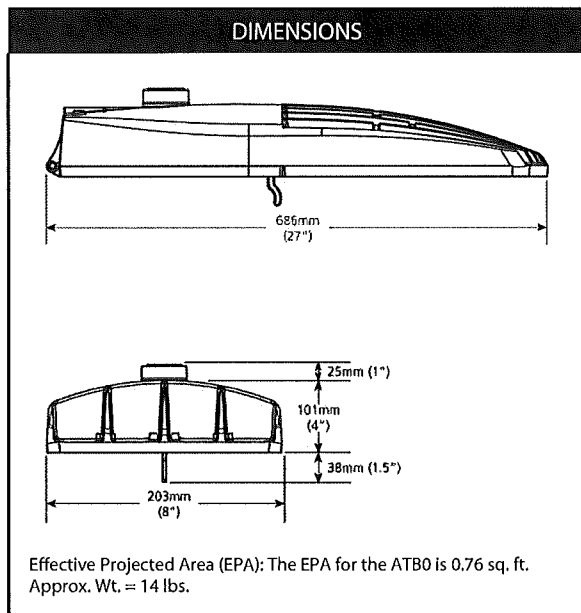
## Roadway Lighting

### PRODUCT OVERVIEW



#### Applications:

- Roadways
- Off ramps
- Residential streets
- Parking lots



#### Features:

##### OPTICAL

**Same Light:** Performance is comparable to 70-150W HPS roadway luminaires.

**White Light:** Average correlated color temperature (CCT) is 4000K or 5000K with a minimum CRI of 70

**Unique IP66 rated LED light engines** provided 0% uplight and restrict backlight to within sidewalk depth, providing optimal application coverage and optimal pole spacing. Available in Type II, III, IV, and V roadway distributions

##### ELECTRICAL

**Long Life:** LED light engines are rated >100,000 hours at 25°C, L70. Electronic driver has a rated life of 100,000 hours at a 25°C ambient

**Lower Energy:** Saves an average of 40-50% over comparable HPS platforms.

**Robust Surge Protection:** Acuity's proprietary SPD provides IEEE/ANSI C62.41 Category C (10kV/5kA) level of protection

##### MECHANICAL

**Easy to Maintain:** Includes standard AEL lineman-friendly features such as tool-less entry, tool-less NEMA photocontrol receptacle, terminal block and quick disconnects. Bubble level located inside the electrical compartment for easy leveling at installation. The electrical platform and durable housing materials provide superior longevity and reduce the need for maintenance

**Rugged die-cast aluminum housing** is polyester powder-coated for durability and corrosion resistance. Rigorous five-stage pre-treating and painting process yields a finish that achieves a scribe creepage rating of 8 (per ASTM D1654) after over 1000 hours exposure to salt fog chamber (operated per ASTM B117)

**Mast arm mount** provides easy, secure installation and is adjustable for arms from 1-1/4" to 2" (1-5/8" to 2-3/8" O.D.) diameter. Wildlife shield is cast into the housing (not a separate piece)

**Die-cast trigger latch** on doorframe allows for tool-less entry and enables easy and secure opening with one hand

##### CONTROLS

**NEMA photocontrol receptacle** is standard; tool-less "lift and turn" receptacle makes photocontrol orientation easy

**Dimming version** (available with DE option) uses proprietary Acuity Brands components to enable continuous 0-10V dimming down to 10% output via the ROAM® smart controls system (sold separately)

**Photocontrol for solid-state lighting** (available with PCSS option) meets ANSI C136.10 criteria.

##### WARRANTY & STANDARDS

All electrical components warranted for 5 years

Rated for -40°C to 40°C ambient

CSA Certified to U.S. and Canadian standards

Complies with ANSI: C136.2, C136.10, C136.14, C136.31, C136.15, C136.37

Data is considered accurate as of the revision date shown. Information is subject to change without notice.

# Autobahn Series ATB0

## Roadway Lighting

### ORDERING INFORMATION

Example: ATB0 20LED E70 MVOLT R2

Series	Performance Packages	Voltage	Optics
ATB0 Autobahn LED Roadway, Small	20A LED E35 20A Chips, 350 mA Driver 20A LED E53 20A Chips, 525 mA Driver 20A LED E70 20A Chips, 700 mA Driver 20B LED E53 20B Chips, 525 mA Driver 20B LED E70 20B Chips, 700 mA Driver 20B LED E10 20B Chips, 1000 mA Driver 30B LED E53 30B Chips, 525 mA Driver 30B LED E70 30B Chips, 700 mA Driver	120 120V MVOLT Multi-volt, 120-277V 347 347V 480 480V	R2 Roadway Type II R3 Roadway Type III R4 Roadway Type IIII R5 Roadway Type V

Options
---------

Color Temperature (CCT)  
 (blank) 4000K (standard)  
 5K 5000K

Misc.  
 HS House-Side Shield  
 BL External Bubble Level

Mounting  
 (blank) 2-bolt Internal (standard)

Controls  
 (blank) NEMA Photocontrol Receptacle (standard)  
 NR No Photocontrol Receptacle  
 PCSS Solid State Lighting Photocontrol (120-277V)  
 SH Shorting Cap  
 DE Dimming Enabled (0-10V) <sup>1</sup>

Paint  
 (blank) Gray (standard)  
 GI Graphite  
 BK Black  
 BZ Bronze  
 DDB Dark Bronze  
 WH White  
 UP Unpainted

Terminal Block  
 (blank) Terminal Block (standard)  
 T2 Wired to L1 and L2 Position

**Notes:**

1 Dimming controlled via ROAM system (sold separately). Contact factory for details.

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# Autobahn Series ATB0

## Roadway Lighting

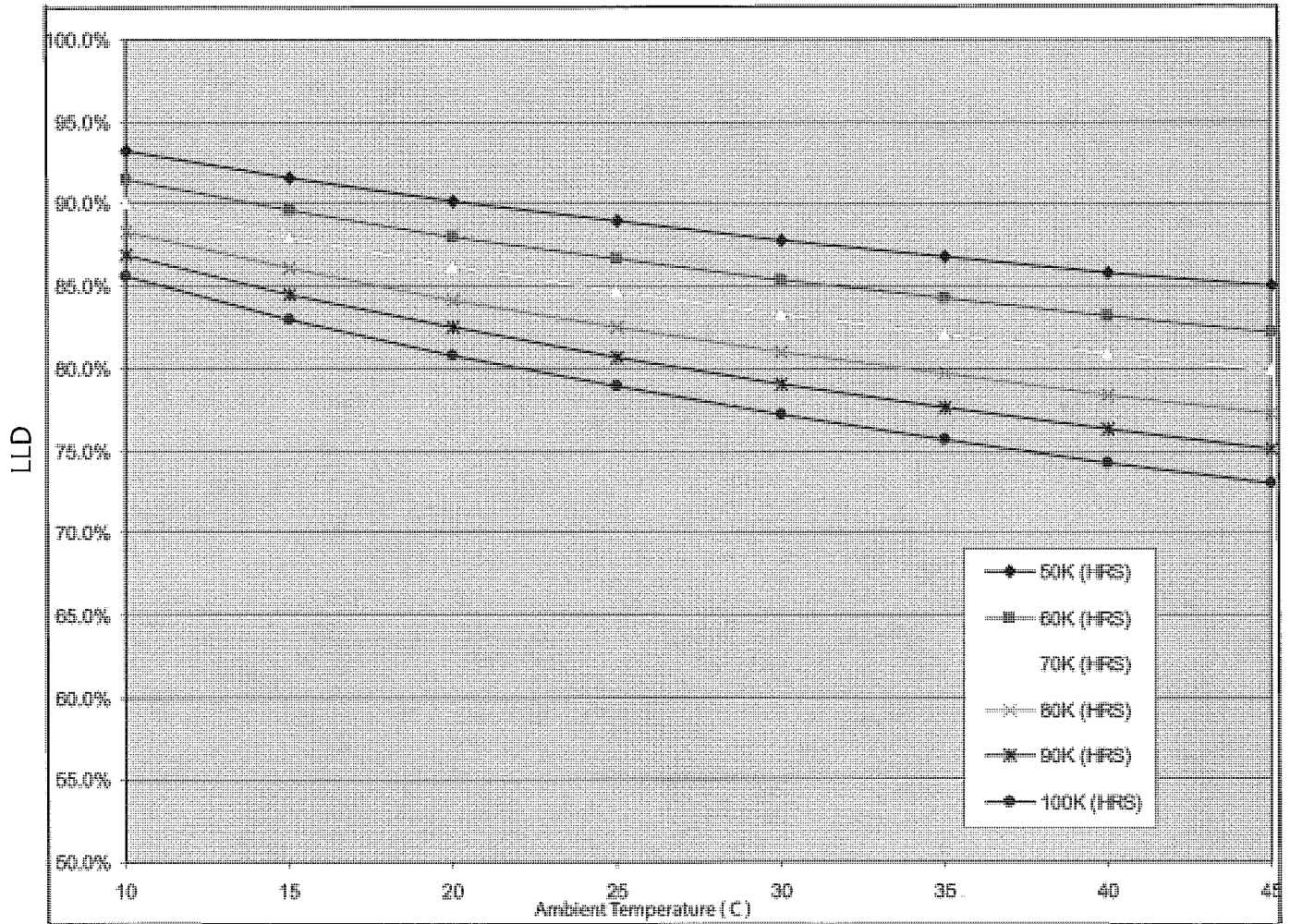
### DESIGN DATA

Performance Package	Drive Current (mA)	Input Watts	Optic	4000K CCT	
				Delivered Lumens	Efficacy (LPW)
20A	350	26	R2	2175	84
	525	37		2999	81
	700	51		3932	77
	350	26	R3	2111	81
	525	37		2910	79
	700	51		3816	75
	350	26	R4	2188	84
	525	37		3017	82
	700	51		3956	78
	350	26	R5	2086	80
	525	37		2876	78
	700	51		3771	74
20B	525	34	R2	3082	91
	700	48		4071	85
	1000	67		5432	81
	525	34	R3	2991	88
	700	48		3951	82
	1000	67		5272	79
	525	34	R4	3100	91
	700	48		4096	85
	1000	67		5465	82
	525	34	R5	2956	87
	700	48		3905	81
	1000	67		5210	78
30B	525	48	R2	4833	101
	700	71		6391	90
	525	48	R3	4690	98
	700	71		6203	87
	525	48	R4	4862	101
	700	71		6429	91
	525	48	R5	4635	97
	700	71		6192	87

\* Manufacturer's projected life calculations are correlated from LM-80 chip data and in situ luminaire thermal testing.



# DESIGN DATA



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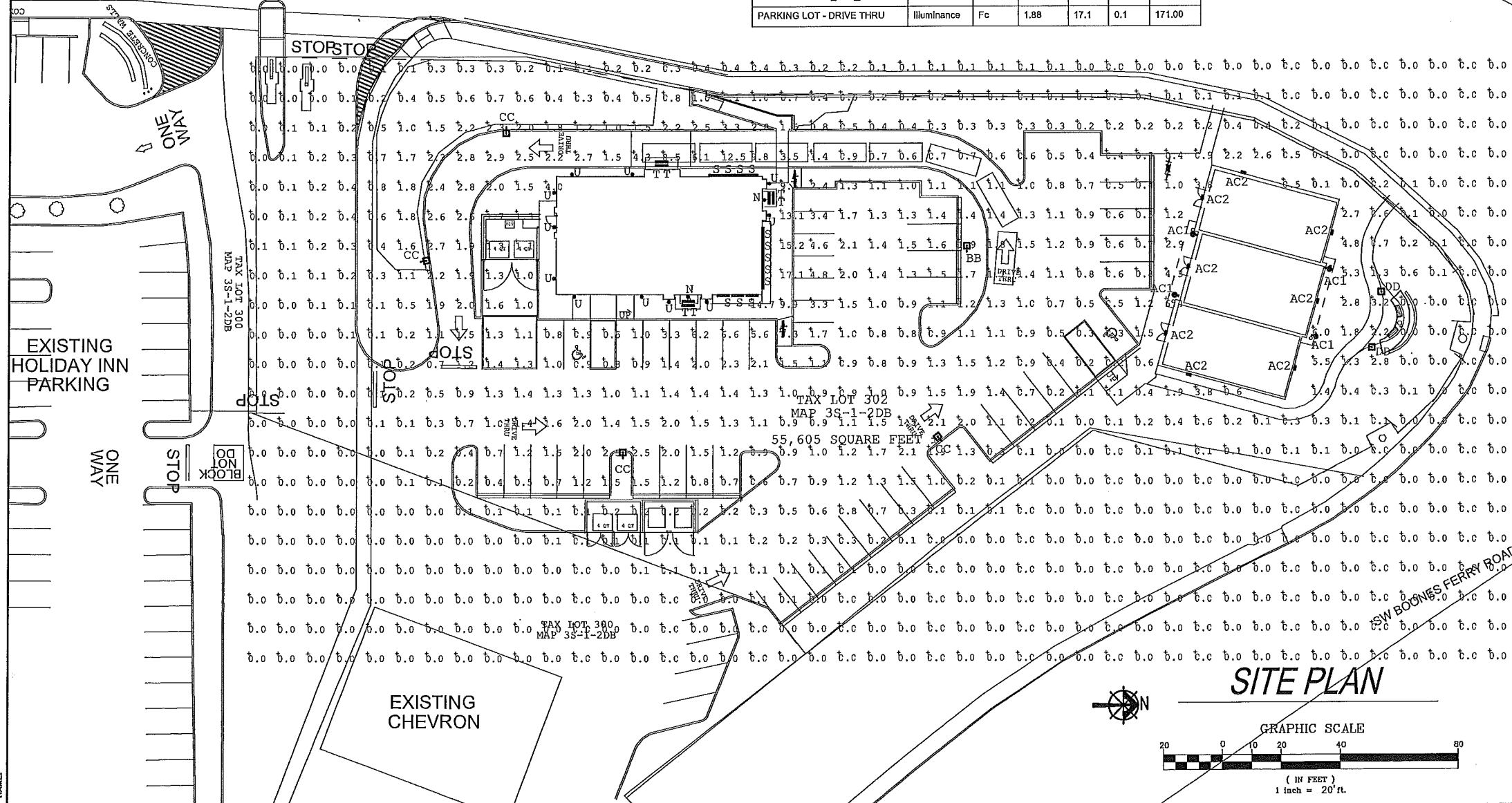
Use these factors to estimate differences in lumen output with variations in ambient temperature. Values shown in the table below are taken in a 25°C ambient; therefore 25°C = 1.00.

Lumen Ambient Temperature (LAT) Factors							
Performance Package	15°C	20°C	25°C	30°C	35°C	40°C	45°C
All	1.02	1.01	1.00	0.99	0.99	0.98	0.98

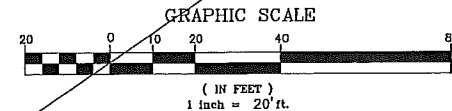
Symbol	Qty	TYPE	Mounting Height	Arrangement	Proposed shielding	Total Lamp Lumens	LLF	CATALOG #	Proposed Wattage	Max. wattage per light zone L22
	2	DD	3.5 ft	SINGLE	SHELDED	3800	0.600	BRL-36-50-MP	32	35
	2	N	10.0 ft	SINGLE	FULL SHELDED	3200	0.700	1101F2642U	42	100
	6	T	10.0 ft	SINGLE	FULL SHELDED	5800	0.750	FSRW-64	(2) 42	100
	12	S	10.0 ft	SINGLE	FULL SHELDED	5800	0.750	SSF-232	(2) 42	100
	11	U	11.6 ft	SINGLE	FULL SHELDED	6800	0.800	WB200-100PMH	100	100
	4	AC1	9 ft	SINGLE	FULL SHELDED	2200	0.700	H91514-100MH	100	100
	8	AC2	9 ft	SINGLE	FULL SHELDED	3200	0.700	TWSL-242TRF-WD	(2) 42	100
	4	CC	25 ft	SINGLE	FULL SHELDED	N.A.	0.900	ATBA20LED70-R2	N.A.	100
	1	BB	25 ft	SINGLE	FULL SHELDED	N.A.	0.900	ATBA20LED70-R2	N.A.	100

Label	CalcType	Units	Avg	Max	Min	Max/Min
GENERAL AREA	Illuminance	Fc	0.71	17.1	0.0	N.A.
VERTICAL CALC_Side_1	Illuminance	Fc	0.09	0.5	0.0	N.A.
VERTICAL CALC_Side_10	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_11	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_12	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_13	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_14	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_15	Illuminance	Fc	0.03	0.1	0.0	N.A.
VERTICAL CALC_Side_16	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_17	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_18	Illuminance	Fc	0.07	0.1	0.0	N.A.
VERTICAL CALC_Side_19	Illuminance	Fc	0.02	0.1	0.0	N.A.
VERTICAL CALC_Side_2	Illuminance	Fc	0.62	2.2	0.0	N.A.
VERTICAL CALC_Side_20	Illuminance	Fc	0.14	0.2	0.1	2.00
VERTICAL CALC_Side_21	Illuminance	Fc	0.07	0.1	0.0	N.A.
VERTICAL CALC_Side_22	Illuminance	Fc	0.10	0.2	0.0	N.A.
VERTICAL CALC_Side_23	Illuminance	Fc	0.02	0.1	0.0	N.A.
VERTICAL CALC_Side_3	Illuminance	Fc	0.47	2.5	0.0	N.A.
VERTICAL CALC_Side_4	Illuminance	Fc	0.11	0.3	0.0	N.A.
VERTICAL CALC_Side_5	Illuminance	Fc	0.06	0.3	0.0	N.A.
VERTICAL CALC_Side_6	Illuminance	Fc	0.09	0.2	0.0	N.A.
VERTICAL CALC_Side_7	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_8	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_9	Illuminance	Fc	0.10	0.1	0.1	1.00
PARKING LOT - DRIVE THRU	Illuminance	Fc	1.88	17.1	0.1	171.00

NOTE:  
EXTERIOR LIGHTING WILL BE CONTROLLED BY ASTRONOMICAL TIME CLOCK AND PHOTOCELL TO MET THE REQUIRED L22 LIGHTING REQUIREMENTS



PARKING LOT - DRIVE THRU  
Illuminance (Fc)  
Average = 1.88  
Maximum = 17.1  
Minimum = 0.1  
Max/Min Ratio = 171.00



PHOTOMETRIC SITE PLAN  
SCALE: 3/32"=1'-0"

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KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

**CB ANDERSON ARCHITECTS** LLC

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2011  
Fax 782-5624

**Wilsonville Devco LLC**  
c/o Integrated Investment Partners  
PO Box 916  
Portland, OR 97207  
(503) 688-9600

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**PHOTOMETRIC SITE PLAN**

**SE1.0**

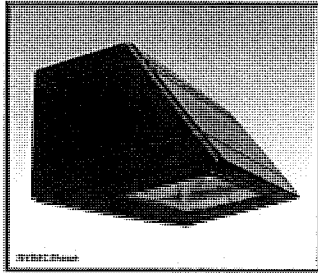
FILE NAME:





**TRAPEZOIDAL WALL SCONCE**

**TYPE AC-1**



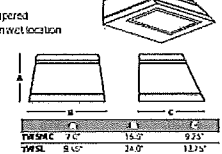
The TWS Series trapezoidal wall sconce offers unique styling and maximum versatility with two sizes, multiple light sources, and a variety of finishes. The downlight-only design features a maximum cut-off distribution with exceptional horizontal illumination.

**Fixture Specifications**

**FEATURES**

- Durable cast aluminum housing
- Integrated design eliminates high angle brightness
- Luminaires finished in a variety of powder coat paints
- Compactly sealed, flat tempered glass lens suitable for use in wet location
- Slips complete with lamp

**DIMENSIONS**



**ORDERING INFORMATION**

SAMPLE CATALOG NUMBER

Finish	Wattage	Mounting	Color	Options
TWSX	XXXXXX	WD	XX	XXX

Finish	Wattage	Mounting	Color	Options
TWSX	XXXXXX	WD	XX	XXX



Performance Designed Lighting Products  
1065 Johnson Drive • Buffalo Grove, IL 60089 • Toll-Free: 800-544-4848 • Phone: 847-275-0527 • Fax: 847-275-0542

**ORDERING INFORMATION**

Product Family	Finish	Wattage	Mounting	Color	Options
BSL	XXXXXX	WD	XX	XXX	

- All lamps are medium base, 100W (100W is available for use in U.S. markets only).
- 100W lamps are available in 100W and 150W.
- 150W lamps are available in 150W and 200W.
- 200W lamps are available in 200W and 250W.
- 250W lamps are available in 250W and 300W.
- 300W lamps are available in 300W and 350W.
- 350W lamps are available in 350W and 400W.
- 400W lamps are available in 400W and 450W.
- 450W lamps are available in 450W and 500W.
- 500W lamps are available in 500W and 550W.
- 550W lamps are available in 550W and 600W.
- 600W lamps are available in 600W and 650W.
- 650W lamps are available in 650W and 700W.
- 700W lamps are available in 700W and 750W.
- 750W lamps are available in 750W and 800W.
- 800W lamps are available in 800W and 850W.
- 850W lamps are available in 850W and 900W.
- 900W lamps are available in 900W and 950W.
- 950W lamps are available in 950W and 1000W.

COOPER Lighting  
www.cooperlighting.com

**DESCRIPTION**

The McGraw-Edison BSL/BRL Bollard Series, available in heights from 24" to 42", has crisp, clean lines which blend with any architectural setting. Constructed of anodized, heavy-duty aluminum and finished with a tough polyester powder coat finish, the McGraw-Edison Bollard Series is gasketed to seal out external contaminants. UL 1009 listed and CSA certified for wet locations.

**SPECIFICATION FEATURES**

**Construction:** TOP: Rugged, minimum 5/32" thick cast aluminum top cap secured via a concealed stainless steel allen screw with twist removal mechanism for lamp access. Flow through ventilation assure cool to the touch top. LID/VERS: Cast Aluminum Lower Housing provides sharp cutoff delivering no direct light above 90°. Louvers are secured to the shade via tempered stainless steel rods and fasteners. LOWER HOUSING: Nominal 1/4" thick aluminum extruded housing. Bollard housing is secured to the base with fasteners, concealed screw covers for smooth, uncluttered appearance. BASE: Rugged cast aluminum. Completely concealed.

**McGraw-Edison**

Category #	Type
	DD
Project	Date
Comments	
Prepared by	

**Electrical:** HID High Power Factor ballast for 20% starting. CFL Electronic ballast for 0% starting. Product is factory mounted to the base. Quick disconnect provided between lamp and electrical assembly. Metal Housing and High Pressure Sodium lamp sources up to 100W and up to 42W Compact Fluorescent sources.

**Optical LAMP ENCLOSURE:** One piece tempered glass with internal flutes for even distribution of illumination. Decorative colored glass covers for smooth, uncluttered appearance. GLOBE is fully gasketed via EPDM material. Socket is porcelain, medium base for HID lamp sources and polycarbonate/FBT GX24q 4 base for compact fluorescent lamps.

**Mounting:** Base mounts onto foundation with three (3) 1/2" x 12 1/2" anchor bolts on a 5" dia. bolt circle (a certified 5.75" x 3.125" wide entrance opening provided).

**Finish:** Premium fade and abrasion resistant TGIC Polyester Powder Coat Finish. Standard colors are Black, Grey, Bronze, White, Dark Platinum and Graphite Metallic. Other finish colors available including all RAL matches.

**BSL/BRL BOLLARD**

26 - 100W  
Pulse Start Metal Halide  
High Pressure Sodium  
Compact Fluorescent  
Incandescent  
PATHWAY LUMINAIRE

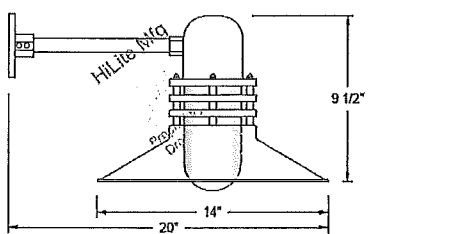


**WATTAGE TABLE**

Lamp Type	Wattage
High Pressure Sodium (HPS)	26, 36, 46, 56, 66, 76, 86, 96, 106
Compact Fluorescent (CFL)	10, 15, 20, 25, 30, 35, 40, 45, 50, 55, 60, 65, 70, 75, 80, 85, 90, 95, 100
Incandescent (INC)	100W



**H-91514-B**  
**Mars Collection**  
Job Name:  
Type: TYPE AC-2  
Quantity:



**FINISH** - Offered in exceptional finishes, comprised of: polyester/polished powder coat, baking enamel liquid, raw metal, or galvanized finishes. Standard finishes are: 91(Black), 93(White), 95(Dark Green), 96(Galvanized), BR47(Powder Coat Rust), BK01(Black Texture), GN20(Powder Coat Patina).  
Upgraded finishes are: 29, 60, 82, 90, 92, 94, 97, 99, 100, 103, 104, 105, 110, 112, 113, 114, 115, 117, 118, 119, 120, 127, 128, 129, 133, 134, 135, 139, 98, 101, 102, 127, 138, 139, 140, 121, 122, 123, 124, 125, 126, 24, 44, 48, 49.

For interior finish of fixture refer to color chart on pages 344-346.

**REFLECTOR** - Heavy duty, spun shade, aluminum 8081-0 and/or 1100-D, galvanized 22 gauge, steel 2022 gauge, copper 032010 and brass 032010 construction. Dependent on finish.

**SOCKETS/LAMPS** - Available in: Incandescent - rated 100 watt max/120 volt, medium base. Compact Fluorescent(CFL) - rated 13/18/26/32 watt max/120/277 volt, GX24Q base. Metal Halide (MH) - rated 30/50/70/100 watt max/120/208/240/277 volt, medium base, 4KV socket. High Pressure Sodium(HPS) - rated 50/70/100 watt max/120/277 volt, medium base. Light-Emitting Diode (LED) - rated 20W LED max. See LED specifications for complete part number.

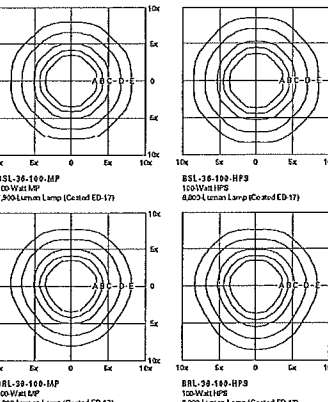
**ACCESSORIES** - CAL(Clear Acrylic Layer), BAL(Blue Acrylic Layer), RAL(Red Acrylic Layer) and GHAL(Green Acrylic Layer) available.

**LENS OPTION** - CLR(Clear), RIB(Clear Ribbed), FR(Frost), RD(Red), BL(Blue), GRN(Green), AM(Amber) and AMC(Amber Crackle) available.

MADE IN THE U.S.A. Suitable for wet location.

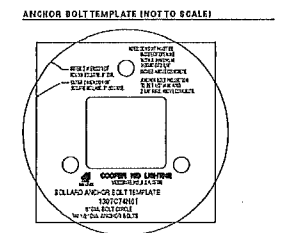


**PHOTOMETRICS**



**Footcandle Table**  
Simplified footcandle table and beam spread for footcandle values of equal footcandle distance. Distance in units of mounting height.

Mounting Height	Beam Diameter	Footcandle
10'	10'	100
20'	20'	25
30'	30'	44
40'	40'	62
50'	50'	81
60'	60'	100



**ORDERING INFORMATION**

Product Family	Finish	Wattage	Mounting	Color	Options
BSL	XXXXXX	WD	XX	XXX	

- 100W lamps are medium base, 100W (100W is available for use in U.S. markets only).
- 100W lamps are available in 100W and 150W.
- 150W lamps are available in 150W and 200W.
- 200W lamps are available in 200W and 250W.
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- 900W lamps are available in 900W and 950W.
- 950W lamps are available in 950W and 1000W.

COOPER Lighting  
www.cooperlighting.com

**ARCHITECTS**

**CB ANDERSON ARCHITECTS PLLC**

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2911  
Fax 782-5924

**CLIENT**

**Wilsonville Devco LLC**  
c/o Integrated Investment Partners  
PO Box 916  
Portland, OR 97207  
(503) 666-9600

**PROJECT**

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**DRAWING TITLE**

**SITE LIGHTING SPECIFICATIONS**

**REVISIONS**

**STAMP**

DATE: December 5, 2012  
REQ. SUBMITTAL DATE:  
DATE:  
DATE:  
DATE:

BID DATE:  
CKE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE ARCHITECT OR WILSONVILLE OR OTHER RELATED REGULATORY AGENCY

**SE1.3**

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14





## Chicago Title Insurance Company of Oregon

1211 SW 5th Avenue, Suite 2130, Portland, OR 97204  
(503)973-7400 FAX (503)248-0324

### PRELIMINARY REPORT

ESCROW OFFICER: Kelly Norton  
TITLE OFFICER: Tony Schadle

ORDER NO.: 472512495862KN-CT50  
**FIRST SUPPLEMENTAL**

TO: Chicago Title Company of Oregon  
1211 SW 5th Avenue, Suite 2130  
Portland, OR 97204

OWNER/SELLER: South Sea, LLC

BUYER/BORROWER: Sunnyside Devco, LLC

**PROPERTY ADDRESS:** SE Corner of SW Boones Ferry & SW 95th Ave.  
Wilsonville, Oregon 97070

**EFFECTIVE DATE:** April 16, 2012, 05:00 PM

1. THE POLICY AND ENDORSEMENTS TO BE ISSUED AND THE RELATED CHARGES ARE:

	<u>AMOUNT</u>	<u>PREMIUM</u>
Owner's Standard	795,000.00	\$ 1,793.00
Governmental Service Fee		\$ 25.00

2. THE ESTATE OR INTEREST IN THE LAND HEREINAFTER DESCRIBED OR REFERRED TO COVERED BY THIS REPORT IS:

A Fee

3. TITLE TO SAID ESTATE OR INTEREST AT THE DATE HEREOF IS VESTED IN:

South Sea, L.L.C., an Oregon limited liability company

4. THE LAND REFERRED TO IN THIS REPORT IS SITUATED IN THE CITY WILSONVILLE IN THE COUNTY OF WASHINGTON, STATE OF OREGON, AND IS DESCRIBED AS FOLLOWS:

SEE EXHIBIT "ONE" ATTACHED HERETO AND MADE A PART HEREOF



**PRELIMINARY REPORT**  
(Continued)

Order No.: 472512495862KN-CT50

**EXHIBIT "ONE"**

A tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, in the Southeast one-quarter of Section 2, Township 3 South, Range 1 West of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, being further described as follows:

Commencing at the Southeast corner of Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, recorded in Book 31 at Page 14 in the Plat Records of Washington County, Oregon; thence South 89°38'33" West, along the South line of said Lot 7, a distance of 379.33 feet to a point 12.00 feet East of the East line of Parcel I as described in Deed from John Q. Hammons to the State of Oregon, by and through its Department of Transportation, Document No. 95-027726, recorded April 21, 1995 (hereinafter referred to as "ODOT"); thence North 00°09'24" East parallel to said East line, 18.00 feet to the true point of beginning; thence continuing North 00°09'24" East along said Easterly line, 341.16 feet; thence along the arc of a 116.16 foot radius curve to the right, through a central angle of 48°43'29", an arc length of 98.78 feet, the chord of which bears North 24°31'08" East, 95.83 feet; thence along the arc of a 45.00 foot radius curve to the right, through a central angle of 67°23'57", an arc length of 52.94 feet, the chord of which bears North 82°35'16" East 49.94 feet; thence along the arc of a 100.00 foot radius curve to the right, through a central angle of 37°13'18", an arc length of 64.96 feet, the chord of which bears South 45°05'58" East, 63.83 feet to a point on the Westerly line of Boones Ferry Road as described in said "ODOT" Deed; thence along the said Westerly line along the arc of a tangent 595.65 foot radius reverse curve to the left, the radius bears North 63°30'41" East, through a central angle of 02°45'38", an arc length of 28.70 feet, the chord of which bears South 27°52'08" East 28.70 feet; thence non-tangent South 15°09'35" West 83.41 feet; thence South 38°02'13" East, 122.78 feet; thence leaving said Westerly line, South 51°57'47" West, 20.00 feet; thence South 20°40'49" West, 186.07 feet to a point that is 18.00 feet measured at right angles from the South line of said Lot 7; thence parallel to said South line of Lot 7, South 89°38'33" West 121.22 feet to the true point of beginning.

EXCEPTING THEREFROM that portion thereof conveyed to the City of Wilsonville for right-of-way purposes in Warranty Deed recorded November 23, 2009 as Fee No. 2009-102082, Washington County Deed Records.

**AS OF THE DATE OF THIS REPORT, ITEMS TO BE CONSIDERED AND EXCEPTIONS TO COVERAGE IN ADDITION TO THE PRINTED EXCEPTIONS AND EXCLUSIONS IN THE POLICY FORM WOULD BE AS FOLLOWS:**

**GENERAL EXCEPTIONS:**

1. Taxes or assessments which are not shown as existing liens by the records of any taxing authority that levies taxes or assessments on real property or by the Public Records; proceedings by a public agency which may result in taxes or assessments, or notices of such proceedings, whether or not shown by the records of such agency or by the Public Records.
2. Facts, rights, interests or claims which are not shown by the Public Records but which could be ascertained by an inspection of the Land or by making inquiry of persons in possession thereof.
3. Easements, or claims of easement, not shown by the Public Records; reservations or exceptions in patents or in Acts authorizing the issuance thereof; water rights, claims or title to water.
4. Any encroachment, encumbrance, violation, variation, or adverse circumstance affecting the Title that would be disclosed by an accurate and complete land survey of the Land. The term "encroachment" includes encroachments of existing improvements located on the Land onto adjoining land, and encroachments onto the Land of existing improvements located on adjoining land.
5. Any lien for services, labor or material heretofore or hereafter furnished, or for contributions due to the State of Oregon for unemployment compensation or worker's compensation, imposed by law and not shown by the Public Records.

**SPECIFIC ITEMS AND EXCEPTIONS:**

6. Unpaid Property Taxes with partial payment are as follows:  
Fiscal Year: 2011-2012  
Original Amount: \$10,586.85  
Unpaid Balance: \$7,057.90, plus interest if any  
Levy Code: 088.08  
Account No.: R2087171  
Map No.: 3S12DB-00302  
  
Prior to close of escrow, please contact the Tax Collector's Office to confirm all amounts owing, including current fiscal year taxes, supplemental taxes, escaped assessments and any delinquencies.
7. City Liens, if any, in favor of the City of Wilsonville. None found as of January 19, 2012.
8. Limited Access Provisions contained in Deed to the State of Oregon, by and through its State Highway Commission, which, among other things, provides that no right or easement of right of access to, from or across the State Highway other than expressly therein provided for shall attach to the abutting property;  
Recording Date: May 21, 1969  
Book: 744  
Page: 156
9. Waiver of Remonstrance and Consent to Local Improvement District:  
Purpose: Street  
Recording Date: February 6, 1974  
Book: 961  
Page: 997

10. Waiver of Remonstrance and Consent to Local Improvement District:  
Purpose: Street  
Recording Date: February 6, 1974  
Book: 962  
Page: 1
11. Easement for the purpose shown below and rights incidental thereto as set forth in a document:  
In favor of: Adjacent property owner  
Purpose: Ingress, egress and parking  
Recording Date: May 16, 1979  
Recording No.: 79-018830  
Affects: The Westerly portion
12. Limited Access Provisions along with Easements for water, gas, electric, communication service lines, sign and drainage, contained in Deed to the State of Oregon, by and through its State Highway Commission, which, among other things, provides that no right or easement of right of access to, from or across the State Highway other than expressly therein provided for shall attach to the abutting property;  
Recording Date: April 21, 1995  
Recording No.: 95-027726
13. Reciprocal Easement Agreement, including the terms and provisions thereof;  
Recording Date: January 16, 1997  
Recording No.: 97-005009
14. Easement for the purpose shown below and rights incidental thereto as set forth in a document:  
In favor of: Exxon of Wilsonville, LLC  
Purpose: Sanitary sewer and storm drainage facilities  
Recording Date: April 30, 2002  
Recording No.: 2002-051320  
Affects: The Northeasterly portion  
  
An area affecting a portion of said Easement was released by instrument;  
Recording Date: November 23, 2009  
Recording No.: 2009-102085
15. Common Ingress and Egress Easement, including the terms and provisions thereof;  
Recording Date: April 30, 2002  
Recording No.: 2002-051321
16. Easement for the purposes shown below and rights incidental thereto as set forth in a document:  
In favor of: The City of Wilsonville  
Purpose: Sanitary sewer, storm drain or water pipelines  
Recording Date: March 12, 2003  
Recording No.: 2003-036755  
Affects: A 15 foot strip along the Easterly lot line
17. Restrictive Covenant, including the terms and provisions thereof;  
Recording Date: March 10, 2005  
Recording No.: 2005-025345
18. Agreement for Grant of Easement, including the terms and provisions thereof;  
Recording Date: March 31, 2006  
Recording No.: 2006-037608

19. A Deed of Trust to secure an indebtedness in the amount shown below,  
Amount: \$600,000.00  
Dated: April 3, 2006  
Grantor: South Sea, L.L.C., an Oregon limited liability company  
Trustee: West Coast Trust  
Beneficiary: West Coast Bank  
Loan No.: 4000991  
Recording Date: April 3, 2006  
Recording No: 2006-038268

And modified by instrument;  
Recording Date: April 17, 2007  
Recording No: 2007-042679

And modified by instrument;  
Recording Date: April 23, 2008  
Recording No: 2008-037089

And modified by instrument;  
Recording Date: May 11, 2009  
Recording No: 2009-041081

A Partial Release from said Deed of Trust;  
Recording Date: November 24, 2009  
Recording No: 2009-102471

And modified by instrument;  
Recording Date: November 4, 2010  
Recording No: 2010-088854

And modified by instrument;  
Recording Date: November 7, 2011  
Recording No: 2011-078762

NOTE: Said maturity date was extended to October 10, 2014.

20. Easement for the purpose shown below and rights incidental thereto as set forth in a document:  
In favor of: The City of Wilsonville  
Purpose: Public utility  
Recording Date: November 23, 2009  
Recording No: 2009-102083  
Affects: The Westerly portion
21. Easement for the purpose shown below and rights incidental thereto as set forth in a document:  
In favor of: The City of Wilsonville  
Purpose: Slope  
Recording Date: November 23, 2009  
Recording No: 2009-102084  
Affects: The Westerly portion
22. Easement for the purpose shown below and rights incidental thereto as set forth in a document:  
In favor of: The City of Wilsonville  
Purpose: Private utility  
Recording Date: November 23, 2009  
Recording No: 2009-102086  
Affects: The Northerly portion

23. If requested to issue an extended coverage ALTA lenders policy, the following matters must be addressed:
- a) The rights of tenants holding under unrecorded leases
  - b) \*\*\* Deleted \*\*\*
  - c) Matters disclosed by a statement as to parties in possession and as to any construction, alterations or repairs to the Land within the last 75 days. The Company must be notified in the event that any funds are to be used for construction, alterations or repairs.

24. **\*\*ADDED\*\***

Potential encroachment matters set forth in survey prepared by Northwest Surveying, Inc., dated March 27, 2012, Job No. 787 as follows:

- [1.] a 24" diameter storm pipe across the property without benefit of an easement
- [2.] a waterline is constructed outside of its easement by up to 12.5'
- [3.] an underground natural gas line across property without benefit of an easement
- [4.] a 25'+/- high sign with power near the southwest corner
- [5.] sidewalk extends up to 3.0' outside of granted easement area
- [6.] a 6' high cyclone fence extends up to 7.0 feet onto property

ADDITIONAL REQUIREMENTS AND NOTES

- A. NOTE: Effective January 1, 2008, Oregon law (ORS 314.258) mandates withholding of Oregon income taxes from sellers who do not continue to be Oregon residents or qualify for an exemption. Please contact your Escrow Closer for further information.
- B. NOTE: A copy of the terms and provisions of the operating agreement for the limited liability company set forth below should be furnished for our examination prior to closing. Any conveyance or encumbrance of said company's property must be executed by all of the members unless otherwise provided for in the operating agreement. In addition, if there have been any changes in membership from the date of original creation of the limited liability company to the present date, copies of approval of withdrawal and/or acceptance of such member should be furnished for our examination.  
Limited Liability Company: South Sea, L.L.C.
- C. NOTE: A copy of the terms and provisions of the operating agreement for the limited liability company set forth below should be furnished for our examination prior to closing. Any conveyance or encumbrance of said company's property must be executed by all of the members unless otherwise provided for in the operating agreement. In addition, if there have been any changes in membership from the date of original creation of the limited liability company to the present date, copies of approval of withdrawal and/or acceptance of such member should be furnished for our examination.  
Limited Liability Company: Sunnyside Devco, LLC
- D. NOTE: There are no matters against the party(ies) shown below which would appear as exceptions to coverage in a title insurance product:  
Party: Sunnyside Devco, LLC
- E. NOTE: Any transfer of the herein described property is subject to the payment of Washington County Transfer Tax at the rate of \$1.00 per \$1,000.00 or fraction thereof of stated consideration.
- F. NOTE: No search has been made or will be made for water, sewer, or storm drainage charges unless the city/service district claims them as liens (i.e., foreclosable) and reflects them on its lien docket at the date of closing. Buyers should check with the appropriate city bureau or water/service district and obtain a billing cutoff. Such charges must be adjusted outside of escrow.

- G. In addition to the standard policy exceptions, the exceptions enumerated above shall appear on the final 2006 ALTA policy unless removed prior to issuance.
  
- H. THE FOLLOWING NOTICE IS REQUIRED BY STATE LAW; YOU WILL BE REVIEWING, APPROVING AND SIGNING IMPORTANT DOCUMENTS AT CLOSING. LEGAL CONSEQUENCES FOLLOW FROM THE SELECTION AND USE OF THESE DOCUMENTS. YOU MAY CONSULT AN ATTORNEY ABOUT THESE DOCUMENTS. YOU SHOULD CONSULT AN ATTORNEY IF YOU HAVE QUESTIONS OR CONCERNS ABOUT THE TRANSACTION OR ABOUT THE DOCUMENTS. IF YOU WISH TO REVIEW TRANSACTION DOCUMENTS THAT YOU HAVE NOT SEEN, PLEASE CONTACT THE ESCROW AGENT.



NOTICE



29799 SW Town Center Loop E  
Wilsonville, Oregon 97070  
(503) 682-1011  
(503) 682-1015 Fax Administration  
(503) 682-7025 Fax Community Development

December 12, 2012

Josh Veentjer  
Wilsonville Devco LLC  
PO Box 916  
Portland, OR 97207

Application Numbers: DB12-0074 through DB12-0076 Carl's Jr. and Commercial Building  
Legal: 25250 SW 95th Ave., the property described as Tax Lot 0302, Section 2DB, Township 3  
South, Range 1 West, Willamette Meridian, City of Wilsonville, Washington County, Oregon.

Status: Notice that Your Application IS NOT COMPLETE.

Dear Mr. Veentjer:

You are listed as the authorized representative on a City of Wilsonville Site Development Application form submitted regarding the property described above. Your application materials were received by the City on November 30, 2012 in application for a Class 3 Stage II Final Plan, Site Design Review, and Master Sign Plan for Fast Food Restaurant and Multi-tenant Commercial Building.

The materials submitted with the application form discussed above were found to be incomplete, when reviewed regarding the applicable provisions of ORS 227.178(2) and Subsection 4.035(.05) WC, due to the following missing items 1-25:

**General**

1. Pursuant to Section 4.011 documentation that no liens exist on the property.
2. Pursuant to Subsection 4.140 (.03) A. signature of the owner of all tracts of land included in the application. A portion the landscape area west of the Carl's Jr. restaurant is on the property owned by Lapoint Business Group LLC. Mr. Lapoint needs to be a joint applicant to include development on his land.
3. Pursuant to Subsection 4.035 (.04) A. 6 sheet A1.0 "Architectural Site Plan" at a standardized scale. The scale on the full sized sheet indicated it is 1 inch equals 20 feet. However, the sheet was reproduced in a manner that is no longer 1:20 scale.



## **Parking and Circulation**

4. Pursuant to Subsection 4.140 (.09) D., 4.155 (.03) B. 5., 4.177 (.01) E. 3., 4.421(.01) C. and 4.430 (.02) G., drawings to scale indicating circulation, parking, and turning movements, as applicable, for the following as well as the related access easements:
  - Passenger vehicles
  - Fire and rescue apparatus
  - Fuel delivery trucks
  - Delivery trucks for restaurant and multi-tenant commercial building
  - Busses, RV's, trailers, and other large format vehicles
  - Pedestrians (circulation internally, and from Holiday Inn for guests and potentially drivers/passengers of larger format vehicles)

Note: Mitigation and Recommendations of the DKS traffic report also suggests the applicant provide turning templates. Specifically it asks the turning templates show how passengers from drive-thru can make 180 degree right turn without extending into the oncoming traffic lanes and show how trucks can stay in north bound lanes turning out onto 95<sup>th</sup> Avenue as well as likely on-site truck maneuvering.

5. Pursuant to Subsection 4.421 (.01) C. documentation regarding the recommendation on page 20 of the DKS Traffic Report to move back the stop bars at various locations to limit pedestrian/vehicle conflicts.
6. Pursuant to Subsection 4.421 (.01) C. demonstrating special attention given to pedestrian/vehicle conflicts in the drive-thru lane. The pedestrian crossing of the drive-thru lane is a high risk location for pedestrian/vehicle conflicts. Striping can fade over time, so staff recommends articulating the crossing by making it concrete or otherwise a different material from the drive lane to create a very durable and visible demarcation of the pedestrian crossing.

## **Signs**

7. Pursuant to Subsections 4.156.02 (.05) D. 2. and 5., (.06) B., (.07) A. and C., 4.156.08 (.01) C., C. 1., and C. 1. b. materials supporting the collocation of the Carl's Jr. sign on the relocated freestanding Chevron sign. Information is not provided demonstrating how the proposed sign satisfies the current sign regulations. The sign, as proposed, likely requires modification to the Chevron Master Sign Plan and a waiver to the allowed sign area. Pursuant to the development agreement also include information on the sign, access, and maintenance agreement and approval from Chevron corporate for the Carl's Jr. sign to be collocated. The collocation of the Carl's Jr. sign may require a building permit to review the engineering of the existing pole sign structure with the additional load.
8. Pursuant to Subsection 4.156.02 (.07) and (.07) A. 1. definition of flexibility, including placement, for signage in the scenario of future rebranding or tenant change for Carl's Jr. restaurant.

9. Pursuant to Subsections 4.156.02 (.07) A. 3., B. 1. and B. 2. more definition and guidance on materials, design, and placement of building signs. The goal is appropriate guidelines to ensure consistent and compatible sign design while allowing the “right” amount of flexibility. Also, consideration needs to be given to sign placement for different tenant configurations in the multi-tenant building (scenarios where building is occupied by 1, 2, or 3 tenants as stated on page 8 of the submitted narrative). Subsection 4.440 (.01) D. requires, the location, size, design, material, color and methods of illumination of all exterior signs, including those on retail building be identified.
10. Pursuant to Subsection 4.156.03 (.01) B. define shapes used to measure area of odd shaped signs consistent with this this subsection, specifically the Carl’s Jr. sign on top of the proposed new freestanding sign and Carl’s Jr. wall signs.
11. Pursuant to 4.156.02 (.05) D. 2. information demonstrating the proposed monument and freestanding signs on the northern part of the site can be constructed within the easements in which they are shown. Alternatively, the signs can be relocated outside the easements.
12. Pursuant to Subsections 4.156.02 (.05) D. 2. and 5., and 4.156.05 (.02) E. information addressing sign code compliance of the drive-thru menu board. In order to be exempt from sign permitting requirements the drive-thru menu board needs to be screened from off-site view. Landscaping or fencing meeting the high screen standard (Subsection 4.176 (.02) F.) would need to be used for a sufficient distance along 95<sup>th</sup> Avenue to block off-site view of the menu board. The landscaping is required to be 6 feet tall and 95% opaque year-round within three (3) years of planting. Alternatively, the sign could be included in the Master Sign Plan request as a non-exempt sign, which would require waivers to the sign regulations.
13. Pursuant to Subsection 4.156.08 (.01) K. information on building addresses for freestanding signs. All freestanding signs need to include addresses of related buildings unless otherwise approved in writing by the City and Fire District.
14. Pursuant to Subsection 4.156.08 (.03) B. information reflecting the area of the monument sign being subtracted from the allowed area for freestanding signs. Please show this in sign tables and calculations for the Master Sign Plan.

### **Landscaping**

15. Pursuant to Subsection 4.155 (.03) B. 2. information demonstrating all landscape islands meet the eight foot by eight foot (8’ x 8’) dimensional standard. One (1) landscape island immediately northeast of the Carl’s Jr. restaurant does not meet the dimensional standard.
16. Pursuant to Subsection 4.176 (.06) A. 3. information demonstrating the maximum area of turf or lawn is not exceeded. Turf or lawn cannot be used to cover more than ten percent (10%) of the landscaped area. The proposed landscape plan shows well in excess of ten percent (10%) of the landscaped area in turf grass.

17. Pursuant to Subsection 4.176 (.02) D. accurate information regarding street tree requirements along 95<sup>th</sup> Avenue. The section of 95<sup>th</sup> Avenue along the property as well as Boones Ferry Road are arterials. The submitted narrative identifies 95<sup>th</sup> as a collector. Street trees need to be 3” caliper along these streets rather than 2” caliper shown in the landscape plans.

**Outdoor Lighting**

18. Pursuant to Subsections 4.199.40 (.01) A. and 4.199.50 (.01) C. information on all outdoor luminaires. Cut sheets need to be provided for, and lighting plans and other compliance documents need to show all outdoor luminaires including building mounted lighting such as those shown on Sheets A5.0 and AC2.1
19. Pursuant to Subsections 4.199.40 (.01) B. 2. and 4.199.50 (.01) D. plan check sheet demonstrating compliance with the *Oregon Energy Efficiency Specialty Code, Exterior Lighting* for the proposed outdoor lighting.
20. Pursuant to Subsection 4.199.40 (.01) B. 3. information on mounting height for all building mounted luminaires.
21. Pursuant to Subsection 4.199.40 (.01) B. 1. information demonstrating that all proposed outdoor luminaires comply with Table 7 of the Outdoor Lighting Ordinance. This includes the specific wattage and shielding of each proposed outdoor luminaire. Staff requests this be in a table format similar to the table below. If other design considerations conflict with this requirement, you may want to explore demonstrating compliance with the Outdoor Lighting Ordinance using the performance method.

Example of Table Demonstrating Compliance with Table 7 of the Outdoor Lighting Ordinance

Luminaire	Proposed Shielding	Proposed Wattage	Maximum Wattage Based on Shielding: Lighting Zone LZ2
Luminaire A	Fully Shielded	100	100
Luminaire B	Fully Shielded	100	100
Luminaire C	Shielded	25	35
Luminaire D	Partly Shielded	35	39
Luminaire E	Shielded	25	35
Luminaire F	Fully Shielded	75	39

**Mixed Solid Waste and Recycling Storage Area**

22. Pursuant to Subsection 4.430 (.04) A. a letter from allied waste approving mixed solid waste and recycling areas and access.
23. Pursuant to Subsection 8.210 (9) Wilsonville Code and Subsections 4.430 (.04) B. information on required covers for mixed solid waste and recycling storage areas. The

City's stormwater regulations require the waste and recycling enclosures be covered. Wilsonville Code Subsection 8.210 (9) reads, "Outside storage areas for grease, oil, waste products, recycling, garbage, and other sources of contaminants shall be a covered enclosure adequately sized to allow all containers to be accessible. No drainage is allowed to enter the storm sewer system." Subsection 4.430 (.04) B. requires vertical clearance to be at least eight feet (8').

### **Site Design Review**

24. Pursuant to Subsection 4.440 (.01) D. information on color, texture, and material for glazing on both buildings, detention pond fencing, wall for placement of monument sign, exterior site furnishings such as benches and waste baskets for the public, and covers and related supports for mixed solid waste and recycling enclosures.
25. Findings for Site Design Review, Subsections 4.400 through 4.440, refer to another development project. Please update these findings to specifically address the subject application.

The above items 1-25 must be addressed in order to complete the applications. Please provide ten (10) copies of the revised project narrative, findings, and reduced 11" by 17" plans, full sheet plans drawn to scale and folded and ten (10) cd's of the project narrative, findings, and plans. When you have resubmitted the application materials, staff will have up to 30 days in which to determine whether the application is complete. ORS 227.178.

If you have any questions, please contact me at 503-682-4960, or at [pauly@ci.wilsonville.or.us](mailto:pauly@ci.wilsonville.or.us)

Sincerely,



Daniel Pauly, AICP  
Associate Planner  
City of Wilsonville

cc:

Craig Anderson  
CB Anderson Architects  
7209 Greenwood Ave. N.  
Seattle, WA 98103

Ben Altman  
SFA Design Group  
9020 SW Washington Sq. Dr., Ste. 505  
Portland, OR 97223

December 12, 2012

Dear Mr. Veentjer:

As indicated in the attached correspondence, your applications:

Case File DB12-0074 through DB12-0076: Carl's Jr. and Commercial Building

has been deemed to be incomplete.

You must acknowledge, in writing, your intent to provide the material required to complete the application, as identified in the attached correspondence, dated December 12, 2012. To do this, please sign below and return this acknowledgment by December 26, 2012, to:

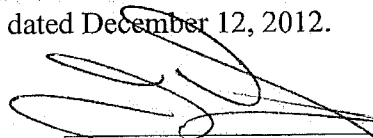
Planning Division – Attention: Daniel Pauly  
Community Development Department  
City of Wilsonville  
29799 SW Town Center Loop Drive E  
Wilsonville, OR 97070

If you indicate your intent to complete the application, you will have 180 days from the date of the submittal (November 30, 2012) to provide the required material. If you fail to submit the required material within 180 days (May 29, 2013), your application will be deemed void. The case files regarding the applications will then be closed.

If you do not return this acknowledgment, such action will be considered to be a refusal under the meaning accorded in ORS 227.178(2). Your applications will then be processed based upon the information you have previously submitted. Note that failure to submit sufficient evidence or material to demonstrate compliance with the applicable criteria is grounds for denial of the application.

**ACKNOWLEDGEMENT**

I  intend /  refuse) to provide the additional material identified in correspondence from the Division of Planning and Development, dated December 12, 2012.

  
\_\_\_\_\_  
Signed and Acknowledged (Applicant)

12 / 12 / 12  
\_\_\_\_\_  
Date



**Ben Altman**

---

**From:** Pauly, Daniel [pauly@ci.wilsonville.or.us]  
**Sent:** Monday, November 26, 2012 3:54 PM  
**To:** Ben Altman  
**Cc:** Edmonds, Blaise  
**Subject:** RE: Carl's Jr Application Fees

Good Afternoon Ben

I concur with your fee calculations, including the fee credit.

Regards



Daniel Pauly, AICP  
Associate Planner

City of Wilsonville Planning Division  
503-682-4960  
pauly@ci.wilsonville.or.us

Disclosure: Messages to and from this E-mail address may be subject to Oregon Public Records Law.

---

**From:** Edmonds, Blaise  
**Sent:** Monday, November 26, 2012 8:21 AM  
**To:** Pauly, Daniel  
**Subject:** FW: Carl's Jr Application Fees

Hi Dan, this one will be yours, please respond.

Thank you,

**Blaise Edmonds**

Manager of Current Planning  
City of Wilsonville  
29799 SW Town Center Loop E  
Wilsonville, OR 97070

503-682-4960 Business  
503-682-7025 Fax  
[edmonds@ci.wilsonville.or.us](mailto:edmonds@ci.wilsonville.or.us)

**DISCLOSURE NOTICE:** Messages to and from this e-mail address may be subject to the Oregon Public Records Law.

---

**From:** Ben Altman [mailto:baltman@sfdag.com]  
**Sent:** Friday, November 23, 2012 10:24 AM  
**To:** Edmonds, Blaise  
**Subject:** Carl's Jr Application Fees

Blaise: I wanted to confirm the application fees for the Carl's Jr site at the north end. My calculation is as follows:

11/26/2012

<b>1.</b> PDC Stage II, Final Development Plan	\$1,920
<b>a.</b> for 1.28 acres @ \$240	\$ 307.20
<b>b.</b> 2,867 sq. ft. Carl's Jr. and 3,150 sq. ft. Office/Retail Building	
<b>c.</b> Total 6,017 sq. ft. @ .024	\$ 144.41
2. Master Sign Plan; and	\$ 960
3. Architectural and Site Design Review	<u>\$1,280</u>
Total	\$4,611.61

However, I also understand that under the right-of-way acquisition (George Brice) there is an application fee credit of \$10,621 applied to this property. So the actually application fee due would be zero dollars.

Please confirm.

Thanks,

**Ben Altman**  
Senior Planner/Project Manager

**SFA DESIGN GROUP, LLC**  
STRUCTURAL | CIVIL | LAND USE PLANNING | SURVEYING  
9020 SW Washington Square Dr., Suite 505 Portland, OR 97223  
P (503) 641-8311  
F (503) 643-7905  
E [ballman@sfadg.com](mailto:ballman@sfadg.com)  
[www.sfadesigngroup.com](http://www.sfadesigngroup.com)

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# CITY OF WILSONVILLE

29799 SW Town Center Loop East  
Wilsonville, OR 97070  
Phone: 503.682.4960  
Fax: 503.682.7025  
Web: [www.ci.wilsonville.or.us](http://www.ci.wilsonville.or.us)

## Planning Division Development Permit Application

Final action on development application or zone change is required within 120 days in accordance with provisions of ORS 227.175

A pre application conference is normally required prior to submittal of an application. Please visit the City's website for submittal requirements

Incomplete applications will not be scheduled for public hearing until all of the required materials are submitted.

Pre-Application meeting date: 2/16/12

### TO BE COMPLETED BY APPLICANT:

Please PRINT legibly

Legal Property Owner's Name:

Wilsonville Devco, LLC

Authorized Representative:

Address: 4188 SW Greenleaf Dr.

Address: \_\_\_\_\_

Portland, OR 97221

Phone: (503) 201-1309

Phone: \_\_\_\_\_

Fax: \_\_\_\_\_

Fax: \_\_\_\_\_

E-mail: Josh@iipcorp.com

E-mail: \_\_\_\_\_

Property Owner or  
Authorized Signature:

Name  
Printed Josh Veentjer

Date 9/7/12

Title: Managing member

### Site Location and Description:

Project Address if Available: \_\_\_\_\_

Suite/Unit \_\_\_\_\_

Project Location: SE Corner of SW 95th + SW Boones Ferry Rd.

Tax Map #(s): 3812DB-00302 Tax Lot #(s): \_\_\_\_\_

County:  Washington  Clackamas

Request: \_\_\_\_\_

Project Type: Class I  Class II  Class III

Residential

Commercial

Industrial

Other (describe below)

### Application Type:

Annexation

Appeal

Comp Plan Map Amend

Conditional Use

Final Plat

Major Partition

Minor Partition

Parks Plan Review

Plan Amendment

Planned Development

Preliminary Plat

Request to Modify Condition

Request for Special Meeting

Request for Time Extension

Signs

Site Design Review

SROZ/SRIR Review

Staff Interpretation

Stage I Master Plan

Stage II Final Plan

Type C Tree Removal Plan

Tree Removal Permit (B or C)

Temporary Use

Variance

Villebois SAP

Villebois PDP

Villebois FDP

Waiver

Zone Map Amendment

Other

### FOR STAFF USE ONLY:

Application Rec'd: \_\_\_\_\_ Fee: \_\_\_\_\_ Check #: \_\_\_\_\_ Application Complete: \_\_\_\_\_ By: \_\_\_\_\_

File No (s) \_\_\_\_\_

**Land Use Application  
City of Wilsonville**

**Boones Ferry Pointe  
Carl's Jr. and Retail Center  
includes Shared Access, Egress and Signage  
with Chevron Site**

**Stage II Final Development Plans;  
Master Sign Plan; and  
Site Design Review**

**November 30, 2012  
Revised for Completeness  
January 21, 2013**

**APPLICANT/OWNER:  
Wilsonville Devco, LLC  
PO Box 916  
Portland, OR 97207  
Contact: Josh Veentjer  
josh@iipcorp.com**

**APPLICANT/OWNER:  
LaPoint Business Group  
10618 Crosby Road, NE  
Woodburn, OR 97071  
Garry LaPoint  
gl@eoni.com**

**APPLICANT'S REPRESENTATIVES:**

**CB Anderson, Project Architects  
7209 Greenwood Ave. N.  
Seattle, WA 98103  
Contact: Craig Anderson  
206-782-2911  
craig@cba-arch.com**

**SFA Design Group, Project Planning  
9020 SW Washington Square Dr., Ste 505  
Portland, OR 97223  
Contact: Ben Altman  
503-641-8311  
baltman@sfadg.com**

**FACT SHEET:**

**Project Name:** Boones Ferry Pointe

**Type of Application:** Stage II Development Plan; Master Sign Plan; and Site Design Review for Carl's Jr. and 3,150 square foot Retail Building. The Master Sign Plan incorporates portions of the Chevron Signage

**Site Location:** SW 95<sup>th</sup> Avenue

**Tax Lots:** T3S R1W, Map 2DB, Lot 302

**Land Area:** 55,605 square feet; or 1.30 Acres

**Comp. Plan/Zoning:** Commercial/PDC, Planned Development Commercial

**Applicants/Owners:** Wilsonville Devco, LLC and LaPoint Business Group

**Design Team:**

Project Planning

Ben Altman, SFA Design Group

Project Architect

CB Anderson

Craig Anderson - Primary Contact

Cameron Fultz

Civil Structural/Engineering

Kittredge Engineers

Chris Kittredge, PE

Engineering MEP

Priest Engineering

Arwin Priest

Brett Wixson

Landscape Architect

Lauchlin R. Buthune Associates, Inc

Lauchlin Bethune

Sign Resource Identity Group

Michael Ehrenberger

Geo-Tech

GeoPacific Engineering, Inc.

Beth, Rapp, PE

Traffic Engineer

DSK Associates

Scott Mansur

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## **I. Introduction**

This application is submitted on behalf of Josh Veentjer, Wilsonville Devco, LLC representing the owner and applicant for Tax Lot 302.

However, this application also includes some property (joint access) owned by LaPoint Business Group, dba North Wilsonville Chevron. In addition to the joint access, this application includes a Master Sign Plan, which incorporates the freestanding Chevron signs. Therefore Mr. LaPoint has signed as a Co-Applicant.

The requested Land Use actions include the following:

1. PDC Stage II, Final Development Plans, for a 2,867 square foot Carl's Jr. and 3,150 square foot Retail Building, including modified access & egress;
2. Master Sign Plan, including Freestanding Chevron signs; and
3. Architectural and Site Design Review, for two buildings, parking, landscaping, and trash enclosures.

The subject site is Tax Lot 302, Map T3S R1W 2DB. This property was original identified as Phase II of the LaPoint Center, of which Phase I was the existing Chevron Station and C Store. Wilsonville Devco, LLC currently owns the property, which was purchased from George Brice, who previously purchased it from Mr. LaPoint.

The Carl's Jr. site contains approximately 55,605 net square feet following the public acquisition of additional right-of-way for improvements on 95<sup>th</sup> Avenue and Boones Ferry Road. This site is currently vacant and there are no trees or significant vegetation on the site. This site has been the subject of a few different development plans, none of which ever received final development permit approval.

Prior applications did not move forward primarily because of traffic capacity issues at the 95<sup>th</sup>/Boones Ferry intersection. However, street improvements have recently been completed through a City/ODOT jointly funded project.

### **Development Agreement**

Prior to submittal of this application, the applicant has worked with the City and the owner of the adjacent Chevron Station, and the Holiday Inn to refine access, egress, and site circulation. The conclusions reached by the parties have been recorded in a Development Agreement, a copy of which is included in the application documents, see Index Tab.

The Development Agreement provides for the driveway configuration, which will provide two inbound lanes, after the sign island and two outbound lanes, with a 50 foot north bound curb radius to accommodate trucks. It also provides for reconfiguration of the access and egress points for the Holiday Inn. In addition the Agreement provides for relocation of the existing freestanding Chevron Sign and provision for adding a panel for Carl's Jr.

## **Proposed Development – Boones Ferry Pointe**

This Land Use application addresses the proposed PDC, Stage II Final Development Plans for the two buildings, a Master Sign Plan, incorporating the Chevron freestanding signs, and the Site Design Review.

### **Design Team**

The Planned Development Regulations require a Design Team. The Professional Design Team for this project includes the following:

- Ben Altman, SFA Design Group, Planner
- Craig Anderson & Cameron Fultz CB Anderson, Architecture
- Chris Kittredge, Kittredge Engineers, Civil Engineering & Surveying
- Priest Engineering, MEP Engineer
- Lauchlin Bethune Associates, Landscape Architect
- GeoPacific, Geo-Technical Engineer
- Michael Ehrenberger, Sign Resources Identity Group, Signage

### **Land Use and Development History**

City of Wilsonville Records for the LaPoint Center includes the following planning and land use approval:

1. The Stage I Master Plan 97DB28

A records search revealed the above reference Design Review file related to the existing Chevron Station. As part of that review the Stage I Preliminary Plan labeled the area north of the fuel station as 'Phase II', which is the Subject Site. As such, the Planning Staff concluded that no modification of that master plan is necessary for the proposed new development. Except pages from the staff report, including the drawing that illustrates the Phasing Plan is attached see Index Tab - Prior Approvals.

Also attached are two pages of the staff report regarding a development proposal in 2006 that was never approved. The value in including this unapproved proposal is that it shows how the Stage I Preliminary Plan was treated in the application.

The Carl's Jr. site shares access with the Chevron station and the Holiday Inn. A portion of the access/egress area is owned by the owner of the Chevron station. Therefore Mr. LaPoint is a joint applicant.

### **Surrounding Development**

This property is located in the area originally master planned as the Commerce Circle Industrial Park. However, it is now separated from the Commerce Circle area by 95<sup>th</sup> Avenue. The AGC Building is located west across 95<sup>th</sup> from the site.

The site abuts in intersection of 95<sup>th</sup> Avenue and Boones Ferry Road, and is located immediately north of the Holiday Inn and west of the Chevron Station.

To the east of the site is Boones Ferry Road.

### **Existing Development – Current Stage I Master Plan**

This property was original identified as Phase II of the LaPoint Center, of which Phase I was the existing Chevron Station and C Store. However, the original Stage I Master Plan did not specify any particular development for this portion of the site. The only specific development approved was the Stage II Final Development Plans for the Chevron Station (originally Exxon). The Chevron site is developed with pump islands and a C-Store.

The original property was Partitioned, creating Tax Lot 302. The partitioned property was subsequently sold to another party (George Brice). Mr. Brice made several unsuccessful attempts at development proposals, all failing due to lack of traffic capacity. Recently the property was acquired by Wilsonville Devco, LLC the current applicant.

### **Existing Access and Road Improvements**

The subject site has excellent arterial street access, with frontage on 95<sup>th</sup> Avenue and on Boones Ferry Road. There is only one existing driveway on 95<sup>th</sup> Avenue, which serves the subject site and the two adjacent developments including the Holiday Inn and Chevron Station. There is no direct access to Boones Ferry Road.

- 95<sup>th</sup> Avenue is classified as a north/south Major Collector. It extends between Boones Ferry Road and Boeckman Road.
- Boones Ferry Road is classified as a Major Arterial, is under the jurisdiction of ODOT, and crosses the I-5 Freeway at the Elligsen Interchange.

Through a jointly funded project the City of Wilsonville and ODOT are just now completing street and intersection improvements adjacent to the subject site. These improvements provide for double left turns from Boones Ferry to 95<sup>th</sup>, and double right turns from 95<sup>th</sup> to Boones Ferry. The improvements also modify the intersection with Commerce Circle North, limiting it to right-in and right-out movements. And, they include the modified curb cut design established in the Development Agreement.

### **Existing Utilities**

The subject site is currently served by a full range of urban services. All services were extended to the site as part of the development of the Chevron site.

### **Proposed – Stage II, Final Development Plans – Carl’s Jr. and Retail Building**

The Site Development Plan provides for a 2,867 square foot Carl’s Jr. Restaurant, together with a 3,150 square foot Retail Building. No tenants have been identified for the retail building at this time, but it is designed to accommodate up to three tenants.

The development is proposed to be known as **Boones Ferry Pointe**:

- The Carl's Jr. Restaurant building will be located in the southwestern portion of the site. It will contain approximately 2,867 square feet, and will include a drive-up service window. The building is designed as a (18 foot) one-story structure, with varied parapet heights. The higher parapets will accommodate the proposed building signage.

The building will be constructed with a combination of brick and horizontal artisan siding, with colored awnings over the windows. It will include a drive-up window, and will have its own trash enclosure.

- The Retail Building will be constructed in the northern portion of the site and will contain approximately 3,150 square feet. This building is anticipated to be ultimately occupied by 1-3 tenants; however, no tenants have been identified at this time.

The building will be constructed with similar materials and colors as the Carl's Jr. building.

- Other site plan details include:
  - A landscaped patio north of the Retail Building, including a Master Site Sign (Boones Ferry Pointe) and a 30 foot tall flag pole. This landscaped patio area will create an attractive entry node for the commercial development district;
  - Free Standing Sign, one northeast of the Retail Building and a new Carl's Jr. panel added to the relocated Chevron Pole Sign;
  - Driveway connection between the Chevron site and the Carl's Jr site; and
  - There will be a total of three trash enclosures, one for the Carl's Jr. and Dual Trash Enclosures to serve the Retail Building and to relocate and enlarge the existing enclosure for the Chevron Site.

**Table 1**  
**Stage II, Final Development Plans**  
**Carl's Jr. and Retail Building**  
**Land Area Allocation - Square Feet**

Use	Lot Coverage Square Feet	Percent of Site
Building	6,017	11
Landscaping	16,139	29
Paving & Parking	33,449	60
Total	55,605	100

## Compliance Report Format

In the following sections, compliance with applicable Plan policies and Code provisions are addressed. Specific Comprehensive Plan policies and Code provisions determined to be applicable to this application are presented in *10 pt Bold Italic*, with our compliance response presented in 12 pt regular type.

## II. Comprehensive Plan Compliance

The subject property is designated for commercial use on the Comprehensive Plan Map. Consistent with the Commercial Plan designation, the current and proposed uses for the property are commercial.

This property does not contain any identified or designated:

- Flood plain;
- Natural hazards areas;
- SROZ; or
- Area of Special Concern.

### COMMERCIAL DEVELOPMENT

Commercial areas designated on the Plan recognize and reinforce existing development patterns, at the north (Elligsen Road) and south (Wilsonville Road). The Plan also recognizes the commercial development potential within the Charbonneau District, and the need for complementary commercial uses within or near the industrial area of the City. All commercial districts are planned in the form of centers or complexes rather than as a strip development along major streets. Five types of commercial centers have been recommended in Wilsonville's Comprehensive Plan since 1971. They are:

Town Center;  
Service Centers;  
Office Complexes;  
Commercial Recreation Centers; and  
Neighborhood Commercial Sites.

Service Centers are primarily related to the motoring public and should be located at the freeway interchanges, particularly the Stafford Interchange. These centers would be the sites for motels, restaurants, automobile and truck service centers, and other large site users dependent on easy access for freeway travelers. Such centers may also be incorporated into industrial developments. Service centers will be "on view" to a maximum number of visitors to the City and, accordingly, their appearance and their physical and visual relationship to abutting land uses are critical. Such uses should not compete for the same retail market as that intended to be served by the Town Center.

**Policy 4.1.2 The City of Wilsonville shall encourage commercial growth primarily to serve local needs as well as adjacent rural and agricultural lands.**

Implementation Measure 4.1.2.a Encourage commercial uses which are compatible with the residential nature of the community, and are complementary to or supportive of industrial development in the City.

Implementation Measure 4.1.2.b Provide opportunities for a basic mix of needed goods and services.

Implementation Measure 4.1.2.c Encourage a rate of commercial development consistent with serving the needs of residents of the City and adjacent rural and agricultural lands.

Implementation Measure 4.1.2.d Cluster commercial activity near the freeway interchanges and encourage service or freeway-oriented commerce to locate near the Stafford Interchange. Encourage retail and other local-oriented commerce to locate in commercial districts along Wilsonville Road to minimize transient traffic impacts on the Wilsonville Interchange.

Implementation Measure 4.1.2.e Maintain the area south of the Willamette River for residential needs and with a residential character consistent with the amended Charbonneau Master Plan (which includes some commercial development).

Implementation Measure 4.1.2.f The City, in accordance with Title 4 of the Metro Urban Growth Management Functional Plan, will encourage development of lands designated by Metro as "Employment" and "Industrial" areas to include supportive retail development. Commercial uses in those areas can be expected to include some limited retail uses, primarily to serve the needs of people working or living in the immediate area and office complexes housing technology-based industries. Where the City has already designated land for commercial development within Metro's employment areas, the City has been exempted from Metro development standards.

Implementation Measure 4.1.2.g The location and development of commercial areas within the community should be given very careful consideration. Although they may occupy a relatively small percentage of the total land area, commercial developments customarily occur at points of maximum traffic movement and, therefore, have a tremendous impact on people's impressions of the visual quality of the community. If Wilsonville is to retain an image as a desirable place to live, its commercial areas must reflect that quality.

Implementation Measure 4.1.2.h Non-commercial uses may be permitted within a planned development commercial zone, provided that the predominant uses remain commercial. In many locations, the development of residential uses is appropriate and desirable in upper floors, while ground-floor uses remain commercial.

Implementation Measure 4.1.2.i As existing businesses are renovated and new ones are constructed, the Development Review Board will require high standards of compatibility with surrounding development, landscaping, architecture, and signage. The ability of a site to function properly in relation to the surrounding area will be emphasized.

Implementation Measure 4.1.2.j Neighborhood commercial, limited to convenience goods and services for local residents and workers, may be permitted as part of a Planned Development in a residential or industrial area provided the following criteria are met:

1. Sites shall be separated from other commercial uses by at least one-half (1/2) mile.
2. Each neighborhood commercial area shall be limited to no more than 5% of the total planned development acreage (gross) or one acre, whichever is less.
3. Sites shall have direct access to a street of at least a collector classification.
4. Pedestrian access to surrounding development areas should also be provided.
5. Sites shall not include more than one quadrant of an intersection and will not result in undue traffic congestion.

Implementation Measure 4.1.2.k In order to assure compliance with Metro standards, retail uses with more than 60,000 square feet of gross leasable floor area per building or business shall not be permitted within areas zoned for industrial development.



## RESPONSE

Consistent with the Comprehensive Plan the area surrounding the subject site is designated for a combination of commercial and industrial uses. The subject site and immediately adjacent properties has been designated for commercial development, as they are clustered near the Elligsen Freeway Interchange. Because this specific area has been designated commercial it is exempted from Metro development standards for employment areas.

The proposed restaurant and retail uses are consistent with serving primarily the growth of local needs as well as adjacent rural and agricultural lands. These commercial uses are compatible with the residential nature of the community, and are complementary to or supportive of surrounding industrial development.

The proposed site development has been attractively designed and reflects off-site visual sensitivity to the surrounding commercial and industrial development. The site design and landscaping also recognizes this site has high visibility located at a major entry point to the City and the 95<sup>th</sup> Avenue Industrial District. Therefore extra effort has been made to create an attractive focal point at the north end of the site.

### *Public Facilities and Services*

**GOAL 3.1:** *To assure that good public facilities and services are available with adequate capacity to meet community needs, while also assuring that growth does not exceed the community's commitment to provide adequate facilities and services.*

**Primary facilities and services include:** *those which significantly impact public health and safety and are directly linked to the land development process, in terms of service capacity, location, and design, or directly affect public health and safety. Therefore, adequate provisions must be made for these facilities/services prior to or concurrently with urban level development. These facilities and services include:*

*Sanitary sewer; Water service; Roads and transportation; Storm drainage; Fire protection; and Police protection and public safety.*

### *Timing – Concurrency Issues*

**Policy 3.1.2** *The City of Wilsonville shall provide, or coordinate the provision of facilities and services concurrent with need (created by new development, redevelopment, or upgrades of aging infrastructure).*

**Implementing Measure 3.1.2.a** *Urban development will be allowed only in areas where necessary facilities and services can be provided.*

**Implementing Measure 3.1.2.b** *Development, including temporary occupancy, that threaten the public's health, safety, or general welfare due to a failure to provide adequate public facilities and services, will not be permitted. Development applications will be allowed to proceed on the following basis:*

- 1.** *Planning approvals may be granted when evidence, including listing in the City's adopted Capital Improvements Program, supports the finding that facilities/services will be available within two years. Applicants may be encouraged or required to plan and complete development in phases, in order to assure that the rate of development does not exceed the capacity of needed facilities/services.*

2. *Building permits will be issued when planning approvals have been granted and funding is in place to assure completion of required facilities/services prior to occupancy. Applicants must sign a statement acknowledging that certificates of occupancy will not be given until adequate facilities/services, determined by the Building Official, after consulting with the City Engineer, are in place and operational. Parks, recreation facilities, streets and other transportation system improvements may be considered to be adequately in place and operational if they are listed in the City's adopted Capital Improvements Program, or other funding is committed for their completion within two years. In such cases, water, sewer, and storm drainage facilities must be available, to the satisfaction of the City Engineer, on at least a temporary basis, prior to occupancy.*
3. *Final certificates of occupancy will not be given until required facilities/services are in place and operational. Temporary certificates of occupancy may only be granted when the Building Official determines, after consulting with the City Engineer, that needed facilities and services will be in place and operational at the conclusion of the time period specified in the temporary certificate of occupancy. Nothing in this policy is intended to indicate that a temporary certificate of occupancy will be granted without assurance of full compliance with City requirements.*

#### *Paying for Needed Facilities and Services*

*Policy 3.1.3 The City of Wilsonville shall take steps to assure that the parties causing a need for expanded facilities and services, or those benefiting from such facilities and services, pay for them.*

*Implementing Measure 3.1.3.a Developers will continue to be required to pay for demands placed on public facilities/services that are directly related to their developments. The City may establish and collect systems development charges (SDC's) for any or all public facilities/services, as allowed by law. An individual exception to this standard may be justified, or SDC credits given, when a proposed development is found to result in public benefits that warrant public investment to support the development.*

## **RESPONSE**

The subject site is already served by a full range of urban services. Proposed new development will only require simple on-site connections to sewer, water, and storm drainage systems. The existing city systems are adequate to support the proposed new level of development, which will have minimal systems impacts. No major system improvements are needed to support this development.

Water is availability via the line along the eastern edge of the site within the Boones Ferry Road right-of-way.

There are three existing fire hydrants in this general area:

- One is located at the northeast corner of the AGC Building property, across 95<sup>th</sup> Avenue;
- One is located on the north side of the Chevron pump islands; and
- One is located in the northeast side of the Holiday Inn parking lot.

The Chevron hydrant is the closest to the site at approximately 145-215 feet from the two proposed buildings.

Based on discussion with TVF&R, they are going to require a new hydrant, as shown on the utility drawing. The buildings will not be equipped with sprinklers so there will not be any FDC's provided.

Sanitary sewer is available from the same line in 95<sup>th</sup> Avenue that serves the Chevron site.

Storm water is managed on-site, utilizing a water quality/detention pond, located just north of the site entry. The majority of the storm water leaving the site flows southwest under 95<sup>th</sup> Avenue to a wetland area just south of the AGC Building. The site now has excellent drainage, with no evidence of localized ponding.

Private utilities provide electrical power, phone, cable, and gas to the site.

Therefore utilities services and facilities are adequate to serve the proposed development consistent with Plan Policies and Implementing Measures.

#### ***Fire Protection Plan***

***Policy 3.1.8. The City of Wilsonville shall continue to coordinate planning for fire safety with the Tualatin Valley Fire Rescue District.***

***Implementing Measure 3.1.8.a All development plans, as approved by the Development Review Board, shall be approved by the City's Building Division for consistency with the state Uniform Fire Code (as amended by the Tualatin Valley Fire and Rescue District and subsequently adopted by the City of Wilsonville).***

***Implementing Measure 3.1.8.c. The City shall require that all buildings be designed to a maximum, fire flow rating of 3,000 GPM at 20 p.s.i. or such other standard as may be agreed to by the City and Fire District.***

## **RESPONSE**

The Tualatin Valley Fire and Rescue District serves Wilsonville. The site is within approximately ½ mile from the Elligsen Road Fire Station, which is now being reconstructed and modernized.

Based on discussion with TVF&R, they are going to require a new hydrant, as shown on the utility drawing. The buildings will not be equipped with sprinklers so there will not be any FDC's provided.

Therefore Policy 3.1.8 and Implementing Measures 3.1.8a & c are met.

#### ***Road and Transportation***

***Implementing Measure 3.1.6.a. The Transportation Master Plan shall be used to establish the design standards for each arterial and major collector street. The conceptual location of new major streets will also be identified. However, actual alignments may vary from the conceptual alignments based on detailed engineering specifications, design considerations, and consideration of impacts of the road alignments on neighborhoods and natural resources, provided that the intended function of the street is not altered. While local residential streets are considered a part of the Transportation Master Plan, they are not typically shown in detail in the Plan. The alignment of local streets shall be evaluated on a project-by-project basis, but***

*must function in coordination with the overall purposed of the Transportation Master Plan. Other streets not shown on the Plan may also be considered, if determined necessary for safe and convenient traffic circulation or increased connectivity.*

*Implementing Measure 3.1.6.b. The Transportation Master Plan shall be use to establish the Functional Street Classification System and the physical characteristics (right-of-way and pavement width, curbs, sidewalks etc.) of the various street classifications.*

*Implementing Measure 3.1.6.c. All streets shall be designed and developed in accordance with the Master Plan and street standards, except that the Development Review Board or City Council may approve specific modifications through the Planned Development process. Such modifications shall be made in consideration of existing traffic volumes and the cumulative traffic generation potential of the land uses being developed. Ata minimum, all streets must be developed with sufficient pavement width to provide two lanes of traffic, unless designated for one-way traffic flow. However, adequate emergency vehicle access and circulation must be provided.*

*Implementing Measure 3.1.6.d. Where the City Council officially designates truck routes, these streets shall be developed to arterial street construction standards and be posted as truck routes.*

*Implementing Measure 3.1.6.k. Individual developments shall be responsible for providing all collector and local streets. However, there may be cases where collector streets are found to benefit the entire community to a degree that warrants public participation in funding those collector streets. Developers and property owners of developing property shall also collectively assume the responsibility for providing "extra capacity" to the existing street system. To insure development of an adequate street system, the City shall collect a Systems Development Charge as development occurs. Funds collected shall be allocated through the Capital Improvements Plan as needed to provide extra capacity service.*

*Implementing Measure 3.1.6.o. The City shall take the following steps to reduce VMT's and overall reliance on single occupancy vehicles:*

- 1. Review all and use/development proposals with regard to transportation impacts. All development proposals shall be required to pay for a transportation impact analysis, unless specifically waived by the City's Community Development Director because the information is not needed.*
- 2. Seek to minimize traffic congestion at the freeway interchanges as well as on local arterial and collector streets.*
- 3. Seek to reduce the number and length of home-to-work trips.*
- 4. Seek a balanced mix of activities which encourage consolidation of automobile oriented trips and encourage design and location of complementary activities that support public transit, ride-share programs, and use of other alternative modes of transportation.*
- 5. Require large developments and high employment and/or traffic generators to design for mass transit and to submit programs to the City indicating how they will reduce transportation impacts. All such proposals shall be subject to review by SMART and, if applicable, ODOT. Maximum parking limits shall be used in conformity with Metro standards.*
- 6. Seek location of a permanent park-and-ride station as well as commitment from Tri-Met to upgrade transit service to the greatest extent possible, in coordination with SMART. Note the potential need for a commuter rail station in conjunction with the park-and-ride.*
- 7. Accommodate the expected growth in population and employment and the resulting transportation needs, the City (et. sic) by expanding local bus service in the residential and employment areas, continue to improve arterial and collector street networks, a bikeway system, ride-sharing programs including carpools and vanpools and encourage staggered or flex-time, work-hour programs.*

8. *Take steps to improve connectivity between neighborhoods and between residential areas and traffic generator locations. Also, work to provide more and better options for travel from one side of the freeway, the railroad, and major drainage courses to the other. It is recognized that alignment decisions for streets may cause concerns for adjacent property owners or residents, whose suggestions may help to improve plans or designs. The testimony of neighboring property owners shall not be the sole justification to postpone the construction of planned streets.*
9. *Increase densities and intensities of development in or near the Town Center area and in other locations where transportation systems can meet those needs.*
10. *Improve the balance between housing, employment, and commercial activities within the City in order to reduce commuting.*

## **RESPONSE**

The bolded text above is from the Comprehensive Plan. However, the City has also adopted a Transportation System Plan (TSP), dated 2003, which contains additional goals, policies and implementing measures. Some of the Comprehensive Plan policies and implementing measures are carried forward to the TSP, some with slightly different wording. In response to transportation issues we have primarily addressed the applicable policies and implementing measures from the TSP.

The subject site has excellent arterial street access, with frontage on 95<sup>th</sup> Avenue and on Boones Ferry Road. There is only one existing driveway on 95<sup>th</sup> Avenue, which serves the subject site and the two adjacent developments including the Holiday Inn and Chevron Station. There is no direct access to Boones Ferry Road.

- 95<sup>th</sup> Avenue is classified as a north/south Major Collector. It extends between Boones Ferry Road and Boeckman Road.
- Boones Ferry Road is classified as a Major Arterial, is under the jurisdiction of ODOT, and crosses the I-5 Freeway at the Elligsen Interchange.

As previously noted, a Development Agreement has been signed and recorded providing for specific design and configuration of the access, egress, and cross-site circulation. The proposed Development Plan reflects the access, egress, and cross-site circulation established in the Agreement.

Through a jointly funded project the City of Wilsonville and ODOT are just now completing street and intersection improvements adjacent to the subject site. These improvements provide for double left turns from Boones Ferry to 95<sup>th</sup>, and double right turns from 95<sup>th</sup> to Boones Ferry. The improvements also modify the intersection with Commerce Circle North, limiting it to right-in and right-out movements. And, they include the modified curb cut design established in the Development Agreement.

As part of preparing the Development Agreement, DKS has previously completed the Traffic Impact Analysis for the proposed project. A copy of the Report is included with the application documents. The street improvements outlined above and the on-site access, egress, and cross-site circulation were all addressed and found to satisfy the City's D LOS standard.

## **Conclusion – Urban Services**

All necessary primary urban services are and will be available, with adequate capacities, to support the proposed development. The proposed development is consistent with the public facilities and transportation system policies and implementing measures.

The development will further pay SDC's proportional to the scope of impact or additional demand created by the proposed development. It will therefore proportionately contribute to maintaining adequate facility and service capacities for the required urban services.

## **CONCLUSION – Comprehensive Plan Compliance**

Based on the response findings above, we believe we have demonstrated that the proposed Development Plans will be in full compliance with the applicable policies, objectives, and implementing measures of the Comprehensive Plan and Transportation System Plan.

### III. Development Code Compliance

Consistent with the industrial designation in the Comprehensive Plan, the subject property is zoned PDC, Planned Development Commercial. The PDC and other applicable zoning standards are addressed in the following sections. To simplify the format, Code provisions are generally addressed in the order in which they appear in the Code.

#### **Section 4.116. Standards Applying To Commercial Developments In Any Zone.**

**Any commercial use shall be subject to the applicable provisions of this Code and to the following:**

**(.01) Commercial developments shall be planned in the form of centers or complexes as provided in the City's Comprehensive Plan. As noted in the Comprehensive Plan, Wilsonville's focus on centers or complexes is intended to limit strip commercial development.**

**(.02) Where the land use map of Wilsonville's Comprehensive Plan calls for "Office Commercial" development, not less than 60% of the total square footage of the ground floors of buildings within the development shall be in office use. Total floor area dedicated to retail use shall not exceed 30%. On-site parking may be limited in order to control traffic generation.**

**(.03) Where the land use map of Wilsonville's Comprehensive Plan calls for "Commercial/Industrial mixed use" development, not more than 50% of the total floor area of the development shall consist of retail space.**

**(.04) Where the land use map of Wilsonville's Comprehensive Plan calls for "Residential/Commercial mixed use" development, not less than 50% of the total floor area of the development shall consist of residential units.**

### **RESPONSE**

The site is located within a Commercial District, with service commercial uses clustered near the 95<sup>th</sup> Avenue intersection with Boones Ferry Road. This area is not specifically designated for office commercial, commercial/industrial mixed-use, or residential/commercial mixed-use therefore the floor area restrictions are not applicable.

**(.05) All businesses, service or processing, shall be conducted wholly within a completely enclosed building; except for:**

**A. The sale of automotive fuel, lubricants, and fluids at service stations.**

**B. Car washes and car vacuum bays.**

**C. Off-street parking for customers and employees and off-street loading.**

**D. Outdoor seating areas associated with food and drink establishments on private property, or on public easements, provided the area and activities conform to ADA standards and do not interfere with public uses, safety, access or circulation.**

**E. Temporary staging of inventory, as shall be authorized through a site development permit, complying with the following additional minimum development and performance standards:**

**1. The staging area shall be screened by a fully sight obscuring fence or planting, high wall, high berm or high screen landscape standard as specified in Section 4.176 - Landscaping Screening and Buffering;**

**2. All parts of the staged inventory shall be completely concealed on all sides from public view at the right-of-way line; and**

**3. The staged inventory shall be relocated into a completely enclosed structure of the primary retail operation within 48 hours of placement.**



F. Exterior sales that are specifically authorized through temporary use permit approval, subject to conditions of approval. Exterior sales that may be permitted are those that are limited in time duration, such as sidewalk sales, grand openings, or farmers' markets. [Section 4.116(.05) amended by Ord. No. 580, 4/4/05.]

G. Exterior sales areas, complying with the following minimum development and performance standards:

1. The sales area shall be accessory to, and shall not exceed 5% of the floor area of the primary retail operation.
2. The sales area shall be completely covered by a permanent structure of a design, construction and architecture compatible with that of the structure of the primary retail operation.
3. All required ADA and pedestrian access ways and circulation aisles shall remain clear at all times.
4. For new development, the Development Review Board may grant a waiver to allow exterior sales area of up to 10% of the floor area of the primary retail operation, provided that findings can be made that:
  - a. The expanded covered area has received approval through a Stage II/Site Design Review process.
  - b. The expanded area does not detract from the overall character of the development or the surrounding neighborhood.
  - c. Partial walls are required for screening large or bulky items.
5. For Development existing on December 21, 2005, the Planning Director, pursuant to a Class II Administrative Review Process, may grant a waiver to allow exterior sales areas of up to 10% of the floor area of the primary retail operation, provided that findings can be made that:
  - a. The expanded area does not detract from the overall character of the area,
  - b. Partial walls are required for screening large or bulky items.[Section 4.116(.05) amended by Ord. 601, 11/21/05]

## RESPONSE

All businesses, service or processing, will be conducted wholly within a completely enclosed building consistent with this Section, except for related parking and the proposed outdoor seating areas associated with the food and drink establishment, as allowed.

(.06) In any Commercial Development directly across the street from any Residential District, the loading facilities shall be at least twenty (20) feet from the street, shall be sited whenever practicable at the rear or side, and if facing a residential area, shall be properly screened. Screening shall be provided in a manner that is compatible with the adjacent residential development in terms of quality of materials and design. Such screening shall effectively minimize light glare and noise levels to those of adjacent residential areas.

## RESPONSE

This site is not located near any residential area. Therefore this criterion does not apply.

(.07) Uses shall be limited to those which will meet the performance standards specified in Section 4.135(.05), with the exception of 4.135(.05)(M.)(3.).

(.08) Corner lots shall conform to the vision clearance standards set forth in Section 4.177.

(.09) Trailer, trailer houses, mobile coaches, or any altered variation thereof shall not be used for the purpose of conducting a trade or calling or for storage of material unless approved for such purpose as a temporary use.

## **RESPONSE**

The site has been designed to comply with these performance criteria.

(.10) Commercial developments generally.

A. No structure shall be erected closer than the right-of-way line then existing or the officially planned right-of-way of any public, county, or state road.

B. Minimum Front Yard Setback: None required except when front yard abuts a more restrictive district. When front yard abuts a more restrictive district, setbacks shall be the same as the abutting district.

C. Minimum Rear Yard Setback: None required except when rear yard abuts a more restrictive district. When rear yard abuts a more restrictive district, setbacks shall be the same as for the abutting district.

D. Minimum Side Yard Setback: None required except when side yard abuts a more restrictive district. When side yard abuts a more restrictive district, setbacks shall be one and one-half (1 1/2) times the setback required for the abutting district.

E. Maximum Building Height: Thirty-five (35) feet, unless taller buildings are specifically allowed in the zone.

F. Minimum Lot Size: No limitation, save and except as may otherwise be affected by other provisions of this Code.

G. Maximum Lot Coverage: No limitation, save and except as may otherwise be affected by other provisions of this Code.

H. Minimum Street Frontage: No limitation, save and except as may be necessary to provide minimum access requirements.

## **RESPONSE**

The site and buildings have been designed to comply with these design and dimensional standards. The development site shares access with the Holiday Inn and the Chevron station. A Development Agreement has been executed between these three parties and the City defining access, egress, and site circulation.

(.12) Off-Street Parking is to be as specified in Section 4.155.

(.13) Signs are subject to the standards of Sections 4.156.01 through .156.11.  
[Amended by Ord. No. 704, 6/18/12]

## **RESPONSE**

These two items are addressed later in this report under those specific Code Sections.

**Section 4.118. Standards applying to all Planned Development Zones:**

**(.01) Height Guidelines:** In “S” overlay zones, the solar access provisions of Section 4.137 shall be used to determine maximum building heights. In cases that are subject to review by the Development Review Board, the Board may further regulate heights as follows:

- A. Restrict or regulate the height or building design consistent with adequate provision of fire protection and fire-fighting apparatus height limitations.
- B. To provide buffering of low density developments by requiring the placement of three or more story buildings away from the property lines abutting a low density zone.
- C. To regulate building height or design to protect scenic vistas of Mt. Hood or the Willamette River.

**RESPONSE**

The City has not applied and “S” overlay zone to this area. Therefore the special height limitations are not applicable. The buildings have been designed consistent with the standard 35 foot height standard.

**(.02) Underground Utilities** shall be governed by Sections 4.300 to 4.320. All utilities above ground shall be located so as to minimize adverse impacts on the site and neighboring properties.

**RESPONSE**

All utilities serving the site will be placed underground, except for allowed transformers and the storm water pond.

**(.03) Notwithstanding the provisions of Section 4.140 to the contrary, the Development Review Board, in order to implement the purposes and objectives of Section 4.140, and based on findings of fact supported by the record may:**

**A. Waive the following typical development standards:**

- 1. minimum lot area;
- 2. lot width and frontage;
- 3. height and yard requirements;
- 4. lot coverage;
- 5. lot depth;
- 6. street widths;
- 7. sidewalk requirements;
- 8. height of buildings other than signs;
- 9. parking space configuration;
- 10. minimum number of parking or loading spaces;
- 11. shade tree islands in parking lots, provided that alternative shading is provided;
- 12. fence height;
- 13. architectural design standards;
- 14. transit facilities; and
- 15. solar access standards, as provided in Section 4.137.

**B. The following shall not be waived by the Board, unless there is substantial evidence in the whole record to support a finding that the intent and purpose of the standards will be met in alternative ways:**

- 1. open space requirements in residential areas;
- 2. minimum density standards of residential zones;
- 3. minimum landscape, buffering, and screening standards;

C. The following shall not be waived by the Board, unless there is substantial evidence in the whole record to support a finding that the intent and purpose of the standards will be met in alternative ways, and the action taken will not violate any applicable federal, state, or regional standards:

1. maximum number of parking spaces;
2. standards for mitigation of trees that are removed;
3. standards for mitigation of wetlands that are filled or damaged; and
4. trails or pathways shown in the Parks and Recreation Master Plan.

D. Locate individual building, accessory buildings, off-street parking and loading facilities, open space and landscaping and screening without reference to lot lines; and

E. Adopt other requirements or restrictions, inclusive of, but not limited to, the following:

1. Percent coverage of land by buildings and structures in relationship to property boundaries to provide stepped increases in densities away from low density development.
2. Parking ratios and areas expressed in relation to use of various portions of the property and/or building floor area.
3. The locations, width and improvement of vehicular and pedestrian access to various portions of the property, including portions within abutting street or private drive. [amended by Ord. 682, 9/9/10]
4. Arrangement and spacing of buildings and structures to provide appropriate open spaces around buildings.
5. Location and size of off-street loading areas and docks.
6. Uses of buildings and structures by general classification, and by specific designation when there are unusual requirements for parking, or when the use involves noise, dust, odor, fumes, smoke, vibration, glare or radiation incompatible with present or potential development of surrounding property.  
Such incompatible uses may be excluded in the amendment approving the zone change or the approval of requested permits.
7. Measures designed to minimize or eliminate noise, dust, odor, fumes, smoke, vibration, glare, or radiation which would have an adverse effect on the present or potential development on surrounding properties.
8. Schedule of time for construction of the proposed buildings and structures and any stage of development thereof to insure consistency with the City's adopted Capital Improvements Plan and other applicable regulations.
9. A waiver of the right of remonstrance by the applicant to the formation of a Local Improvement District (LID) for streets, utilities and/or other public purposes.
10. Modify the proposed development in order to prevent congestion of streets and/or to facilitate transportation.
11. Condition the issuance of an occupancy permit upon the installation of landscaping or upon a reasonable scheduling for completion of the installation of landscaping. In the latter event, a posting of a bond or other security in an amount equal to one hundred ten percent (110%) of the cost of the landscaping and installation may be required.
12. A dedication of property for streets, pathways, and bicycle paths in accordance with adopted Facilities Master Plans or such other streets necessary to provide proper development of adjacent properties.

## RESPONSE

The applicant is not requesting any Waivers from the base standards, except for sign area, which is addressed under the Master Sign Plan provisions. Therefore this section is not applicable.

**(.09) Habitat-Friendly Development Practices.** To the extent practicable, development and construction activities of any lot shall consider the use of habitat-friendly development practices, which include:

- A. Minimizing grading, removal of native vegetation, disturbance and removal of native soils, and impervious area;
- B. Minimizing adverse hydrological impacts on water resources, such as using the practices described in Part (a) of Table NR-2 in Section 4.139.03, unless their use is prohibited by an applicable and required state or federal permit, such as a permit required under the federal Clean Water Act, 33 U.S.C. §§1251 et seq., or the federal Safe Drinking Water Act, 42 U.S.C. §§300f et seq., and including conditions or plans required by such permit;
- C. Minimizing impacts on wildlife corridors and fish passage, such as by using the practices described in Part (b) of Table NR-2 in Section 4.139.03; and
- D. Using the practices described in Part (c) of Table NR-2 in Section 4.139.03.  
[Section 4.118(.09) added by Ord. # 674 11/16/09]

## RESPONSE

The site has been previously rough graded and there is no significant native vegetation. The site does not contain any SROZ and no fish or wildlife habitats are associated with this property.

The site has been designed consistent with the Habitat-Friendly practices. The storm system design provides for on-site water quality and volume control which protects the down stream wetland area south of the AGC Building.

**Section 4.131. PDC - Planned Development Commercial Zone.**

The requirements of a PDC Zone shall be governed by Section 4.140, Planned Development Regulations, and as otherwise set forth in this Code.

**(.01) The following shall apply to any PDC zone:**

- A. Uses that are typically permitted:
  - 1. Retail business, goods and sales.
  - 2. Wholesale showrooms.
  - 3. Offices and clinics.
  - 4. Service establishments.
  - 5. Any use allowed in a PDR Zone or PDI Zone, provided the majority of the total ground floor area is commercial, or any other commercial uses provided that any such use is compatible with the surrounding uses and is planned and developed in a manner consistent with the purposes and objectives of Section 4.140. However, the uses listed as prohibited below shall not be permitted.
  - 6. Accessory uses, buildings, and structures customarily incidental to any of the aforesaid principal permitted uses.
  - 7. Temporary buildings or structures for uses incidental to construction work, which buildings or structures shall be removed upon completion or abandonment of the construction work.
  - 8. Churches.

9. Those uses that are listed as typically permitted in Section 4.131.05(.03), as well as the following additional uses when conducted entirely within enclosed buildings:

- a. Automotive machine shops
- b. Automotive detail shops
- c. Repair shops for:
  - i. electronics;
  - ii. boats;
  - iii. appliances;
  - iv. light equipment;
  - v. yard equipment;
  - vi. other related types of repair shops.
- d. Fabrication shops including:
  - i. cabinets;
  - ii. sheet metal;
  - iii. counter tops;
  - iv. closet systems;
  - v. other related types of work.
- e. Marine equipment – supply and repair

## **RESPONSE**

The City has not adopted an “S” Overlay zone for this commercial district. Therefore the solar access provisions are not applicable. The proposed uses are consistent with the allowed uses in the PDC zone.

Therefore the development complies with the applicable provisions of this section. Both uses are permitted within the PDC zone.

### **(.02) Prohibited uses.**

**A. No body/fender repair shops shall be permitted unless all operations are conducted entirely within enclosed buildings and meet the performance standards of Section 4.135(.05). The storage and parking of damaged vehicles shall be screened to assure that they are not visible off-site.**

## **RESPONSE**

The proposed uses are not identified as Prohibited.

### **(.03) Block and access standards:**

**1. The Development Review Board shall determine appropriate conditions of approval to assure that adequate connectivity results for pedestrians, bicyclists, and motor vehicle drivers. Consideration shall be given to the use of public transit as a means of meeting access needs.**

## **RESPONSE**

This development relies upon the existing adjacent street pattern and does not create any new blocks. As previously noted, a Development Agreement has been executed for this project, which defines access, egress and on-site circulation. The agreed upon modified access design for curb radii has already been incorporated into the recently completed street improvements on 95<sup>th</sup> Avenue. The Site Plan provides for safe and convenient pedestrian circulation.

While transit is not expected to be a major mode of travel for this development, there is Smart Transit service available on 95<sup>th</sup> Avenue on the Bus 5 route, with a stop just south of the site at the intersection of Commerce Circle South and 9<sup>th</sup> Avenue. There is also a link to the Tri Met 96 bus on Commerce Circle South.

Therefore this criterion has been met.

**Section 4.133.00. Wilsonville Road Interchange Area Management Plan (IAMP) Overlay Zone**

**Section 4.133.01. Purpose**

**The purpose of the IAMP Overlay Zone is the long-range preservation of operational efficiency and safety of the Wilsonville Road Interchange, which provides access from and to Interstate 5 for residents and businesses in south Wilsonville. The Wilsonville Road Interchange is a vital transportation link for regional travel and freight movement and provides connectivity between the east and west side of the community. Preserving capacity and ensuring safety of this interchange and the transportation system in its vicinity is essential to existing businesses and residents in the southern parts of the city and to the continued economic and community growth and development in the vicinity of Wilsonville Road and the interchange.**

**Section 4.133.02. Where These Regulations Apply**

**The provisions of this Section shall apply to land use applications subject to Section 4.004, Development Permit Required, for parcels wholly or partially within the IAMP Overlay Zone, as shown on Figure I-1. Any conflict between the standards of the IAMP Overlay Zone and those contained within other chapters of the Development Code shall be resolved in favor of the Overlay Zone.**

**RESPONSE**

This development is not located within the Wilsonville Road Interchange Management Area. Therefore this criterion does not apply.

However, it is noted that this site is located near the Elligsen Interchange. The City and ODOT have not yet adopted any similar management provisions for this interchange, but could in the near future, given the recent completion of road improvements at Boones Ferry and 95<sup>th</sup> Avenue. In this regard, the City and ODOT are both aware of this development, and in particular, the shared access agreement that has recently been recorded. With the recent right-of-way acquisition appropriate access control has been established. Therefore we believe appropriate access management issues have already been adequately addressed.

***4.140 Planned Development Regulations.***

***(.01) Purpose .***

***A. The provisions of Section 4.140 shall be known as the Planned Development Regulations. The purposes of these regulations are to encourage the development of tracts of land sufficiently large to allow for comprehensive master planning, and to provide flexibility in the application of certain regulations in a manner consistent with the intent of the Comprehensive Plan and general provisions of the zoning regulations and to encourage a harmonious variety of uses through mixed use design within specific developments thereby promoting the economy of shared public services and facilities and a variety of complimentary activities***



*consistent with the land use designation on the Comprehensive Plan and the creation of an attractive, healthful, efficient and stable environment for living, shopping or working.*

**B. It is the further purpose of the following Section:**

- 1. To take advantage of advances in technology, architectural design, and functional land use design;*
- 2. To recognize the problems of population density, distribution and circulation and to allow a deviation from rigid established patterns of land uses, but controlled by defined policies and objectives detailed in the comprehensive plan;*
- 3. To produce a comprehensive development equal to or better than that resulting from traditional lot land use development.*
- 4. To permit flexibility of design in the placement and uses of buildings and open spaces, circulation facilities and off-street parking areas, and to more efficiently utilize potentials of sites characterized by special features of geography, topography, size or shape or characterized by problems of flood hazard, severe soil limitations, or other hazards;*
- 5. To permit flexibility in the height of buildings while maintaining a ratio of site area to dwelling units that is consistent with the densities established by the Comprehensive Plan and the intent of the Plan to provide open space, outdoor living area and buffering of low-density development.*
- 6. To allow development only where necessary and adequate services and facilities are available or provisions have been made to provide these services and facilities.*
- 7. To permit mixed uses where it can clearly be demonstrated to be of benefit to the users and can be shown to be consistent with the intent of the Comprehensive Plan.*
- 8. To allow flexibility and innovation in adapting to changes in the economic and technological climate.*

## **RESPONSE**

The subject site was originally part of the Chevron Master Plan, and was identified as Phase 2. The previous section of this narrative addressed compliance with the applicable provisions of the PDC zoning regulations.

This section addresses compliance with the general provisions of the zoning regulations and demonstrates how the proposed development is consistent with the stated intent of the PD zoning, which is, *“to encourage a harmonious variety of uses through mixed use design within specific developments thereby promoting the economy of shared public services and facilities and a variety of complimentary activities consistent with the land use designation on the Comprehensive Plan and the creation of an attractive, healthful, efficient and stable environment for living, shopping or working”*.

### ***(.02) Lot Qualification.***

***A. Planned Development may be established on lots which are suitable for and of a size to be planned and developed in a manner consistent with the purposes and objectives of Section 4.140.***

***B. Any site designated for development in the Comprehensive Plan may be developed as a Planned Development, provided that it is zoned “PD.” All sites which are greater than two (2) acres in size, and designated in the Comprehensive Plan for commercial, residential, or industrial use shall be developed as Planned Developments, unless approved for other uses permitted by the Development***

*Code. Smaller sites may also be developed through the City's PD procedures, provided that the location, size, lot configuration, topography, open space and natural vegetation of the site warrant such development.*

*(.03) Ownership.*

*A. The tract or tracts of land included in a proposed Planned Development must be in one (1) ownership or control or the subject of a joint application by the owners of all the property included. The holder of a written option to purchase, with written authorization by the owner to make applications, shall be deemed the owner of such land for the purposes of Section 4.140.*

*B. Unless otherwise provided as a condition for approval of a Planned Development permit, the permittee may divide and transfer units or parcels of any development. The transferee shall use and maintain each such unit or parcel in strict conformance with the approval permit and development plan.*

## **RESPONSE**

At 1.28 acres, the subject site is less than 2 acres. However, all commercial development is reviewed under the planned development regulations, regardless of lot size. Therefore the PDC and PD regulations apply.

This application is being submitted on behalf of the property owner, Wilsonville Devco, LLC. LaPoint Business Group, owner of the Chevron station is a Joint Applicant, covering site design elements specifically related to joint access/egress and circulation, signage, and trash enclosures.

*(.04) Professional Design.*

*A. The applicant for all proposed Planned Developments shall certify that the professional services of the appropriate professionals have been utilized in the planning process for development.*

*B. Appropriate professionals shall include, but not be limited to the following to provide the elements of the planning process set out in Section 4.139:*

- 1. An architect licensed by the State of Oregon;*
- 2. A landscape architect registered by the State of Oregon;*
- 3. An urban planner holding full membership in the American Institute of Certified Planners, or a professional planner with prior experience representing clients before the Development Review Board, Planning Commission, or City Council; or*
- 4. A registered engineer or a land surveyor licensed by the State of Oregon.*

*C. One of the professional consultants chosen by the applicant from either 1, 2, or 3, above, shall be designated to be responsible for conferring with the planning staff with respect to the concept and details of the plan.*

*D. The selection of the professional coordinator of the design team will not limit the owner or the developer in consulting with the planning staff.*

## **RESPONSE**

The Planned Development Regulations require a Design Team. The Professional Design Team for this project is listed on Pages 2 & 5.

## PDC Stage II, Final Development Plan

### *(.09) Final Approval (Stage II).*

- A. *Unless an extension has been granted by the Development Review Board, within two (2) years after the approval or modified approval of a preliminary development plan (Stage I), the applicant shall file with the City Planning Department a final plan for the entire development or when submissions in stages has been authorized pursuant to Section 4.035 for the first unit of the development, a public hearing shall be held on each such application as provided in Section 4.013.*
- B. *After such hearing, the Development Review Board shall determine whether the proposal conforms to the permit criteria set forth in this Code, and shall approve, conditionally approved, or disapprove the application.*
- C. *The final plan shall conform in all major respects with the approved preliminary development plan, and shall include all information included in the preliminary plan plus the following:*
  - i. *The location of water, sewerage and drainage facilities;*
  - ii. *Preliminary building and landscaping plans and elevations, sufficient to indicate the general character of the development;*
  - iii. *The general type and location of signs;*
  - iv. *Topographic information as set forth in Section 4.035;*
  - v. *A map indicating the types and locations of all proposed uses; and*
  - vi. *A grading plan.*
- D. *The final plan shall be sufficiently detailed to indicate fully the ultimate operation and appearance of the development or phase of development. However, Site Design Review is separate and more detailed review of proposed design features, subject to the standards of Section 4.400.*
- E. *Copies of legal documents required by the Development Review Board for dedication or reservation of public facilities, or for creation of non-profit homeowner's association, shall be submitted.*

## RESPONSE

This application includes a request for Stage II Final Development Plan approval. As previously noted, the Stage I Master Plan for the Chevron site simply listed this area as Phase 2, so to that extent the Stage II plans substantially conform in all major respects with the Stage I Master Plan. The application includes all required information, including the following:

1. The location of water, sewer, and storm drainage;
2. Preliminary building and landscaping plans and elevations, sufficient to indicate the general character of the development;
3. The general type and location of signs;
4. Topographic information as set forth in Section 4.035;
5. A site plan indicating the types and locations of all proposed uses;
6. Access & Egress modifications; and
7. A grading plan.

Sufficient details have been provided so as to indicate fully the ultimate operation and appearance of the proposed Carl's Jr. and Retail Building.

There are no proposed or required dedications of additional rights-of-way or reservations of public facilities. All right-of-way and street improvements have already been provided, and improvements completed.

The proposed improvements to this site will complete development of all vacant land in the immediately surrounding commercial district. This site is situated at the visual focal point entering the 95<sup>th</sup> Avenue Business District. Therefore an emphasis was given to providing a high quality visual environment, including coordinated signage. The two buildings are designed with similar materials for a coordinated appearance, and are also compatible with and complimentary to the adjacent Chevron and Holiday Inn.

The total square footage of the new buildings is 6,017 square feet. The landscaping covers 16,139 square feet or 29% of the site. However, the Chevron site includes an existing C-Store containing 3,139 square feet and the Chevron fuel islands, with canopy covering 3,468 square feet. The site landscaping and parking has been configured to compliment that of the Chevron site and provides for cross-circulation between the two properties.

This is commercial property not residential, all under single ownership, so there is no homeowner's association proposed. There is however the executed Development Agreement linked to the other two adjacent properties.

Therefore, we believe the proposed development and application satisfies all the Stage II requirements consistent with Code standards.

- E. A planned Development permit may be granted by the Development Review Board only if it is found that the development conforms to all the following criteria, as well as the Planned Development Regulations in Section 4.140:*
- a. The location, design, size, and uses, both separately and as a whole, are consistent with the Comprehensive Plan, and with any other applicable plan, development map or Ordinance adopted by the City Council.*
  - b. That the location, design, size, and uses are such that traffic generated by the development can be accommodated safely and without congestion in excess of level of service "D" defined in the highway capacity manual published by the national Highway Research Board on existing or immediately planned arterial or collector streets and will, in the case of commercial or industrial developments, avoid traversing local streets.*
  - c. That the location, design, size and uses are such that the residents or establishments to be accommodated will be adequately served by existing or immediately planned facilities and services.*

## **Conclusion – Planned Development Permit and PDC Zoning**

Based on the above findings the proposed development is found to comply with the applicable provisions of Section 4.140(.09). The above findings demonstrate that the location, design, size, and uses, both separately and as a whole, are consistent with the Comprehensive Plan.

The findings within this report, together with the DKS Traffic Impact Study, further demonstrate that the traffic generated by the proposed development can be accommodated safely and without congestion in excess of level of service "D" on existing arterial and collector streets. Further, this non-residential development has direct access to major collector and arterial streets so traversing of local residential streets will be avoided.

The executed Development Agreement includes considerations related to access, egress, vehicle queuing, site circulation and parking. The proposed site plan incorporates those elements agreed to in the executed document, including consideration of comments listed in the DKS report.

The above findings also demonstrate that the proposed development will be adequately served by all primary public facilities and services.

Therefore, Stage II approval should be granted, as requested.

### **General Development Regulations**

#### *Section 4.154. Bicycle, Pedestrian and Transit Facilities.*

*NOTE: Completion of Section 4.154 has been postponed pending the completion of the Transportation Systems Plan.*

### **RESPONSE**

Provisions for bicycles and pedestrians are provided as part of the overall site design. There are public sidewalks along three sides of this site along both 95<sup>th</sup> Avenue and Boones Ferry Road. The site design provides for multiple sidewalk connections from on-site to off-site.

Staff noted concerns regarding the pedestrian access that crosses the Carl's drive-up lane. The concern was that typical painted striping tends to fade and over time diminishes the visibility for the intended safe crossing.

We do not believe this is a high risk crossing. This crossing is expected to have very low usage, given the typical amount of foot traffic in the area. In addition it is crossing a drive-up lane where vehicles are moving very slowly and often stopped in a queue line.

Never-the-less, to ensure pedestrian safety the cross-walk has been modified to enhance its visibility and to provide more long lasting and durable markings. The cross-walk will be stamped asphalt to create a clearly visible pattern. The stamped pattern will also extend the durability of the painted striping. In addition vertical cross-walk signs (flexible posts) will be installed to emphasize the crossing's visibility.

The Site Development Plans show the location and design of the bicycle parking. A total of three double bike racks will be provided, two at the Carl's Jr. site and one at the retail building. These locations provide convenient linkage with the adjacent Smart transit

stops. In addition pedestrian walkways have been provided from the parking areas to the front doors of the two buildings.

Smart Transit service is currently provided on 95<sup>th</sup> Avenue by the Bus 5 route. There is a bus stop about 600 feet south of the site at the intersection of Commerce Circle South and 95th Avenue. There is also a link to the Tri Met 96 bus, which stops across the street on Commerce Circle South.

**Section 4.155. General Regulations - Parking, Loading and Bicycle Parking.**

**(.01) Purpose:**

*A. The design of parking areas is intended to enhance the use of the parking area as it relates to the site development as a whole, while providing efficient parking, vehicle circulation and attractive, safe pedestrian access.*

*B. As much as possible, site design of impervious surface parking and loading areas shall address the environmental impacts of air and water pollution, as well as climate change from heat islands.*

*C. The view from the public right of way and adjoining properties is critical to meet the aesthetic concerns of the community and to ensure that private property rights are met. Where developments are located in key locations such as near or adjacent to the I-5 interchanges, or involve large expanses of asphalt, they deserve community concern and attention.*

**(.02) General Provisions:**

*A. The provision and maintenance of off-street parking spaces is a continuing obligation of the property owner. The standards set forth herein shall be considered by the Development Review Board as minimum criteria.*

*1. The Board shall have the authority to grant variances or planned development waivers to these standards in keeping with the purposes and objectives set forth in the Comprehensive Plan and this Code.*

*2. Waivers to the parking, loading, or bicycle parking standards shall only be issued upon a findings that the resulting development will have no significant adverse impact on the surrounding neighborhood, and the community, and that the development considered as a whole meets the purposes of this section.*

*B. No area shall be considered a parking space unless it can be shown that the area is accessible and usable for that purpose, and has maneuvering area for the vehicles, as determined by the Planning Director.*

*C. In cases of enlargement of a building or a change of use from that existing on the effective date of this Code, the number of parking spaces required shall be based on the additional floor area of the enlarged or additional building, or changed use, as set forth in this Section. Current development standards, including parking area landscaping and screening, shall apply only to the additional approved parking area.*

*D. In the event several uses occupy a single structure or parcel of land, the total requirement for off-street parking shall be the sum of the requirements of the several uses computed separately, except as modified by subsection "E," below.*

*E. Owners of two (2) or more uses, structures, or parcels of land may utilize jointly the same parking area when the peak hours of operation do not overlap, provided satisfactory legal evidence is presented in the form of deeds, leases, or contracts securing full and permanent access to such parking areas for all the parties jointly using them. [Amended by Ord. # 674 11/16/09]*

*F. Off-street parking spaces existing prior to the effective date of this Code may be included in the amount necessary to meet the requirements in case of subsequent enlargement of the building or use to which such spaces are necessary.*

*G. Off-Site Parking. Except for single-family dwellings, the vehicle parking spaces required by this Chapter may be located on another parcel of land, provided the parcel is within 500 feet of the use it serves and the DRB has approved the off-site parking through the Land Use Review. The distance from the parking area to the use shall be measured from the nearest parking space to the*

*main building entrance, following a sidewalk or other pedestrian route. The right to use the offsite parking must be evidenced in the form of recorded deeds, easements, leases, or contracts securing full and permanent access to such parking areas for all the parties jointly using them.*

*(.03) Minimum and Maximum Off-Street Parking Requirements:*

*A. Parking and loading or delivery areas shall be designed with access and maneuvering area adequate to serve the functional needs of the site and shall:*

- 1. Separate loading and delivery areas and circulation from customer and/or employee parking and pedestrian areas. Circulation patterns shall be clearly marked.*
- 2. To the greatest extent possible, separate vehicle and pedestrian traffic.*

*B. Parking and loading or delivery areas shall be landscaped to minimize the visual dominance of the parking or loading area, as follows:*

- 1. Landscaping of at least ten percent (10%) of the parking area designed to be screened from view from the public right-of-way and adjacent properties.*

*This landscaping shall be considered to be part of the fifteen percent (15%) total landscaping required in Section 4.176.03 for the site development.*

- 2. Landscape tree planting areas shall be a minimum of eight (8) feet in width and length and spaced every eight (8) parking spaces or an equivalent aggregated amount.*

*a. Trees shall be planted in a ratio of one (1) tree per eight (8) parking spaces or fraction thereof, except in parking areas of more than two hundred (200) spaces where a ratio of one (1) tree per six (six) spaces shall be applied as noted in subsection (.03)(B.)(3.). A landscape design that includes trees planted in areas based on an aggregated number of parking spaces must provide all area calculations.*

*b. Except for trees planted for screening, all deciduous interior parking lot trees must be suitably sized, located, and maintained to provide a branching minimum of seven (7) feet clearance at maturity.*

- 3. Due to their large amount of impervious surface, new development with parking areas of more than two hundred (200) spaces that are located in any zone, and that may be viewed from the public right of way, shall be landscaped to the following additional standards:*

*a. One (1) trees shall be planted per six (6) parking spaces or fraction thereof. At least twenty-five percent (25%) of the required trees must be planted in the interior of the parking area.*

*b. Required trees may be planted within the parking area or the perimeter, provided that a minimum of forty percent (40%) of the canopy dripline of mature perimeter trees can be expected to shade or overlap the parking area. Shading shall be determined based on shadows cast on the summer solstice.*

*c. All parking lots in excess of two hundred (200) parking spaces shall provide an internal pedestrian walkway for every six (6) parking aisles. Minimum walkway clearance shall be at least six (6) feet in width. Walkways shall be designed to provide pedestrian access to parking areas in order to minimize pedestrian travel among vehicles.*

*Walkways shall be designed to channel pedestrians to the front entrance of the building.*



*d. All parking lots viewed from the public right of way shall have a minimum twelve (12) foot landscaped buffer extending from the edge of the property line at the right of way to the edge of the parking area. Buffer landscaping shall meet the low screen standard of 4.176(.02)(D) except that trees, groundcovers and shrubs shall be grouped to provide visual interest and to create view openings no more than ten (10) feet in length and provided every forty (40) feet. Notwithstanding this requirement, view of parking area that is unscreened from the right of way due to slope or topography shall require an increased landscaping standard under 4.176(.02) in order to buffer and soften the view of vehicles as much as possible. For purposes of this section, "view from the public right of way" is intended to mean the view from the sidewalk directly across the street from the site, or if no sidewalk, from the opposite side of the adjacent street or road.*

*e. Where topography and slope condition permit, the landscape buffer shall integrate parking lot storm water treatment in bioswales and related plantings. Use of berms or drainage swales are allowed provided that planting areas with lower grade are constructed so that they are protected from vehicle maneuvers. Drainage swales shall be constructed to Public Works Standards.*

*f. In addition to the application requirements of section 4.035(.04)(6)(d), where view of signs is pertinent to landscape design, any approved or planned sign plan shall accompany the application for landscape design approval.*

*4. Be designed for safe and convenient access that meets ADA and ODOT standards. All parking areas which contain ten (10) or more parking spaces, shall for every fifty (50) standard spaces, provide one ADA-accessible parking space that is constructed to building code standards, Wilsonville Code 9.000.*

*5. Where possible, parking areas shall be designed to connect with parking areas on adjacent sites so as to eliminate the necessity of utilizing the public street for multiple accesses or cross movements. In addition, on-site parking shall be designed for efficient on-site circulation and parking.*

*6. In all multi-family dwelling developments, there shall be sufficient areas established to provide for parking and storage of motorcycles, mopeds and bicycles. Such areas shall be clearly defined and reserved for the exclusive use of these vehicles.*

*7. On-street parking spaces, directly adjoining and on the same side of the street as the subject property, may be counted towards meeting the minimum offstreet parking standards.*

*8. Tables 5, below, shall be used to determine the minimum and maximum parking standards for various land uses. The minimum number of required parking spaces shown on Tables 5 shall be determined by rounding to the nearest whole parking space. For example, a use containing 500 square feet, in an area where the standard is one space for each 400 square feet of floor area, is required to provide one off-street parking space. If the same use contained more than 600 square feet, a second parking*

**RESPONSE**

The parking and delivery areas have been designed with access and maneuvering areas adequate to serve the functional needs of the site, and also for consistency with the Development Agreement. Care has been given to the extent practicable to separate vehicle and pedestrian traffic.

For these sizes of buildings the code does not require separate loading docks. As is typical for these types of uses, delivery trucks utilized available parking spaces. Delivery times are typically scheduled so as to not conflict with peak customer traffic periods.

Circulation patterns will be clearly marked including provisions for safe and convenient access that meet ADA and ODOT standards. Because the parking for this facility is less than 50 spaces only one ADA space has been provided for each building.

**Table 2  
Proposed Parking**

<b>Type of Parking</b>	<b>Carl's Jr.</b>	<b>Retail Building</b>
Auto	27	19
ADA	1	1
Bike	4	2
<b>TOTAL</b>	<b>28</b>	<b>20</b>

The parking and delivery areas will be appropriately landscaped to minimize their off-site visual impacts, consistent with the ten percent (10%) code standard. The landscaping plan provides for one shade tree planting area eight (8) feet in width and length for every eight (8) parking spaces or an equivalent aggregated amount. A total of 6 bike racks will be provided consistent with Code standards as set forth in Table 5 of the Code, shown below.

The applicable off-street parking standards, excerpted from Table 5, Section 4.155, are as shown below.

**Section 4.155  
CODE Table 5**

<i>Use</i>	<i>Parking Minimum</i>	<i>Parking Maximum</i>	<i>Bicycle Minimum</i>	<i>Truck Loading</i>
<i>Fast Food</i>	<i>9.9 per 1,000 sq ft</i>	<i>14.9 per 1,000 sq ft</i>	<i>minimum 4</i>	<i>&lt;5,000 sq ft = 0</i>
<i>Commercial Retail</i>	<i>4.1 per 1,000 sq ft</i>	<i>6.2 per 1,000 sq ft</i>	<i>1 per 4,000 sq ft Min 2</i>	<i>&lt;5,000 sq ft = 0</i>

Parking has been designed using the fast food and commercial retail standards. Applying the Code standards to this development the calculated parking requirements compared to the proposed parking are reflected in Table 5.

**Table 3  
Proposed Parking  
Compared to Code**

Building	Floor Area/1,000 sq. ft.	Minimum Required Spaces/1,000 sq. ft.	Parking Spaces		Proposed ADA Spaces	Required Bicycle Spaces/1,000 sq. ft.	Proposed Bicycle Spaces
			Required	Provided			
Carl's Jr.	2,867	9.9	28	27	1	Min. 42	4
Retail Building	3,150	4.1	13	19	1	1/5,000 Min. 2	2
<b>Total</b>	6.017		41	46	2	6	6

The code requires a minimum of 41 spaces. The maximum number of spaces allowed calculates to 61. The development plan provides 46, plus 2 ADA parking spaces, which complies within the allowed range of minimum and maximum code standards.

In addition, provisions for 6 bicycle spaces will also be provided. Therefore the proposed parking meets code standards, and no waiver is necessary or requested.

*4.155(.04) A loading berth shall contain space twelve (12) feet wide, thirty-five (35) feet long, and have a height clearance of fourteen (14) feet. Where the vehicles generally used for loading and unloading exceed these dimensions, the required length of these berths shall be increased to accommodate the larger vehicles.*

**RESPONSE**

For these sizes of buildings the code does not require separate loading docks. As is typical for these types of uses, delivery trucks utilized available parking spaces. Delivery times are typically scheduled so as to not conflict with peak customer traffic periods. Curb radii and travel lanes have been designed to accommodate anticipate truck access, circulation and maneuvering.

Therefore all of the applicable parking and truck loading criteria have been met.

*H. Signs: As provided in Section 4.156.*

**(.07) Master Sign Plans.** A Master Sign Plan is required for non-residential developments with three (3) or more tenants. In creating a Master Sign Plan thought should be given to needs of initial tenants as well as the potential needs of future tenants.

**A. Master Sign Plan Submission Requirements:** Applications for Master Sign Plans shall include ten (10) paper and electronic copies of all the submission requirements for Class II and III Sign Permits and the following in addition to all required fees:

1. A written explanation of the flexibility of the Master Sign Plan for different potential tenant space configurations over time;
2. A written explanation of the extent to which different sign designs, including those incorporating logos, stylized letters, multiple lines of text, non-straight baselines, or different materials and illumination will be allowed and if allowed how the flexibility of the master sign plan will allow these different sign designs over time;

3. A written explanation of how the sign plan provides for a consistent and compatible sign design throughout the subject development.

**B. Master Sign Plan Review Criteria:** In addition to the review criteria for Class II and Class III Sign Permits, Master Sign Plans shall meet the following criteria:

1. The Master Sign Plan provides for consistent and compatible design of signs throughout the development; and
2. The Master Sign Plan considers future needs, including potential different configurations of tenant spaces and different sign designs, if allowed.

**C. Modifications of a Master Sign Plan:** Modifications of a Master Sign Plan, other than Minor and Major Adjustments, shall be reviewed the same as a new Master Sign Plan.

## RESPONSE

As noted, the subject site shares access with the adjacent Chevron Station and C-Store. Under the executed Development Agreement the applicant intends to add a Carl's Jr. sign box to the existing Chevron freestanding sign on 95<sup>th</sup> Avenue. Because this involves the Chevron site, City Staff recommended a combined Master Sign Plan. The intent is to accommodate a variety of potential tenants on the two sites, without have to continually amend each of the Master Sign Plans.

The owner of the Chevron station has signed the application as a Co-Applicant. The Master Sign Plan has been revised to incorporate the existing freestanding Chevron signs. The revised Master Plan provides more details relative to the allowed tenant signage including maximum area, locations illumination, etc., as addressed in more detail below.

The Site Monument Sign (Boones Ferry Pointe) has been factored into the sign area for the freestanding signs, as addressed in the Tables 4 & 5 below. The areas of the all signs have been calculated using rectangles and circles. The Landscaping Plan has been revised to provide screening consistent with Section 4.176(.02)F, thereby exempting the menu board from the sign area calculations. And, building addressing has been added to the freestanding signs, as well as on the buildings.

However, with this revision it is now necessary to request a Waiver from the Code for total sign area allowed, as addressed below.

Rather than completely replacing the Chevron's Master Sign Plan, it was determined appropriate to only include the freestanding signs and not all of the building signs. Therefore the Master Sign Plan provides for signage for the two new buildings, plus incorporates the two existing freestanding Chevron signs, and then adds two more freestanding signs. Under this plan the Master Sign Plan anticipates a total occupancy of six tenants within the three buildings and the Chevron fuel island. The package includes on site complex monument sign indentifying the development as Boones Ferry Pointe.

The Master Sign Plan is anchored with two corporate franchise approved sign packages, one for the Chevron station and one for Carl's Jr. The coordinated Master Sign Plan does not include all of the Chevron signage, just the west freestanding sign (95<sup>th</sup> Ave.), which

will include a Carl's Jr. sign. In addition it includes building mounted signs for the two new buildings, a site monument sign, and a freestanding sign, to include panels for Carl's Jr. 3-Retail tenants, and the Coca Cola Fountain Mart (C-Store).

The Master Sign Plan incorporates the two existing freestanding Chevron Signs, with the addition of the new signage for Carl's Jr. and the Retail Building. The package includes the following:

- Two existing Chevron freestanding signs, with LED price panels.
  - One faces Boones Ferry Road northeast of the C-Store.
  - One faces 95<sup>th</sup> Avenue just north of the driveway.
    - Relocated to refuge island in modified driveway
    - Carl's Jr. sign box added
- One new freestanding sign for the complex located north of the Retail Building.
  - Includes Carl's Jr.; 3 –Retail Tenants; and Coca Cola Fountain Mart (C-Store)
- A complex Monument Sign (Boones Ferry Pointe) located on the curved plaza wall.
- Addressing on the freestanding signs and buildings.
- Building signs on each elevation.

The Plan specifies the location for each sign, together with the maximum allowed area, cabinet style, and back lighting requirements.

The signs for the Carl's building total are located on the towers over the doors, as shown on the elevation drawings. The sign areas shown are the maximum allowed (36 sq. ft. each) for any future tenant. However, the sign package identifies specific Carl's Jr. signs as approved by corporate.

The signs for the Retail Building are designed to compliment the Carl's Jr. signage, while providing appropriate flexibility for potential tenants. Specific sign locations over doorways and windows are shown on the building elevations. Each sign panel provides for a maximum sign area of 24 square feet.

The applicant is not imposing any specific limitations on signs within these designated spaces, except that the signs must be in a box cabinet with back lighting. It is anticipated that each tenant will have their own unique sign, incorporating logos, stylized letters, multiple lines of text, non-straight baselines, or different materials and illumination, etc. These specifications will be approved through a Type I sign permit at the time of tenant modifications.

The proposed Coordinated Master Sign Plan provides for consistent and compatible design of signs throughout the development; and appropriately considers future needs, including potential different occupants and configurations of tenant spaces and individual sign design preferences, within approved guidelines. The sign design guidelines establish the maximum allowed sign area for both building mounted and freestanding signs, generally defined in rectangular forms, and maximum individual tenant sign areas.

The Master Sign Plan consists of Four Freestanding Signs:

- 95<sup>th</sup> Avenue, West Side: Relocated Chevron Sign, and add Carl's Jr. sign @ 32 sq. ft. Total area is 114 square feet. The LED prices panels are exempt @ 33 sf. ft.
- 95<sup>th</sup> Avenue, North Side: Includes sign panels for Carl's Jr. at 18 sq. ft., 3 Retail Tenants, at 6 sq. ft. each, and Coca Cola Fountain Mart at 12 sq. ft.
- Boones Ferry, East Side: Existing Chevron, with LED prices no change, at 82 sq. ft.
- 95<sup>th</sup>/Boones Ferry intersection: Site Monument, Boones Ferry Pointe at 13 square feet.

**Table 4**  
**Master Sign Plan**  
**Coordinated Freestanding Signs**  
**Carls' Jr., 3 Retail Tenants, Chevron, and Fountain Mart C-Store**

Signs	Location	Site Frontage or Building Length	Sign Area Square Footage	Total Sign Area *	
<b>Coordinated Freestanding</b>	95 <sup>th</sup> Avenue Boones Ferry	480 feet 180 feet			
Chevron	95 <sup>th</sup> Avenue		82	82	
Carl's Jr	K - Add to Relocated Chevron Sign		1 @ 32	32	
Retail Building	Northeast Corner		1 @ 49.48	49	
BFP Monument	North of Retail		13	13	
			<b>Freestanding Total</b>	<b>176</b>	
					<b>Total Carl's Jr. Sign Area *</b>
<b>Carl's Jr Building</b> 2,867 SF		72 feet long 41 feet wide			
	A - Over Entry Tower south		Max 36 Carl's 1 @ 6.25	36	6.25
	B - Over Entry Towers north, east & west		3 - Max 36 Carl's 3 @ 30	108	90
			<b>Building Total</b>	<b>144</b>	<b>96.25</b>

Signs	Location	Site Frontage or Building Length	Sign Area Square Footage	Total Sign Area *	
<b>Retail Building 3,150 SF</b>	Over Entry Doors & Windows	70 feet long 48 feet wide			
	north & south	façade 22.5-25 feet	6 @ 24	144	
	east & west	facade 40 feet	2 @ 36	72	
			<b>Building Total</b>	<b>216</b>	
			<b>Total Site</b>	<b>536</b>	

\* The areas of the all signs have been calculated using rectangles and circles.

**Section 4.156.08. Sign Regulations in the PDC, PDI, and PF Zones.**

**(.01) Freestanding and Ground Mounted Signs:**

*A. One freestanding or ground mounted sign is allowed for the first two-hundred (200) linear feet of site frontage. One additional freestanding or ground mounted sign may be added for through and corner lots having at least two-hundred (200) feet of frontage on one street or right-of-way and one-hundred (100) feet on the other street or right-of-way.*

*B. The allowed height above ground of a freestanding or ground mounted sign is twenty (20) feet except as noted in 1-2 below.*

*1. The maximum allowed height above ground for signs along the frontage of Interstate 5, and parallel contiguous portions of streets, as identified in Figure S-4, associated with multiple tenants or businesses may be increased by three (3) feet for each tenant space of ten thousand (10,000) square feet or more of gross floor area up to a maximum of thirty-five (35) feet.*

*2. The allowed height above ground for signs in the PDC-TC Zone, Old Town Overlay Zone, and PDI Zone is eight (8) feet, except those signs along the frontage of Interstate 5 and parallel contiguous portions of streets identified in Figure S-4.*

*C. The maximum allowed area for each freestanding or ground-mounted sign is determined based on gross floor area and number of tenant spaces:*

*1. For frontages along streets other than those indicated in 2 below sign area allowed is calculated as follows:*

*a. The sign area allowed for signs pertaining to a single tenant:*

Gross Floor Area in a Single Building	Maximum Allowed Sign Area
<i>Less than 11,000 sq. ft</i>	<i>.32 sq. ft.</i>
<i>11,000-25,999 sq. ft.</i>	<i>32 sq. ft. + 2 sq. ft. per 1000 sq. ft. of floor area greater than 10,000 rounded down to the nearest 1,000 sq. ft.</i>
<i>26,000 sq. ft. or more</i>	<i>64 sq. ft.</i>

*i. For PF (Public Facility) zoned properties adjacent to residential zoned land the maximum allowed area is thirty-two (32) square feet.*



*b. The maximum allowed sign area for signs pertaining to multiple tenants or businesses is thirty-two (32) square feet plus the following for each tenant space:*

<i>Gross Floor Area of Tenant Space</i>	<i>Additional Allowed Sign Area for Tenant Space</i>
<i>Less than 1,000 sq. ft.</i>	<i>3 sq. ft.</i>
<i>1,000-10,999</i>	<i>3 sq. ft. + 3 sq. ft. per 1,000 sq. ft. of floor area rounded down to the nearest 1,000 sq. ft.</i>
<i>11,000 sq. ft. or more</i>	<i>32 sq. ft.</i>

- i. The total sign area shall not exceed two hundred (200) square feet, except in the PDC-TC Zone, Old Town Overlay Zone, and PDI Zone the total sign area shall not exceed eighty (80) square feet.*
- ii. Though the maximum allowed sign area is calculated based on number of tenant spaces and their size, the content of the sign and area used for different content is at the discretion of the sign owner, except for required addressing.*

## **RESPONSE**

The Carl's Jr. site has a total of approximately 460 linear feet of frontage along 95<sup>th</sup> Avenue and about 180 feet of frontage along Boones Ferry Road.

The Chevron site only has 11 feet of frontage on 95<sup>th</sup> Avenue, but also has approximately 230 feet of frontage on Boones Ferry Road. It is noted that the Carl's Jr. site was originally part of the Chevron property, but was subsequently partitioned off.

The combined sites have 471 feet of frontage on 95<sup>th</sup> Avenue and 410 of frontage on Boones Ferry Road. Therefore they qualify for two freestanding signs.

The existing Chevron sign on 95<sup>th</sup> Avenue, which includes LED price panels, will be relocated within the pedestrian refuge island in the entry drive. A Carl's Jr. sign box, at 32 square feet, will be added to the inside of the pole, for a total sign area of 114 square feet. The sign height will remain at 20 feet.

In addition, a second freestanding sign is proposed north east of the Retail Building. This sign will have a total area of 49.48 square feet. There are four tenants identified, Carl's Jr. at 22.48 square feet and three potential tenants in the Retail Building each at 9 square feet. The total sign height will be 20 feet.

For the two freestanding sign, the site and sign locations are generally at the same grade as the street, so no height adjustment is necessary.

However, with the inclusion of the existing Chevron freestanding sign on 95<sup>th</sup> Avenue, it is necessary to request a Waiver from the Code for total sign area allowed. The existing Chevron sign is larger than would be allowed for the new development alone, see Table 5 below.

## Historical Perspective

In order to understand the proposed coordinated Master Sign Plan it is important to provide some historical perspective. At the time the Chevron property was originally planned (Exxon) and developed it was combined under the same ownership with the Carl's Jr. site. The original approved plans envisioned a second phase on the northern section of the property. The existing Chevron sign on 95<sup>th</sup> Avenue was placed for best visibility, with combined signage for Phase 2 envisioned.

Subsequent to the initial site development, as an Exxon station, the northern portion of the property was partitioned off and sold. The partitioning and sale included an agreement for the existing sign to remain (now off-site), still anticipating combined signage for the future development.

Then prior to the submittal for the currently proposed development (Carl's Jr & Retail Building) there was considerable negotiations between the adjoining property owners and the City, which resulted in a Development Agreement. The Agreement included combined use of the existing freestanding sign, as well as relocating it to accommodate the modifications to the ingress/egress configuration.

In addition, subsequent to the approval of the Chevron Master Sign Plan (DB07-0056) the City amended the Sign Code. The amended Code changed the amount and method of calculating allowed signage. It also separated the allowed signage providing different standards for freestanding versus building signage, and modified Master Sign Plan provisions.

The Code revisions overall eliminated many identified problems with the old standards, and improved general administration. However, unfortunately, the new freestanding sign provisions make it a little more difficult to provide for combined signage for the two properties. Therefore it has been determined that a Waiver is necessary to accommodate the proposed coordinated freestanding signs, including the Site Monument.

First, the new Code allows one freestanding sign, with the first 200 feet of street frontage. Then it allows a second sign, if the property has at least 100 feet of frontage on a second street.

The Carl's Jr. site has approximately 460 linear feet of frontage along 95<sup>th</sup> Avenue and another 180 feet of frontage along Boones Ferry Road. Therefore this property qualifies for two freestanding signs under the new Code.

The Chevron site only has 11 feet of frontage on 95<sup>th</sup> Avenue, but also has approximately 230 feet of frontage on Boones Ferry Road. It technically only qualified for one freestanding sign under the old Code, but was allowed the second sign based on the original frontage, prior to the Carl's Jr. site being partitioned.

However, under the new Code, if the two sites were combined for master plan purposes, they would still only qualify for two freestanding signs. Recognizing the Chevron's existing two signs the Development Agreement included a provision to add a Carl's Jr.

sign box to the existing 95<sup>th</sup> Avenue sign. As a reciprocal action, the applicant agreed to allow Mr. LaPoint to add a sign panel on the proposed new north sign to identify the Coca Cola Fountain Mart C-Store.

It is noted that with the original Exxon approval, the two signs included the Fountain Mart. But when the station was converted to Chevron and the new LED price signs were approved the Fountain Mart panels were dropped.

Second, the new Code sets new standards for the sign area allowed for freestanding signs, based on building square footage as follows:

Allowed Freestanding Sign Area: 32 sq ft, Plus 3 sq ft, + 3/1000, rounded to nearest 1,000 sq. ft.  
Total 6 Tenants, with Chevron and C-Store

Freestanding Signs

**Table 5  
Allowed and Proposed  
Freestanding Signs**

<b>Tenants (# tenants)</b>	<b>Building Area Sq. Ft.</b>	<b>Additional Sign Area Allowed over Base Sq. Ft.</b>	<b>Proposed 95<sup>th</sup> Avenue West</b>	<b>Proposed Boones Ferry North</b>
Base Area		32		
Carl's Jr. (1)	2,867	12	<b>32</b>	<b>18</b>
Retail Building (3)	3,150	12	<b>0</b>	<b>18</b>
BFP Monument		0	<b>0</b>	<b>13</b>
Chevron Canopy (1)	3,536	15	<b>82 Existing</b>	
C-Store (1)	2,267	9	<b>0</b>	<b>12</b>
Total		80 65 less Chevron	<b>114</b>	<b>61</b>

1. The Chevron sign has 3 exempt fuel price panels = 33 sq. ft.
2. A separate freestanding sign for Carl's Jr. could be 44 sq. ft.

Based on the summary table, the proposed new north sign complies with the new standards, including the Boones Ferry Pointe monument sign and accounting for 5 tenants, Carl's, 3-Retail and the Fountain Mart C-Store.

However, the 95<sup>th</sup> Avenue sign is 2 square feet over the allowed area just with the existing Chevron sign. Adding the Carl's Jr. sign puts this sign 34 square feet of the allowed area.

If no consideration for joint signage were considered the Carl's Jr. site would be eligible for a 44 square foot sign just by itself, or up to an 56 square foot sign for the 4 tenants anticipated in the 2 building complex.

The applicants believe it is better to combine the signage than to add an additional sign. Therefore we are requesting a Waiver to the allowed area for the 95<sup>th</sup> Avenue sign, thus allowing for the addition of the 32 square foot Carl's Jr. sign box on the east side of the post.

### Waiver Criteria

*(.08) Waivers and Variances. Waivers and variances are similar in that they allow deviation from requirements such as area, and height from ground. They differ in that waivers are granted by the DRB as part of a comprehensive review of the design and function of an entire site to bring about an improved design and variances are granted by either the Planning Director or DRB to relieve a specific hardship caused by the regulations.*

*A. Waivers. The DRB may grant waivers for sign area, sign height from ground (no waiver shall be granted to allow signs to exceed thirty-five (35) feet in height), number of signs, or use of electronic changeable copy signs in order to better implement the purpose and objectives of the sign regulations as determined by making findings that all of the following criteria are met:*

- 1. The waiver will result in improved sign design, in regards to both aesthetics and functionality.*
- 2. The waiver will result in a sign or signs more compatible with and complementary to the overall design and architecture of a site, along with adjoining properties, surrounding areas, and the zoning district than signs allowed without the waiver.*
- 3. The waiver will result in a sign or signs that improve, or at least do not negatively impact, public safety, especially traffic safety.*
- 4. Sign content is not being considered when determining whether or not to grant a waiver.*

### **RESPONSE**

The waiver is necessary to account for the existing approved Chevron signs, while adding a Carl's Jr. sign to the 95<sup>th</sup> Avenue sign and a Coca Cola Fountain Mart sign to the new north freestanding sign. The Waiver is further required to allow for factoring in the proposed Complex Monument sign. The combined signage results in improved coordinated signage for the combined uses, while maintaining aesthetics and functionality.

The waiver will result in a more coordinated sign program for the 6 tenants occupying the two properties. The coordinated design provided for complementary design and a reduced number of independent signs. The coordinated design is complementary to the surrounding architecture adjoining properties, surrounding areas, and the zoning district than signs that would be allowed without the waiver.

The waiver will result in a coordinated sign package that provides good identity for all on-site tenants while not negatively impacting public safety, especially traffic safety.

The Waiver allows for a Coordinate Master Plan, which defines allowable signage for a variety of existing or future tenants, without limiting content as a basis for granting the waiver. Therefore all Waiver Approval Criteria are met.

**2. Signs fronting Interstate 5 and parallel contiguous street sections, as identified in Figure S-4.**

**D. Pole or sign support placement shall be installed in a full vertical position.**

**E. Freestanding and ground mounted signs shall not extend into or above public rights-of-way, parking areas, or vehicle maneuvering areas.**

**RESPONSE**

There are no proposed signs that front on Interstate 5 or a parallel contiguous street, so sub-section 2 does not apply.

The freestanding signs are designed so that neither the sign or its support are installed in a full vertical position. Further the Freestanding signs are located on the site so that they do not extend into or above public rights-of-way, parking areas, or vehicle maneuvering areas.

*F. The location of free standing or ground mounted signs located adjacent to or near the Public Right-of-Way shall be in compliance with the City's Public Works Standards for sight distance clearance. Prior to construction, the location of the sign shall be approved by the City of Wilsonville Engineering Division.*

*G. Freestanding and ground mounted signs shall be designed to match or complement the architectural design of buildings on the site.*

*H. For freestanding and ground mounted signs greater than eight (8) feet in height, the width of the sign shall not exceed the height.*

*I. Along street frontages in the PDC-TC Zone and Old Town Overlay Zone monument style signs are required.*

*J. Freestanding and ground mounted signs shall be no further than fifteen (15) feet from the property line and no closer than two (2) feet from a sidewalk or other hard surface in the public right-of-way.*

*K. Except for those signs fronting Interstate 5, freestanding and ground mounted signs shall include the address number of associated buildings unless otherwise approved in writing by the City and the Fire District.*

*L. When a sign is designed based on the number of planned tenant spaces it shall remain a legal, conforming sign regardless of the change in the number of tenants or configuration of tenant spaces.*

**RESPONSE**

The proposed free standing signs are located adjacent compliance with the City's Public Works Standards for sight distance clearance. The applicant understands that prior to construction, the location of the sign must be approved by the City of Wilsonville Engineering Division.

The freestanding signs have been designed to match or complement the architectural design of buildings on the site. The proposed height and widths of the two signs area consistent with the Code standards.

This site is not within the PDC-TC Zone or the Old Town Overlay Zone therefore monument style signs are not required.

The freestanding signs are not further than fifteen (15) feet from the property line and no closer than two (2) feet from a sidewalk or other hard surface in the public right-of-way.

The signs do not front Interstate 5.

The applicant understands that when a sign is designed based on the number of planned tenant spaces it remains a legal, conforming sign regardless of the change in the number of tenants or configuration of tenant spaces.

**(.02) Signs on Buildings.**

**A. Sign Eligible Facades:** Building signs are allowed on a facade of a tenant space or single tenant building when one or more of the following criteria are met:

1. The facade has one or more entrances open to the general public;
2. The facade faces a lot line with frontage on a street or private drive with a cross section similar to a public street, and no other buildings on the same lot obstruct the view of the building facade from the street or private drive; or
3. The facade is adjacent to the primary parking area for the building or tenant.

**B. Sign Area Allowed:**

1. The sign area allowed for all building signs on a sign eligible facade is shown in the table below:

<b>Linear Length of Façade (feet)</b>	<b>Sign Area Allowed*</b>
Less than 16	Area equal to linear length
16 to 24	24 sq. ft.
Greater than 24 to 32	32 sq. ft.
Greater than 32 to 36	Area equal to linear length
Greater than 36 to 72	36 sq. ft.
Greater than 72	36 sq. ft. plus 12 sq. ft. for each 24 linear feet or portion thereof greater than 72 up to a maximum of 200 sq. ft.

\*Except as noted in 2. through 5. below

2. The sign area allowed for facades with a primary public entrance or with a frontage along a public street dominated by windows or glazing may be increased by transferring to the facade up to one half (1/2) the sign area allowed for adjacent facades up to fifty (50) square feet. In no case shall the allowed sign area exceed an area equal to the linear length of the facade.

3. The sign area allowed is increased as follows for signs at separate building entrances:

- a. For building entrances open to the general public located at least fifty (50) feet apart on the same facade, the sign area allowed is increased by fifty (50) percent up to fifty (50) square feet.
- b. For building entrances located less than fifty (50) feet apart on the same facades, the sign area allowed is increased by twenty (20) percent up to twenty (20) square feet.

4. For businesses occupying multiple buildings in a campus setting, sign area shall be limited to that allowed for the largest building, which may then be distributed throughout the campus.

*5. If a façade otherwise not sign eligible faces a lot line with frontage on Interstate 5, the applicant can transfer sign area allowed from one (1) of the locations described in a. and b. below. In no case shall the allowed sign area exceed an area equal to the allowed sign area for a sign eligible façade of the same linear length.*

*a. The freestanding sign along the Interstate 5 frontage. This generally involves placing building signs on the subject façade in lieu of installing a freestanding sign.*

*b. Adjacent façade up to fifty (50) square feet, when a majority of the adjacent façade from which the sign area is being transferred is visible from Interstate 5.*

*6. Calculating linear length of a façade for the purpose of determining maximum sign area allowed. For facades of a single tenant building the length the façade measured at the building line, except as noted in a. and b. below. For multitenant buildings the width of the façade of the tenant space shall be measured*

## **RESPONSE**

The proposed building signs all meet, at least, one of the criteria in (.02).1-3 above. They either face a street or a parking lot, or they are over an entry door.

The Carl's Jr. Building has two facades that are 40 feet wide and two that are 70 feet long. The building is designed to accommodate a single tenant. The east and west elevations are allowed 36 square feet, while the north and south elevations are also allowed 36 square feet. This is the maximum allowed area for each elevation.

However, for the Carl's Jr the actual proposed signage allocates 30 square feet to each of the east, west, and north elevations, with 6.25 square feet on the south elevation.

The Retail Building has two facades that are 45 feet wide and two that are 70 feet long. The building is designed to accommodate up to three tenants, with anticipated tenant facades of 25 and 2 @ 22.5 feet. The Master Plan allocates 24 square feet for each tenant on the north and south elevations, and 36 square feet for the two end tenants, which will have either the east or west facades available for signage.

Therefore the propose Master Sign Plan complies with the sign area and locations allowed.

### *4.167 General Regulations - Access, Ingress and Egress*

*(.01) Each access onto streets shall be at defined points as approved by the City and shall be consistent with the public's health, safety and general welfare. Such defined points of access shall be approved at the time of issuance of a building permit if not previously determined in the development permit.*

## **RESPONSE**

As previously discussed access and egress for the site is provided by a joint use single curbcut on 95<sup>th</sup> Avenue. The design and configuration for the joint driveway is defined in a Development Agreement entered into by the applicant, the Holiday Inn, Chevron Station and the City.



#### **4.175 Public Safety and Crime Prevention**

- (.01) All developments shall be designed to deter crime and insure public safety.*
- (.02) Addressing and directional signing shall be designed to assure identification of all buildings and structures by emergency response personnel, as well as the general public.*
- (.03) Areas vulnerable to crime shall be designed to allow surveillance. Parking and loading areas shall be designed for access by police in the course of routine patrol duties.*
- (.04) Exterior lighting shall be designed and oriented to discourage crime.*

### **RESPONSE**

The applicant is of course concerned about deterring crime and ensuring public safety throughout the site. The Site Design provides for appropriate addressing and directional signage designed to assure easy identification of this multi-tenant development. Attention has been given to avoid creating areas vulnerable to crime, and to the extent practical the design allows for natural surveillance from the adjacent street and abutting uses.

The buildings have been designed to be aesthetically pleasing. Vegetative screening has been provided to buffer and accent the building from the road and to create a visual focal point at the entry to this business district.

The parking areas have also been designed for easy access and surveillance by City police in their course of routine patrols. Exterior lighting has been minimized, but designed and oriented to discourage crime. See also Sections 4.199 and 4.400 Outdoor lighting discussion below.

Therefore the proposed development complies with these standards.

#### **4.176 Landscaping and Buffering**

##### *(.02) Landscaping and Screening Standards.*

*A. Subsections "C" through "I," below, state the different landscaping and screening standards to be applied throughout the City. The locations where the landscaping and screening are required and the depth of the landscaping and screening is stated in various places in the Code.*

*B. All landscaping and screening required by this Code must comply with all of the provisions of this Section, unless specifically waived or granted a Variance as otherwise provided in the Code. The landscaping standards are minimum requirements; higher standards can be substituted as long as fence and vegetation height limitations are met. Where the standards set a minimum based on square footage or linear footage, they shall be interpreted as applying to each complete or partial increment of area or length (e.g., a landscaped area of between 800 and 1600 square feet shall have two trees if the standard calls for one tree per 800 square feet.*

##### *C. General Landscaping Standard.*

*1. Intent. The General Landscaping Standard is a landscape treatment for areas that are generally open. It is intended to be applied in situations where distance is used as the principal means of separating uses or developments and landscaping is required to enhance the intervening space. Landscaping may include a mixture of ground cover, evergreen and deciduous shrubs, and coniferous and deciduous trees.*

*2. Required materials. Shrubs and trees, other than street trees, may be grouped. Ground cover plants must fully cover the remainder of the landscaped area (see Figure 21: General Landscaping). The General Landscaping Standard has two different requirements for trees and shrubs:*

*a. Where the landscaped area is less than 30 feet deep, one tree is required for every 30 linear feet.*

*b. Where the landscaped area is 30 feet deep or greater, one tree is required for every 800 square feet and two high shrubs or three low shrubs are required for every 400 square feet.*

**D. Low Screen Landscaping Standard.**

**1. Intent.** *The Low Screen Landscaping Standard is a landscape treatment that uses a combination of distance and low screening to separate uses or developments. It is intended to be applied in situations where low screening is adequate to soften the impact of one use or development on another, or where visibility between areas is more important than a total visual screen. The Low Screen Landscaping Standard is usually applied along street lot lines or in the area separating parking lots from street rights-of-way.*

**2. Required materials.** *The Low Screen Landscaping Standard requires sufficient low shrubs to form a continuous screen three (3) feet high and 95% opaque, year-round. In addition, one tree is required for every 30 linear feet of landscaped area, or as otherwise required to provide a tree canopy over the landscaped area. Ground cover plants must fully cover the remainder of the landscaped area. A three (3) foot high masonry wall or a berm may be substituted for the shrubs, but the trees and ground cover plants are still required. When applied along street lot lines, the screen or wall is to be placed along the interior side of the landscaped area. (See Figure 22: Low Screen Landscaping).*

## **RESPONSE**

The landscape design at the Wilsonville Retail Center and Carl's Jr. will consist of two landscape types: Streetscape and Perimeter.

The streetscape, around the retail buildings and restaurant will consist of ornamental plantings including hedges, perennials, shade trees and fine lawn. The landscape design around the buildings reflect the building architecture lines through low hedges at the base of the building up to the window mullion, small conifer uprights in the corners to frame facades and perennials in front of the hedges for summer color. Annuals will be changed out twice a year to provide year round color at the entry. Street trees will be a columnar maple with brilliant fall color and strong branching form in the winter. The landscape is also designed to mature successfully reflecting current maintenance practices conducted in the field.

The eastern perimeter landscape and the slope to the fence bordering the property line to the east will be planted with native trees, shrubs and ground cover. This area will evolve to fill in requiring very little maintenance once established. This native planting area will also be irrigated until establishment and will not require additional water after establishment.

The landscape design for the Wilsonville Retail Building and the Carl's Jr. Restaurant will include a wide variety of plantings with brilliant seasonal color and beautiful seasonal blooms back dropped by an evergreen foundation and dappled with shade which will make the landscape attractive throughout all seasons.

*(.03) **Landscape Area.** Not less than fifteen (15%) of the total lot area, shall be landscaped with vegetative plant materials. The ten (10%) parking area landscaping required by Section 4.155(B)(1) is included in the fifteen percent (15%) total lot landscaping requirement. Landscaping shall be located in at least three separate and distinct area of the lot, on of which must be in the contiguous frontage area. Planting areas shall be encouraged adjacent to buildings and off-street parking areas. Materials to be installed shall achieve a balance between various plant forms, textures, and heights. The installation of native plant materials shall be used whenever practicable.*

*(.04) **Buffering and Screening.** Additional to the standards of this subsection, the requirements of Section 4.137.5 (Screening and Buffering Overlay Zone) shall also be applied, where applicable.*

## **RESPONSE**

The proposed site development plan provides for total landscaping coverage of 16,139 square feet, or 29% of the site area. Therefore the plan exceeds the minimum 15% standard.

The landscaping is designed to physically separate the two portions of the site, and to accent each building. Additional landscaping is provided in the parking and trash enclosure planter island that separates the subject site from the Chevron property. This landscaped island also serves to channel on-site traffic flows between the two sites. In addition, the landscaped feature at the north end utilizes an otherwise non-usable are of the site to create an attractive visual focal point for the entry into the 95<sup>th</sup> Avenue industrial district.

There has not been a Screening and Buffering Overlay Zone established for this site, so subsection (.04) does not apply. However appropriate vegetative screening has been provided to soften the view of the building and equipment yard from 95<sup>th</sup> Avenue and Boones Ferry Road view points. The Landscaping Plan has been revised to provide screening consistent with Section 4.176(.02) F., thereby exempting the menu board from the sign area calculations.

The Landscaping Plan has been revised to provide screening consistent with Section 4.176(.02)F, thereby exempting the menu board from the sign area calculations.

The Landscaping Plan includes the water quality pond, and provides a total of 16,139 square feet of planted area. This amount of landscaping allows for a maximum (@ 10%) of 1,614 square feet of lawn area.

However, in conversations with Planning Staff it has been determined that, given the corner lot and large area of street frontage, it would be appropriate to allow more than just the minimum lawn area.

Based on staff comments, the Landscaping Plan has been modified to increase ground cover and storm swale plantings. This reduces the lawn area to 2,080 square feet or 13% of the total landscaped area.

The landscaping is provided in 4 areas around the perimeter of the site and building and includes shade tree islands within the parking area. This design is consistent with and complimentary to the established landscape designs for the two adjacent properties, and is also consistent with code standards, as described herein.

*(.06) Plant Materials.*

*C. Where a proposed development includes buildings larger than twenty-four (24) feet in height or greater than 50,000 square feet in footprint area, the Development Review Board may require larger or more mature plant materials:*

*1. At maturity, proposed trees shall be at least one-half the height of the building to which they are closest, and building walls longer than 50 feet shall require tree groups located no more than fifty (50) feet on center, to break up the length and height of the façade.*

*2. Either fully branched deciduous or evergreen trees may be specified depending upon the desired results. Where solar access is to be preserved, only solar-friendly deciduous trees are to be used. Where year-round sight obscuring is the highest priority, evergreen trees are to be used.*

*3. The following standards are to be applied:*

*a. Deciduous trees:*

*i. Minimum height of ten (10) feet; and*

*ii. Minimum trunk diameter (caliper) of 2 inches (measured at four and one-half [4 1/2] feet above grade).*

*b. Evergreen trees: Minimum height of twelve (12) feet.*

*E. Types of Plant Species.*

*1. Existing landscaping or native vegetation may be used to meet these standards, if protected and maintained during the construction phase of the development and if the plant species do not include any that have been listed by the City as prohibited. The existing native and non-native vegetation to be incorporated into the landscaping shall be identified.*

*2. Selection of plant materials. Landscape materials shall be selected and sited to produce hardy and drought-tolerant landscaping. Selection shall be based on soil characteristics, maintenance requirements, exposure to sun and wind, slope and contours of the site, and compatibility with other vegetation that will remain on the site. Suggested species lists for street trees, shrubs and groundcovers shall be provided by the City of Wilsonville.*

## **RESPONSE**

The project landscape architect provided the previous narrative outlining the design and planting materials. Landscaping consists of a combination of ground covers, ornamental shrubs, and a variety of deciduous and evergreen trees.

All proposed planting areas will have automatic irrigation systems installed to ensure the proper establishment of the proposed plantings and, more importantly, the long-term survival of all the plants.

Therefore the proposed landscaping complies with the applicable design criteria.

*Section 4.177. Street Improvement Standards.*

*Note: This section is expected to be revised after the completion of the Transportation Systems Plan.*

*(.01) Except as specifically approved by the Development Review Board, all street and access improvements shall conform to the Transportation Systems Plan and the Public Works Standards, together with the following standards:*

*A. All street improvements and intersections shall conform to the Public Works Standards and shall provide for the continuation of streets through specific developments to adjoining properties or subdivisions.*

*B. All streets shall be developed with curbs, utility strips and sidewalks on both sides; or a sidewalk on one side and a bike path on the other side.*

*1. Within a Planned Development the Development Review Board may approve a sidewalk on only one side. If the sidewalk is permitted on just one side of the street, the owners will be required to sign an agreement to an assessment in the future to construct the other sidewalk if the City Council decides it is necessary.*

## **RESPONSE**

Site access, egress and site circulation have been designed consistent with the Development Agreement.

Street improvements have already been completed providing adequate traffic capacity for the proposed development consistent with the City's "D" LOS standard, see also DKS Traffic Impact Report.

*Section 4.179. Mixed Solid Waste and Recyclables Storage in New Multi-Unit Residential and Non-Residential Buildings.*

*(.01) All site plans for multi-unit residential and non-residential buildings submitted to the Wilsonville Development Review Board for approval shall include adequate storage space for mixed solid waste and source separated recyclables.*

*(.02) The floor area of an interior or exterior storage area shall be excluded from the calculation of building floor area for purposes of determining minimum storage requirements.*

*(.03) The storage area requirement shall be based on the predominant use(s) of the building. If a building has more than one of the uses listed herein and that use occupies 20 percent or less of the floor area of the building, the floor area occupied by that use shall be counted toward the floor area of the predominant use(s). If a building has more than one of the uses listed herein and that use occupies more than 20 percent of the floor area of the building, then the storage area requirement for the whole building shall be the sum of the requirement for the area of each use.*

*(.04) Storage areas for multiple uses on a single site may be combined and shared.*

*(.05) The specific requirements are based on an assumed storage height of four feet for seven feet may be used to accommodate the same volume of storage in a reduced floor space. Where vertical or stacked storage is proposed, the site plan shall include drawings to illustrate the layout of the storage area and dimensions for the containers.*

*(.06) The specific requirements for storage area are as follows:*

*A. Multi-unit residential buildings containing five-ten units shall provide a minimum storage area of 50 square feet. Buildings containing more than ten residential units shall provide an additional five square feet per unit for each unit above ten.*

*B. Non-residential buildings shall provide a minimum storage area of ten square feet, plus:*

1. Office: Four square feet per 1,000 square feet gross floor area (GFA);
2. Retail: Ten square feet per 1,000 square feet GFA;
3. Wholesale / Warehouse / Manufacturing: Six square feet per 1,000 square feet GFA; and
4. Other: Four square feet per 1,000 square feet GFA.

(.07) The applicant shall work with the City's franchised garbage hauler to ensure that site plans provide adequate access for the hauler's equipment and that storage area is adequate for the anticipated volumes, level of service and any other special circumstances which may result in the storage area exceeding its capacity. The hauler shall notify the City by letter of their review of site plans and make recommendations for changes in those plans pursuant to the other provisions of this section.

## RESPONSE

The development plan provides adequate storage space for mixed solid waste and source separated recyclables, which is provided in a combination of interior space and exterior storage area.

There is one screened enclosure (15'x18' inside) located at the southeast corner of the Carl's Jr. building. Two additional enclosures are provided in the landscaped island along the common property line with the Chevron Station. One of these enclosures is relocated for the Chevron Station and the other will serve the Retail Building. One enclosure is 18' x 11' and the other is 19' x 11'.

These storage area designs have been coordinated with Allied Waste Service, the City's franchised garbage hauler. The applicant has made minor revisions to the trash enclosures based on comments from Allied Waste, and has obtained a letter from Allied Waste approving the revised trash enclosure designs, see attached letter at Index Tab.

- a. The hinges have been designed as 180 degree hinges regardless of the door swing shown. We are showing some stops at 120 degrees right now at the Chevron and the retail trash enclosures, but they can be moved to a larger degree if desired.
- b. The minimum width for access to the trash enclosure is 12'. So if the trash enclosure is 5'+ feet wider, it seems to me that 90 degrees is not a problem.
- c. The CJ trash enclosure is 17'-6" wide at the opening and used 90 degree stops due to the side walk, building and landscaping. However, it is unrealistic to expect the doors to open at 180 degrees in this location since the building prevents one door and the landscaping requirements the other.

**Table 6**  
**Mixed Solid Waste and Recyclables Storage**

Type Use	Storage Area Standard Square Feet	Proposed Square Feet
Carl's Jr @ 2,867 sf	4 sq ft/1,000 = 11	270
Retail Building @ 3,150 sf	10 sq ft/1,000 = 32	198
Chevron C-Store @ 3,468 sf	10 sq ft/1,000 = 35	209
<b>Total</b>	78	677

Therefore the proposed development plan complies with the standards for mixed solid waste and source separated recyclables.

#### **4.199 OUTDOOR LIGHTING**

##### **4.199.20. Applicability:**

**(.01) This Ordinance is applicable to:**

- A. Installation of new exterior lighting systems in public facility, commercial industrial and multi-family housing projects with common areas.**
- B. Major additions or modifications (as defined in this Section) to existing exterior lighting systems in public facility, commercial industrial and multi-family housing projects with common areas.**

**(.02) Exemption. The following luminaires and lighting systems are EXEMPT from these requirements:**

- A. Interior lighting**
- B. Internally illuminated signs**
- F. Building Code required exit path lighting**
- G. Lighting specifically for stairs and ramps**
- K. Code required Signs**
- M. Landscape lighting**

##### **4.199.30 Lighting Overlay Zones.**

**(.01) The designated Lighting Zone as indicated on the Lighting Overlay Zone Map for a commercial, industrial, multi-family, or public facility parcel or project shall determine the limitations for lighting systems and fixtures as specified in this Ordinance.**

**(.02) The Lighting Zones shall be:**

- A. LZ 0. Critical dark environments.**
- B. LZ 1. Developed areas in City and State parks, recreation areas, SROZ wetland and wildlife habitat areas: developed areas in natural settings; sensitive night environments; and rural areas.**
- C. LZ 2. Low-density suburban neighborhoods and suburban commercial districts, industrial parks and districts. This zone is intended to be the default condition for the majority of the City.**
- D. LZ 3. Medium to high-density suburban neighborhoods and districts, major shopping and commercial districts as depicted on the Lighting Overlay Zone Map.**
- E. LZ 4. Reserved for limited applications with special lighting requirements.**

##### **4.199.40. Lighting Systems Standards for Approval.**

**(.01) Non-Residential Uses and Common Residential Areas.**

- A. All outdoor lighting shall comply with either the Prescriptive Option or the Performance Option below.**

**(.03) In addition to the above submittal requirements, Applicants using the Performance Method shall submit the following information as part of the permit set plan review:**

- A. Site plan showing horizontal isocandle lines, or the output of a point-by-point computer calculation of the horizontal illumination of the site, showing property lines and light levels immediately off of the subject property.**
- B. For each side of the property, the output of a point-by-point vertical footcandle calculation showing illumination in the vertical plane at the property line from grade to at least 10 feet higher than the height of the tallest pole.**
- C. Lighting plans shall be prepared by a qualified licensed engineer.**

##### **4.199.60. Major Additions or Modifications to Pre-Existing Sites.**



*(01.) Major Additions. If a major addition occurs on a property, all of the luminaires on the site shall comply with the requirements of this Section. For purposes of this subsection, the following are considered to be major additions:*

*A. Additions of 50 percent or more in terms of additional dwelling units, gross floor area, seating capacity, or parking spaces, either with a single addition or with cumulative additions after July 2, 2008.*

*B. Modification or replacement of 50 percent or more of the outdoor lighting luminaries' within a 5-year timeframe existing as of July 2, 2008.*

**Table 9: Performance Method**

<i>Lighting Zone</i>	<i>Maximum percentage of direct uplight lumens</i>	<i>Maximum Light Level at Property Line</i>	
		<i>Horizontal plane at grade (foot candles - fc)</i>	<i>Vertical plane facing the site in question, from grade to mounting height of highest mounted luminaire (foot candles - fc)</i>
<i>LZ 2</i>	<i>5%</i>	<i>0.2 fc</i>	<i>0.4 fc</i>

**From Table 10: Curfew: Lighting Zone Curfew Time**

*LZ 2 10:00 PM (2200 hours)*

**RESPONSE**

The City has adopted new outdoor lighting standards, Section 4.199. These relatively new regulations set standards for the intensity of outdoor lighting, and there are also curfew provisions, aimed at lower artificial light levels at night (dark sky).

Section 4.199.30(.02) establishes lighting zones. The subject site is within LZ 2, as identified on the Lighting Zone Map. This zone applies to low-density suburban neighborhoods and suburban commercial districts, industrial parks and districts, and is the default condition for the majority of the City. This zone has a 10:00 PM curfew, which calls for lower lighting levels after 10:00 PM.

The development site is within a developed commercial and industrial district and within the PDC, Planned Development Commercial Zone. This development is subject to the provisions of these new regulations, as defined above. The following findings and supporting drawings demonstrate that the proposed external lighting associated with this new development has been designed to comply with this section utilizing the "Performance Option".

However, this section also provides an exception for businesses that operate continuously (Exception 3). The Carl's Jr. will be a 24 hour, 7 day/week operation so this exception is applicable.

Even with this exception, the Development Plans provide for lighting that is energy efficient. The plan calls for 5, 25 foot tall parking lot lights around the Carl's Jr. and 2, 36 foot tall lights at the north end Plaza. The proposed lighting is reflected in Table 6

below. These lights meet City standards to the greatest degree practicable, while accounting for on-site safety and security.

The Electrical Plan set provides the photometrics and details for all luminaires indicated on the submitted cut sheets. The lights are controlled by timers and photocells to dim lighting levels after 10:00PM consistent with Code requirements.

The pole mounted luminaires for the parking area are not all set back from all property lines at least 3 times the mounting height. The lights do qualify for exemptions as follows:

- The subject property abuts property with the same base PDC zone and LZ2 lighting zone. Therefore no setback from the common lot lines is required
- The luminaires are for the purpose of parking lot lighting and are located less than 3 mounting heights from the property line. However, these lights are directionally controlled which provide essentially full shielding (zero intensity) at the adjoining property. And the adjoining property is not residential.

See Lighting Plans for details. This proposed lighting is considered adequate for general evening hour access safety and security.

**Table 8: Maximum Lighting Mounting Height In Feet**

<i>Lighting Zone</i>	<i>Lighting for private drives, driveways, parking, bus stops and other transit facilities</i>	<i>Lighting for walkways, bikeways, plazas and other pedestrian areas</i>	<i>All other lighting</i>
<i>LZ 0</i>	<i>20</i>	<i>8</i>	<i>4</i>
<i>LZ 1</i>	<i>25</i>	<i>12</i>	<i>4</i>
<i>LZ 2</i>	<i>40</i>	<i>18</i>	<i>8</i>
<i>LZ 3</i>	<i>40</i>	<i>18</i>	<i>16</i>
<i>LZ 4</i>	<i>Height limit to be determined by Special Use Permit Only</i>		

*Lighting mounted onto buildings or other structures shall not exceed a mounting height greater than 4 feet higher than the tallest part of the building or structure at the place where the lighting is installed, nor higher than 33.33 percent of the horizontal distance of the light from the nearest property line, whichever is less.*

The subject site is in LZ 2. Below are the mounting heights for the light fixtures, which demonstrating compliance with Table 8:

- Carl's parking area pole-mounted fixtures – 20'
- Drive-up Bollard – 42''
- Retail plaza area – 30'

## **Conclusion - Lighting**

The revised lighting plans and photometrics demonstrate compliance using the Performance option. Based on the above findings and plans submitted this project complies with the outdoor lighting standards.

### ***Section 4.300. General Underground Utilities***

***(.01) The City Council deems it reasonable and necessary in order to accomplish the orderly and desirable development of land within the corporate limits of the City, to require the underground installation of utilities in all new developments.***

***(.02) After the effective date of this Code, the approval of any development of land within the City will be upon the express condition that all new utility lines, including but not limited to those required for power, communication, street lighting, gas, cable television services and related facilities, shall be placed underground.***

***(.03) The construction of underground utilities shall be subject to the City's Public Works Standards and shall meet applicable requirements for erosion control and other environmental protection.***

### ***Section 4.310 Exceptions.***

***Section 4.300 of this Code shall not apply to surface-mounted transformers, surface-mounted connection boxes, wireless communication facilities, and meter cabinets and other appurtenances which are reasonably necessary to be placed above ground, or to temporary utility service facilities during construction, or to high capacity electric and communication feeder lines, or to utility transmission lines operating at 50,000 volts or more.***

### ***Section 4.320. Requirements.***

***(.01) The developer or subdivider shall be responsible for and make all necessary arrangements with the serving utility to provide the underground services (including cost of rearranging any existing overhead facilities). All such underground facilities as described shall be constructed in compliance with the rules and regulations of the Public Utility Commission of the State of Oregon relating to the installation and safety of underground lines, plant, system, equipment and apparatus.***

***(.02) The location of the buried facilities shall conform to standards supplied to the subdivider by the City. The City also reserves the right to approve location of all surface-mounted transformers.***

***(.03) Interior easements (back lot lines) will only be used for storm or sanitary sewers, and front easements will be used for other utilities unless different locations are approved by the City Engineer. Easements satisfactory to the serving utilities shall be provided by the developer and shall be set forth on the plat.***

## **RESPONSE**

There are no existing overhead facilities that require undergrounding, as part of this development.

All new utility lines, including water, sewer, storm, power, communication, street lighting, gas, cable television services and related facilities, are designed to be placed underground. The construction of these underground utilities is designed consistent with the City's Public Works Standards and the rules and regulations of the Public Utility Commission of the State of Oregon. The construction plans will also be designed to meet applicable requirements for erosion control and other environmental protection.

Appropriate interior easements have already been or will be provided, as specified by the City engineer, needed for storm or sanitary sewers.

Therefore the proposed development complies with the provisions of this section.

## **CONCLUSION – ZONING and STAGE II DEVELOPMENT PERMIT**

Based on the Plans submitted and the findings provided herein the proposed development plan for the Data Center has been demonstrated to comply with all applicable PDI and Planned Development code standards.

### **IV. SITE DESIGN REVIEW**

#### ***1.400. Purposed.***

*(.01) Excessive uniformity, inappropriate or poor design of the exterior appearance of structures and signs and the lack of proper attention to site development and landscaping in the business, commercial, industrial and certain residential areas of the City hinders the harmonious development of the City, impairs the desirability of residence, investment or occupation in the City, limits the opportunity to attain the optimum use in value and improvements, adversely affects the stability and value of property, produces degeneration of property in such areas and with attendant deterioration of conditions affecting the peace, health and welfare, and destroys a proper relationship between the taxable value of property and the cost of municipal services thereof.*

*(.02) The City Council declares that the purposes and objectives of site and development requirements and the site design review procedures are to:*

- A. Assure that Site Development Plans are designed in a manner that insures proper functioning of the site and maintains a high quality visual environment;*
- B. Encourages originality, flexibility and innovation in site planning and development, including the architecture, landscaping and graphic design of said development;*
- C. Discourages monotonous, drab, unsightly, dreary and inharmonious developments;*
- D. Conserve the City's natural beauty and visual character and charm by assuring that structures, signs and other improvements are properly related to their sites, and to surrounding sites and structures, with due regard to the aesthetic qualities of the natural terrain and landscaping, and that proper attention is given to exterior appearances of structures, signs and other improvements;*
- E. Protect and enhance the City's appeal and thus support and stimulate business and industry and promote the desirability of investment and occupancy in business, commercial and industrial purposes;*
- F. Stabilize and improve property values and prevent blighted areas and, thus increase tax revenues;*
- G. Insure that adequate public facilities are available to serve development as it occurs and that proper attention is given to site planning and development so as to not adversely impact the orderly, efficient and economic provisions of public facilities and services;*
- H. Achieve the beneficial influence of pleasant environments for living and working on behavioral patterns and, thus decrease the cost of government services and reduce opportunities for crime through careful consideration of physical design and site layout under defensible space guidelines that clearly define all areas as either public, semi-public, or private, provide clear identity of structures and opportunities for easy surveillance of the site that maximize resident control of behavior – particularly crime;*

- I. Foster civic pride and community spirit so as to improve the quality and quantity of citizen participation in local government and in community growth, change and improvements;*
- J. Sustain the comfort, health, tranquility and contentment of residents and attract new residents by reason of the City's favorable environment and, thus, to promote and protect the peace, health and welfare of the City.*

## **RESPONSE**

In designing the Center, the goal was to identify with the general environment of commercial developments along Wilsonville Road and Argyle Square while also adding unique personality to the development and proper identity for the planned tenants. Being at a prominent intersection, it was decided to create a planter at the north corner of the property to acknowledge this gateway into one of the City's major employment centers and to add a sense of place within the community.

Rather than constructing one larger 4 tenant building, two buildings of smaller scale are proposed more in keeping with existing retail buildings. The anchor tenant will be in one building and up to 3 tenants will be in the slightly larger separate structure. These two smaller wood-framed buildings use traditional exterior materials typically found on smaller scale structures, further reinforcing their location in the Wilsonville's small town setting.

Both buildings were designed to complement each other rather than copy each other. Additionally, special attention was given to the retail building's massing relative to its orientation to the intersection and adjacent interstate.

The same color of brick is used as the primary accent material in both buildings. These two buildings use the same siding, similar steel canopies and awnings creating a common vocabulary, but have some significant variations in proportion and form, so they are not perceived as identical.

Care was taken to maintain brand identity for the anchor tenant, Carl's Jr. while also integrating into the business park environment. The prototype Carl's Jr. building is quite contemporary, so a full exterior redesign was necessary for this Wilsonville location.

The prototypical towers denoting building entries with signage located at the top were retained, but are constructed of brick giving them a more traditional appearance. Accent and siding colors are from the prototype Carl's Jr. design, but the awnings along with the more traditional horizontal & vertical siding are unique design elements to this location. Steel canopies using 12" C-Channels are used at these entry points and over the Drive-Thru Window where the awnings are of a contemporary style giving contrast to the traditional siding and creating welcoming highlights appropriate for a quick-serve restaurant.

The multi-tenant retail building features a large bricked gable element in the middle third of the main facades. These gables express themselves both from the parking lot as well as on the façade facing the intersection. They rise above the side parapet walls to a traditional pitched roof continuous over the center of the building.

A large gable roof addressed both internal and external visual orientation with emphasis towards the intersection giving purpose to a "Boones Ferry Pointe" gateway sign. The gable shape of the roof provides more interest than a flat roof when viewed from the higher elevation of the adjacent arterial street.

The gabled roof will have dark bronze standing seam metal roof appropriate for a commercial building. This dominant design element creates interest when viewed upon from the higher interstate highway instead of a flat roof, and also differentiates itself from the Carl's Jr. building. Windows are placed on all four sides of this building to make it attractive from all directions. Similar to the Carl's Jr. building, a brick wainscot wraps around the building. The same exterior siding paint colors will be used on both buildings, but the retail structure will not have the red accents of Carl's Jr.

On both buildings, the vertical siding is a board and batten design and is used toward the top of the facades and band on the building. This is accomplished with durable composite concrete panels and then using 1x2 battens of similar material spaced on 1 foot centers, then painted the same color. The board and batten on the retail building also serves as a band for tenant signage.

Also on both buildings, the horizontal siding with an 6" exposure is a newer lap composite concrete board product that is twice as thick as the common concrete horizontal lap siding and that provides deep shadow lines to achieve a higher level of aesthetics and reinforces the traditional style.

Steel horizontal canopies denote guest entry points for all Carl's Jr. doors, and on one of the three retail tenant entry points. These steel canopies are detailed similar to one another on both buildings, but the scale difference creates distinction between the two buildings.

Also, both buildings have sloped awning elements over windows and some entry doors on the retail building. The fabric awnings on Carl's Jr. are stepper in form and have more of a contemporary look while the awnings on the retail building are flatter in slope and use semi-contemporary steel knee braces for support.

In designing each structure, care was taken to compose each façade in pleasing color and proportion. The brick elements of the Carl's Junior create proportional balance with the horizontal and vertical siding elements that are of complimentary colors. The tallest portions of the brick peak at the entry portal areas that are suitable for identification of signage and/or pedestrian access. We have been careful not to use too much brick so as to make the material unremarkable. The layers of siding changing from horizontal to vertical elements adjacent to the brick towers compliments brick pattern and accentuates the brick material used. The caps at the two pedestrian entry towers crown elements further drawing attention to their purpose and material usage.

The colors have also been carefully chosen to achieve the same effect in reference to the brick's dark red color. The various beige colors of the siding stand in contrast to the brick in hue color and light-dark value and yet complement each other in their muted chromatic brightness. The red horizontal trim band and awning elements are designed to

complement the brick in an inverted fashion from the siding. The contrast element in this case is the chromatic brightness of the color while the hue color is similar to the red brick and the light-dark value is complementary to the siding's light-dark value. A second type of canopy-awning is also used at access areas that are dark and much muted in color to further balance the composition.

The retail building elements take a similar vocabulary using similar colors with some significant variations. The two new buildings on the site will appear to be related, but not identical. Rather than using a parapet style tower feature as was done at the Carl's Jr. building, the retail building will use a gabled tower feature. Higher brick placement is prominent at the tower elevations as well to balance out the two primary facades: the entry and the north façade facing the intersection with the Boones Ferry Point monument sign. At both buildings, the horizontal siding is the Artisan product <http://www.artisanluxury.com/> that provides deep shadow lines to achieve a higher level of aesthetics.

Two types of canopies are also used in the retail building, but again with variation. The horizontal canopy is similar to the Carl's Jr. building but the more prominent sloping canopies are designed to stand in contrast. Colors of siding and brick remain in the same family but are slightly different in appearance. The color philosophy for the retail building follows a similar approach of contrasting colors, textures and patterns while yet incorporating horizontal and vertical elements in each area.

To summarize, using different mass & form for each building while using the same exterior materials create an overall identity to the center that creates a harmonious statement to this development that integrates gracefully into the fabric of the commercial Wilsonville town center district.

Pedestrian circulation from parking areas to the front doors of the two buildings has been provided. And, as noted, there are public sidewalks almost completely surrounding this property.



The development plans insure that adequate public facilities are available to serve development. At the same time proper attention is given to site planning and development so as to not adversely impact the orderly, efficient and economic provisions of public facilities and services. Appropriate water quality and quantity control provisions have been designed into the storm drainage system consistent with the discharge carrying capacity of the storm system in 95<sup>th</sup> Avenue.

The proposed building and site improvements are designed to compliment and enhance the existing business park environment, and therefore will maintain consistency with the Design Review Criteria and Objectives listed above. These improvements are designed to ensure the site functions safely, properly and efficiently.

**4.421. Criteria and Application of Design Standards.**

*(.01) The following standards shall be utilized by the Board in reviewing the plans, drawings, sketches and other documents required for Site Design Review. These standards are intended to provide a frame of reference for the applicant in the development of site and building plans as well as a method of review for the Board. These standards shall not be regarded as inflexible requirements. They are not intended to discourage creativity, invention and innovation. The specifications of one or more particular architectural style is not included in these standards. (Even in the Boones Ferry Overlay Zone, a range of architectural styles will be encouraged.)*

- A. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.*
- B. Relation of Proposed Buildings to Environment. Proposed structures shall be located and designed to assure harmony with the natural environment, including protection of steep slopes, vegetation and other naturally sensitive areas for wildlife habitat and shall provide buffering from less intensive uses in accordance with Sections 4.139 and 4.139.5. The achievement of such relationship may include enclosure of space in conjunction with other existing buildings or other proposed buildings and the creation of focal points with respect to avenues of approach, street access or relationships to natural features such as vegetation or topography.*
- C. Drives, Parking, and Circulation. With respect to vehicular and pedestrian circulation, including walkways, interior drives and parking, special attention shall be given to location and number of access points, general interior circulation, separation of pedestrian and vehicular traffic, and arrangement of parking areas that are safe and convenient and, insofar as practicable, do not detract from the design of proposed buildings and structures and the neighboring properties.*
- D. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties of (et sic) the public storm drainage system.*
- E. Utility Service. Any utility installations above ground shall be located so as to have a harmonious relation to neighboring properties and site. The proposed method of sanitary and storm sewage disposal from all buildings shall be indicated.*
- F. Advertising Features. In addition to the requirements of the City's sign regulations, the following criteria should be included: the size, location, design, color, texture, lighting and materials of all exterior signs and outdoor advertising structures or features shall not detract from the design of proposed buildings and structures and the surrounding properties.*

**G. *Special Features. Exposed storage area, exposed mechanical installations, surface areas, truck loading areas, utility buildings and structures and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall be required to prevent their being incongruous with the existing or contemplated environment and its surrounding properties. Standards for screening and buffering are contained in Section 4.176.***

**(.02) *The standards of review outlined in Sections (a) through (g) shall also apply to all accessory buildings, structures, exterior signs and other site features, however related to the major buildings or structures.***

**(.03) *The Board shall also be guided by the purposes of Section 4.400 and such objectives shall serve as additional criteria and standards.***

## **RESPONSE**

The area of the proposed development has already been substantially modified from its natural state by prior rough grading and right-of-way acquisition and improvements. Within the development site, there are no naturally sensitive areas or wildlife habitats that require protection or buffering (SROZ). But, the proposed development minimizes additional changes, insofar as practicable, while adding appropriate aesthetic enhancements with building and landscape architecture.

The property is located at a prominent arterial intersection adjacent to the Elligsen Road Freeway Interchange and therefore the site is highly visible from the adjacent roadways.

This site is surrounded by a combination of commercial and industrial uses and shares access with the adjacent Chevron Station and Holiday Inn. There are no nearby residential uses.

As previously discussed, the existing shared access will be modified consistent with an executed Development Agreement.

The driveway circulation and parking areas will be enhanced by paving and landscape islands to improve traffic flow channelization between the site and the Chevron development. Site improvements will also enhance pedestrian circulation, together with the creation of safe and convenient parking areas provided in close relationship to the building. Appropriate ADA access is also provided consistent with code requirements.

Special attention has been given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. In that regard on-site water quality treatment and detention at the southwest corner of the site is provided.

All utilities installation, except for above ground transformers and lighting, will be placed underground as required by code. Preliminary utility plans show the method of sanitary and storm sewage disposal.

4.420 *Location, Design and Access Standards for Mixed Waste and Recycling Areas.*

*(.01) The following locations, design and access standards for mixed solid waste and recycling storage areas shall be applicable to the requirements of Section 4.179 of the Wilsonville City Code.*

*(.02) Location Standards.*

- A. To encourage its use, the storage for source separated recyclables shall be co-located with the storage area for residual mixed solid waste.*
- B. Indoor and outdoor storage areas shall comply with Uniform Building and Fire Code requirements.*
- C. Storage area space requirements can be satisfied with a single location or multiple locations and can combine both interior and exterior locations.*
- D. Exterior storage areas can be located within interior side yards or rear yard areas. Minimum setback shall be three (3) feet. Exterior storage areas shall not be located within a required front yard setback, including double frontage lots.*
- E. Exterior storage areas shall be located in central and visible locations on a site to enhance security for users.*
- F. Exterior storage areas can be located in a parking area if the proposed use provides at least a minimum number of parking spaces required for the use after deducting the area used for storage. Storage areas shall be appropriately screened according to the provisions of Section 4.430(.03), below.*

*(.03) Access Standards.*

- A. Access to storage areas can be limited for security reasons. However, the storage areas shall be accessible to users at convenient times of the day and to collect service (et sic) personnel on the day and approximate time they are scheduled to provide collection service.*
- B. Storage areas shall be designed to be easily accessible to collection trucks and equipment, considering paving, grad and vehicle access. A minimum of ten (10) feet horizontal clearance and eight feet of vertical clearance if the storage area is covered.*
- C. Storage areas shall be accessible to collection vehicles without requiring backing out or a driveway onto a public street. If only a single access point is available to the storage area, adequate turning radius shall be provided to allow collection vehicles to safely exit the site in a forward motion.*
- D. Pursuant to Subsection 8.210 (9) Wilsonville Code and Subsections 4.430 (.04) B. information on required covers for mixed solid waste and recycling storage areas. The City's stormwater regulations require the waste and recycling enclosures be covered. Wilsonville Code Subsection 8.210 (9) reads, "Outside storage areas for grease, oil, waste products, recycling, garbage, and other sources of contaminants shall be a covered enclosure adequately sized to allow all containers to be accessible. No drainage is allowed to enter the storm sewer system." Subsection 4.430 (.04) B. requires vertical clearance to be at least eight feet (8').*

## RESPONSE

Two solid waste and recycling storage areas have been carefully designed to encourage proper use. One enclosure, located at the southeast corner of the Carl's Jr. Building is for that use only. The other enclosure is a double unit, one side for the New Retail Building, and the other side is a replacement (relocated) for the Chevron and C-Store.

The design has been coordinated with Allied Waste Services to ensure convenient access for their collection trucks. It is noted that Allied has been serving the Chevron site for many years and has had problems accessing the structure without conflicting with pump island traffic. Therefore the existing facility will be removed and replaced, thereby improving access.

The design of the two enclosures has been modified to provide the required roof covers to protect the storm water system from contaminants, per Sections 4.430(.04) and 8.210(9).

The applicant has obtained a letter from Allied Waste approving the proposed trash enclosure designs.

## CONCLUSION – Site and Architectural Design Review

This report has provided documentation of compliance with the applicable design review standards. Therefore this project should be approved as requested.

## V. TREE PRESERVATION AND REMOVAL

### *Section 4.600. Purpose and Declaration*

*(.01) Rapid growth, the spread of development, need for water and increasing demands upon natural resources have the effect of encroaching upon, despoiling, or eliminating many of the trees, other forms of vegetation, and natural resources and processes associated therewith which, if preserved and maintained in an undisturbed and natural condition, constitute important physical, aesthetic, recreational and economic assets to existing and future residents of the City of Wilsonville.*

## RESPONSE

There are no significant trees and no designated protected Resource on the site. Therefore this Section is not applicable.

## **VI. FINAL CONCLUSION**

Based on the findings presented herein, and supported by the accompanying documentation and plans, the proposed development has been demonstrated to comply with all applicable Comprehensive Plan and Development Code provisions and standards.

Compliance has been demonstrated for updated Stage I Master Plan, Stage II Final Development Plans, Design Review, Outdoor Lighting, and Tree Protection and Mitigation.

Therefore the applicant respectfully requests approval as requested.

Submittal date: July 18, 1997  
120 day review limit: December 15, 1997 (30 days was added to the 120 day limit because the application is incomplete.)

**RECOMMENDED FINDINGS**

**STAGE I PRELIMINARY PLAN,  
STAGE II FINAL PLAN  
and  
SITE AND DESIGN**

**Site Identification:**

1. The project site is located on the southwest corner of SW Boones Ferry Road and Commerce Circle North being more particularly described as Tax Lots 300 and 400 in Section 2DB, T3S, R1W, Washington County, Wilsonville, Oregon.

**Vicinity Information:**

2. North: Riverwood Industrial Campus  
East: Stafford Interchange  
West: AGC office building  
South: Holiday Inn

**Description of Project:**

3. Review a two phase, Stage I Preliminary Plan, Phase I, Stage II Final Plan, Site and Design Plans, and review proposed signs for development of a Chevron service station/convenience market. The project site is located at the southwest corner of SW Boones Ferry Road and Commerce Circle North.

**Project Data:**

4. Project Data:

Phase I site area: 43,560 sf @ 1 acre  
Gross site area: The original site was surveyed at 2.486 acres or 108,277 square feet. The City of Wilsonville is requesting the dedication of an additional 12' travel lane along 95th Avenue reducing the site area by 6,480 square feet to 101,797 sf.

**Building Area**

Convenience Store: 2,176 sf  
Service Pumps Canopy: 6,584 sf  
Total..... 8,760 sf @ 20%

Parking and Drives: 24,560 sf @ 56%  
Landscaping: 16,914 sf @ 24%

4

970328

**Stage II Final Plan  
Review Standards**

5. The operable review criteria for Stage II site development including a preliminary subdivision plat is found in Subsection 4.139(4): "*A Planned development permit may be granted by the Planning Commission only if it is found that the development conforms to all the following criteria, as well as to the Planned Development Regulations in Sections 4.130 to 4.140.*"
- a. The location, design, size and uses, both separately and as a whole, are consistent with the Comprehensive Plan, and with any other applicable plan, development map or Ordinance adopted by the City Council.*
- b. That the location, design, size, and uses are such that traffic generated by the development at the most probable used intersection(s) can be accommodated safely and without congestion in excess of level service D defined in the highway capacity manual published by the National Highway Research Board on existing or immediately planned arterial or collector streets and will, in the case of commercial or industrial developments, avoid traversing local streets.*
- c. That the location, design, size and uses are such that the residents or establishments to be accommodated will be adequately served by existing or immediately planned facilities and services.*

**Response findings to Subsection 4.139(4)(a) Land Use:**

**Comprehensive Plan and Zone Designations:**

6. The project site is designated "Commercial". The subject property is not identified in an area of special concern in the Comprehensive Plan. The property is zoned Planned Development Commercial (PDC).

Furthermore, Section 4.160 Criteria 1 to 10 sets forth design standards for development of services stations:

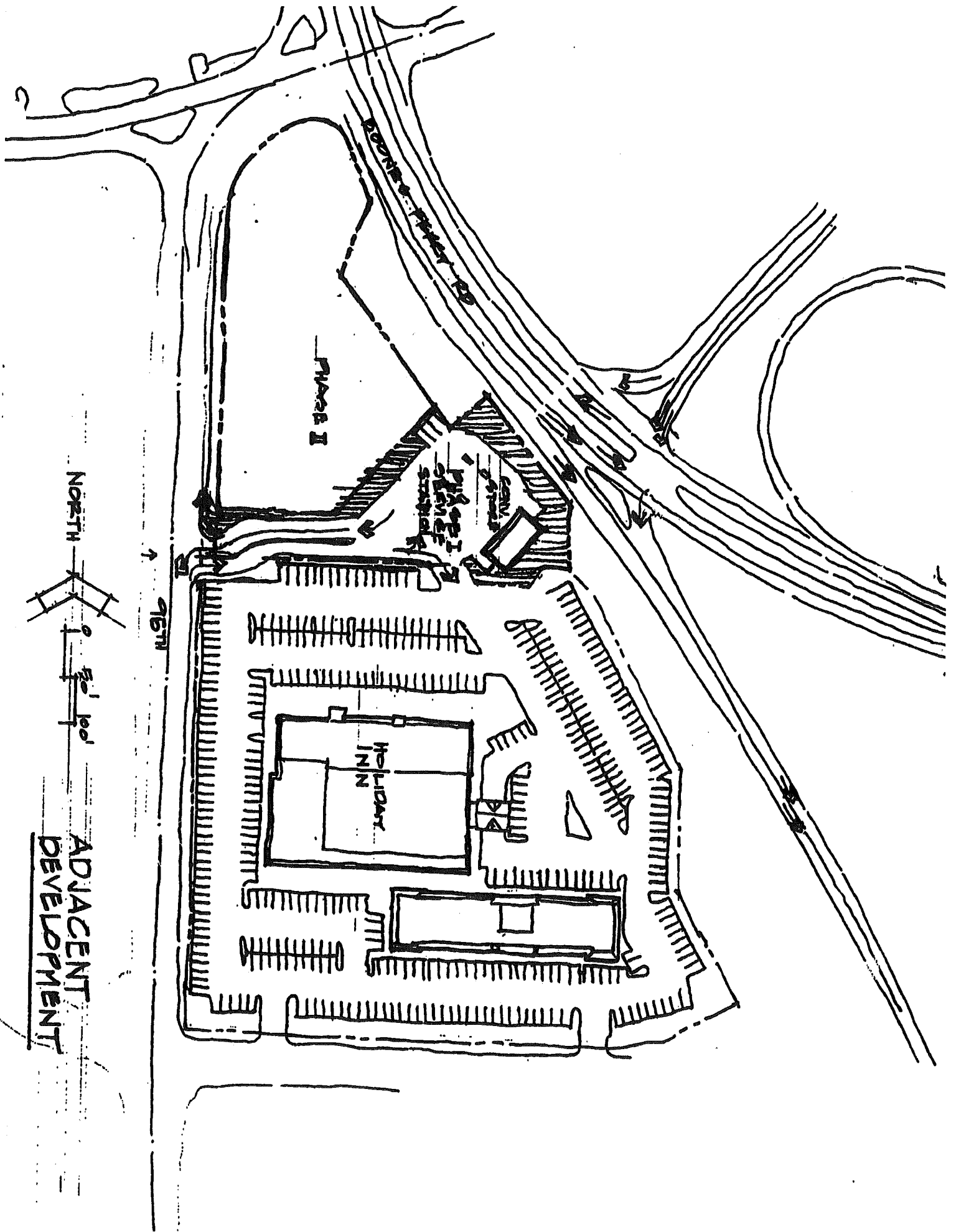
**Criterion (1):**

*The Planning Commission shall disapprove all applications for new stations unless a need shall be established and the design, location and use is compatible with and does not adversely impact the surrounding uses.*

**Response finding:**

7. The applicant has provided new evidence which satisfies Subsection 4.160(1). See Exhibit J for market study. There is currently in the Stafford Interchange vicinity existing Chevron and BP stations. A "Tarr" card lock station is under construction on Ridder Road which is one mile west of the project site.





Phase I

Plymouth  
office  
building

Garage

Holiday  
INN

NORTH

96th

Riverside Drive

ADJACENT  
DEVELOPMENT

0 50' 100'



**Exhibit 'A'**  
**WILSONVILLE PLANNING DIVISION**  
**STAFF REPORT**  
**DEVELOPMENT REVIEW BOARD PANEL 'A'**  
**QUASI-JUDICIAL PUBLIC HEARING**

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**Public Hearing Dates:** August 14, 2006, November 13, 2006  
**Date of Reports:** August 4, 2006, amended November 3, 2006  
**Application Numbers:** Request A: DB06-0041 (Stage II Final Plan)  
Request B: DB06-0043 (Site Design Review)  
Request C: DB06-0057 (Waiver to building height)  
Request D: DB06-0042 (Master Sign Plan)  
**Property Owner:** South Sea, LLC  
**Applicant's Representative:** John D. Annand II  
**Professional Design Team:** John D Annand II, Architect

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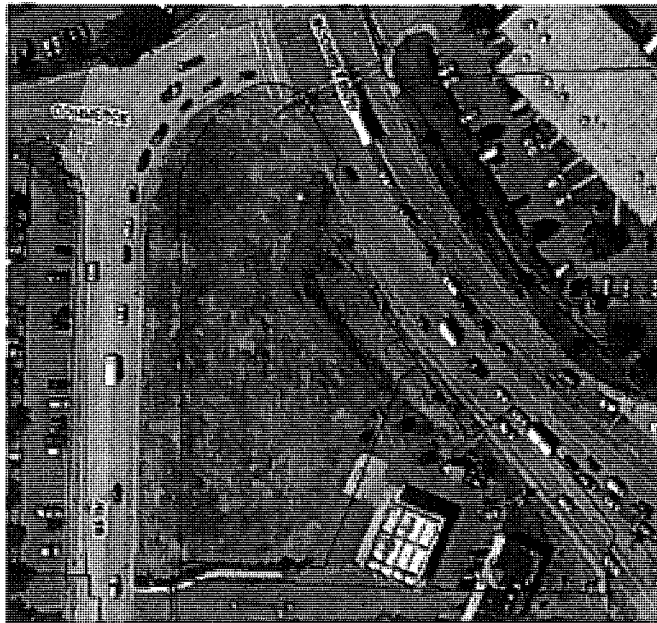
**Request:** John D. Annand II, acting as applicant for South Sea, LLC, proposes the development of a three (3) story, 43,500 square foot office building with under-structure parking on Tax Lot 302.

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**Staff Recommendation:** Deny Requests A, B, C and D without prejudice per Sections 4.024.01 and .02(A through E).

**Project Location:** The subject property is located southwest corner of 95<sup>th</sup> Avenue and SW Boones Ferry Road, more specifically described as Tax Lot 302, in Section 2DB; Township 3S, Range 1W; Washington County; Wilsonville, Oregon.

**Vicinity Map:** The project site is the undeveloped parcel shown below:



## FINDINGS OF FACT

**1. Adjacent land uses:**

Compass Direction	Zone:	Existing Use:
North:	PDI	Industrial/office condominium complex
East:	PDC	I-5, Stafford Interchange
South:	PDC	La Point-Chevron service station and Holliday Inn
West:	PDC	AGC Building

- 2. Comprehensive Plan and Zone Maps:** The site has a Comprehensive Plan designation of Commercial and is zoned Planned Development Commercial (PDC).
- 3. Previous Planning Approvals:** The subject property is part of the Edwards Business Center Industrial Park Master Plan, which envisioned a variety of industrial and commercial uses. The Master Plan designated the subject property for commercial development and it did not specify the type of commercial use. This master plan serves as the Stage I master plan per Subsection 4.104(.07) for the proposed project. The proposed use is consistent with the Edwards Business Center Industrial Park Master Plan.
- 4. Natural Characteristics:** The subject site has fairly level terrain and it is treeless.
- 5. Streets:** The subject property is fronted on the north and west by 95<sup>th</sup> Avenue and on the east by SW Boones Ferry Road. The subject property will share the existing private drive that provides access to the adjacent Chevron service station and Holiday Inn. No other access can be provided to the subject site.
- 6. Review Procedures:** The required public notices have been sent and all proper notification procedures have been satisfied.
- 7. Previous land use approvals:**

Edwards Business Center Industrial Park Plat – Stage I  
97DB28 Stage II, Site Design Review



01738562201200681010210219

I, Richard Hobernicht, Director of Assessment and Taxation and Ex-Officio County Clerk for Washington County, Oregon, do hereby certify that the within instrument of writing was received and recorded in the book of records of said county.

Richard Hobernicht, Director of Assessment and Taxation, Ex-Officio County Clerk



*Eight*

After recording, return to:  
City Recorder  
City of Wilsonville  
29799 SW Town Center Loop E.  
Wilsonville OR 97070

**DEVELOPMENT AGREEMENT**

This Development Agreement is entered into by and between the City of Wilsonville ("City"), Wilsonville Devco LLC, an Oregon limited liability company ("Developer"), and two neighboring impacted businesses, LaPoint Business Group, LLC, an Oregon limited liability company, operating a Chevron gasoline station and a convenience store ("LaPoint"), and WHI Hotel, LLC, an Oregon limited liability company operating as a Holiday Inn hotel ("Holiday Inn"). The effective date of this Development Agreement is July 3<sup>rd</sup>, 2012 ("Effective Date"). All of the foregoing parties are referred to collectively as the "Parties" and in the singular as a "Party." "Shared Driveway Parties" are all of the Parties listed above, excluding the City.

**RECITALS**

- A. Developer proposes to construct a Carl's Jr. fast food restaurant and other yet to be determined retail ("Development") on its property located adjacent to the Holiday Inn and LaPoint businesses ("Developer Property"). The locations of the LaPoint property, Holiday Inn property, and Developer Property are legally described and depicted on the map attached hereto as Exhibit A, and are collectively referred to as the "Neighboring Properties."
- B. The proposed Development is located adjacent to 95<sup>th</sup> Avenue in Wilsonville, Oregon and would be accessed via 95<sup>th</sup> Avenue. The Neighboring Properties, including the Developer Property, share a common driveway ("Shared Driveway") that allows for access to 95<sup>th</sup> Avenue.
- C. The City intends to make certain improvements to 95<sup>th</sup> Avenue this summer ("Roadway Improvements"), whether or not this Development Agreement is entered into. Based on a traffic study recently conducted for the City by DKS Associates in conjunction with the proposed Development, dated May 2012, DKS, Developer, and the City have discussed a driveway configuration requiring certain enhancements and modifications be made to the proposed Roadway Improvements in order to better serve the proposed Development ("Enhanced Roadway Improvements"). The proposed Enhanced Roadway Improvements will cost the City approximately Forty Thousand Dollars to Sixty Thousand Dollars (\$40,000-\$60,000) more than the current scope of work for the Roadway Improvements.

- D. The City is willing to make the Enhanced Roadway Improvements in consideration for Developer making certain on-site improvements to the Neighboring Properties in order to help relieve congestion on the Shared Driveway and to make the traffic flow more smoothly to and from 95<sup>th</sup> Avenue (“Developer Improvements”). In consideration of the City making the Enhanced Roadway Improvements, Developer has agreed to make the Developer Improvements, more particularly described in **Section III** below.
- E. Holiday Inn and LaPoint support the Enhanced Roadway Improvements and Developer Improvements and agree to fully cooperate with the City and Developer in the construction of the Enhanced Roadway Improvements and Developer Improvements.
- F. Developer will be solely responsible for all costs associated with the Developer Improvements.

## **AGREEMENT**

In consideration of the foregoing Recitals, and incorporating all of the above Recitals by reference in this Development Agreement as if fully set forth herein, and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, all of the above-named Parties agree as follows:

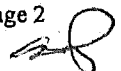
### **I. NEW DEVELOPMENT**

Developer intends to construct a retail development on Developer’s Property, which may contain a Carl’s Jr. fast food restaurant and other retail stores. Nothing in this Development Agreement ensures that Developer’s proposed development will be approved by the City. Developer will be required to go through all application and permitting processes required by the City for commercial development and to pay all fees required by the City to be paid for such commercial development in order to obtain approval to move forward with Developer’s proposed Development (“Development Approval”). Nothing contained herein is a guarantee that Development Approval will be granted by the City.

### **II. CITY’S ENHANCED ROADWAY IMPROVEMENTS (City Obligations)**

In consideration for Developer making the Developer Improvements, described in **Section III**, the City agrees to make the Enhanced Roadway Improvements generally depicted on **Exhibit B** and generally described as follows:

- Install a concrete sidewalk along the length of the Shared Driveway adjacent to 95<sup>th</sup> Avenue in the right-of-way.
- Bring storm manhole in right-of-way to grade.
- Install curb along northeast return of the Shared Driveway and 95<sup>th</sup> Avenue.
- Install concrete commercial Shared Driveway for access to 95<sup>th</sup> Avenue that lies within right-of-way north of the existing driveway back of curb;



- Transition existing sidewalk north of and adjacent to access drive to meet grades of new concrete driveway.
- Paint and maintain an approximately 50 foot "DO NOT BLOCK DRIVEWAY" signage on 95<sup>th</sup> Avenue at the inbound and outbound lanes, using eight inch or larger lettering.
- Allow one (1) inbound and two (2) outbound lanes of traffic into the Shared Driveway area at 95<sup>th</sup> Avenue within the right-of-way. (First 20 feet is one-way inbound and then converts to two lanes within the Shared Driveway.)
- Provide required legal notice to the Parties with respect to any changes being made to their access to 95<sup>th</sup> Avenue and an opportunity for the Parties to provide comments, understanding the Oregon Department of Transportation has the final authority with respect to any roadway modifications.
- City to stripe egress within the right-of-way.

### III. DEVELOPER IMPROVEMENTS (Developer Obligations)

In consideration of the City's installation of the above-described Enhanced Roadway Improvements, Developer agrees to make the Developer Improvements generally depicted on **Exhibit B** and generally described as follows:

- Remove and install curb, gutter, and storm facilities, as necessary, to close the existing Holiday Inn egress to the Shared Driveway and create a new egress. The new egress shall include a driveway that is 16.6 feet wide and Developer shall place "Do Not Block" signage across twelve feet of space in front of the driveway so that cars in the stacking lane do not block Holiday Inn's egress driveway, as depicted on **Exhibit B**.
- Widen the existing Holiday Inn ingress from the Shared Driveway by approximately 5 feet by removing and installing the curb surrounding the Holiday Inn sign and replacing with crushed rock base and asphalt drive.
- Remove a parking stall island and existing tree on Holiday Inn property and replace with asphalt parking with section matching existing parking lot section.
- Add a parking stall island on Holiday Inn property.
- Narrow parking stall island on Holiday Inn property by removing and installing curb and asphalt.
- Remove the median island in the Shared Driveway and install asphalt.
- Install an entry landscape island and a pedestrian refuge island in the middle of the relocated portion of the Shared Driveway.
- Widen the Shared Driveway to four (4) lanes.
- Stripe the Shared Driveway to four (4) lanes, excepting the Shared Driveway entrance (right-of-way), which will be striped to three (3) lanes to discourage travel.
- Remove curb along LaPoint's western property line and install asphalt, as depicted on **Exhibit B**.
- Install stop bars and signs at Developer Property egress to the Shared Driveway and at Holiday Inn egress to the Shared Driveway, as depicted on **Exhibit B**.
- Provide construction easement to the City in order to allow the City to install NE curb return of Shared Driveway, to 95<sup>th</sup>, and for sidewalk transition and driveway construction.



- Adjust manholes and area inlets in the Shared Driveway to grade.
- All construction within the Shared Driveway or on Holiday Inn property shall be of the same standard as existing construction.
- Relocate Chevron sign and security cameras to either the Landscape Entry Island, centered North/South but as close to the right-of-way as LaPoint wishes to place the signage, as long as it does not overhang the right-of-way, including the sidewalk or, in the alternative, to be located on the southwest corner of Developer's Property next to or within the detention pond location to be agreed upon between Developer and LaPoint and as close to the right-of-way as LaPoint wishes to place the signage, as long as it does not overhang the right-of-way, including the sidewalk. If sign is relocated to Developer's Property, Developer shall provide LaPoint with a sign, access and maintenance easement. LaPoint will seek approval from Chevron to enter into a shared signage agreement with Developer for Developer's proposed fast food sign.
- If a shared signage agreement is entered into between Developer and LaPoint/Chevron, each party will be responsible for the maintenance of their own signage and will share in the maintenance cost or replacement of the pole equally, if any.
- Agreements between LaPoint and Developer, to be agreed upon in terms of markings and placement in order to dedicate the right-hand ingress lane for use as a stacking area for LaPoint gasoline station customers. LaPoint reserves the right to install reflective buttons or flexisticks to further identify the stacking area if gasoline station traffic later warrants, at LaPoint's sole cost and expense.
- Provide Holiday Inn a non-exclusive easement for egress over Developer's property located within the Shared Driveway.
- Preparation of **Exhibit B**, to this Development Agreement, as approved by all Parties and as attached hereto.
- Share one third of the cost of annual maintenance of all striping and signage painted within the boundary of the Shared Driveway.
- Provide an easement for and allow LaPoint to construct a trash enclosure and recycling area on Developer's property, adjacent to the proposed trash enclosure on Developer's eastern property line. LaPoint's trash enclosure must be constructed with like kind materials, craftsmanship and resemble the same aesthetic look as Developer's trash enclosure.
- Developer and LaPoint have agreed to amend the existing easement agreement by vacating a portion of Developer's easement over LaPoint's property in exchange for LaPoint granting Developer a reciprocal cross-easement on Developer's eastern property line and LaPoint's western property line for curb cuts between the properties and LaPoint's access to trash enclosure on Developer's Property, as illustrated on **Exhibit C** attached hereto. The amended easement agreement will be part of a separate agreement between Developer and LaPoint.
- Developer will include LaPoint's trash enclosure as part of Developer's plans for the purpose of DRB review and permit approval. The cost of the trash enclosure permit and construction will be at LaPoint's sole cost and expense.

The foregoing Developer Improvements shall be made at Developer's sole expense and are agreed to be a reasonable and agreed upon exchange for the Enhanced Roadway Improvements that the City has agreed to make in order to improve traffic flow to and from the Neighboring

Properties onto 95<sup>th</sup> Avenue. The foregoing Developer Improvements must be completed, inspected by the City, and deemed complete by the City before the City will issue any temporary occupancy permits to Developer, assuming Development Approval. In addition, regardless of whether Development Approval by the City is granted or denied, Developer will be legally obligated to make the Developer Improvements set forth herein, unless the City agrees otherwise, in its sole discretion to release Developer from any or all of the foregoing obligations, because the City will be constructing the Enhanced Roadway Improvements in consideration of and in reliance upon this Development Agreement, including Developer's agreement to make the Developer Improvements in exchange for the Enhanced Roadway Improvements. Developer's obligations hereunder will therefore run with the land and this Agreement will be recorded against all of the Neighboring Properties.

#### **IV. LAPOINT AGREEMENT (LaPoint Obligations)**

In consideration for the City's installation of the above-described Enhanced Roadway Improvements and Developer's construction of the Developer Improvements, both as generally described herein and generally depicted on **Exhibit B**, LaPoint agrees to allow the following with respect to its property, as also generally depicted on **Exhibit B**:

- Allow the current Chevron sign and light pole with security cameras to be relocated as described in Section III above.
- Cooperate with Developer in making all of the required improvements to the Shared Driveway, at Developer's cost, as provided for in Section III and as depicted on **Exhibit B**.
- Allow the Holiday Inn egress driveway portion of its easement to be relocated to the location on LaPoint property, as described above and as depicted on **Exhibit B**.
- Allow pavement signage to be installed by Developer across a twelve-foot area directly in front of the new egress driveway reading DO NOT BLOCK.
- Provide construction easement to allow construction of private ingress and egress improvements, as depicted on **Exhibit B**.
- Share one third of the cost of annual maintenance of all striping and signage painted within the boundary of the Shared Driveway.
- Pay all of the costs associated with the installation and maintenance of any reflective buttons or flexisticks to identify the stacking area.

Except for installation and maintenance of reflective buttons or flexisticks, which shall be LaPoint's responsibility, all of the foregoing work described in this Section IV will be done by Developer, at Developer's cost. LaPoint and Developer will work cooperatively with respect to timing of the foregoing removals and installations.

## V. HOLIDAY INN AGREEMENT (Holiday Inn Obligations)

In consideration for the City's installation of the above-described Enhanced Roadway Improvements and Developer's construction of the Developer Improvements, both as generally described herein and generally depicted on **Exhibit B**, Holiday Inn agrees to allow the following with respect to its property, as also generally depicted on **Exhibit B**:

- Allow its current egress driveway to the Shared Driveway to be vacated and permanently closed and replaced by Developer, at Developer's cost and expense, with a new egress. The new egress shall include a driveway that is 16.6 feet wide, as depicted on **Exhibit B**.
- Relinquish three (3) parking spaces to accommodate the Developer Improvements, including relocation of Holiday Inn's easement to egress.
- Allow its existing ingress to be widened by approximately 5 feet in order to better accommodate trucks access.
- Restripe its parking stalls, as needed, at Holiday Inn's expense.
- Provide construction easement to allow construction of private ingress and egress improvements, as depicted on **Exhibit B**.
- Share one third of the cost of annual maintenance of all striping and signage painted within the boundary of the Shared Driveway.

Except for stall restriping (which shall be Holiday Inn's responsibility), all of the foregoing work described in this Section V will be done by Developer, at Developer's cost and expense. Holiday Inn and Developer will work cooperatively with respect to timing of the foregoing improvements.

## VI. Obligations of All Shared Driveway Parties

All Shared Driveway Parties will use good faith reasonable efforts not to unreasonably interfere with or impede Shared Driveway usage. Developer and Holiday Inn agree that the right ingress lane will be used primarily by LaPoint as a stacking lane for gasoline station customers. The left ingress lane shall be used primarily by customers of Developer's Property and LaPoint's C-Store customers, vendors, diesel pumps and fuel deliveries. Holiday Inn recognizes and agrees that the egress driveway across the LaPoint property may occasionally be temporarily blocked by fuel trucks, RV's, delivery trucks, or large trucks with trailers from time-to-time entering the site, however, such blockage will be short term as the vehicles maneuver into place as the egress driveway crosses over the stacking lane. An area twelve feet (12 ft) in width will be clearly marked with DO NOT BLOCK letters painted on the pavement. LaPoint will use reasonable good faith efforts to monitor public compliance with this signage and require customers to move out of the DO NOT BLOCK area but Holiday Inn recognizes and agrees that at times a customer may fail to abide by the signage and LaPoint shall not have liability for such blockage. Holiday Inn customers will not be allowed to cut into the stacking lane and if such customers wish to get gasoline, they will be required to circle around to the end of the stacking lane. Developer and LaPoint may make minor revisions to striping, signage and traffic flow within the Shared Driveway area as they agree as long as such minor revisions do not impact traffic coming to or from 95<sup>th</sup> Avenue, do not impede the Holiday Inn ingress or egress, are not in violation of Development Review Board conditions of approval, and are in compliance with City permit

requirements. Redirection of traffic that could create a negative impact on traffic flow to and from 95<sup>th</sup> Avenue must be approved, in writing by the City, in accordance with City permitting requirements. The Parties agree that except as modified hereby, all ingress, egress and common area easements and other agreements between some or all of the Parties with respect to or relating to use of the Shared Driveway and Neighboring Properties shall remain in place (collectively "Shared Driveway/Neighboring Property Agreements"). Wherever those Shared Driveway/Neighboring Property Agreements directly conflict with the provisions of this Development Agreement, this Development Agreement will control.

## **VI. MISCELLANEOUS PROVISIONS**

### **Section 6.1 – Further Assurances**

Each Party shall execute and deliver any and all additional drawings, specifications, documents, and other assurances, and shall do any and all acts and things reasonably necessary in connection with the performance of its obligations hereunder, in good faith to carry out the intent of the Parties hereto. Developer understands and agrees that no occupancy permit will be granted for the Development until the Developer Improvements have been completed and approved by the City as meeting the requirements set forth herein.

### **Section 6.2 – Modification or Amendment**

No amendment, change, or modification of this Development Agreement shall be valid unless in writing and signed by the Parties hereto.

### **Section 6.3 – Relationship**

Nothing herein shall be construed to create an agency relationship or a partnership or joint venture between the Parties.

### **Section 6.4 – Maintenance**

Nothing contained herein is intended to address anything concerning maintenance of the Shared Driveway. Maintenance is an issue to be negotiated between the owners of the Neighboring Properties who use the Shared Driveway.

### **Section 6.5 – Burden and Benefit**

The covenants and agreements contained herein shall be binding upon and inure to the benefit of the Parties and their successors and assigns.

### **Section 6.6 – No Continuing Waiver**

The waiver of any Party of any breach of this Development Agreement shall not operate or be construed to be a waiver of any subsequent breach.



**Section 6.7 – Applicable Law**

This Development Agreement shall be governed by and construed under the laws of the State of Oregon. Jurisdiction is in Clackamas County, Oregon.

**Section 6.8 – Legal Fees**

If any Party commences legal proceedings, including arbitration, mediation, or bankruptcy, for any relief against any other Party arising out of or related to this Development Agreement, or the breach thereof, the losing Party shall pay the prevailing Party's legal costs and expenses, including, but not limited to, arbitration costs, reasonable attorneys' fees, and expert witness fees, as determined by the court or the arbitrator at the trial level or on any appeal.

**Section 6.9 – Time of Essence**

Time is expressly declared to be of the essence of this Development Agreement.

**Section 6.10 – Notices**

All notices, demands, consents, approvals, and other communications which are required or desired to be given by any Party to each other hereunder shall be in writing and shall be faxed, hand delivered, or sent by overnight courier or United States Mail at its address set forth below, or at such other address as such Party shall have last designated by notice to the other. Notices, demands, consents, approvals, and other communications shall be deemed given when delivered, three (3) days after mailing by United States Mail, or upon receipt if sent by courier; provided, however, that if any such notice or other communication shall also be sent by telecopy or fax machine, such notice shall be deemed given at the time and on the date of machine transmittal.

To City:                   City of Wilsonville  
                                  Attn: City Engineer  
                                  29799 SW Town Center Loop East  
                                  Wilsonville OR 97070

To Developer:           Wilsonville Devco, LLC  
                                  Attn: Josh Veentjer, Managing Member  
                                  4188 SW Greenleaf Drive  
                                  Portland OR 97221

To LaPoint:             LaPoint Business Group, LLC  
                                  Attn: Garry L. LaPoint, Registered Agent  
                                  850 Lawson Ave  
                                  Woodburn OR 97071



To Holiday Inn:        Holiday Inn, Portland South Hotel & Convention Ctr.  
                                  Attn: General Manager  
                                  25425 SW 95<sup>th</sup> Ave  
                                  Wilsonville OR 97070

**Section 6.11 – Rights Cumulative**

All rights, remedies, powers, and privileges conferred under this Development Agreement on the Parties shall be cumulative of and in addition to, but not restrictive of or in lieu of, those conferred by law.

**Section 6.12 – Counterparts**

This Development Agreement may be executed in several counterparts, each of which shall be deemed an original, and all of such counterparts together shall constitute one and the same instrument.

**Section 6.13 – No Third-Party Beneficiaries and No Assignment**

None of the duties and obligations of any Party under this Development Agreement shall in any way or in any manner be deemed to create any rights in any person or entity other than the Parties hereto or their respective heirs, successors and assigns.

**Section 6.14 – Obligations Run with Land**

This Development Agreement shall run with the land and be binding upon any successors and assigns of any of the Parties hereto.

**Section 6.15 – Dispute Resolution**

**6.15.1 Mediation.** All disputes arising out of this Development Agreement shall first be submitted to mediation. Any Party desiring mediation shall provide the other Parties with a written notice (the “Request to Mediate”), which shall set forth the nature of the dispute. The Parties shall in good faith cooperate in the selection of a mediator and may adopt any procedural format that seems appropriate for the particular dispute. In the event a written settlement agreement is not executed by the Parties, in the Parties’ sole discretion, within twenty (20) days from the date of the Request to Mediate, or such longer time frame as may be agreed upon in writing by the Parties, any Party may make demand for arbitration pursuant to the following paragraph.

**6.15.2 Arbitration or Litigation.** Any dispute arising under this Development Agreement which is not resolved through mediation, may be submitted by any Party to arbitration, to be conducted in Wilsonville, Oregon before a single arbitrator selected by mutual agreement of the Parties. The arbitrator shall have substantial experience in commercial real estate and construction disputes. If the

Parties are unable to mutually select an arbitrator within twenty (20) days, then any Party may file an action in Clackamas County Circuit Court in lieu of arbitration and there will be no obligation to arbitrate. If arbitrated, judgment upon the arbitrator's award may be entered in any court having jurisdiction of the matter.

**Section 6.16 – Representations and Warranties**

Each of the Parties hereto represents and warrants that he/she is the authorized representative of the owners of each respective Neighboring Property and hereby warrants full authority to enter into this Agreement and bind all persons with ownership interest in the respective properties. The Parties signing below also hereby warrant that entry into this Development Agreement and the enforcement of its terms will not violate any loan covenants or other agreements pertaining to any of the land or improvements impacted hereby.

**Section 6.17 – Legal Review**

All of the Parties to this Development Agreement hereby affirm that they have been represented in the negotiation hereof by their own independent legal counsel who have reviewed this Development Agreement and advised their respective client concerning the same. Therefore it shall be interpreted accordingly and shall not be construed against the drafter. Any revisions that the Shared Driveway Parties wish to make to their respective easements not otherwise covered by this Development Agreement will be negotiated between them and the City will not be a party thereto.

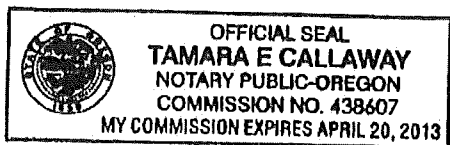
IN WITNESS WHEREOF, the Parties have hereunto set their hands as of the day and year first written above.

WILSONVILLE DEVCO, LLC  
an Oregon limited liability company

By: [Signature]  
Josh Veentjer  
As Its: Managing Member

STATE OF OREGON            )  
  ) ss.  
County of Clackamas    )

This instrument was acknowledged before me on August 3, 2012,  
by Josh Veentjer, as Managing Member of  
Wilsonville Devco, LLC.



Tamara E. Callaway  
Notary Public – State of Oregon

[Handwritten initials]

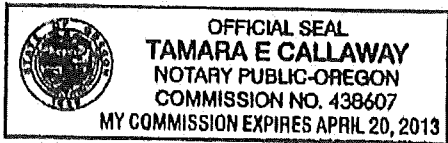


LAPOINT BUSINESS GROUP, LLC,  
an Oregon limited liability company

By: [Signature]  
Garry L. LaPoint  
As Its: Active Member

STATE OF OREGON )  
 ) ss.  
County of Clackamas )

This instrument was acknowledged before me on August 3, 2012,  
by Garry L. LaPoint, as Active Member of  
LaPoint Business Group, LLC.



Tamara E. Callaway  
Notary Public - State of Oregon

WHI HOTEL, LLC,  
an Oregon limited liability company



By: [Signature]  
Sungmin Park  
As Its: Owner

STATE OF OREGON )  
 ) ss.  
County of Clackamas )

This instrument was acknowledged before me on 07/27/12, 2012,  
by Sung-min Park, as Owner of  
WHI Hotel, LLC.

Melissa Marie Lopez FKA Melissa Marie Lopez  
Notary Public - State of Oregon

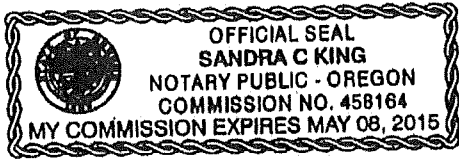
CITY OF WILSONVILLE,  
an Oregon municipal corporation

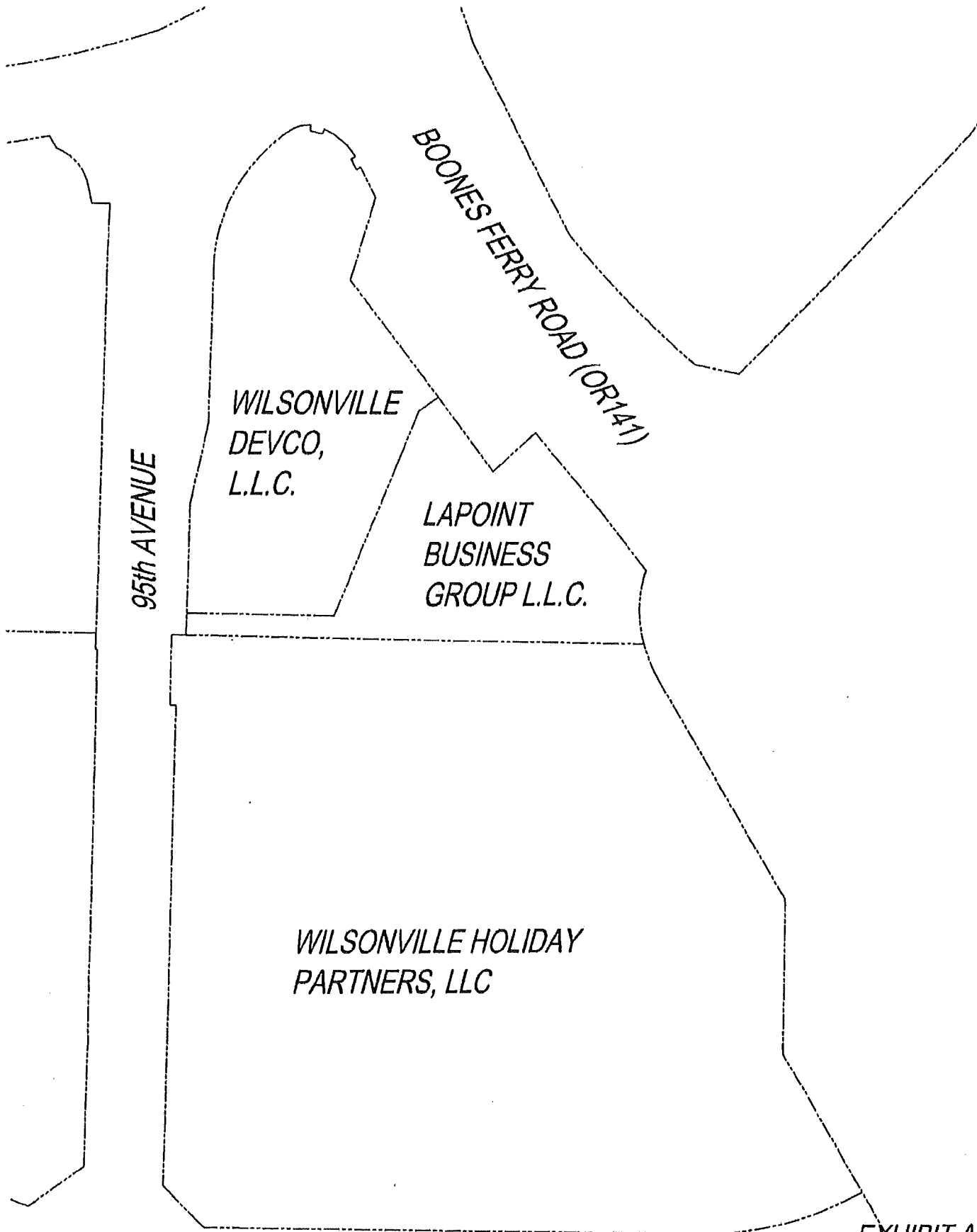
By: *Bryan A Cosgrove*  
Bryan Cosgrove  
As Its: City Manager

STATE OF OREGON            )  
  ) ss.  
County of Clackamas        )

This instrument was acknowledged before me on August 6, 2012,  
by Bryan Cosgrove, as the City Manager of the City of Wilsonville.

*Sandra C King*  
Notary Public – State of Oregon





95th AVENUE

WILSONVILLE  
DEVCO,  
L.L.C.

BOONES FERRY ROAD (OR141)

LAPPOINT  
BUSINESS  
GROUP L.L.C.

WILSONVILLE HOLIDAY  
PARTNERS, LLC

EXHIBIT A

Handwritten signature or initials, possibly 'SR', located at the bottom right of the page.

**EXHIBIT "A"**

A tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, in the Southeast one-quarter of Section 2, Township 3 South, Range 1 West of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, being further described as follows:

Commencing at the Southeast corner of Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, recorded in Book 31 at Page 14 in the Plat Records of Washington County, Oregon; thence South 89°38'33" West, along the South line of said Lot 7, a distance of 379.33 feet to a point 12.00 feet East of the East line of Parcel I as described in Deed from John Q. Hammons to the State of Oregon, by and through its Department of Transportation, Document No. 95-027726, recorded April 21, 1995 (hereinafter referred to as "ODOT"); thence North 00°09'24" East parallel to said East line, 18.00 feet to the true point of beginning; thence continuing North 00°09'24" East along said Easterly line, 341.16 feet; thence along the arc of a 116.16 foot radius curve to the right, through a central angle of 48°43'29", an arc length of 98.78 feet, the chord of which bears North 24°31'08" East, 95.83 feet; thence along the arc of a 45.00 foot radius curve to the right, through a central angle of 67°23'57", an arc length of 52.94 feet, the chord of which bears North 82°35'16" East 49.94 feet; thence along the arc of a 100.00 foot radius curve to the right, through a central angle of 37°13'18", an arc length of 64.96 feet, the chord of which bears South 45°05'58" East, 63.83 feet to a point on the Westerly line of Boones Ferry Road as described in said "ODOT" Deed; thence along the said Westerly line along the arc of a tangent 595.65 foot radius reverse curve to the left, the radius bears North 63°30'41" East, through a central angle of 02°45'38", an arc length of 28.70 feet, the chord of which bears South 27°52'08" East 28.70 feet; thence non-tangent South 15°09'35" West 83.41 feet; thence South 38°02'13" East, 122.78 feet; thence leaving said Westerly line, South 51°57'47" West, 20.00 feet; thence South 20°40'49" West, 186.07 feet to a point that is 18.00 feet measured at right angles from the South line of said Lot 7; thence parallel to said South line of Lot 7, South 89°38'33" West 121.22 feet to the true point of beginning.

EXCEPTING THEREFROM that portion thereof conveyed to the City of Wilsonville for right-of-way purposes in Warranty Deed recorded November 23, 2009 as Fee No. 2009-102082, Washington County Deed Records.

**EXHIBIT "A"**

**PARCEL I:**

A parcel of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, in the South one-half Section 2, Township 3 South, Range 1 West, of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, being further described as follows:

Commencing at the Southeast corner of said Lot 7; thence South 89°38'33" West, along the South line of said lot, a distance of 391.33 feet to the East line of Parcel 1 in Deed from John Q. Hammons, to the State of Oregon, by and through its Department of Transportation (herein after referred to as "ODOT"); thence North 00°09'24" East, along said "ODOT" Deed, a distance of 359.27 feet; thence continuing along said "ODOT" Deed, along the arc of a curve to the right, said curve having a radius of 128.16 feet, arc length of 140.62 feet, central angle of 062°51'50", a chord bearing of North 31°35'19" East, a chord length of 133.67 feet to the intersection with the South line of SW Commerce Circle as dedicated in the plat of EDWARDS BUSINESS INDUSTRIAL PARK; thence non-tangent North 70°34'24" East, along said street, a distance of 20.97 feet, and along the arc of a curve to the right, said curve having a radius 25.00 feet, arc length of 32.72 feet, central angle of 074°59'06", a chord bearing of South 71°56'03" East, and a chord length of 30.43 feet to the intersection with the West line of Boones Ferry as described in said "ODOT" Deed; thence along said "ODOT" Deed, along the arc of a non-tangent curve to the left, said curve having a radius of 1,001.93 feet, arc length of 12.00 feet, central angle of 000°41'10", a chord bearing of South 24°13'24" East, and a chord length of 12.00 feet to the intersection with the East line of said Lot 7; thence along the East line of said Lot 7, along the arc of a non-tangent curve to the left, said curve having a radius of 595.65 feet, arc length of 85.44 feet, central angle of 008°13'08", a chord bearing of South 25°08'24" East, and a chord length of 85.36 feet to the Westerly line of Boones Ferry Road as described in said "ODOT" Deed; thence non-tangent, along said Westerly line South 15°09'35" West, a distance of 83.41 feet, South 38°02'13" East, a distance of 200.44 feet, North 46°33'47" East, a distance of 48.10 feet, South 40°56'40" East, a distance of 81.06 feet, and along the arc of a non-tangent curve to the right, said curve having a radius of 2,837.79 feet, arc length of 17.49 feet, central angle of 00°21'11", a chord bearing of South 38°36'45" East, and a chord length of 17.49 feet to a point 100.00 feet North of, when measured at right angle to, the South line of said Lot 7; thence continuing along said "ODOT" Deed, along the arc of a non-tangent curve to the right, said curve having a radius of 2,837.79 feet, arc length of 48.51 feet, central angle of 00°58'46", a chord bearing of South 37°56'47" East, and a chord length of 48.51 feet, to the East line of said Lot 7; thence along the arc of a curve to the left, said curve having a radius of 116.96 feet, arc length of 62.30 feet, central angle of 030°31'07", a chord bearing South 00°03'01" West, and a chord length of 61.56 feet to the point of beginning.

EXCEPTING THEREFROM a tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, in the South one-half Section 2, Township 3 South, Range 1 West, of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, being further described as follows:

Commencing at the Southeast corner of said Lot 7; thence South 89°38'33" West, along the South line of said lot, a distance of 379.33 feet to a point 12 feet Easterly of the East line of Parcel 1 in Deed from John Q. Hammons to the State of Oregon, by and through its Department of Transportation, Fee No. 95027726, April 21, 1995 (herein after referred to as "ODOT"); thence North 00°09'24" East a distance of 12.00 feet parallel to and 12.00 feet Easterly of said "ODOT" line to the true point of beginning; thence North 00°9'24" East, parallel to and 12.00 feet Easterly of said "ODOT" line, a distance of 347.16 feet; thence along the arc of a curve to the right, said curve having a radius of 116.16 feet, arc length of 101.04 feet, central angle of 49°50'12", a chord bearing of North 25°04'30" East, and a chord length of 97.88 feet to a point of compound curvature; thence along the arc of a curve to the right, said curve having a radius of 45.00 feet, arc length of 53.94 feet, central angle of 33°01'29", a chord bearing South 71°56'03" East, and a chord length of 30.43 feet to a point of compound curvature; thence along the arc of a curve to the right, said curve having a radius of 100.00 feet, arc length of

## EXHIBIT "A"

(Continued)

61.13 feet, central angle of  $35^{\circ}01'29''$ , a chord bearing of South  $43^{\circ}49'18''$  East, and a chord length of 60.18 feet to the intersection with the West line of Boones Ferry Road as described in said "ODOT" Deed and a point on a non-tangent curve to the left, said point having a radial bearing of North  $63^{\circ}41'28''$  East; thence along said "ODOT" Deed, along the arc of said non-tangent curve to the left, said curve having a radius of 595.65, arc length of 30.57 feet, central angle of  $02^{\circ}56'25''$ , a chord bearing of South  $27^{\circ}46'44''$  East, and a chord length of 30.56 feet to along the Westerly line of Boones Ferry Road as described in said "ODOT" Deed; thence along said Westerly line South  $15^{\circ}09'35''$  West, a distance of 83.41 feet; thence South  $38^{\circ}02'13''$  East, a distance of 120.44 feet; thence South  $57^{\circ}57'47''$  West, a distance of 55.00 feet; thence South  $20^{\circ}29'49''$  West, a distance of 171.35 feet to a point that is 12 feet from, when measured at right angles, to the South line of said Lot 7; thence South  $89^{\circ}38'33''$  West, a distance of 97.95 feet, more or less, to the true point of beginning.

ALSO EXCEPTING THEREFROM that portion conveyed to Prairie Corp., an Oregon corporation, by instrument recorded July 19, 2000 as Fee No. 2000-48398, more particularly described as follows:

A tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, in the Southeast one-quarter of Section 2, Township 3 South, Range 1 West, of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, being further described as follows:

Commencing at the Southeast corner of said Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, recorded in Book 31, Page 14 in the Plat Records of Washington County, Oregon; thence South  $89^{\circ}38'33''$  West, along the South line of said Lot 7, a distance of 379.33 feet to a point 12.00 feet East of the East line of Parcel I as described in the Deed from John Q. Hammons to the State of Oregon, by and through its Department of Transportation, Document Number 95027726, recorded April 21, 1995 (herein after referred to as "ODOT"); thence North  $00^{\circ}09'24''$  East parallel to said East line, 18.00 feet; thence North  $89^{\circ}38'33''$  East parallel to said South line of Lot 7, 95.10 feet to the true point of beginning; thence North  $20^{\circ}29'49''$  East, 170.00 feet; thence North  $57^{\circ}57'47''$  East, 55.00 feet to the Westerly line of Boones Ferry Road as described in said "ODOT" Deed; thence along said Westerly line South  $38^{\circ}02'13''$  East, 2.34 feet; thence leaving said Westerly line South  $51^{\circ}57'47''$  West, 20.00 feet; thence South  $20^{\circ}40'49''$  West, 186.07 feet to a point 18.00 feet Northerly when measured at right to the said South line of Lot 7; thence South  $89^{\circ}38'33''$  West parallel to said South line of Lot 7, 26.13 feet, more or less, to the true point of beginning.

ALSO EXCEPTING THEREFROM that portion thereof conveyed to State of Oregon, by and through its Department of Transportation, in Deed recorded April 21, 1995, as Fee No. 95027726.

FURTHER EXCEPTING THEREFROM that portion dedicated to the City of Wilsonville for street purposes by instrument recorded March 7, 2003 as Fee No. 2003-034138.

### PARCEL II:

A tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK, recorded in Book 31, Page 14 in the Plat Records of Washington County, Oregon; thence South  $89^{\circ}38'33''$  West, along the South line of said Lot 7, a distance of 379.33 feet to a point 12.00 feet East of the East line of Parcel I as described in the Deed from John Q. Hammons to the State of Oregon, by and through its Department of Transportation, Document Number 95027726, recorded April 21, 1995; thence North  $00^{\circ}09'24''$  East parallel to said East line, 18.00 feet to the true point of beginning; thence North  $89^{\circ}38'33''$  East parallel to said South line of Lot 7, 95.10 feet; thence South  $20^{\circ}29'49''$  West, 6.42 feet to a point 12.00 feet Northerly when measured at right angles to the said South line of Lot 7; thence South  $89^{\circ}38'33''$  West parallel to said South line of Lot 7, 92.87 feet, more or less, to a point 12.00 feet East of the said East line of Parcel I; thence North  $00^{\circ}09'24''$  East parallel to said East line, 6.00 feet to the true point of beginning.

**EXHIBIT "A"**  
(Continued)

EXCEPTING THEREFROM that portion dedicated to the City of Wilsonville for street purposes by instrument recorded March 7, 2003 as Fee No. 2003-034138.





**EXHIBIT "A"**

**PARCEL I:**

A portion of Lot 8, EDWARDS BUSINESS INDUSTRIAL PARK NO. 2, a duly recorded subdivision in Washington County, Oregon located in the Southeast one quarter of Section 2, Township 3 South, Range 1 West of the Willamette Meridian, in the City of Wilsonville, County of Washington and State of Oregon, said portion of Lot 8 being more particularly described as follows:

Beginning at a 2 inch iron pipe at the initial point of said Edwards Business Industrial Park No. 2, said initial point being on the Westerly line of S.W. Frontage Road, also known as Lower Boones Ferry Road; thence following the boundary of said Lot 8, along the arc of a 117.00 foot radius non-tangent curve left through a central angle of 15°30'25", an arc distance of 31.67 feet (the chord of which bears South 22°58'48" East, a distance of 31.57 feet) to a 5/8 inch iron rod (Plat record: central angle, 15°30'50"; radius 117 feet; arc length 31.68 feet; chord, South 22°56'17" East, a distance of 31.58 feet); thence South 30°34'43" East a distance of 213.04 feet to a 5/8 inch iron rod (Plat record: South 30°34'26" East a distance of 212.92 feet); thence South 00°05'07" East a distance of 130.27 feet to a 5/8 inch iron rod (Plat record: South 00°07'50" East a distance of 130.27 feet); thence South 30°06'16" east a distance of 136.08 feet to a 5/8 inch iron rod (Plat record: South 30°03'30" East a distance of 136.04 feet); thence South 59°37'27" West a distance of 13.17 feet to a 5/8 inch iron rod (Plat record: South 59°56'30" West a distance of 13.10 feet); thence Southwesterly along the arc of a 243.00 foot radius curve right through a central angle of 29°42'03" an arc distance of 125.97 feet to a 5/8 inch iron rod (the chord of which bears South 74°47'32" West a distance of 124.56 feet) (Plat record: central angle 29°42'03"; radius 243.00 feet; arc length 125.97 feet; chord, South 74°47'32" West, 124.56 feet); thence South 89°38'33" West (Plat record: South 89°38'33" West) a distance of 410.16 feet (Survey Number 26,398 Record: 410.17 feet) to a 5/8 inch iron rod at the intersection of the Southerly boundary of said Lot 8 and the Easterly right of way line of 95th Avenue; thence North 44°12'00" West a distance of 50.04 feet to a 5/8 inch iron rod 36.00 feet opposite and Easterly of Engineers Centerline Station 66+45.00, when measured at right angles to the centerline of 95th Avenue; thence North 00°08'50" East a distance of 405.00 feet to a 5/8 inch iron rod 36.00 feet opposite and Easterly of Engineer's Centerline Station 70+50.00 when measured at right angle to the centerline of 95th Avenue; thence North 89°51'10" West a distance of 5.00 feet to a 5/8 inch iron rod 31.00 feet opposite and Easterly of said Engineer's Centerline Station 70+50.00; thence North 00°08'50" East a distance of 58.91 feet to a 5/8 inch iron rod marking the intersection of the Easterly right of way line of 95th Avenue and the Northerly boundary of said Lot 8; thence leaving the Easterly right of way line of 95th Avenue and following the Northerly line of said Lot 8 North 89°37'29" East (Plat record: bearing North 89°38'33" East) a distance of 391.26 feet to the 2 inch iron pipe marking the initial point of said Edwards Business Industrial Park No. 2 and the point of beginning of this described tract of land.

**PARCEL II:**

Easement rights as set forth in Reciprocal Easement Agreement dated December 27, 1996 and recorded January 6, 1997 as Records's Fee No. 97-005009, described as follows:

A tract of land located in Lot 7, EDWARDS BUSINESS INDUSTRIAL PARK as recorded in Book 38, Page 14, Washington County, Oregon Plat Records, being situated in the Southeast one quarter of Section 2, Township 3 South, Range 1 West of the Willamette Meridian, Washington County, Oregon, said tract of land being more particularly described as follows:

Beginning at a 2 inch iron pipe marking the initial point of said Edwards Business Industrial Park; thence South 89°37'29" West (Plat record: Bearing South 89°38'33" West) along the Southerly boundary of said Lot 7, a distance of 391.26 feet to a point marking the intersection of the Southerly line of said Lot 7 and the Easterly line

**EXHIBIT "A"**  
(Continued)

of 95th Avenue as acquired by the State of Oregon and the true point of beginning of this described tract of land; thence North  $00^{\circ}08'50''$  East along said Easterly line, 20.00 feet; thence South  $89^{\circ}51'10''$  East leaving said Easterly right of way, 51.00 feet; thence South  $53^{\circ}16'00''$  East, 32.38 feet to a point on the Southerly line of said Lot 7; thence South  $89^{\circ}37'29''$  West along the Southerly line of said Lot 7, a distance of 77.00 feet to the true point of beginning.







**EARNEST MONEY AGREEMENT  
FOR PURCHASE OF LAND**

Wilsonville, Oregon  
October ~~24~~, 2009

Sellers: South Sea, LLC  
8655 SW Citizens Drive, Suite 104  
Wilsonville, Oregon 97070

Buyer: City of Wilsonville  
29799 SW Town Center Loop East  
Wilsonville, Oregon 97070

**RECITALS**

The City of Wilsonville is a municipal corporation with the power of condemnation. Buyer desires to acquire right-of-way for the 95<sup>th</sup> Avenue & Boones Ferry Road Intersection Project (the "Project"). Sellers own the real property described as tax lot 0302 of Section 02DB Township 3S, Range 1W (the "Site"). The property to be acquired is described in Exhibits A & B to this Agreement (the "Property").

Under the threat of condemnation, Sellers have agreed to sell the Property to Buyer upon the terms and conditions set forth in this Agreement.

**AGREEMENT**

1. **Purchase and Sale.** Sellers agree to sell to Buyer and Buyer agrees to purchase from Sellers the Property, including improvements and any damages to the remainder for One Hundred Seven Thousand Five Three Hundred Forty and No/100 Dollars ~~(\$107,534.00)~~ (the "Purchase Price"). This is comprised of the following:

Fee Acquisition (8,131 SF)	<del>8,131.00.00</del>
In Settlement: Reimbursement for Expenses Incurred after 10/2006	<del>913,940.00</del>
<b>Rounded Total Value Paid</b>	<del>922,071.00</del>

Buyer shall pay Sellers the entire Purchase Price in cash upon closing.

Further, to satisfy the requirements of a preexisting condition of development, Seller agrees to convey to Buyer a Public Utility Easement and a Slope Easement for no additional financial consideration.

Buyer's obligation to close the purchase of the Property shall be subject to and conditioned upon Sellers ability to secure and record a Release of Sewer Easement from Exxon of Wilsonville, I.L.C. Sellers shall further secure and record the conveyance of a Private Utility Easement benefitting Exxon of Wilsonville, I.L.C.

**2. Additional Consideration.** In addition to the payment of the Purchase Price, but expressly upon the condition that Seller conveys the Property to Buyer pursuant to this Agreement, Buyer agrees to the following Site Specific Items:

- A. Credit waiver for future City development application and review fees in the amount of Ten Thousand Six Hundred Twenty One and No/100 (\$10,621.00) dollars.

**3. Title Report.** Chicago Title Insurance Company (the "Escrow Agent") has prepared a Status of Record Title report (the "SORT Report") for the Property dated March 5, 2009. Sellers agree to convey the Property to Buyer subject only to exceptions 1 ~ 9 (one through and including nine), exceptions 11 ~ 14 (eleven through and including fourteen) and exception 16 (sixteen) described in the SORT Report (the "Permitted Exceptions"). As stated above, a Release of Sewer Easement will be needed per exception 10 (ten). A partial release of interest will be needed from West Coast Bank per exception 15 (fifteen).

**4. Condition Precedent to Purchase.** Buyer's obligation to close the purchase of the Property shall be subject to and conditioned upon Sellers' ability to convey the Property to Buyer subject only to the Permitted Exceptions and the accuracy of Sellers' representations and warranties made in this Agreement as of the closing date. Sellers shall have until escrow closing to cause the extinguishment of all exceptions other than the Permitted Exceptions.

**5. Representations and Warranties.** Sellers represent and warrant to Buyer that:

(a) To Sellers' knowledge, without the duty of investigation or inquiry, the Property is not in violation of any agreements, covenants, conditions or restrictions affecting the Property.

(b) Sellers have received no notice from any governmental agency having jurisdiction in the matter of any violation of any statute, law, ordinance, deed restriction or rules or regulations with respect to the existence, construction, maintenance or operation of the Property.

(c) Sellers are not foreign persons within the meaning of Section 1445 of the Internal Revenue Code of 1986, and at closing Sellers shall deliver a certification to this effect to Buyer.

(d) To Sellers' knowledge, without the duty of investigation or inquiry, there are no material hidden or latent defects that exist in or on the Property.

(e) There are no actions, claims or proceedings pending or, to Sellers' actual knowledge, threatened by any party against Sellers in connection with the Property or against the Property, nor to Sellers' knowledge is there any basis for any such action or proceeding.

(f) Sellers have the legal power, right and authority to enter into this Agreement and to consummate the transactions contemplated herein. The individual executing this instrument on behalf of Sellers have the legal power, right and actual authority to bind Sellers to the terms and conditions of this Agreement.

(g) The foregoing representations and warranties shall be true and correct at closing, and to the extent that full performance of such representations and warranties is not required prior to or at closing, they shall survive the closing and be fully enforceable thereafter.

**6. Closing, Escrow, Prorates.** If the conditions set forth in paragraph 3 above have been timely satisfied or waived, the purchase and sale shall close on the date designated by Sellers in a written notice to Buyer (but in no event later than 60 days after execution of this agreement) given at least ten (10) days before the designated closing date. The purchase of the Property shall be closed in escrow by the Escrow Agent. Prior to closing, each party will deposit with the Escrow Agent the funds, a partial release of property interest, a Release of Sewer Easement, documents and instructions necessary for closing. The cost of the escrow shall be Buyer's responsibility. Current real property taxes on the Property will be prorated between the parties as of the date of closing. Buyer shall be entitled to possession of the Property on the closing date.

**7. Broker.** Parties agree that Seller will be responsible for compensating any broker or salespersons Seller may have retained for this transaction.

**8. Full Payment.** Sellers acknowledge that the Purchase Price and Buyer's completion of the Site Specific Items are just and full compensation for a fee taking of real property, including any and all damages, costs, liabilities or other obligations which Sellers may have otherwise been entitled to in the event a condemnation action had been filed. Sellers agree to defend, indemnify and hold harmless Buyer from any and all claims of any tenants of the Site related to the purchase of the Property by Buyer.

**9. General and Miscellaneous Provisions.**

(a) **Prior Agreements.** This instrument is the entire, final and complete agreement of the parties pertaining to the sale and purchase of the Property, and supersedes and replaces all written and oral agreements heretofore made or existing by

and between the parties or their representatives insofar as the Property is concerned. Neither party shall be bound by any promises, representations and agreements except as are herein expressly set forth.

(b) **Time is of the Essence.** Time is expressly made of the essence of each provision of this Agreement.

(c) **Notices.** Any notice required or permitted under this Agreement shall be in writing and shall be given when actually delivered in person or forty-eight (48) hours after having been deposited in the United States mail as certified or registered mail addressed to the addresses set forth below the names of the parties on the first page hereof, or to such other address as one party may indicate by written notice to the other party.

(d) **Attorney Fees and Costs.** In the event legal action is commenced in connection with this Agreement, the prevailing party in such action shall be entitled to recover its reasonable attorney fees and costs incurred therein.

(e) **Nonwaiver.** Failure by either party at any time to require performance by the other party of any of the provisions hereof shall in no way affect the party's rights hereunder to enforce the same, nor shall any waiver by the party of the breach hereof be held to be a waiver of any succeeding breach or a waiver of this nonwaiver clause.

(f) **No Merger.** The obligations set forth in this Agreement shall not merge with the transfer or conveyance of title to any party of the Property but shall remain in effect until fulfilled.

(g) **Amendments.** This Agreement may be amended, modified or extended without new consideration but only by written instrument executed by both parties.

(h) **Governing Law.** This Agreement shall be construed in accordance with and governed by the laws of the state of Oregon.

(i) **Severability.** If any portion of this Agreement shall be invalid or unenforceable to any extent, the validity of the remaining provisions shall not be affected thereby.

(j) **Counting of Days.** Whenever a time period is set forth in days in this Agreement, the first day from which the designated period of time begins to run shall not be included. The last day of the period so computed shall be included, unless it is a Saturday or legal holiday, including Sunday, in which event the period runs until the end of the next day which is not a Saturday or legal holiday.



(k) **Number, Gender and Captions.** In construing this Agreement, it is understood that if the context so requires the singular pronoun shall be taken to mean and include the plural, the masculine, the feminine and the neuter, and that generally all grammatical changes shall be made, assumed and implied to individuals and/or corporations and partnerships. All captions and paragraph heading used herein are intended solely for convenience of reference and shall in no way limit any of the provisions of this Agreement.

(l) **Recording.** Upon request of either party, the parties shall execute in a form sufficient for recording a memorandum of this Agreement, which may be recorded at the expense of the party requesting the same.

(m) **Binding Effect.** The covenants, conditions and terms of the Agreement shall extend to and be binding upon and inure to the benefit of the heirs, personal representatives, successors and assign of the parties hereto.

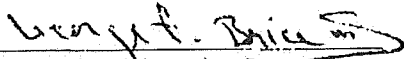
(n) **Execution in Counterparts.** This Agreement may be executed in any number of counterparts and by different parties hereto on separate counterparts, each of which counterparts, when so executed and delivered, shall be deemed to be an original and all of which counterparts, taken together, shall constitute but one and the same Agreement.

(o) **Assignment.** The Buyer shall have the right to assign its rights and obligations under this Agreement to the City of Wilsonville, a municipal corporation of the state of Oregon ("City"). The City, as assignee, shall succeed to all the rights and remedies under this Agreement, including but not limited to the specific performance of this Agreement. If Buyer assigns its rights and obligations under this Agreement to the City, Sellers thereafter shall tender all Sellers performance to and for the benefit of the City, including, but not limited to delivery of the deed, in the name of the City.

**10. Zoning and Land Use.** THE PROPERTY DESCRIBED IN THIS INSTRUMENT MAY NOT BE WITHIN A FIRE PROTECTION DISTRICT PROTECTING STRUCTURES. THE PROPERTY IS SUBJECT TO LAND USE LAWS AND REGULATIONS, WHICH, IN FARM OR FOREST ZONES, MAY NOT AUTHORIZE CONSTRUCTION OR SITING OF A RESIDENCE AND WHICH LIMIT LAWSUITS AGAINST FARMING OR FOREST PRACTICES AS DEFINED IN ORS 30.930 IN ALL ZONES. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USED AND EXISTENCE OF FIRE PROTECTION FOR STRUCTURES.

**SELLERS:**

SOUTH SEA, LLC

  
George Brice, Managing Partner

**BUYER:**

CITY OF WILSONVILLE

By: \_\_\_\_\_  
Arlene Loble, City Manager

EXHIBIT "A"

14582LD1C  
1-28-09  
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**MacKay & Sposito, Inc.**

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**LEGAL DESCRIPTION  
BOONES FERRY ROAD/95TH AVE INTERSECTION IMPROVEMENTS  
RIGHT OF WAY DEDICATION  
CITY OF WILSONVILLE**

A parcel of land lying in Lot 7, "Edwards Business Industrial Park" in the Southeast quarter of Section 2, Township 3 South, Range 1 West, W.M., Washington County, Oregon and being a portion of that property described in that deed to South Sea, LLC, an Oregon Limited Liability Company, recorded as Document No. 2000-058349, Film Records of Washington County, being further described as follows:

Commencing at the centerline intersection of SW 95<sup>th</sup> Avenue and SW Commerce Circle being Station 65+77.83 as surveyed and shown in Washington County Survey Number 31128; thence North 01°48'08" East along the centerline of SW 95<sup>th</sup> Avenue as shown on said survey a distance of 530.75 feet; thence South 88°42'44" East a distance of 43.00 feet to the Easterly right of way line of said SW 95<sup>th</sup> Avenue, being the East line of that Tract of land described in Document No. 2003-34138; thence along said Easterly right of way line, the Southerly right of way line of SW Commerce Circle and the Westerly right of way line of Boones Ferry Road the following courses:

North 01°48'08" East a distance of 109.96 feet to the **True Point of Beginning**; thence continuing North 01°48'08" East a distance of 249.21 feet to the point of curvature of a 116.16 foot radius curve; thence along said curve to the right, through a central angle of 48°47'35", an arc distance of 98.92 feet (having a chord bearing of North 26°11'55" East, a distance of 95.96 feet) to a point of compound curvature with a 45.00 foot radius curve, also being a point on the Southerly right of way line of SW Commerce Circle; thence along said curve to the right, through a central angle of 67°27'08", an arc distance of 52.98 feet (having a chord bearing of North 84°19'17" East, a distance of 49.97 feet) to a point of compound curvature with a 100.00 foot radius curve; thence along said curve to the right, through a central angle of 37°15'00", an arc distance of 65.01 feet (having a chord bearing of South 43°19'39" East, a distance of 63.87 feet) to the point of reverse curvature with a 595.65 foot radius curve, also being a point on the Westerly right of way

EXHIBIT "A"

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1-28-09

mjf

line of Boones Ferry Road as surveyed and shown in Washington County Survey Number 26,108; thence along said curve to the left, through a central angle of  $02^{\circ}45'12''$ , an arc distance of 28.62 feet (having a chord bearing of South  $26^{\circ}04'45''$  East, a distance of 28.62 feet); thence South  $16^{\circ}51'53''$  West, a distance of 11.09 feet to a point on an arc of a 863.64 foot radius non-tangent curve having a radial bearing of North  $62^{\circ}53'14''$  East;

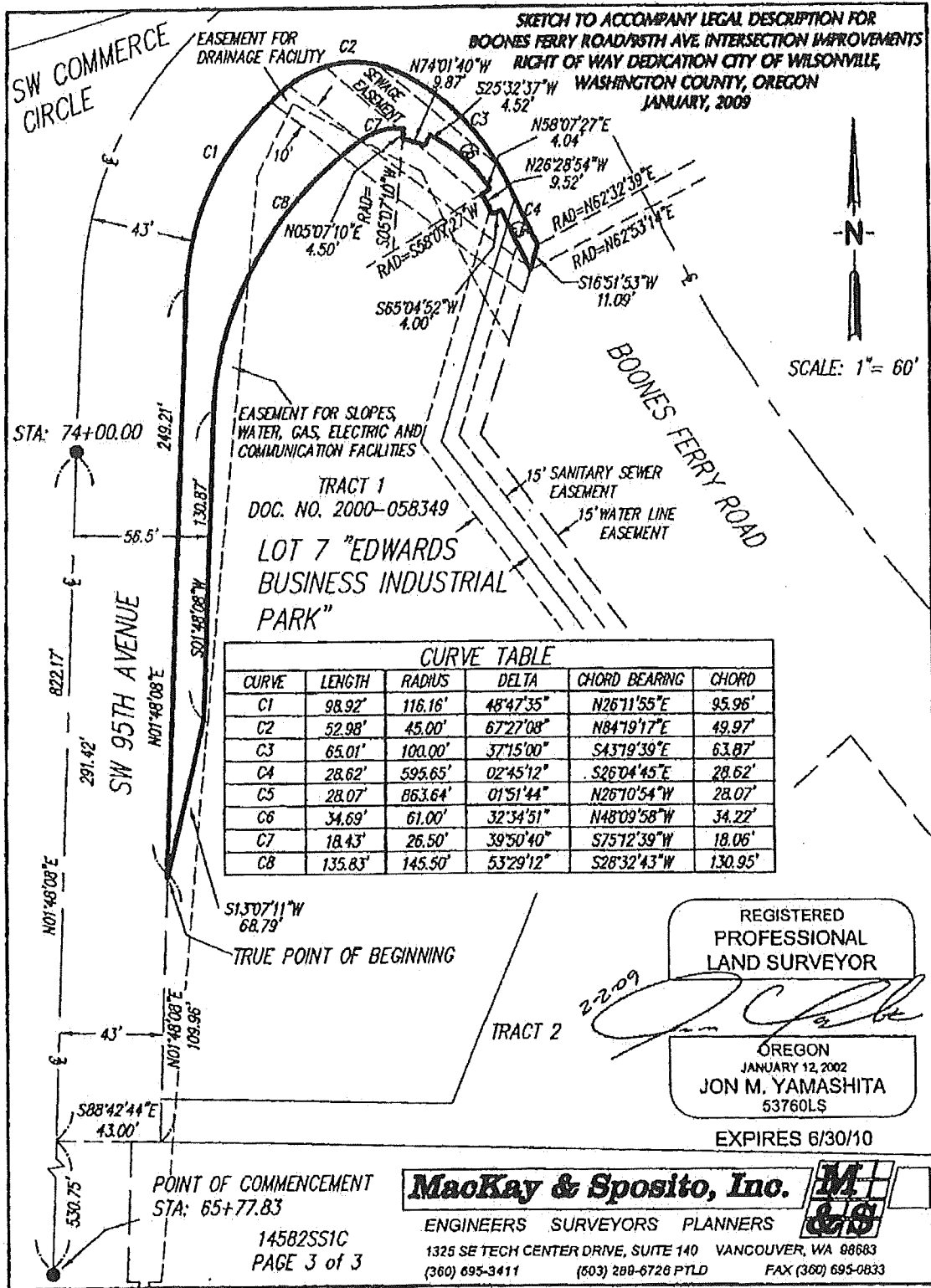
thence leaving said Westerly right of way line along said curve to the right, through a central angle of  $01^{\circ}51'44''$ , an arc distance of 28.07 feet (having a chord bearing of North  $26^{\circ}10'54''$  West, a distance of 28.07 feet); thence South  $65^{\circ}04'52''$  West a distance of 4.00 feet; thence North  $26^{\circ}28'54''$  West a distance of 9.52 feet; thence North  $58^{\circ}07'27''$  East a distance of 4.04 feet to a point on an arc of a 61.00 foot radius non-tangent curve having a radial bearing of South  $58^{\circ}07'27''$  West; thence along said curve to the left, through a central angle of  $32^{\circ}34'51''$ , an arc distance of 34.69 feet (having a chord bearing of North  $48^{\circ}09'58''$  West, a distance of 34.22 feet); thence South  $25^{\circ}32'37''$  West a distance of 4.52 feet; thence North  $74^{\circ}01'40''$  West a distance of 9.87 feet; thence North  $05^{\circ}07'10''$  East a distance of 4.50 feet to a point on an arc of a 26.50 foot radius non-tangent curve having a radial bearing of South  $05^{\circ}07'10''$  West; thence along said curve to the left, through a central angle of  $39^{\circ}50'40''$ , an arc distance of 18.43 feet (having a chord bearing of South  $75^{\circ}12'39''$  West, a distance of 18.06 feet to a point of compound curvature with a 145.50 foot radius curve; thence along said curve to the left, through a central angle  $53^{\circ}29'12''$ , an arc distance of 135.83 feet (having a chord bearing of South  $28^{\circ}32'43''$  West, a distance of 130.95 feet) to a point 56.50 feet easterly of, when measured at right angles to, the centerline of said SW 95<sup>th</sup> Avenue; thence South  $01^{\circ}48'08''$  West parallel with said centerline a distance of 130.87 feet; thence South  $13^{\circ}07'11''$  West a distance of 68.79 feet to the **True Point of Beginning**.

Containing 8,131 sq. ft., more or less.

Subject to easements and restrictions of record.



EXHIBIT "B"







Report for

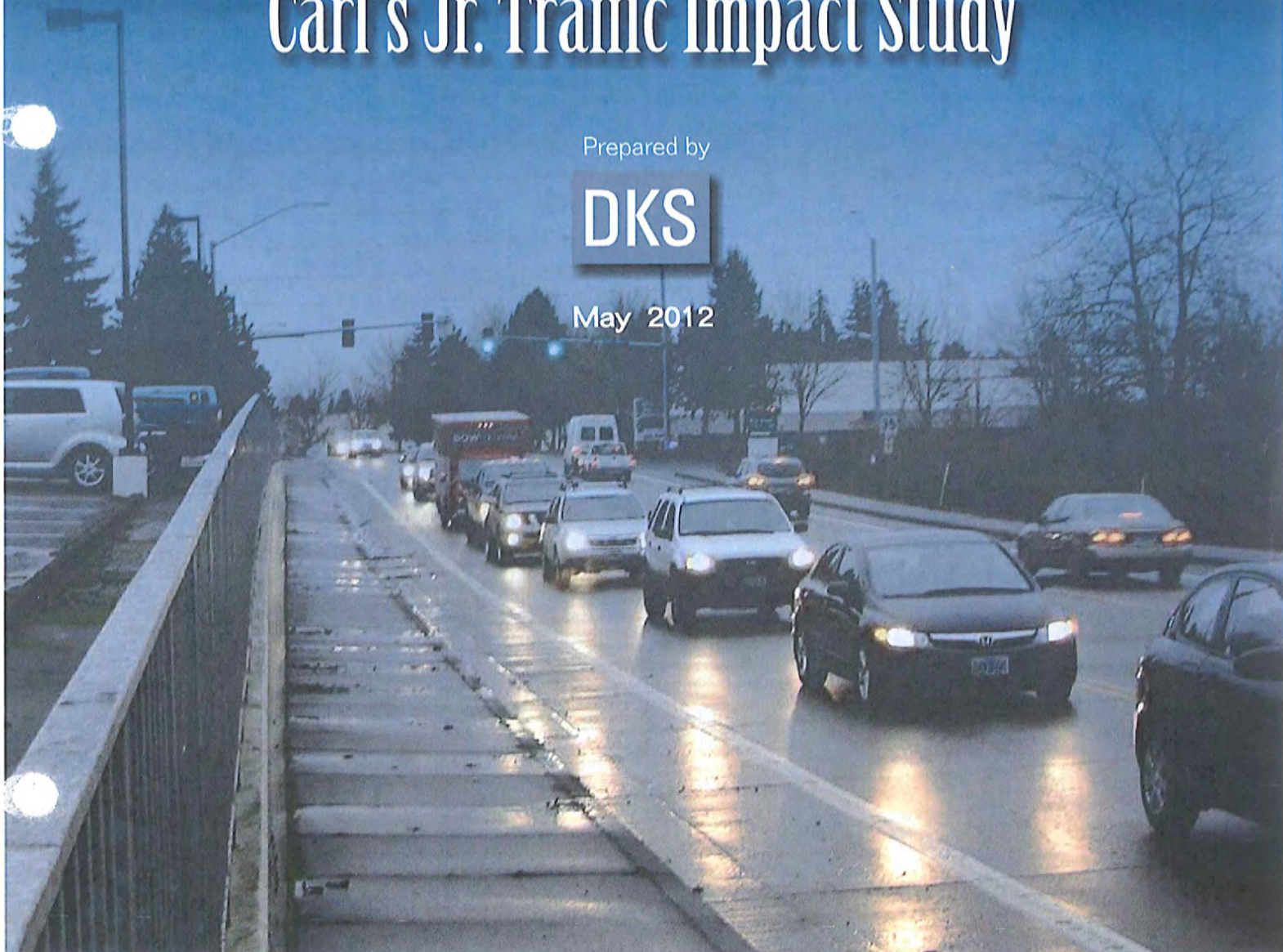


# Carl's Jr. Traffic Impact Study

Prepared by

**DKS**

May 2012







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## CHAPTER 1: INTRODUCTION AND SUMMARY

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This study evaluates the transportation impacts for the proposed Carl's Jr. Restaurant and retail space on the southeast corner of the Boones Ferry Road/95<sup>th</sup> Avenue intersection in Wilsonville Oregon. Figure 1 shows the site location, which shares access with the existing Chevron gas station and Holiday Inn that are immediately east and south of the site respectively. The site plan for the proposed development is provided in the appendix.

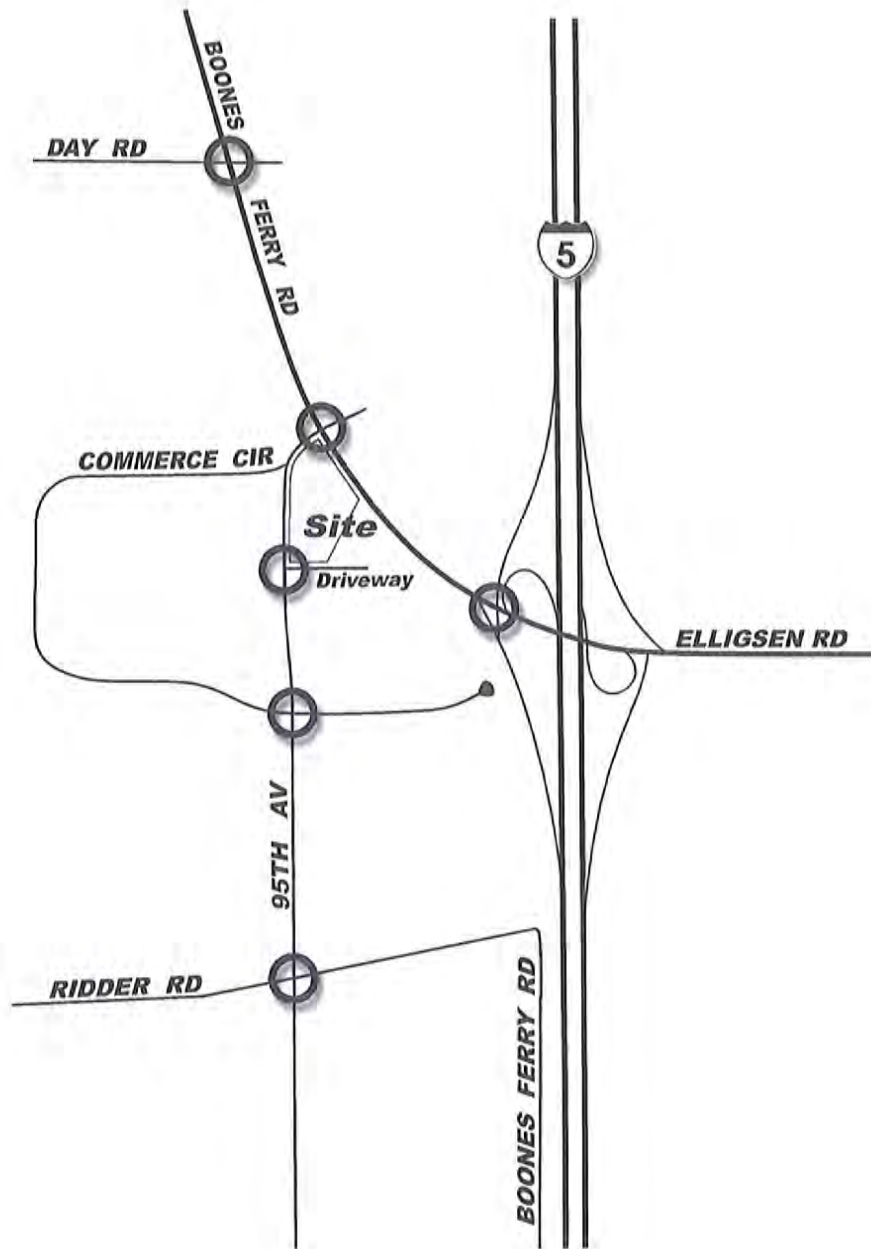
The purpose of this transportation impact study (TIS) is to recommend mitigation measures that offset the transportation impacts that the proposed restaurant and retail space would have on the nearby transportation network. It also evaluates the site's internal transportation facilities. The focus of the impact analysis is on the study intersections identified by City of Wilsonville staff.<sup>1</sup> These study intersections are shown in Figure 1 and include the following:

- Elligsen Road/I-5 Southbound Ramp
- Boones Ferry Road/Day Road
- Boones Ferry Road/95<sup>th</sup> Avenue
- 95<sup>th</sup> Avenue/Commerce Circle (southern intersection)
- 95<sup>th</sup> Avenue/Ridder Road
- 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access

The chapters in this report document the existing conditions, project impacts, and recommended mitigations associated with the proposed project in the identified study area. In this first chapter, Table 1 is provided immediately following Figure 1 and lists important characteristics of the study area and proposed project. The remaining sections of this chapter provide a summary of key information and findings related to the proposed development, project traffic impact, planned improvements, and recommended mitigations.

---

<sup>1</sup> Mike Ward, City of Wilsonville, February 22, 2012.




**LEGEND**  
 - Study Intersection



Figure 1

**STUDY AREA**

**Table 1: Key Study Area and Proposed Carl's Jr. Characteristics**

Characteristics	Information
<b>Study Area</b>	
Number of Study Intersections	Six
Analysis Period	Weekday P.M. Peak Hour (one hour between 3 p.m. and 5 p.m.)
<b>Proposed Carl's Jr. and Retail Space</b>	
Fast-Food Restaurant w/Drive-Through	2,786 SF
Specially Retail Center	3,150 SF
Total P.M. Peak Hour Project Driveway Trips	112 (56 in, 56 out)
Net-New P.M. Peak Hour Project Trips for Site	60 (30 in, 30 out)
Net-New P.M. Peak Hour Project Trips Through the I-5/Elligsen Road Interchange Area	32 additional site trips through interchange area
Vehicle Access Points	Shares existing site access onto 95 <sup>th</sup> Avenue with Holiday Inn and Chevron

### Proposed Development

Based on information provided by the project sponsor, the proposed development site is 1.30 acres and includes a 2,786 square-foot Carl's Jr. Restaurant (including a drive-up window and indoor seating). In addition, the development includes a 3,375 square-foot retail business at the north end of the site. The development will access 95<sup>th</sup> Avenue using the driveway that serves the existing Chevron gas station and Holiday Inn, and driveway modifications will be constructed as part of the site development.

### Project Traffic Impact

Project traffic impacts were evaluated at the study intersections for the weekday p.m. peak hour. The impact analysis includes trip generation, trip distribution, p.m. peak hour project trips through the two City of Wilsonville I-5 interchange areas, and future traffic operating conditions at the study intersections. The analysis also includes scenarios that account for Stage II approved developments in the area, including those under construction or built but not yet occupied. In addition, the impact analysis assumed the completion of the Boones Ferry Road/95<sup>th</sup> Avenue intersection improvement project, which is currently under contract and expected to be completed later this year.

To determine project impact at the study intersections, traffic operating conditions were analyzed during the weekday p.m. peak hour. The analysis was performed using 2000 *Highway Capacity Manual (HCM)* methodology<sup>2</sup> for signalized and unsignalized intersections for the following scenarios:

<sup>2</sup> 2000 *Highway Capacity Manual*, Transportation Research Board, Washington DC, 2000.



- Existing Weekday PM Peak Hour
- Existing + Project (includes traffic from Carl's Jr. and north retail building)
- Existing + Stage II (includes traffic from other developments that have Stage II approval or are under construction)
- Existing + Project + Stage II

The study intersection operating conditions for the three future scenarios are listed in Table 2. The study intersections at public streets (i.e., excluding the project driveway) are expected to meet the City's and ODOT's applicable operating standards. Because the project driveway is not a public street, it is not required to meet the City's LOS D operating standards. However, the City's primary concern is ensuring that driveway operations do not negatively impact the City's public streets or public safety. Long delays and queuing at the private access can lead to undesired driver behavior. Because additional driveway traffic is being added by the proposed project, delays for outbound traffic are expected to worsen to greater than 50 seconds per vehicle.

The constraints of the project site and adjacent roadway limit the ability to fully resolve the minor street delay concerns. However, delays can be minimized if sufficient storage is provided for both the left-turn and right-turn lanes exiting the driveway. Under the driveway's current configuration, there is only sufficient room (approximately 50 feet) for up to two left-turning vehicles to wait in the short left-turn lane. The proposed site plan extends the site's exiting turn lanes so that it is more than three times longer (approximately 150 feet) and would accommodate up to six vehicles.

**Table 2: Future Project and Stage II Intersection Operations Comparison**

Intersection	Operating Standard	Existing + Project			Existing + Stage II			Existing + Stage II + Project		
		Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C
<b>Signalized</b>										
Elligsen Rd/I-5 SB Ramp	LOS D, 0.85 v/c	14.9	B	0.54	15.2	B	0.54	15.3	B	0.56
Boones Ferry Road/Day Rd	LOS D, 0.99 v/c	19.3	B	0.65	19.1	B	0.65	19.3	B	0.65
Boones Ferry Rd/95 <sup>th</sup> Ave	LOS D, 0.99 v/c	20.3	C	0.55	20.1	C	0.55	20.4	C	0.56
95 <sup>th</sup> Ave/Commerce Cir	LOS D	11.7	B	0.68	11.8	B	0.69	11.9	B	0.69
95 <sup>th</sup> Ave/Ridder Rd	LOS D	5.5	A	0.42	5.5	A	0.42	5.5	A	0.43
<b>Unsignalized</b>										
95 <sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access	N/A <sup>a</sup>	>50	B/F	>1.0	>50	B/F	>1.0	>50	B/F	>1.0
<u>Signalized Intersections:</u> LOS = Level of Service of Intersection V/C = Volume-to-Capacity Ratio of Intersection					<u>Unsignalized Intersections:</u> LOS = Level of Service of Major Street/Minor Street V/C = Volume-to-Capacity Ratio of Worst Movement					
<b><u>Bold Underlined</u></b> values do not meet standards.										

<sup>a</sup> The City's LOS D standard does not apply to private driveways or roadway approaches; however, LOS D operations or better are preferred.

Traffic simulation was also performed to evaluate the northbound queuing on 95<sup>th</sup> Avenue and to provide additional estimates regarding the delay at the project driveway. The simulations suggest that minimal queuing will occur on 95<sup>th</sup> Avenue under the “Existing + Project + Stage II” traffic scenario once the 95<sup>th</sup> Avenue/Boones Ferry Road intersection improvements are completed. These queues are not expected to extend back past the project driveway, which will allow free-flow movement on 95<sup>th</sup> Avenue adjacent to the project site.

On the driveway approach to 95<sup>th</sup> Avenue, the traffic simulation suggests that the average vehicle delay at the project driveway was between 45 and 50 seconds, which falls in the level of service E range (rather than the LOS F calculated from the HCM operations analysis). While the simulation estimates slightly better operations than the HCM analysis, traffic leaving the project site can still expect to experience longer delays than exist today, due primarily to the additional traffic from the proposed development.

### Site Plan Evaluation

A site evaluation was performed for the proposed development based on the preliminary site plan provided by the project sponsor. The evaluation considered site access, internal vehicular circulation, pedestrian facilities, and parking. One of the site’s primary concerns related to its access to 95<sup>th</sup> Avenue. Therefore, the proposed site plan assumes reconstruction of the driveway, and the proposed driveway improvements have been coordinated with City staff and DKS Associates.<sup>3</sup> Various additional mitigations have been identified for the site and are listed in the next section of this chapter.

### Recommended Mitigations

To preserve the operations of the study area roadways and provide safe access to the proposed Carl’s Jr. and surrounding land uses, it is recommended that a series of transportation mitigation measures be performed. The following project related measures would typically be required as conditions of approval if the project were approved:

#### Site Access

- The site driveway should be reconstructed as shown on the site plan.
- The reconstructed site driveway should meet American Association of State Highway and Transportation Officials (AASHTO) intersection sight distance requirements<sup>4</sup> as measured from 14.5 feet back from the edge of pavement. Based on 95<sup>th</sup> Avenue’s 35 mph speed limit, the site driveway would require a minimum of 390 feet of sight distance. The sight triangles at the driveways should be clear from vegetation, signs, and other obstructions that may limit intersection sight distance. Prior to occupancy, intersection sight distance at the site driveway will need to be verified, documented, and stamped by a registered professional Civil or Traffic Engineer licensed in the State of Oregon.

<sup>3</sup> Meeting at the City of Wilsonville, April 11, 2012.

<sup>4</sup> *Geometric Design of Highways and Streets*, AASHTO, 2004; Case B1, p. 661.



### **Internal Vehicular Circulation**

- Turn templates for passenger vehicles should be provided by the applicant to verify that vehicles leaving the drive-thru lane are able to make the sharp 180-degree right turn without extending into the oncoming traffic lanes.
- Turn templates for trucks should be provided by the applicant to show that adequate turning radius will be available on the northeast corner of the 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas driveway to allow right-turning trucks to stay within the northbound travel lane on 95<sup>th</sup> Avenue (instead of requiring them to either mount the curb or use an opposing southbound travel lane). Turn templates may also be helpful to show the likely on-site truck maneuvering.

### **Pedestrian Facilities**

- All sidewalks within the site should conform to ADA requirements.<sup>5</sup>
- Stop bars at the site egress should be located so that they stop vehicles prior to the pedestrian crossing to reduce pedestrian/vehicle conflicts.

### **Parking**

- Six bicycle parking spaces should be provided on the new site for it to be in compliance with City Code.

---

<sup>5</sup>ADA Accessibility Guidelines for Buildings and Facilities, Department of Justice, January 2004.

## CHAPTER 2: EXISTING CONDITIONS

This chapter provides documentation of existing study area conditions, including the project site, study area roadway network, existing traffic volumes, existing p.m. peak hour traffic operations, and recent collision history. Supporting details are provided in the appendix.

### Project Site

The 1.30 acre project site is located on the southeast corner of the Boones Ferry Road/95<sup>th</sup> Avenue intersection in Wilsonville, Oregon, and is currently vacant. The development will access 95<sup>th</sup> Avenue using the driveway that serves the existing Chevron gas station and Holiday Inn.

### Study Area Roadway Network

Key study area roadways are listed in Table 3 along with their functional classifications and other important roadway characteristics. The functional classifications for City of Wilsonville streets are provided in the City of Wilsonville Transportation System Plan (TSP).<sup>6</sup> The three main functional classes are local (more access but less mobility), collector (balanced access and mobility), and arterial (less access but more mobility).

**Table 3: Study Area Roadway Characteristics**

Roadway	Wilsonville Classification	Cross Section	Posted Speed	On-Street Parking	Sidewalks	Bike Lanes
Boones Ferry Road <sup>a</sup>	Major Arterial	4-5 Lanes	35 mph	No	Yes	Yes
Elligsen Road <sup>a</sup>	Major Arterial	4-5 Lanes	35 mph	No	Yes	Yes
95 <sup>th</sup> Avenue	Minor Arterial	3 Lanes	35 mph	No	Both Sides	Yes
Ridder Road	Major Collector	3 Lanes	35 mph	Yes	Yes	No
Day Road	Major Collector	3 Lanes	35 mph	No	Yes	Yes
Commerce Circle	Local	2 Lanes	25 mph	No	No	No

<sup>a</sup> The portions of Boones Ferry Road and Elligsen Road in the study area are under ODOT jurisdiction.

### Existing Traffic Volumes and Operations

Existing p.m. peak hour traffic operations were analyzed at the six existing study intersections, which were selected in consultation with City of Wilsonville staff<sup>7</sup>:

- Elligsen Road/I-5 Southbound Ramp
- Boones Ferry Road/Day Road
- Boones Ferry Road/95<sup>th</sup> Avenue
- 95<sup>th</sup> Avenue/Commerce Circle (southern intersection)
- 95<sup>th</sup> Avenue/Ridder Road
- 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access

<sup>6</sup> City of Wilsonville Transportation Systems Plan, Figure 4.8, Adopted June 2, 2003.

<sup>7</sup> Mike Ward, City of Wilsonville, February 22, 2012.



Traffic volume turn movement counts were performed at the study intersections for the p.m. peak period (i.e., between 3:00 p.m. and 5:00 p.m.) on January 18, 2012, or March 8, 2012. Balancing adjustments were performed at the intersections following typical traffic engineering practices. The p.m. peak hour traffic volumes analyzed under existing conditions are shown in Figure 2, and the detailed two-hour traffic counts are included in the appendix.

The purpose of intersection analysis is to ensure that the transportation network remains within desired performance levels as required by the City code. Intersections are the focus of the analysis because they are the controlling bottlenecks of traffic flow and the ability of a roadway system to carry traffic efficiently is nearly always diminished in their vicinity.

Before the analysis results of the study intersections are presented, discussion is provided for two important analysis issues: intersection performance measures (definitions of typical measures) and required operating standards (as specified by the agency with roadway jurisdiction).

### ***Intersection Performance Measures***

Level of service (LOS) ratings and volume-to-capacity (v/c) ratios are two commonly used performance measures that provide a good picture of intersection operations. In addition, they are often incorporated into agency mobility standards.

- **Level of service (LOS):** A “report card” rating (A through F) based on the average delay experienced by vehicles at the intersection.<sup>8</sup> LOS A, B, and C indicate conditions where traffic moves without significant delays over periods of peak hour travel demand. LOS D and E are progressively worse operating conditions. LOS F represents conditions where average vehicle delay has become excessive and demand has exceeded capacity. This condition is typically evident in long queues and delays.
- **Volume-to-capacity (v/c) ratio:** A decimal representation (typically between 0.00 and 1.00) of the proportion of capacity that is being used at a turn movement, approach leg, or intersection. It is determined by dividing the peak hour traffic volume by the hourly capacity of a given intersection or movement. A lower ratio indicates smooth operations and minimal delays. As the ratio approaches 1.00, congestion increases and performance is reduced. If the ratio is greater than 1.00, the turn movement, approach leg, or intersection is oversaturated and usually results in excessive queues and long delays.

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<sup>8</sup> A description of Level of Service (LOS) is provided in the appendix and includes a list of the delay values (in seconds) that correspond to each LOS designation.

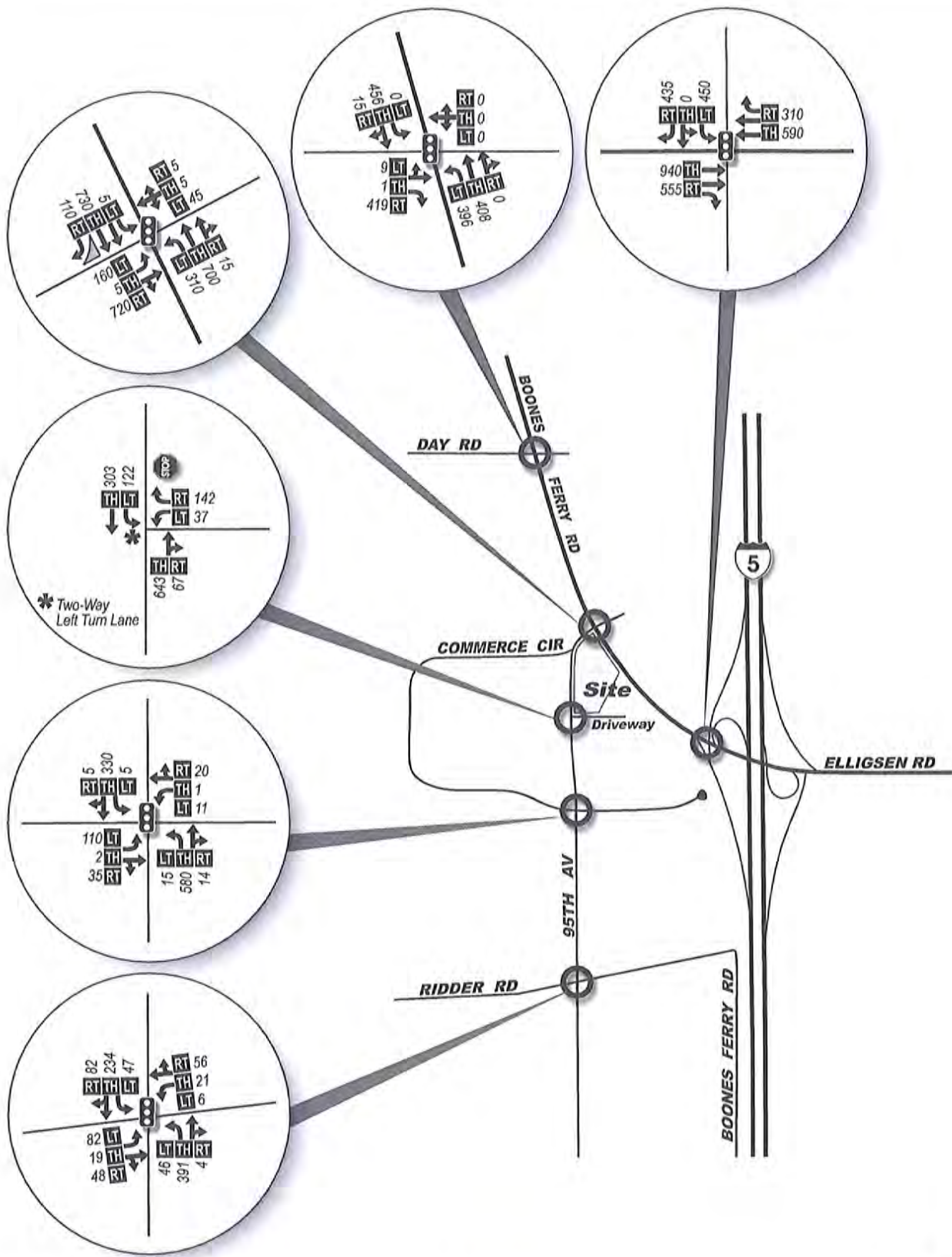


Figure 2

**EXISTING 2012  
PM PEAK HOUR  
TRAFFIC VOLUMES**

**LEGEND**

- Study Intersection
- Traffic Signal
- Stop Sign
- Lane Configuration
- 000* - PM Peak Hour Traffic Volumes
- Volume Turn Movement

**DKS**

No Scale



### Required Operating Standards

The City of Wilsonville requires all study intersections of public streets to meet its minimum acceptable level of service (LOS) standard, which is LOS D for peak periods.<sup>9</sup> While private driveway approaches are not required by City code to meet the City’s LOS standard, lower congestion levels are preferred.

In addition, the study intersections on Boones Ferry Road and Elligsen Road are also required to meet Oregon Department of Transportation (ODOT) *Oregon Highway Plan (OHP)* mobility standards.<sup>10</sup> The *OHP* indicates that I-5 interchange ramps are required to operate with v/c ratios that are no greater than 0.85 and the Boones Ferry Road intersections are required to operate with v/c ratios that are no greater than 0.99.

### Existing Operating Conditions

The existing traffic operations at the study intersections were determined for the p.m. peak hour based on the *2000 Highway Capacity Manual* methodology<sup>11</sup> for signalized and unsignalized intersections. The estimated average delay, level of service (LOS), and volume to capacity (v/c) ratio of each study intersection are shown in Table 4. The Boones Ferry Road/95<sup>th</sup> Avenue intersection does not currently meet the City of Wilsonville’s level of service (LOS) “D” operating standard. However, an improvement project at this intersection is currently under contract and expected to be completed later in 2012.

**Table 4: Existing Study Intersection Operations (P.M. Peak Hour)**

Intersection	Operating Standard	Existing P.M. Peak Hour		
		Delay	LOS	V/C
<b>Signalized</b>				
Elligsen Road/I-5 Southbound Ramp	LOS D, 0.85 v/c	14.8	B	0.52
Boones Ferry Road/Day Road	LOS D, 0.99 v/c	19.8	B	0.64
Boones Ferry Road/95 <sup>th</sup> Avenue	LOS D, 0.99 v/c	59.5	<b>E</b>	0.74
95 <sup>th</sup> Avenue/Commerce Circle (south)	LOS D	7.8	A	0.57
95 <sup>th</sup> Avenue/Ridder Road	LOS D	5.5	A	0.41
<b>Unsignalized</b>				
95 <sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access	N/A <sup>a</sup>	25.1	B/D	0.54
<u>Signalized Intersections:</u> LOS = Level of Service of Intersection V/C = Volume-to-Capacity Ratio of Intersection		<u>Unsignalized Intersections:</u> LOS = Level of Service of Major Street/Minor Street V/C = Volume-to-Capacity Ratio of Worst Movement		
<b><u>Bold Underlined</u></b> values do not meet standards.				

<sup>a</sup> The City’s LOS D standard does not apply to private driveways or roadway approaches; however, LOS D operations or better are preferred (N/A = Not Applicable).

<sup>9</sup> *City of Wilsonville Code*, City of Wilsonville Section 4.140, p.163.

<sup>10</sup> *1999 Oregon Highway Plan*, Oregon Department of Transportation, 1999; Policy 1F (as amended 12/21/2011); Table 7 for typical highway segments and page 79 for interchange ramps.

<sup>11</sup> *2000 Highway Capacity Manual*, Transportation Research Board, Washington DC, 2000.



### Field Observations

Field observations were performed during the p.m. peak hour to observe traffic operations and queuing in the vicinity of the project site.<sup>12</sup> During the visit, significant northbound queues were observed on 95<sup>th</sup> Avenue at the Boones Ferry Road intersection. Even though these queues extended past the Chevron/Holiday Inn driveway, many drivers on 95<sup>th</sup> Avenue were mindful of driveway traffic and stopped to allow entering and exiting maneuvers. Without the yielding by vehicles on 95<sup>th</sup> Avenue, delays could have been much higher for driveway traffic.

The full length of the northbound queues on 95<sup>th</sup> Avenue was approximately 1,500 feet (i.e., extending south past the 95<sup>th</sup> Avenue/Commerce Circle traffic signal). The current improvement project being constructed at the Boones Ferry Road/95<sup>th</sup> Avenue intersection is expected to address this northbound queuing issue.

Observations were also made of the shared Chevron gas station and Holiday Inn driveway, which is shown in Figure 3. One of the primary concerns was the improper use of the Holiday Inn driveway, which includes ingress and egress lanes that are separated by a raised median (see "1"). Multiple drivers were observed entering using the egress lane or exiting using the ingress lane.



**Figure 3: Existing Chevron Gas Station and Holiday Inn Driveway Concerns**

Another concern was how the driveway shifts (see "2"), which often resulted in drivers crossing over the stripped center-line, particularly the westbound left-turn drivers who were avoiding the right-turn queue. Significant queuing has also been observed during the height of the peak period, with exiting queues that back up on the driveway from 95<sup>th</sup> Avenue to the gas station pumps.

### Collision History

The collision histories of the study intersections were obtained for 2007 through 2010 from the Oregon Department of Transportation (ODOT) Crash Analysis and Reporting Unit. Based on the collision data and peak hour traffic counts, collision rates were estimated at the study intersections and are shown in Table 5 along with the breakdown of collisions by severity. As shown, between 2007 and 2010 there were no fatal collisions reported. Also, a collision rate greater than or equal to 1.0 collision per million entering vehicles (MEV) generally indicates a

<sup>12</sup> Traffic operations at the site driveways were observed on January 18, 2012, from 3:30 to 4:30 p.m.

higher than average collision rate and that additional collision analysis should be performed. However, none of the study intersections had a collision rate in excess of 1.0 collision per MEV.

**Table 5: Study Intersection Collisions (2006-2010)**

Intersection	Collisions (by Severity)				Collisions Per year	Collision Rate <sup>b</sup>
	Fatal	Injury	PDO <sup>a</sup>	Total		
<b>Signalized</b>						
Elligsen Road/I-5 Southbound Ramp	0	10	9	19	4.75	0.41
Boones Ferry Road/Day Road	0	5	4	9	2.25	0.37
Boones Ferry Road/95 <sup>th</sup> Avenue	0	6	8	14	3.50	0.34
95 <sup>th</sup> Avenue/Commerce Circle (south)	0	3	2	5	1.25	0.31
95 <sup>th</sup> Avenue/Ridder Road	0	2	8	10	2.50	0.68
<b>Unsignalized</b>						
95 <sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access	0	1	0	1	0.25	0.05

<sup>a</sup> PDO = Property damage only.

<sup>b</sup> Collision rate = average annual collisions per million entering vehicles (MEV); MEV estimates based on p.m. peak-hour traffic count and applicable factors.

## Public Transit Service

South Metro Area Regional Transit (SMART) operates several fixed routes that serve Wilsonville and the surrounding area.<sup>13</sup> Route 5 runs north-south along 95<sup>th</sup> Avenue and has bus stops on Commerce Circle in the project vicinity. Route 5 also connects to SMART Central, where connections from this route can be made to all other SMART routes, to the Westside Express Service (WES) commuter rail, and to other regional transit systems. At Commerce Circle, connections can also be made to TriMet Route 96, which provides service to downtown Portland.

<sup>13</sup> South Metro Area Regional Transit (SMART) operates several fixed routes that serve Wilsonville and make connections to TriMet in Portland, Cherriots in Salem, and Canby Area Transit. The City's transit center, "SMART Central at Wilsonville Station," provides connections to all SMART routes and to TriMet's Westside Express Service (WES) commuter rail station. Besides the new transit center, the other main transfer locations are the Tualatin Park and Ride (Route 2X), Barbur Boulevard Transit Center (Route 2X), Salem Transit Center (Route 1X), and Canby Transit Center (Route 3). In addition, Route 4 provides service on Wilsonville Road and Town Center Loop connecting the east and west city limits, Route 5 runs along the west side of I-5 between SMART Central and Commerce Circle, and Route 6 runs along the east side of I-5 between SMART Central and Argyle Square, serving major employment sites. SMART also operates a shuttle service to and from Villebois and SMART Central as well as a dial-a-ride system that operates on a demand-responsive basis.



## CHAPTER 3: PROJECT IMPACTS

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This chapter reviews the impacts that the proposed Carl's Jr. Restaurant and retail space would have on the study area transportation system in the City of Wilsonville. The focus of the impact analysis is on the study intersections identified by City of Wilsonville staff. These study intersections include the following:

- Elligsen Road/I-5 Southbound Ramp
- Boones Ferry Road/Day Road
- Boones Ferry Road/95<sup>th</sup> Avenue
- 95<sup>th</sup> Avenue/Commerce Circle (southern intersection)
- 95<sup>th</sup> Avenue/Ridder Road
- 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access

Although the development would generate traffic throughout the week, the weekday p.m. peak hour was the main period analyzed since this is when the greatest impact is expected (the sum of project traffic and traffic on adjacent streets is generally greatest during this period). The p.m. peak hour analysis includes trip generation, trip distribution, and future year traffic volumes and operating conditions. This chapter also discusses access issues (i.e., location, spacing, and intersection sight distance) and a site evaluation (i.e., internal vehicular circulation and pedestrian facilities and on-site parking). First, the proposed development is described.

### Proposed Development

Based on information provided by the project sponsor, the proposed development site is 1.30 acres and includes a 2,786 square-foot Carl's Jr. Restaurant (including a drive-up window and indoor seating). In addition, the development includes a 3,375 square-foot retail business at the north end of the site. The development will access 95<sup>th</sup> Avenue using the driveway that serves the existing Chevron gas station and Holiday Inn, and driveway modifications will be constructed as part of the site development.

### Trip Generation and Distribution

Trip generation and distribution were performed for the proposed project site to analyze the impacts to the additional traffic to the transportation network. Trips levels through the City's two I-5 interchanges were also estimated.

#### *Trip Generation*

Trip generation is the method used to estimate the number of vehicles that are added to the site driveways and roadway network by the proposed development during a specified period (i.e., such as the p.m. peak hour). The trip rates used to estimate new p.m. peak hour project trips levels were obtained from the Institute of Transportation Engineers (ITE).<sup>14</sup>

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<sup>14</sup> *Trip Generation, 8<sup>th</sup> Edition*, Institute of Transportation Engineers, 2008

- P.M. peak hour trip generation estimates were performed for the proposed 2,786 square-foot Carl's Jr. restaurant using the ITE rates for Fast Food with Drive-Thru (Land Use Code 934).
- P.M. peak hour trip generation estimates were performed for the proposed 3,375 square-foot retail building on the north end of the project site assuming the building develops as a Specialty Retail Center (ITE Land Use Code 814), which generally consists of a small strip shopping center containing a variety of retail shops that typically specialize in apparel, hard goods and services such a real estate and investment offices, dance studios, florists, and small restaurants. If more intensive uses are proposed for the strip mall than assumed by this traffic study, additional analysis will be needed.

A 10-percent internal trip rate was assumed to account for those customers who are visiting multiple land uses on the site (i.e., those who “trip chaining” by traveling between the Carl's Jr., the specialty retail, the Chevron gas station, and/or Holiday Inn). In addition, both of the proposed land uses attract pass-by trips, which were assumed to be drawn from 95<sup>th</sup> Avenue based on the existing directional (northbound/southbound) traffic volume split.

Table 6 lists the project’s trip generation assumptions. The total site is expected to generate 124 (62 in, 62 out) p.m. peak hour trips. Of these trips, 12 (6 in, 6 out) are expected to be internal trips and 52 (26 in, 26 out) are expected to be pass-by trips. Therefore, the site is expected to add 60 (30 in, 30 out) new p.m. peak hour trips to the study area roadway network.

**Table 6: Carl's Jr. and Retail Center Trip Generation**

Land Use (ITE Code)	Size	P.M. Peak Hour Trip Generation Rate	P.M. Peak Hour Trips		
			In	Out	Total
Fast-Food Restaurant w/Drive-Through Window (934)	2.79 KSF <sup>a</sup>	33.84 trips/KSF <sup>a</sup>	49	45	94
Specialty Retail Center (814)	3.38 KSF <sup>a</sup>	8.88 trips/KSF <sup>a</sup>	13	17	30
<b>Total Trips</b>			<b>62</b>	<b>62</b>	<b>124</b>
<i>Internal Trips</i>			-6	-6	-12
<b>Driveway Trips</b>			<b>56</b>	<b>56</b>	<b>112</b>
<i>Pass-by Trips</i>			-26	-26	-52
<b>New Primary Trips</b>			<b>30</b>	<b>30</b>	<b>60</b>

<sup>a</sup> KSF = 1,000 square feet

### **Trip Distribution**

The trip distribution for the new project traffic was generally based on existing traffic patterns in the study area and is shown in Figure 4. This figure also shows how the resulting project traffic volumes were routed onto the nearby roadway network based on the trip generation and distribution estimates as well as the pass-by trip assumptions.



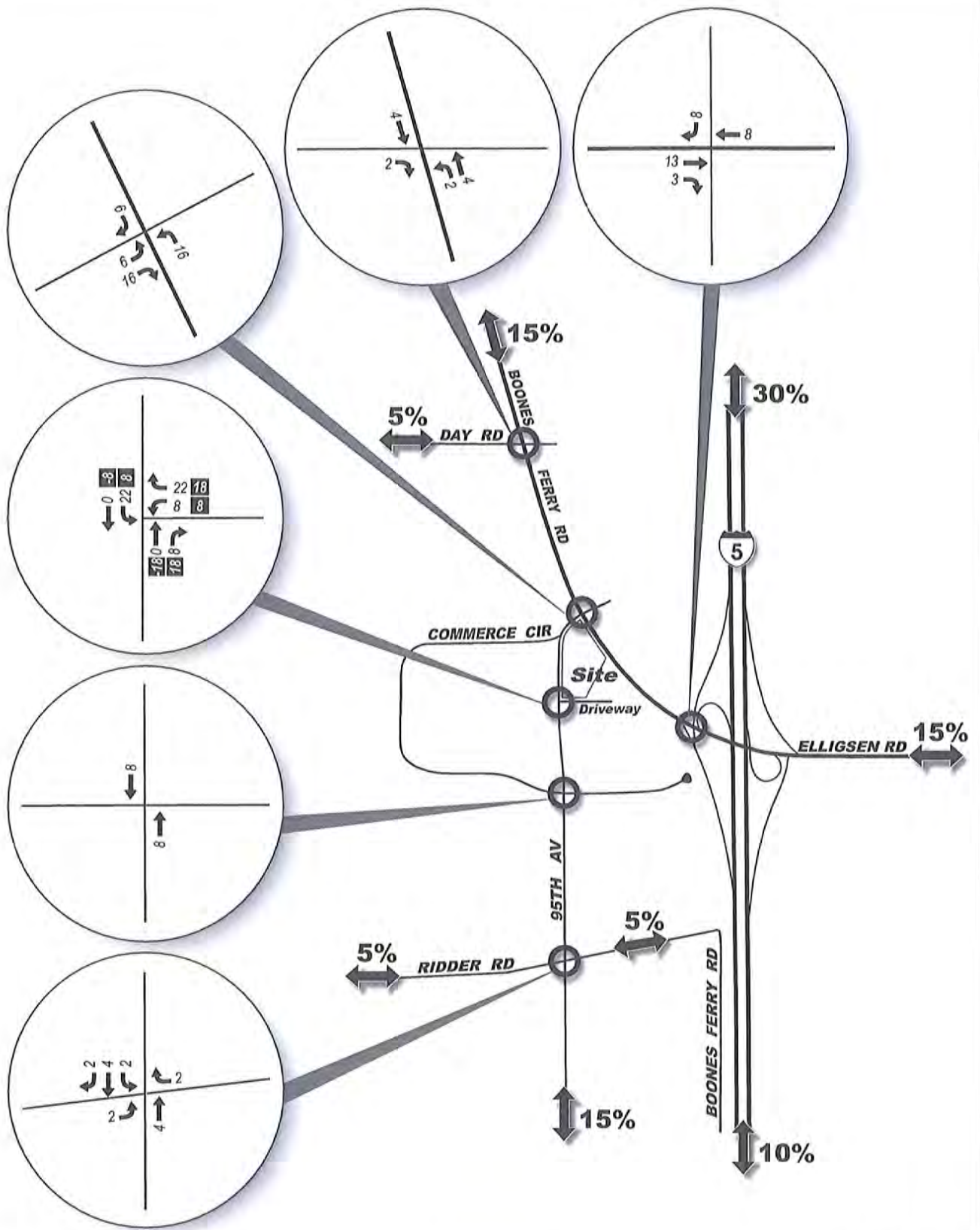


Figure 4

**LEGEND**

- Study Intersection
- Trip Distribution Percentage
- Primary PM Peak Hour Project Volume
- Pass-by PM Peak Hour Project Volume

**DKS**

No Scale

**PROJECT TRAFFIC  
PM PEAK HOUR VOLUMES &  
TRIP DISTRIBUTION**

### **Project Trips through City of Wilsonville Interchange Areas**

The project trips through the two City of Wilsonville I-5 interchange areas (i.e., at Wilsonville Road<sup>15</sup> and Elligsen Road) were estimated based on the trip generation and distribution discussed previously. The proposed Carl's Jr. and retail space would generate 32 p.m. peak hour trips through the I-5/Elligsen Road interchange area. The I-5/Wilsonville Road interchange area would not be impacted by this development.

### **Future Traffic Volumes and Operations**

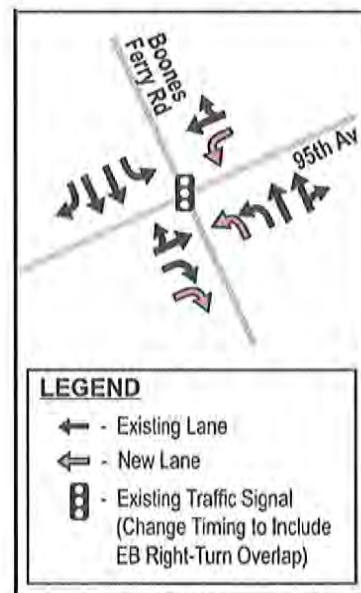
Future operating conditions were analyzed at the study intersections for three traffic scenarios:

- Existing + Project (includes traffic from Carl's Jr. and north retail building)
- Existing + Stage II (includes traffic from other developments that have Stage II approval or are under construction)
- Existing + Project + Stage II

The three future traffic scenarios also assumed the completion of the Boones Ferry Road/95<sup>th</sup> Avenue intersection improvement project, which is currently under contract and expected to be completed later this year. Figure 5 shows a schematic of the planned roadway geometry and traffic control improvements.

Future traffic volumes were estimated at the study intersections for each scenario. The future operating scenarios include various combinations of three types of traffic: existing, project, and stage II. Existing and project traffic have been explained previously in this report. Stage II traffic is estimated based on the list of currently approved Stage II developments, which was provided by City staff.<sup>16</sup> This list and the corresponding p.m. peak hour trip generation estimates for these developments are included in the appendix.

The weekday p.m. peak hour traffic volumes used to analyze the "Existing plus Stage II" and "Existing plus Stage II plus Project" scenarios are shown in Figure 6. The analysis of these scenarios enables the assessment of project impacts.

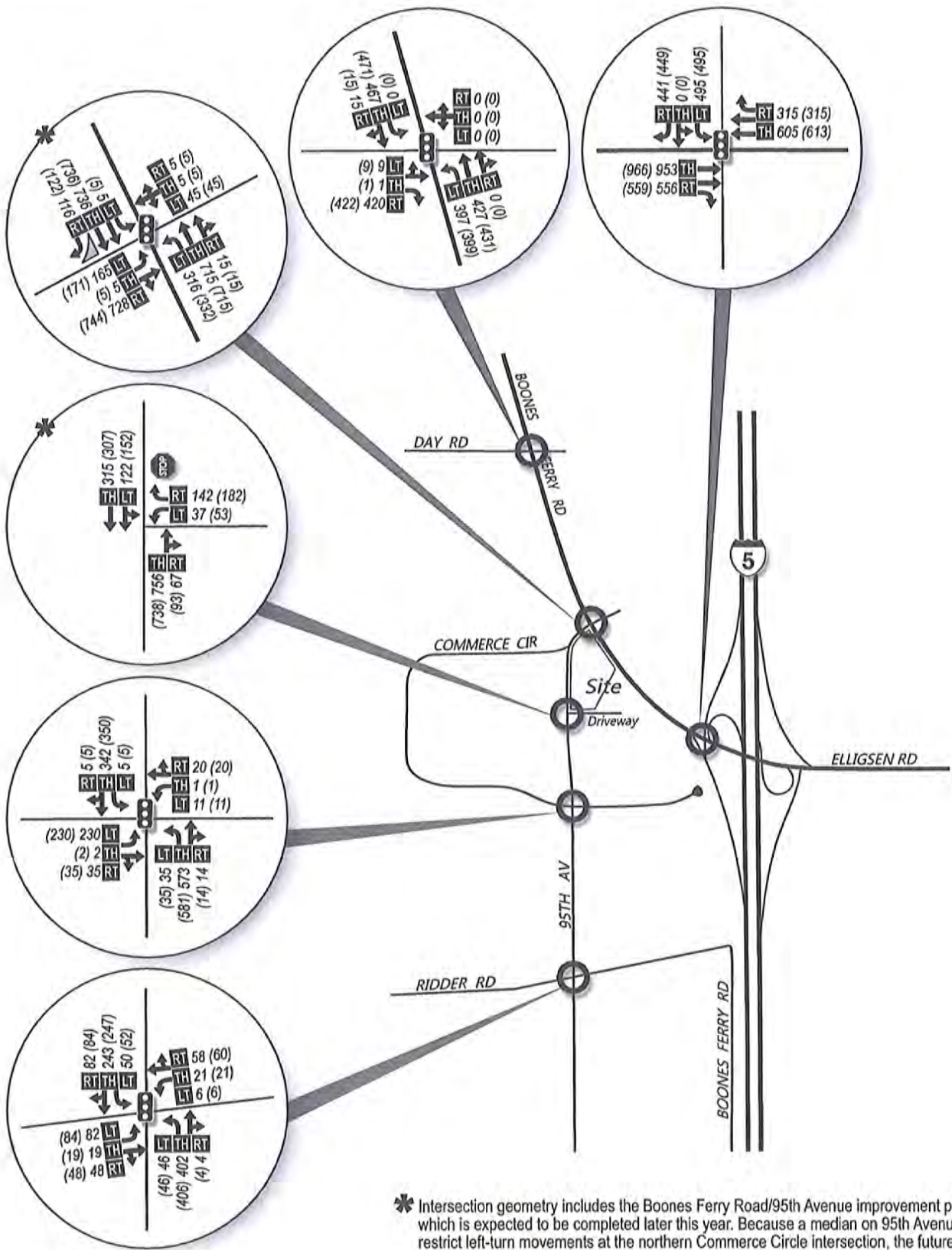


**Figure 5: Boones Ferry Rd/95<sup>th</sup> Ave Intersection Improvement Project**

<sup>15</sup> The I-5/Wilsonville Road interchange area includes the Wilsonville Road/Boones Ferry Road and Wilsonville Road/Town Center Loop West intersections.

<sup>16</sup> Email from Blaise Edmonds, City of Wilsonville, March 8, 2012 (see appendix for Stage II list).





\* Intersection geometry includes the Boones Ferry Road/95th Avenue improvement project, which is expected to be completed later this year. Because a median on 95th Avenue will restrict left-turn movements at the northern Commerce Circle intersection, the future analysis rerouted left-turn volumes to the southern Commerce Circle intersection.

**LEGEND**

- Study Intersection
- Traffic Signal
- Stop Sign
- Lane Configuration
- 000 (000) - Existing + Stage II (+ Project) PM Peak Hour Traffic Volumes
- Volume Turn Movement Left-Thru-Right

**DKS**

**Figure 6**

**EXISTING PLUS STAGE II (PLUS PROJECT) PM PEAK HOUR TRAFFIC VOLUMES**

No Scale



The study intersection operating conditions for the three future scenarios are listed in Table 7. The study intersections at public streets (i.e., excluding the project driveway) are expected to meet the City's and ODOT's applicable operating standards. Due to the current improvement project, the Boones Ferry Road/95<sup>th</sup> Avenue intersection would have improved operations compared to existing, unimproved conditions.

**Table 7: Future Project and Stage II Intersection Operations Comparison**

Intersection	Operating Standard	Existing + Project			Existing + Stage II			Existing + Stage II + Project		
		Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C
<b>Signalized</b>										
Elligsen Rd/I-5 SB Ramp	LOS D, 0.85 v/c	14.9	B	0.54	15.2	B	0.54	15.3	B	0.56
Boones Ferry Rd/Day Rd	LOS D, 0.99 v/c	19.3	B	0.65	19.1	B	0.65	19.3	B	0.65
Boones Ferry Rd/95 <sup>th</sup> Ave	LOS D, 0.99 v/c	20.3	C	0.55	20.1	C	0.55	20.4	C	0.56
95 <sup>th</sup> Ave/Commerce Cir	LOS D	11.7	B	0.68	11.8	B	0.69	11.9	B	0.69
95 <sup>th</sup> Ave/Ridder Rd	LOS D	5.5	A	0.42	5.5	A	0.42	5.5	A	0.43
<b>Unsignalized</b>										
95 <sup>th</sup> Ave/Holiday Inn-Chevron Gas Access	N/A <sup>a</sup>	>50	B/F	>1.0	>50	B/F	>1.0	>50	B/F	>1.0
<u>Signalized Intersections:</u> LOS = Level of Service of Intersection V/C = Volume-to-Capacity Ratio of Intersection					<u>Unsignalized Intersections:</u> LOS = Level of Service of Major Street/Minor Street V/C = Volume-to-Capacity Ratio of Worst Movement					
<b>Bold Underlined</b> values do not meet standards.										

<sup>a</sup> The City's LOS D standard does not apply to private driveways or roadway approaches; however, LOS D operations or better are preferred.

Because the project driveway is not a public street, it is not required to meet the City's LOS D operating standards. However, the City's primary concern is ensuring that driveway operations do not negatively impact the City's public streets or public safety. Long delays and queuing at the private access can lead to undesired driver behavior. Because additional driveway traffic is being added by the proposed project, delays for outbound traffic are expected to worsen to greater than 50 seconds per vehicle.

The constraints of the project site and adjacent roadway limit the ability to fully resolve the minor street delay concerns. However, delays can be minimized if sufficient storage is provided for both the left-turn and right-turn lanes exiting the driveway. Under the driveway's current configuration, there is only sufficient room (approximately 50 feet) for up to two left-turning vehicles to wait in the short left-turn lane. The proposed site plan extends the site's exiting turn lanes so that it is more than three times longer (approximately 150 feet) and would accommodate up to six vehicles.



### **Traffic Simulation**

Traffic simulation was performed in SimTraffic™ to evaluate the northbound queuing on 95<sup>th</sup> Avenue and provide additional estimates regarding the delay at the project driveway. The simulations suggest that minimal queuing will occur on 95<sup>th</sup> Avenue under the “Existing + Project + Stage II” traffic scenario once the 95<sup>th</sup> Avenue/Boones Ferry Road intersection improvements are completed. These queues are not expected to extend back past the project driveway, which will allow free-flow movement on 95<sup>th</sup> Avenue adjacent to the project site. While free-flow movement means that queues are not blocking the driveway, it also means that vehicles entering and exiting the site are not able to benefit from drivers in those queues who stop and allow vehicles to enter and exit the site.

On the driveway approach to 95<sup>th</sup> Avenue, the traffic simulation suggests that the average vehicle delay at the project driveway was between 45 and 50 seconds, which falls in the level of service E range. This only slightly differs from than the LOS F calculated from the operations analysis. Therefore, during the most congested periods, traffic leaving the project site can expect to experience longer delays than exist today even though they will not be blocked by long northbound queues. These higher delays are due primarily to the additional traffic from the proposed development.

### **Site Plan Evaluation**

A site plan showing the proposed buildings and site layout was provided by the project sponsor and is included in the appendix. This site plan was evaluated with consideration for site access, internal vehicular circulation, pedestrian facilities, and parking.

### **Site Access**

Because the section of 95<sup>th</sup> Avenue along the site frontage is classified as a Minor Arterial, the minimum access spacing required by the City is 600 feet.<sup>17</sup> Due to the proximity of the 95<sup>th</sup> Avenue/Boones Ferry Road intersection, the site will only be able to meet the access spacing requirements by using the existing access that is shared by Chevron and Holiday Inn. This access is partially located on each of the three properties. Therefore, coordination with the Chevron and Holiday Inn properties will be an important component of any driveway improvements.

As identified previously in the report (see Figure 3, Table 7, and the discussions that accompany the figure and table), there are multiple concerns with the existing driveway configuration, including the sharp curve, improper use of internal driveways, limited queue storage, and high delays for exiting vehicles. By improving the driveway as part of the Carl’s Jr. site development, the proposed project will provide a significant safety and operational benefits to all users. The proposed site plan assumes reconstruction of the driveway, which includes the proposed driveway improvements that were previously coordinated with City staff and DKS Associates.<sup>18</sup> This coordination was based on a prior memorandum that was prepared to evaluate access alternatives. A copy of this memorandum is provided in the appendix.

<sup>17</sup> City of Wilsonville Transportation Systems Plan, Table 4.o (Access Management Guidelines), Adopted June 2, 2003.

<sup>18</sup> Meeting with DKS, City of Wilsonville engineering staff, and Carl’s Jr. project representatives, April 11, 2012, and follow up emails.



Upon reconstruction of the driveway, assurance should be provided that the site driveway meets American Association of State Highway and Transportation Officials (AASHTO) intersection sight distance requirements<sup>19</sup> as measured from 14.5 feet back from the edge of pavement. Based on 95<sup>th</sup> Avenue's 35 mph speed limit, the site driveway would require a minimum of 390 feet of sight distance. The sight triangles at the driveways should be clear from vegetation, signs, and other obstructions that may limit intersection sight distance. Prior to occupancy, intersection sight distance at the site driveway will need to be verified, documented, and stamped by a registered professional Civil or Traffic Engineer licensed in the State of Oregon.

### ***Internal Vehicular Circulation***

In addition to evaluating site access, a review of internal vehicular circulation was also performed for the site. Three potential concerns were identified:

- Vehicles leaving the drive-thru lane and navigating toward the exit would be required to make a sharp 180-degree right turn. Turn templates should be provided by the applicant to verify that this turn can be executed without forcing vehicles into oncoming traffic.
- If the drive-thru vehicle queue extends beyond the designated drive-thru area, then the queued vehicles could impede circulation of vehicles accessing the retail facility.
- It is not clear how truck maneuvers would be accommodated at the driveway to 95<sup>th</sup> Avenue and within the project site. Turn templates should be provided by the applicant to show that adequate turning radius will be available on the northeast corner of the 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas driveway to allow right-turning trucks to stay within the northbound travel lane on 95<sup>th</sup> Avenue (instead of requiring them to either mount the curb or use an opposing southbound travel lane). Turn templates may also be helpful to show the likely on-site truck maneuvering.

### ***Pedestrian Facilities***

Sidewalks along the north side of the driveway allow pedestrians to safely access the proposed developments. In addition, the site plan identifies a pedestrian refuge that would be installed at the driveway and would separate ingress and egress movements to the site. This pedestrian island is needed due to the wide driveway width; however, the site plan depicts stop bars for the egress lanes that would encroach on the area provided for the pedestrian crossing. These stop bars should be moved back behind the pedestrian crossing to reduce pedestrian/vehicle conflicts.

### ***Parking***

The proposed Carl's Jr. restaurant and the adjacent retail building are required to comply with the City of Wilsonville Planning and Land Development code for the number of vehicular

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<sup>19</sup> *Geometric Design of Highways and Streets*, AASHTO, 2004; Case B1, p. 661.

parking stalls and bicycle parking spaces that are provided on the site.<sup>20</sup> The requirements are based on the land use type and size.

Table 8 provides the vehicles parking requirements. Based on City Code requirements, the site would be required to have between 41 and 62 parking stalls. The site plan provided by the project sponsor shows 48 parking stalls, which falls within the range allowed by City code. The site plan also shows four additional parking stalls that would be constructed in the site vicinity but would be on the Chevron gas station property.

**Table 8: Vehicular Parking Requirements**

Land Use (Size)	Code Required Vehicular Parking Stalls (Min/Max)		Proposed Number of Stalls
	<i>Stalls per KSF</i>	<i>Total Stalls</i>	
Fast Food Restaurant with drive-thru (2.79 KSF)	9.9/KSF to 14.9/KSF	28 to 42	48
Commercial Retail (3.15 KSF)	4.1/KSF to 6.2/KSF	13 to 20	
<b>Total</b>		<b>41 to 62</b>	

Table 9 provides the bicycle parking requirements for the proposed development based on the same assumptions regarding the office versus warehouse use for the industrial space. The proposed development should ensure that it provides the minimum number of bicycle parking spaces required by City Code.

**Table 9: Bicycle Parking Requirements**

Land Use (Size)	Code Required Bicycle Parking Spaces	Minimum Number of Bicycle Parking Spaces Required by Code
Fast Food Restaurant with drive-thru (2.79 KSF)	Minimum of 4	4
Commercial Retail (3.15 KSF)	1 per 4 KSF, minimum of 2	2
<b>Total</b>		<b>6</b>

<sup>20</sup>City of Wilsonville, Planning and Land Development Ordinance, Sections 4.154-4.198, Table 5, Updated Jan. 2010.



## CHAPTER 4: MITIGATIONS AND RECOMMENDATIONS

To preserve the operations of the study area roadways and provide safe access to the proposed Carl's Jr. and surrounding land uses, it is recommended that a series of transportation mitigation measures be performed. The following project related measures would typically be required as conditions of approval if the project were approved:

### Site Access

- The site driveway should be reconstructed as shown on the site plan.
- The reconstructed site driveway should meet American Association of State Highway and Transportation Officials (AASHTO) intersection sight distance requirements<sup>21</sup> as measured from 14.5 feet back from the edge of pavement. Based on 95<sup>th</sup> Avenue's 35 mph speed limit, the site driveway would require a minimum of 390 feet of sight distance. The sight triangles at the driveways should be clear from vegetation, signs, and other obstructions that may limit intersection sight distance. Prior to occupancy, intersection sight distance at the site driveway will need to be verified, documented, and stamped by a registered professional Civil or Traffic Engineer licensed in the State of Oregon.

### Internal Vehicular Circulation

- Turn templates for passenger vehicles should be provided by the applicant to verify that vehicles leaving the drive-thru lane are able to make the sharp 180-degree right turn without extending into the oncoming traffic lanes.
- Turn templates for trucks should be provided by the applicant to show that adequate turning radius will be available on the northeast corner of the 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas driveway to allow right-turning trucks to stay within the northbound travel lane on 95<sup>th</sup> Avenue (instead of requiring them to either mount the curb or use an opposing southbound travel lane). Turn templates may also be helpful to show the likely on-site truck maneuvering.

### Pedestrian Facilities

- All sidewalks within the site should conform to ADA requirements.<sup>22</sup>
- Stop bars at the site egress should be located so that they stop vehicles prior to the pedestrian crossing to reduce pedestrian/vehicle conflicts.

### Parking

- Six bicycle parking spaces should be provided on the new site for it to be in compliance with City Code.

<sup>21</sup> *Geometric Design of Highways and Streets*, AASHTO, 2004; Case B1, p. 661.

<sup>22</sup> *ADA Accessibility Guidelines for Buildings and Facilities*, Department of Justice, January 2004.

# Appendix

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**Site Information**

**Wilsonville Stage II Project List**

**Weekday PM Peak Hour Traffic Counts**

**Level of Service Descriptions**

**HCM Analysis – Existing**

**HCM Analysis – Future**

**ODOT Collision Data**

**Trip Generation and Access Analysis Memorandum**



## Site Information

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**A TRAFFIC STUDY APPLICATION FOR:**

**CARL'S JR. RESTAURANT AND OTHER RETAIL/SERVICE ORIENTED SPACE**

REQUEST FOR: TRAFFIC STUDY

SUBMITTED TO: CITY OF WILSONVILLE  
**ATTN: DEPUTY CITY ENGINEER, STEVE ADAMS**  
COMMUNITY DEVELOPMENT DEPARTMENT  
ENGINEERING AND PLANNING DIVISIONS  
29799 SW TOWN CENTER LOOP EAST  
WILSONVILLE, OREGON 97070

APPLICANT/OWNER: JOSH VEENTJER, MANAGER  
SUNNYSIDE DEVCO, LLC  
PO BOX 916  
PORTLAND, OREGON 97207  
(503) 688-9600 OFFICE  
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AUTHORIZED REPRESENTATIVE: CHRIS KITTREDGE  
KITTREDGE ENGINEERS, LLC  
6565 SW 207<sup>TH</sup> AVENUE  
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(503) 708-3942  
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PROPERTY LOCATION: SE CORNER OF SW 95<sup>TH</sup> AND SW BOONES FERRY RD.

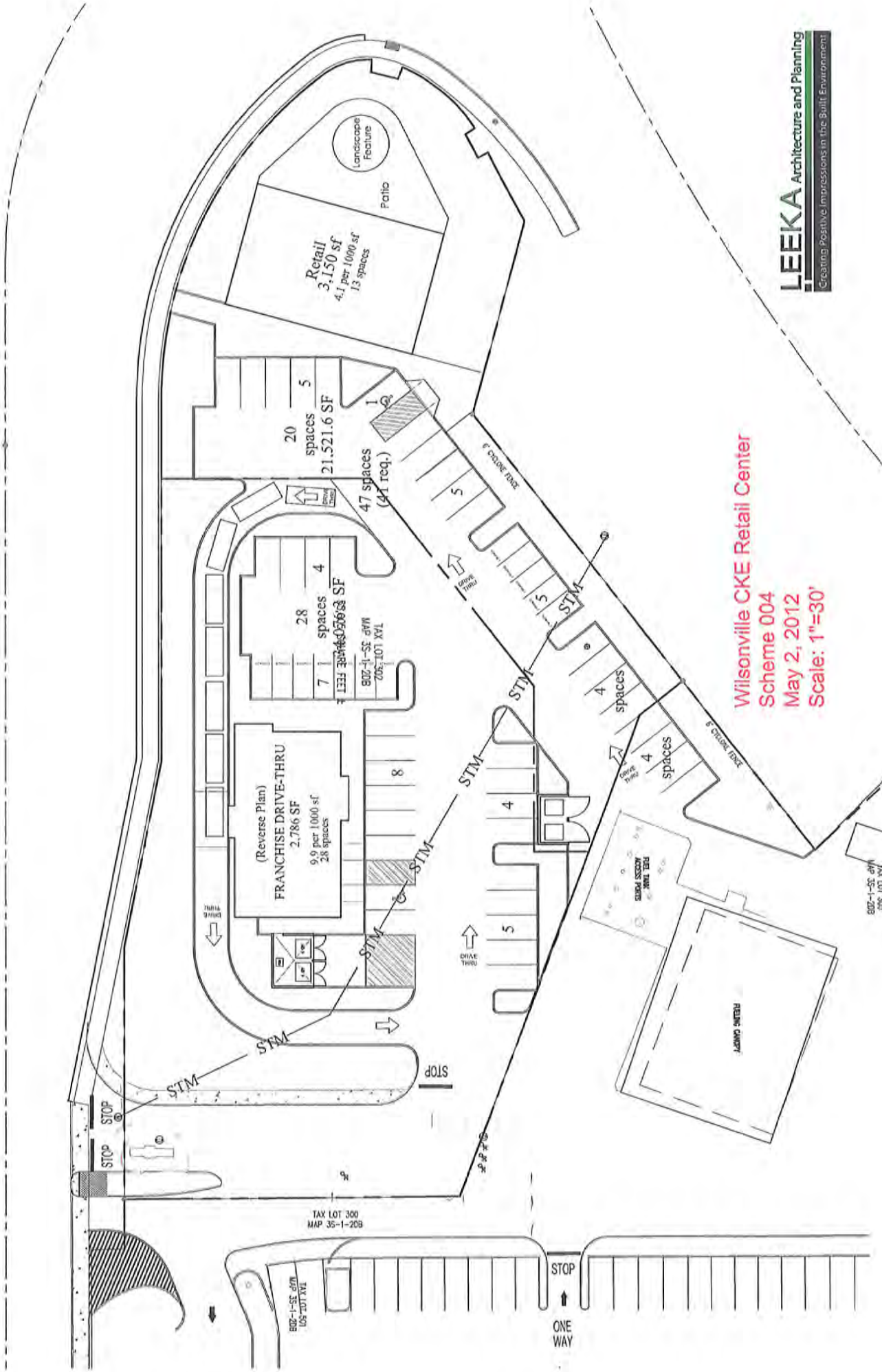
PARCEL INFO: WASHINGTON COUNTY, # R2087171

PROPERTY SIZE: +/- 1.30 ACRES

**I. DESCRIPTION OF PROPOSAL**

The applicant proposes to build a new Carl's Jr. Restaurant and other retail/service oriented building in Wilsonville. The subject site is approximately 1.30 acres and is located on the SE corner of SW 95<sup>th</sup> Avenue and SW Boones Ferry Road. The proposal includes a new +/- 2,786 Sf. restaurant building with drive-through and +/- 3,375 Sf. Retail/Service oriented building with complimentary landscaping, pedestrian connections, a bicycle parking rack, two mixed solid waste/recyclables facilities and an automobile parking area. As illustrated in the preliminary site plan, the proposed site plan design is in accordance with the Wilsonville Design Guidelines and Standards.





Wilsonville CKE Retail Center  
 Scheme 004  
 May 2, 2012  
 Scale: 1"=30'



# Wilsonville Stage II Project List

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**Wilsonville Planning Division  
Stage II Approved, Vested, and Other Projects**

Updated March 8, 2012

Stage II Approved									
Project	Land Use	Status	Size	Total PM Peak Trips	Trip Allocation Percentage		Net New (Primary) PM Peak Hour Trips		
					Internal	Pass-By	Diverted	In	Out
Ash Meadows	MFDU		22				14	7	21
Rivergreen (Phase 3)	SFDU		4				4	2	6
Mercedes Benz (Phase 2)	Auto Dealership	Not built					20	26	46
I-5 Corporate Park (In Focus)	2 story Office bldg W-1	Built, not occupied	70.0 KSF				17	80	97
Town Center Ph III and its trip dedication to Miller Paint store <i>Uses marked with "*" have not been built and their PM peak hour trip sum exceeds remaining vested trip level by 2 trips. It has yet to be determined how to allocate trips between remaining buildings.</i>	Wilsonville Town Center Office (Pad 5)	Built, not occupied	44.0 KSF				18	86	104
	*Fast Food Restaurant (Pad 2)	Not built	2.5 KSF				18	16	34*
	*High Turnover Restaurant (Pad 1)	Not built	7.5 KSF				24	17	41*
	*Miller Paint store	Not built	5.0 KSF				6	6	12*
	<b>Remaining Approved Total</b>								<b>189</b>
Cross Creek Subdivision	Residential	Lots for sale (6 of 13 lots built)	7 lots available				4	2	6
Hydro-Temp	Office/Flex-Space	Not built	60.8 KSF				7	4	11
Copper Creek	Residential	Not built	26 units				44	46	90
Chad Ward building on Kinsman	Manuf., warehouse, office & 5,000 SF retail bldg.	Not built	25.4 KSF				15	8	23
Joe Angel's retail (Wilsonville Retail) on Boones Ferry Rd	Retail (North Bldg)	Not built	11.2 KSF				11	41	52
	Bank (South Bldg)	Not built	3.2 KSF			26%	66	65	131
	<b>Total</b>					26%	53	58	111
Athey Creek Church	Convert existing Diatron Bldg to Church	5 year Temporary Use Permit (TUP)	39.6 KSF				119	123	242



**Wilsonville Planning Division  
Stage II Approved, Vested, and Other Projects**

Updated March 8, 2012

Stage II Approved										
Project	Land Use	Status	Size	Total PM Peak Trips	Trip Allocation Percentage		Net New (Primary) PM Peak Hour Trips			
					Internal	Pass-By	Diverted	In	Out	Total
Abele-Renaissance Subdivision	Residential (single-family)	Not built	33 units					21	12	33
Fred Meyer – Old Town Square	Fred Meyer building (with attached space)	Built and occupied	155.1 KSF	770	8%	15%	42%	158	162	320
	Retail (multiple buildings, one w/ some allowed restaurant use)	Mostly occupied	43.1 KSF	362	17%	15%	42%	54	63	117
	Restaurant (portion of existing church)	Remodeled as a Brew Pub	6.5 KSF	54	26%	15%	42%	10	3	13
	Residential (apartments)	Under construction	52 units	43	40%			16	10	26
Wilsonville Road Business Park <i>Land use sizes are slightly lower than analyzed in TIS. Applicant is requesting Stage I and Stage II approval for both phases and Site Design Review for Phase I only.</i>	<b>Total</b>			<b>43</b>	<b>12.6%</b>		<b>0 of 442 (221, 221)</b>	<b>16 of 238</b>	<b>10</b>	<b>26 of 476</b>
	Phase 1: Industrial, office, service/retail (4 buildings on east parcel)	Built but not occupied	89.8 KSF					13	87	100
	Phase 2 - office (2-story building on west parcel)	Not built	21.7 KSF					15	71	86
<b>Total</b>			111.5 KSF	186				<b>28</b>	<b>158</b>	<b>186</b>
Brenchley Estates Phase I, Parcel I <i>Unit quantities and types are slightly different than those analyzed in TIS, but they are sufficiently close that same trip generation is assumed for Stage II analysis. Also, just going to assume entire Phase I trip generation.</i>	Residential (apartments)	Under construction	324 units					156	84	240
	Clubhouse (residential use)	Under construction	5.4 KSF					N/A	N/A	N/A
	<b>Total</b>				240			<b>156</b>	<b>84</b>	<b>240</b>

**Wilsonville Planning Division  
Stage II Approved, Vested, and Other Projects**

Updated March 8, 2012

Stage II Approved										
Project	Land Use	Status	Size	Total PM Peak Trips	Trip Allocation Percentage			Net New (Primary) PM Peak Hour Trips		
					Internal	Pass-By	Diverted	In	Out	Total
TVFR station @ Elligsen Road <i>Current proposed use generates less p.m. peak hour trips than historical Community Development use (therefore, no Stage II trips added to analysis).</i>	Fire Station and Command Center	Under construction	19.5 KSF	5				0	0	0
The Boone Building	Dentist office, offices and retail.	Under construction	19.5 KSF	65				25	40	65
Mentor Graphics Data center	Data Center	Under Construction	19.7 KSF	0						
SMART	Fleet and operations building	Under construction	12,400	15						
Retherford Meadows SAP-2 East (Villebois)	Residential	Not built	88 lots	89						

**Wilsonville Planning Division  
Stage II Approved, Vested, and Other Projects**

Updated March 8, 2012

Vested Projects with trips through Interchange Areas										
Project	Land Use	Status	Size	Total PM Peak Trips	Trip Allocation Percentage			Net New (Primary) PM Peak Hour Trips		
					Internal	Pass-By	Diverted	In	Out	Total
Villebois	Mixed Use	Building IF built	N/A					266	144	309 WVIC
The Villebois approved projects as shown below are part of the 410 vested trips through the WV Road Interchange Area as shown above (309 trips based on occupied units in SAP-South Phases 1, 2, and 3)										
Villebois SAP-South Phases 2 and 3	Residential	Mostly built, lots for sale	121 units					74	41	115
Villebois SAP-East Phase 1	Residential	Lots for sale (33 of 190 units built)	190 units							
Villebois SAP-Central Phase 1	Residential	Mostly built, lots for sale	394 units							
Villebois SAP-Central Phase 2	Residential and 5 KSF commercial	Charleston built	114-134 (mid 124)							
Villebois SAP-North	Residential	Not built								
Villebois SAP-South Phase 5	Residential	Approved, Not built	27 units							
Villebois SAP-South Phase 6	Residential	22 houses of 81 lots built								
Villebois SAP North Phase 1	Residential	Not built – 144 lots								
Lowrie Primary School	Elementary School	Under construction								

**Wilsonville Planning Division  
Stage II Approved, Vested, and Other Projects**

Updated March 8, 2012

Projects Without Stage II Approval									
Project	Land Use	Status	Size	Total PM Peak Trips	Trip Allocation Percentage		Net New (Primary) PM Peak Hour Trips		
					Internal	Pass-By	Diverted	In	Out
Wilsonville – Old Town	Residential (Phases 1 and 2)	Land use permit expired	10 lots + 10 accessory units						
Brenchley Estates Phase I, Parcel 2	Residential	Not built	32 houses						



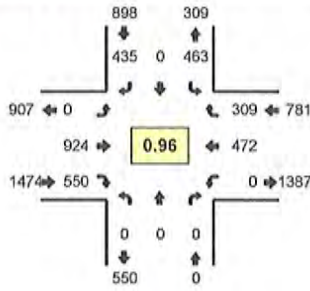
# **Weekday PM Peak Hour Traffic Counts**



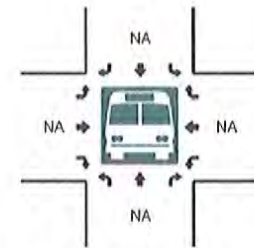
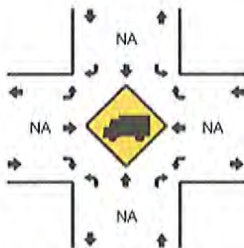
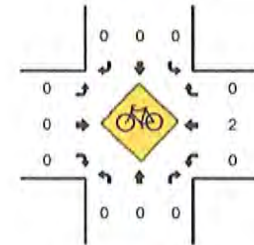
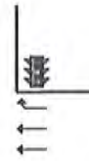
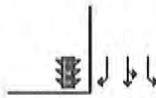
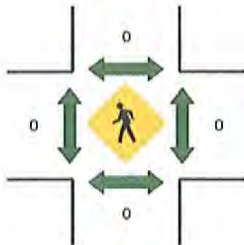
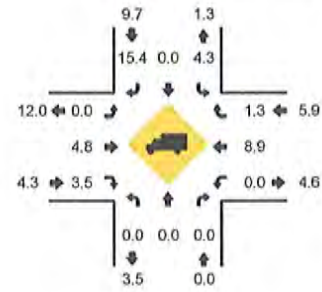


LOCATION: I-5 SB Ramps -- Elligsen Rd  
 CITY/STATE: Wilsonville, OR

QC JOB #: 10723302  
 DATE: Thu, Mar 08 2012



Peak-Hour: 4:00 PM -- 5:00 PM  
 Peak 15-Min: 4:45 PM -- 5:00 PM



5-Min Count Period Beginning At	I-5 SB Ramps (Northbound)				I-5 SB Ramps (Southbound)				Elligsen Rd (Eastbound)				Elligsen Rd (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:00 PM	0	0	0	0	27	0	29	0	0	63	31	0	0	35	28	0	213	
3:05 PM	0	0	0	0	33	0	30	0	0	72	35	0	0	35	21	0	226	
3:10 PM	0	0	0	0	43	0	48	0	0	51	54	0	0	34	24	0	254	
3:15 PM	0	0	0	0	34	0	27	0	0	43	51	0	0	39	22	0	216	
3:20 PM	0	0	0	0	24	0	37	0	0	54	30	0	0	40	19	0	204	
3:25 PM	0	0	0	0	50	0	42	0	0	55	30	0	0	31	16	0	224	
3:30 PM	0	0	0	0	30	0	26	0	0	73	36	0	0	38	23	0	226	
3:35 PM	0	0	0	0	50	0	35	0	0	64	50	0	0	45	23	0	267	
3:40 PM	0	0	0	0	36	0	29	0	0	71	70	0	0	39	21	0	266	
3:45 PM	0	0	0	0	33	0	37	0	0	74	61	0	0	43	21	0	269	
3:50 PM	0	0	0	0	33	0	36	0	0	61	64	0	0	33	19	0	246	
3:55 PM	0	0	0	0	42	0	32	0	0	62	58	0	0	45	27	0	266	2877
4:00 PM	0	0	0	0	35	0	28	0	0	80	49	0	0	31	19	0	242	2906
4:05 PM	0	0	0	0	31	0	42	0	0	80	56	0	0	31	35	0	275	2955
4:10 PM	0	0	0	0	49	0	28	0	0	73	49	0	0	37	26	0	262	2963
4:15 PM	0	0	0	0	36	0	30	0	0	64	50	0	0	35	30	0	245	2992
4:20 PM	0	0	0	0	30	0	41	0	0	68	53	0	0	40	27	0	259	3047
4:25 PM	0	0	0	0	38	0	30	0	0	71	33	0	0	49	26	0	247	3070
4:30 PM	0	0	0	0	34	0	33	0	0	85	30	0	0	49	23	0	254	3098
4:35 PM	0	0	0	0	48	0	31	0	0	102	33	0	0	37	32	0	283	3114
4:40 PM	0	0	0	0	49	0	44	0	0	57	50	0	0	41	25	0	266	3114
4:45 PM	0	0	0	0	30	0	42	0	0	78	47	0	0	37	14	0	248	3093
4:50 PM	0	0	0	0	38	0	45	0	0	83	55	0	0	40	29	0	290	3137
4:55 PM	0	0	0	0	45	0	41	0	0	83	45	0	0	45	23	0	282	3153
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	452	0	512	0	0	976	588	0	0	488	264	0	3280	
Heavy Trucks	0	0	0	0	28	0	40	0	0	44	24	0	0	32	8	0	176	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Bicycles	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Railroad																		
Stopped Buses																		

Comments:

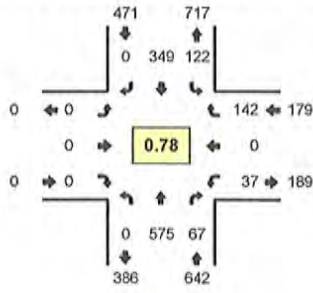


Type of peak hour being reported: Intersection Peak

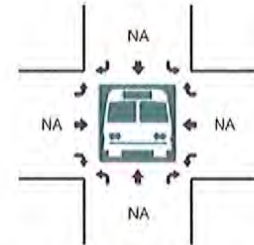
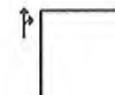
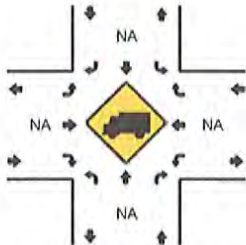
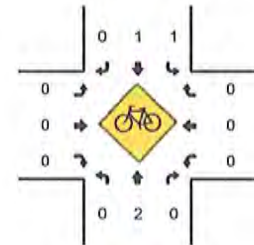
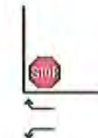
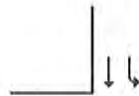
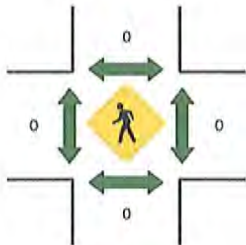
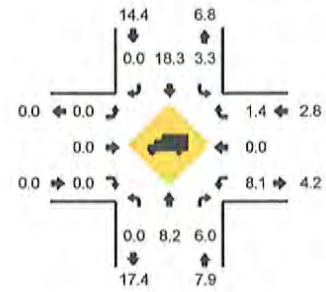
Method for determining peak hour: Total Entering Volume

LOCATION: 95th Ave -- Holiday Inn-Chevron Gas Access  
 CITY/STATE: Wilsonville, OR

QC JOB #: 10723303  
 DATE: Thu, Mar 08 2012



Peak-Hour: 3:25 PM -- 4:25 PM  
 Peak 15-Min: 3:25 PM -- 3:40 PM



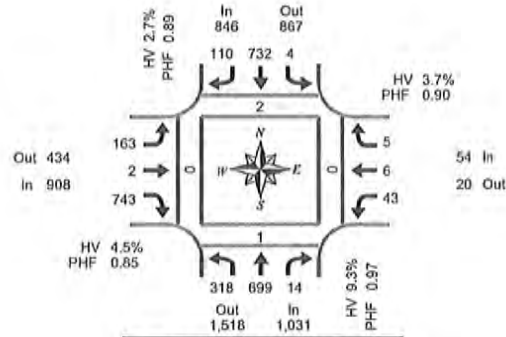
5-Min Count Period Beginning At	95th Ave (Northbound)				95th Ave (Southbound)				Holiday Inn-Chevron Gas Access (Eastbound)				Holiday Inn-Chevron Gas Access (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:00 PM	0	37	3	0	11	25	0	0	0	0	0	0	2	0	10	0	88	
3:05 PM	0	41	4	0	8	26	0	0	0	0	0	0	3	0	10	0	92	
3:10 PM	0	38	5	0	11	23	0	0	0	0	0	0	4	0	10	0	91	
3:15 PM	0	40	2	0	2	32	0	0	0	0	0	0	4	0	11	0	91	
3:20 PM	0	18	4	0	8	33	0	0	0	0	0	0	1	0	9	0	73	
3:25 PM	0	81	6	0	20	62	0	0	0	0	0	0	8	0	15	0	192	
3:30 PM	0	55	5	0	8	25	0	0	0	0	0	0	2	0	13	0	108	
3:35 PM	0	59	4	0	15	23	0	0	0	0	0	0	3	0	9	0	113	
3:40 PM	0	44	5	0	9	20	0	0	0	0	0	0	2	0	16	0	96	
3:45 PM	0	39	2	0	11	35	0	0	0	0	0	0	3	0	10	0	100	
3:50 PM	0	39	6	0	13	32	0	0	0	0	0	0	3	0	13	0	106	
3:55 PM	0	28	7	0	11	24	0	0	0	0	0	0	3	0	11	0	84	1234
4:00 PM	0	62	9	0	5	25	0	0	0	0	0	0	1	0	14	0	116	1262
4:05 PM	0	56	9	0	10	25	0	0	0	0	0	0	2	0	8	0	110	1280
4:10 PM	0	47	5	0	6	26	0	0	0	0	0	0	3	0	11	0	98	1287
4:15 PM	0	29	5	0	8	24	0	0	0	0	0	0	6	0	12	0	84	1280
4:20 PM	0	36	4	0	6	28	0	0	0	0	0	0	1	0	10	0	85	1292
4:25 PM	0	46	6	0	5	35	0	0	0	0	0	0	5	0	7	0	104	1204
4:30 PM	0	63	5	0	8	31	0	0	0	0	0	0	4	0	9	0	120	1216
4:35 PM	0	66	5	0	8	17	0	0	0	0	0	0	1	0	11	0	108	1211
4:40 PM	0	54	1	0	7	26	0	0	0	0	0	0	3	0	11	0	102	1217
4:45 PM	0	49	6	0	15	26	0	0	0	0	0	0	2	0	6	0	104	1221
4:50 PM	0	58	3	0	11	21	0	0	0	0	0	0	4	0	9	0	106	1221
4:55 PM	0	47	7	0	8	19	0	0	0	0	0	0	2	0	15	0	98	1235
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
All Vehicles	0	780	60	0	172	440	0	0	0	0	0	0	52	0	148	0	1652	
Heavy Trucks	0	60	4	0	0	68	0	0	0	0	0	0	8	0	0	0	140	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Bicycles	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Railroad																		
Stopped Buses																		

Comments: count with DVD that shows the driveway throat and interactions between gas station and Holiday Inn driveway traffic (we need to be able to see)

# Total Vehicle Summary



Clay Carney  
(503) 833-2740



**Peak Hour Summary**  
3:55 PM to 4:55 PM

## SW Boones Ferry Rd & SW 95th Ave

Wednesday, January 18, 2012

3:00 PM to 5:00 PM

### 5-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW Boones Ferry Rd				Southbound SW Boones Ferry Rd				Eastbound SW 95th Ave				Westbound SW 95th Ave				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	22	44	1	0	0	50	11	0	11	0	47	0	3	0	1	0	190	0	0	0	0
3:05 PM	24	44	0	0	2	57	7	0	15	0	51	0	5	2	0	0	207	0	0	0	0
3:10 PM	21	35	2	0	0	62	5	0	18	0	48	0	2	0	0	0	193	0	0	0	0
3:15 PM	16	57	0	0	1	54	10	0	13	1	54	0	5	0	0	0	211	0	0	0	0
3:20 PM	26	48	0	0	1	48	13	0	10	0	59	0	1	2	1	0	209	0	0	0	0
3:25 PM	24	50	1	0	0	63	8	0	10	0	60	0	2	0	1	0	219	0	0	0	0
3:30 PM	26	39	1	0	0	51	13	0	12	0	49	0	3	2	0	0	196	0	0	0	0
3:35 PM	28	61	0	0	0	48	11	0	15	3	43	0	0	0	2	0	211	0	0	0	0
3:40 PM	17	68	2	0	2	40	10	0	15	0	54	0	5	1	0	0	214	0	0	0	0
3:45 PM	30	63	0	0	0	57	9	0	18	0	51	0	4	0	0	0	232	0	0	0	0
3:50 PM	19	59	1	0	0	62	4	0	9	0	44	0	0	1	1	0	200	0	0	0	0
3:55 PM	33	58	1	0	1	57	9	0	13	0	57	0	2	0	0	0	231	0	0	0	0
4:00 PM	21	49	2	0	0	53	7	0	11	0	52	0	5	0	1	0	201	0	0	0	0
4:05 PM	26	62	1	0	0	59	7	0	19	0	58	0	6	1	0	0	237	0	0	0	0
4:10 PM	27	51	2	0	0	62	10	0	13	0	49	0	2	0	0	0	216	0	0	0	0
4:15 PM	25	60	1	0	1	61	6	0	15	0	68	0	3	1	1	0	242	0	0	0	0
4:20 PM	29	59	0	0	1	66	10	0	11	1	59	0	6	1	0	0	243	0	0	0	0
4:25 PM	23	66	2	0	0	55	14	0	10	0	75	0	2	0	0	0	247	1	0	0	0
4:30 PM	28	57	2	0	0	57	9	0	17	0	71	0	4	1	0	0	246	1	0	0	0
4:35 PM	31	54	0	0	0	57	7	0	17	0	76	0	5	0	1	0	248	0	0	0	0
4:40 PM	23	55	2	0	0	62	13	0	9	0	60	0	1	1	0	0	226	0	1	0	0
4:45 PM	26	67	1	0	1	73	5	0	14	1	65	0	2	0	1	0	256	0	0	0	0
4:50 PM	26	61	0	0	0	70	13	0	14	0	55	0	5	1	1	0	246	0	0	0	0
4:55 PM	21	44	0	0	0	64	7	0	12	0	54	0	3	0	1	0	206	0	0	0	0
Total Survey	592	1,311	22	0	10	1,388	218	0	321	6	1,357	0	76	14	12	0	5,327	2	1	0	0

### 15-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW Boones Ferry Rd				Southbound SW Boones Ferry Rd				Eastbound SW 95th Ave				Westbound SW 95th Ave				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	67	123	3	0	2	169	23	0	44	0	146	0	10	2	1	0	590	0	0	0	0
3:15 PM	66	155	1	0	2	165	31	0	33	1	173	0	8	2	2	0	639	0	0	0	0
3:30 PM	71	168	3	0	2	139	34	0	42	3	146	0	8	3	2	0	621	0	0	0	0
3:45 PM	82	180	2	0	1	176	22	0	40	0	152	0	6	1	1	0	663	0	0	0	0
4:00 PM	74	162	5	0	0	174	24	0	43	0	157	0	13	1	1	0	654	0	0	0	0
4:15 PM	77	185	3	0	2	182	30	0	36	1	202	0	11	2	1	0	732	1	0	0	0
4:30 PM	82	166	4	0	0	176	29	0	43	0	207	0	10	2	1	0	720	1	1	0	0
4:45 PM	73	172	1	0	1	207	25	0	40	1	174	0	10	1	3	0	708	0	0	0	0
Total Survey	592	1,311	22	0	10	1,388	218	0	321	6	1,357	0	76	14	12	0	5,327	2	1	0	0

### Peak Hour Summary

3:55 PM to 4:55 PM

By Approach	Northbound SW Boones Ferry Rd				Southbound SW Boones Ferry Rd				Eastbound SW 95th Ave				Westbound SW 95th Ave				Total	Pedestrians Crosswalk			
	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes		North	South	East	West
Volume	1,031	1,518	2,549	0	846	867	1,713	0	908	434	1,342	0	54	20	74	0	2,839	2	1	0	0
%HV	9.3%				2.7%				4.5%				3.7%				5.7%				
PHF	0.97				0.89				0.85				0.90				0.96				

By Movement	Northbound SW Boones Ferry Rd				Southbound SW Boones Ferry Rd				Eastbound SW 95th Ave				Westbound SW 95th Ave				Total
	L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	Total	
Volume	318	699	14	1,031	4	732	110	846	163	2	743	908	43	6	5	54	2,839
%HV	11.9%	8.2%	7.1%	9.3%	0.0%	1.6%	10.0%	2.7%	5.5%	0.0%	4.3%	4.5%	0.0%	16.7%	20.0%	3.7%	5.7%
PHF	0.97	0.94	0.70	0.97	0.50	0.89	0.83	0.89	0.87	0.50	0.84	0.85	0.83	0.75	0.63	0.90	0.96

### Rolling Hour Summary

3:00 PM to 5:00 PM

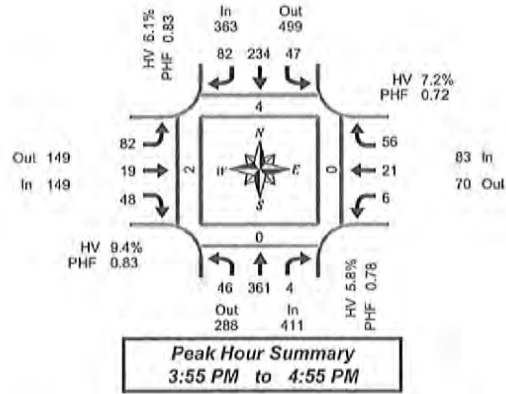
Interval Start Time	Northbound SW Boones Ferry Rd				Southbound SW Boones Ferry Rd				Eastbound SW 95th Ave				Westbound SW 95th Ave				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	286	626	9	0	7	649	110	0	159	4	817	0	32	8	6	0	2,513	0	0	0	0
3:15 PM	293	665	11	0	5	654	111	0	158	4	828	0	35	7	6	0	2,577	0	0	0	0
3:30 PM	304	695	13	0	5	671	110	0	161	4	857	0	38	7	5	0	2,670	1	0	0	0
3:45 PM	315	693	14	0	3	708	105	0	162	1	718	0	40	6	4	0	2,769	2	1	0	0
4:00 PM	306	685	13	0	3	739	108	0	162	2	740	0	44	6	6	0	2,814	2	1	0	0



# Total Vehicle Summary



Clay Carney  
(503) 833-2740



## SW 95th Ave & SW Ridder Rd

Wednesday, January 18, 2012

3:00 PM to 5:00 PM

### 5-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Ridder Rd				Westbound SW Ridder Rd				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	3	28	0	0	3	15	6	0	4	1	1	0	0	2	3	0	66	0	0	0	0
3:05 PM	3	34	0	0	1	19	7	0	6	2	7	0	0	5	6	0	90	0	0	0	0
3:10 PM	2	18	0	0	3	20	3	0	6	2	1	0	0	0	3	0	58	0	0	0	0
3:15 PM	2	22	0	0	2	15	8	0	8	5	1	0	0	3	9	0	75	0	0	0	0
3:20 PM	2	19	0	0	5	14	5	0	9	1	2	0	1	3	2	0	63	0	0	1	0
3:25 PM	0	19	2	0	1	23	8	0	8	3	1	0	0	1	9	0	75	0	0	0	0
3:30 PM	2	24	0	0	4	21	4	0	11	2	5	0	0	2	9	0	84	0	0	0	0
3:35 PM	3	21	1	0	2	20	8	0	15	0	5	0	0	0	4	0	79	0	0	0	0
3:40 PM	4	24	0	0	2	19	4	0	5	1	4	0	0	2	7	0	72	0	0	0	1
3:45 PM	1	25	0	0	2	9	6	0	7	1	2	0	0	3	6	0	62	0	0	0	0
3:50 PM	2	22	0	0	3	18	5	0	2	1	3	0	0	2	4	0	62	0	0	0	0
3:55 PM	4	23	0	0	1	14	4	0	7	0	1	0	0	0	5	0	59	0	0	0	0
4:00 PM	2	37	0	0	5	21	4	0	6	6	5	0	0	1	4	0	91	2	0	0	0
4:05 PM	10	55	0	0	9	13	7	0	8	3	5	0	0	0	2	0	112	0	0	0	0
4:10 PM	3	22	0	0	3	16	8	0	9	1	2	0	0	4	6	0	74	0	0	0	0
4:15 PM	3	19	0	0	5	30	8	0	3	1	5	0	1	0	5	0	80	0	0	0	0
4:20 PM	4	19	0	0	2	21	9	0	4	2	4	0	0	2	11	0	78	0	0	0	0
4:25 PM	4	20	1	0	5	21	8	0	5	0	5	0	3	4	3	0	79	0	0	0	0
4:30 PM	3	39	1	0	1	22	6	0	7	0	4	0	0	0	5	0	88	0	0	0	0
4:35 PM	6	44	0	0	1	25	9	0	13	2	3	0	0	3	5	0	111	0	0	0	0
4:40 PM	0	29	1	0	5	9	7	0	8	1	4	0	2	4	2	0	72	2	0	0	2
4:45 PM	4	25	0	0	8	24	8	0	3	2	6	0	0	1	3	0	84	0	0	0	0
4:50 PM	3	29	1	0	2	18	4	0	9	1	4	0	0	2	5	0	78	0	0	0	0
4:55 PM	1	13	0	0	3	10	3	0	3	4	1	0	0	0	2	0	40	0	0	0	0
Total Survey	71	630	7	0	78	437	149	0	166	42	81	0	7	44	120	0	1,832	4	0	1	3

### 15-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Ridder Rd				Westbound SW Ridder Rd				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	8	80	0	0	7	54	16	0	16	5	9	0	0	7	12	0	214	0	0	0	0
3:15 PM	4	60	2	0	8	52	21	0	25	9	4	0	1	7	20	0	213	0	0	1	0
3:30 PM	9	69	1	0	8	60	16	0	31	3	14	0	0	4	20	0	235	0	0	0	1
3:45 PM	7	70	0	0	6	41	15	0	16	2	6	0	0	5	15	0	183	0	0	0	0
4:00 PM	15	114	0	0	17	50	19	0	23	10	12	0	0	5	12	0	277	2	0	0	0
4:15 PM	11	58	1	0	12	72	25	0	12	3	14	0	4	6	19	0	237	0	0	0	0
4:30 PM	9	112	2	0	7	56	22	0	28	3	11	0	2	7	12	0	271	2	0	0	2
4:45 PM	8	67	1	0	13	52	15	0	15	7	11	0	0	3	10	0	202	0	0	0	0
Total Survey	71	630	7	0	78	437	149	0	166	42	81	0	7	44	120	0	1,832	4	0	1	3

### Peak Hour Summary

3:55 PM to 4:55 PM

By Approach	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Ridder Rd				Westbound SW Ridder Rd				Total	Pedestrians Crosswalk			
	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes		North	South	East	West
Volume	411	288	699	0	363	499	862	0	149	149	298	0	83	70	153	0	1,006	4	0	0	2
%HV	5.8%				6.1%				9.4%				7.2%				6.6%				
PHF	0.78				0.83				0.83				0.72				0.80				

By Movement	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Ridder Rd				Westbound SW Ridder Rd				Total
	L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	Total	
Volume	46	361	4	411	47	234	82	363	82	19	48	149	6	21	56	83	1,006
%HV	19.6%	3.9%	25.0%	5.8%	2.1%	4.7%	12.2%	6.1%	8.5%	10.5%	10.4%	9.4%	16.7%	9.5%	5.4%	7.2%	6.6%
PHF	0.72	0.78	0.50	0.78	0.69	0.81	0.82	0.83	0.73	0.48	0.66	0.83	0.38	0.66	0.64	0.72	0.80

### Rolling Hour Summary

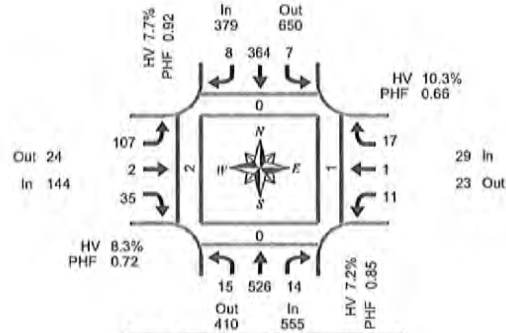
3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Ridder Rd				Westbound SW Ridder Rd				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	28	279	3	0	29	207	68	0	88	19	33	0	1	23	67	0	845	0	0	1	1
3:15 PM	35	313	3	0	39	203	71	0	95	24	36	0	1	21	67	0	908	2	0	1	1
3:30 PM	42	311	2	0	43	223	75	0	82	18	46	0	4	20	66	0	932	2	0	0	1
3:45 PM	42	354	3	0	42	219	81	0	79	16	43	0	6	23	58	0	968	4	0	0	2
4:00 PM	43	351	4	0	49	230	81	0	78	23	48	0	6	21	53	0	987	4	0	0	2

# Total Vehicle Summary



Clay Camey  
(503) 833-2740



**Peak Hour Summary  
3:50 PM to 4:50 PM**

## SW 95th Ave & SW Commerce Cir

Wednesday, February 15, 2012

3:00 PM to 5:00 PM

### 5-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Commerce Cir				Westbound SW Commerce Cir				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	2	36	0	0	1	33	0	0	6	0	0	0	0	0	6	0	84	0	0	0	0
3:05 PM	1	36	2	0	0	35	0	0	3	0	2	0	4	0	1	0	84	0	0	0	0
3:10 PM	0	30	1	0	0	12	2	0	4	0	1	0	2	0	0	0	52	0	0	0	0
3:15 PM	1	32	0	0	0	32	1	0	3	0	1	0	0	0	0	0	70	0	0	0	0
3:20 PM	0	31	0	0	0	46	0	0	1	0	2	0	0	0	1	0	81	0	0	0	0
3:25 PM	4	37	0	0	1	25	0	0	7	0	1	0	2	0	3	0	80	0	0	0	0
3:30 PM	2	58	1	0	1	33	1	0	8	0	5	0	1	0	2	0	112	0	0	0	1
3:35 PM	3	38	0	0	0	24	2	0	6	0	4	0	1	0	1	0	79	0	0	0	0
3:40 PM	1	37	1	0	0	24	3	0	7	0	3	0	0	0	0	0	77	0	0	0	0
3:45 PM	0	35	1	0	3	30	0	0	6	0	4	0	4	0	0	0	83	0	0	0	0
3:50 PM	1	35	1	0	0	33	2	0	6	0	3	0	3	0	0	0	84	0	0	0	0
3:55 PM	4	35	1	0	0	28	0	0	14	0	3	0	1	0	1	0	87	0	0	0	1
4:00 PM	2	64	1	0	1	29	0	0	12	0	2	0	0	1	0	0	112	0	0	0	0
4:05 PM	1	45	1	0	1	29	1	0	10	0	3	0	2	0	2	0	95	0	0	0	1
4:10 PM	0	40	2	0	0	28	3	0	5	0	6	0	0	0	2	0	86	0	0	1	0
4:15 PM	1	35	0	0	0	30	0	0	8	0	0	0	0	0	0	0	74	0	0	0	0
4:20 PM	0	32	3	0	3	37	0	0	2	0	0	0	1	0	1	0	79	0	0	0	0
4:25 PM	1	46	0	0	0	20	1	0	18	1	7	0	1	0	4	0	90	0	0	0	0
4:30 PM	1	63	2	0	0	41	1	0	13	0	5	0	1	0	3	0	130	0	0	0	0
4:35 PM	0	45	1	0	1	27	0	0	4	0	2	0	1	0	1	0	82	0	0	0	0
4:40 PM	3	49	0	0	1	27	0	0	10	0	1	0	0	0	2	0	93	0	0	0	0
4:45 PM	1	37	2	0	0	35	0	0	5	1	3	0	1	0	1	0	86	0	0	0	0
4:50 PM	1	41	1	0	3	21	0	0	5	0	2	0	1	0	0	0	75	0	0	0	0
4:55 PM	0	40	0	0	0	24	3	0	14	0	2	0	0	0	1	0	84	0	0	0	0
Total Survey	30	977	21	0	17	703	20	0	177	2	62	0	26	1	32	0	2,068	0	0	1	3

### 15-Minute Interval Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Commerce Cir				Westbound SW Commerce Cir				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	3	102	3	0	1	80	2	0	13	0	3	0	6	0	7	0	220	0	0	0	0
3:15 PM	5	100	0	0	1	103	1	0	11	0	4	0	2	0	4	0	231	0	0	0	0
3:30 PM	6	133	2	0	2	81	6	0	21	0	12	0	2	0	3	0	268	0	0	0	1
3:45 PM	5	105	3	0	3	91	2	0	26	0	10	0	8	0	1	0	254	0	0	0	1
4:00 PM	3	149	4	0	2	86	4	0	27	0	11	0	2	1	4	0	293	0	0	1	1
4:15 PM	2	113	3	0	3	87	1	0	28	1	7	0	2	0	5	0	252	0	0	0	0
4:30 PM	4	157	3	0	2	95	1	0	27	0	8	0	2	0	6	0	305	0	0	0	0
4:45 PM	2	118	3	0	3	80	3	0	24	1	7	0	2	0	2	0	245	0	0	0	0
Total Survey	30	977	21	0	17	703	20	0	177	2	62	0	26	1	32	0	2,068	0	0	1	3

### Peak Hour Summary

3:50 PM to 4:50 PM

By Approach	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Commerce Cir				Westbound SW Commerce Cir				Total	Pedestrians Crosswalk			
	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes	In	Out	Total	Bikes		North	South	East	West
Volume	555	410	965	0	379	650	1,029	0	144	24	168	0	29	23	52	0	1,107	0	0	1	2
%HV	7.2%				7.7%				8.3%				10.3%				7.6%				
PHF	0.85				0.92				0.72				0.66				0.89				

By Movement	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Commerce Cir				Westbound SW Commerce Cir				Total
	L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	Total	
Volume	15	526	14	555	7	364	8	379	107	2	35	144	11	1	17	29	1,107
%HV	13.3%	7.2%	0.0%	7.2%	0.0%	7.7%	12.5%	7.7%	5.6%	0.0%	17.1%	8.3%	18.2%	###	0.0%	10.3%	7.6%
PHF	0.54	0.84	0.70	0.85	0.58	0.93	0.50	0.92	0.74	0.50	0.63	0.72	0.69	0.25	0.93	0.66	0.89

### Rolling Hour Summary

3:00 PM to 5:00 PM

Interval Start Time	Northbound SW 95th Ave				Southbound SW 95th Ave				Eastbound SW Commerce Cir				Westbound SW Commerce Cir				Interval Total	Pedestrians Crosswalk			
	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes	L	T	R	Bikes		North	South	East	West
3:00 PM	19	440	8	0	7	355	11	0	71	0	29	0	18	0	15	0	973	0	0	0	2
3:15 PM	19	487	9	0	8	361	13	0	85	0	37	0	14	1	12	0	1,046	0	0	1	3
3:30 PM	16	500	12	0	10	345	13	0	102	1	40	0	14	1	13	0	1,067	0	0	1	3
3:45 PM	14	524	13	0	10	359	8	0	108	1	36	0	14	1	16	0	1,104	0	0	1	2
4:00 PM	11	537	13	0	10	348	9	0	106	2	33	0	8	1	17	0	1,095	0	0	1	1



# Level of Service Descriptions

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## TRAFFIC LEVELS OF SERVICE

Analysis of traffic volumes is useful in understanding the general nature of traffic in an area, but by itself indicates neither the ability of the street network to carry additional traffic nor the quality of service afforded by the street facilities. For this, the concept of *level of service* has been developed to subjectively describe traffic performance. Level of service can be measured at intersections and along key roadway segments.

Level of service categories are similar to report card ratings for traffic performance. Intersections are typically the controlling bottlenecks of traffic flow and the ability of a roadway system to carry traffic efficiently is generally diminished in their vicinities. Levels of Service A, B and C indicate conditions where traffic moves without significant delays over periods of peak travel demand. Level of service D and E are progressively worse peak hour operating conditions and F conditions represent where demand exceeds the capacity of an intersection. Most urban communities set level of service D as the minimum acceptable level of service for peak hour operation and plan for level of service C or better for all other times of the day. The *Highway Capacity Manual* provides level of service calculation methodology for both intersections and arterials.<sup>1</sup> The following two sections provide interpretations of the analysis approaches.

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<sup>1</sup> 2000 *Highway Capacity Manual*, Transportation Research Board, Washington D.C., 2000, Chapters 16 and 17.



## UNSIGNALIZED INTERSECTIONS (Two-Way Stop Controlled)

Unsignalized intersection level of service is reported for the major street and minor street (generally, left turn movements). The method assesses available and critical gaps in the traffic stream which make it possible for side street traffic to enter the main street flow. The *2000 Highway Capacity Manual* describes the detailed methodology. It is not unusual for an intersection to experience level of service E or F conditions for the minor street left turn movement. It should be understood that, often, a poor level of service is experienced by only a few vehicles and the intersection as a whole operates acceptably.

Unsignalized intersection levels of service are described in the following table.

Level of Service	Expected Delay	(Sec/Veh)
A	Little or no delay	0-10.0
B	Short traffic delay	>10.1-15.0
C	Average traffic delays	>15.1-25.0
D	Long traffic delays	>25.1-35.0
E	Very long traffic delays	>35.1-50.0
F	Extreme delays potentially affecting other traffic movements in the intersection	> 50

Source: 2000 *Highway Capacity Manual*, Transportation Research Board Washington, D.C.

## SIGNALIZED INTERSECTIONS

For signalized intersections, level of service is evaluated based upon average vehicle delay experienced by vehicles entering an intersection. Control delay (or signal delay) includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. In previous versions of this chapter of the HCM (1994 and earlier), delay included only stopped delay. As delay increases, the level of service decreases. Calculations for signalized and unsignalized intersections are different due to the variation in traffic control. The *2000 Highway Capacity Manual* provides the basis for these calculations.

Level of Service	Delay (secs.)	Description
A	≤10.00	<b>Free Flow/Insignificant Delays:</b> No approach phase is fully utilized by traffic and no vehicle waits longer than one red indication. Most vehicles do not stop at all. Progression is extremely favorable and most vehicles arrive during the green phase.
B	10.1-20.0	<b>Stable Operation/Minimal Delays:</b> An occasional approach phase is fully utilized. Many drivers begin to feel somewhat restricted within platoons of vehicles. This level generally occurs with good progression, short cycle lengths, or both.
C	20.1-35.0	<b>Stable Operation/Acceptable Delays:</b> Major approach phases fully utilized. Most drivers feel somewhat restricted. Higher delays may result from fair progression, longer cycle lengths, or both. Individual cycle failures may begin to appear at this level, and the number of vehicles stopping is significant.
D	35.1-55.0	<b>Approaching Unstable/Tolerable Delays:</b> The influence of congestion becomes more noticeable. Drivers may have to wait through more than one red signal indication. Longer delays may result from some combination of unfavorable progression, long cycle lengths, or high v/c ratios. The proportion of vehicles not stopping declines, and individual cycle failures are noticeable.
E	55.1-80.0	<b>Unstable Operation/Significant Delays:</b> Volumes at or near capacity. Vehicles may wait through several signal cycles. Long queues form upstream from intersection. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are a frequent occurrence.
F	≥80.0	<b>Forced Flow/Excessive Delays:</b> Represents jammed conditions. Queues may block upstream intersections. This level occurs when arrival flow rates exceed intersection capacity, and is considered to be unacceptable to most drivers. Poor progression, long cycle lengths, and v/c ratios approaching 1.0 may contribute to these high delay levels.

Source: *2000 Highway Capacity Manual*, Transportation Research Board, Washington D.C.



# HCM Analysis – Existing

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HCM Signalized Intersection Capacity Analysis  
1: Boones Ferry Road & SW Day Rd

Carl's Jr. TIS  
2012 Existing (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	9	1	419	0	0	0	396	408	0	0	456	15
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	0.96	1.00	0.85	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Flt Protected	1652	1553	1570	3505	1838	1838	1570	3505	1838	1838	1570	3505
Satd. Flow (prot)	0.77	1.00	0.95	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Flt Permitted	1328	1553	1570	3505	1838	1838	1570	3505	1838	1838	1570	3505
Satd. Flow (perm)	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Peak-hour factor, PHF	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Adj. Flow (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
RTOR Reduction (vph)	0	0	73	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	11	378	0	0	0	426	439	0	0	505	0
Confl. Peds. (#/hr)	11%	0%	4%	0%	0%	0%	15%	3%	0%	0%	3%	0%
Heavy Vehicles (%)	Perm	NA	pt-ov	Perm	NA	NA	Prot	NA	Prot	NA	Prot	NA
Turn Type	8	8	8	1	4	4	1	6	1	6	5	2
Protected Phases	8	8	8	1	4	4	1	6	1	6	5	2
Permitted Phases	8	8	8	1	4	4	1	6	1	6	5	2
Actuated Green, G (s)	6.9	48.9	39.0	88.1	46.1	46.1	39.0	88.1	46.1	46.1	39.0	88.1
Effective Green, g (s)	6.9	48.9	39.0	90.1	47.1	47.1	39.0	90.1	47.1	47.1	39.0	90.1
Actuated g/C Ratio	0.07	0.48	0.37	0.86	0.45	0.45	0.37	0.86	0.45	0.45	0.37	0.86
Clearance Time (s)	4.0	4.0	4.0	5.0	5.0	5.0	4.0	5.0	5.0	5.0	4.0	5.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	87	738	c0.24	583	3008	824	583	3008	824	824	583	3008
v/s Ratio Prot	0.01	0.13	0.51	0.73	0.15	0.61	0.73	0.15	0.61	0.61	0.73	0.15
v/s Ratio Perm	46.2	19.1	1.00	28.5	1.2	2.0	28.5	1.2	2.0	2.0	28.5	1.2
Uniform Delay, d1	0.7	0.6	0.6	0.87	0.60	0.60	0.73	0.1	0.6	0.6	0.73	0.1
Progression Factor	46.9	19.7	32.0	0.8	25.4	25.4	46.9	19.7	32.0	0.8	25.4	25.4
Incremental Delay, d2	Level of Service	D	B	C	A	C	D	B	C	A	C	C
Delay (s)	Approach Delay (s)	20.3	0.0	0.0	16.2	25.4	20.3	0.0	0.0	16.2	25.4	25.4
Level of Service	Approach LOS	C	A	A	B	C	C	A	A	B	C	C
Approach Delay (s)	Intersection Summary											
Approach LOS	HCM Average Control Delay	19.8										
	HCM Volume to Capacity ratio	0.64										
	Actuated Cycle Length (s)	105.0										
	Intersection Capacity Utilization	60.2%										
	Analysis Period (min)	15										
	c Critical Lane Group											

HCM Signalized Intersection Capacity Analysis  
2: Boones Ferry Road & 95th Avenue

Carl's Jr. TIS  
2012 Existing (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	160	5	720	45	5	5	310	700	15	5	730	110
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	0.95	1.00	0.85	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Flt Protected	1699	1650	1744	1400	3000	1805	1699	1650	1744	1400	3000	1805
Satd. Flow (prot)	0.75	1.00	0.95	1.00	1.00	1.00	0.75	1.00	0.95	1.00	1.00	1.00
Flt Permitted	1346	1650	1650	390	1400	3000	1346	1650	1650	390	1400	3000
Satd. Flow (perm)	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Peak-hour factor, PHF	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Adj. Flow (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
RTOR Reduction (vph)	0	442	0	0	3	0	0	0	0	0	0	0
Lane Group Flow (vph)	167	313	0	0	54	0	323	744	0	5	760	73
Confl. Peds. (#/hr)	2	1	1	1	2	2	2	2	2	2	2	2
Heavy Vehicles (%)	6%	0%	4%	0%	17%	20%	12%	8%	7%	0%	2%	10%
Turn Type	Perm	NA	NA	Perm	NA	NA	Prot	NA	Prot	NA	Prot	NA
Protected Phases	8	8	8	4	4	4	1	6	1	6	5	2
Permitted Phases	8	8	8	4	4	4	1	6	1	6	5	2
Actuated Green, G (s)	17.0	17.0	17.0	17.0	17.0	17.0	25.6	74.7	1.3	50.4	50.4	50.4
Effective Green, g (s)	17.0	17.0	17.0	17.0	17.0	17.0	25.6	74.7	1.3	50.4	50.4	50.4
Actuated g/C Ratio	0.16	0.16	0.16	0.16	0.16	0.16	0.24	0.71	0.01	0.48	0.48	0.48
Clearance Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	218	267	c0.19	63	341	2134	218	267	63	341	2134	267
v/s Ratio Prot	0.12	0.77	1.17	0.85	0.14	0.14	0.12	0.77	1.17	0.85	0.14	0.14
v/s Ratio Perm	42.1	44.0	1.00	42.8	39.0	5.8	42.1	44.0	1.00	42.8	39.0	5.8
Uniform Delay, d1	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Progression Factor	14.8	110.3	63.7	35.1	0.4	0.4	14.8	110.3	63.7	35.1	0.4	0.4
Incremental Delay, d2	56.9	154.3	106.5	74.2	4.8	4.8	56.9	154.3	106.5	74.2	4.8	4.8
Delay (s)	Level of Service	E	F	F	A	E	E	F	A	E	B	B
Level of Service	Approach Delay (s)	136.7	0.0	0.0	25.8	16.3	136.7	0.0	0.0	25.8	16.3	16.3
Approach Delay (s)	Approach LOS	F	F	F	C	B	F	F	F	C	B	B
Approach LOS	Intersection Summary											
	HCM Average Control Delay	59.5										
	HCM Volume to Capacity ratio	0.74										
	Actuated Cycle Length (s)	105.0										
	Intersection Capacity Utilization	92.3%										
	Analysis Period (min)	15										
	c Critical Lane Group											

HCM Signalized Intersection Capacity Analysis  
 3: I-5 SB On Ramp/I-5 SB Off Ramp & Boones Ferry Road/Sw Elligsen Rd 2012 Existing (PM Peak)

Carl's Jr. TIS  
 2012 Existing (PM Peak)



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SSR
Lane Configurations	0	940	555	0	590	310	0	0	0	450	0	435
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	0.95	1.00	1.00	0.95	1.00	1.00	0.95	0.95	1.00	0.95	1.00	0.95
Lane Util. Factor	1.00	0.85	1.00	1.00	0.85	1.00	1.00	1.00	0.85	1.00	1.00	0.85
Flt. Protected	3438	1553	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Satd. Flow (prot)	3438	1553	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Flt. Permitted	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Peak-hour factor, PHF	0.979	0.979	0.979	0.979	0.979	0.979	0.979	0.979	0.979	0.979	0.979	0.979
Adj. Flow (vph)	0	578	578	0	615	323	0	0	0	469	0	453
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	172
Lane Group Flow (vph)	0	979	578	0	615	323	0	0	0	234	235	281
Heavy Vehicles (%)	0%	5%	4%	0%	9%	1%	0%	0%	0%	4%	0%	15%
Turn Type	NA	Free	Free	NA	Free	Free	NA	Free	Free	Split	NA	Prot
Protected Phases	2	6	6	6	6	6	6	6	6	4	4	4
Permitted Phases	Free	Free	Free	Free	Free	Free	Free	Free	Free	26.1	26.1	26.1
Actuated Green, G (s)	69.9	105.0	105.0	69.9	105.0	105.0	69.9	105.0	105.0	26.1	26.1	26.1
Effective Green, g (s)	70.9	105.0	105.0	70.9	105.0	105.0	70.9	105.0	105.0	26.1	26.1	26.1
Actuated g/C Ratio	0.68	1.00	1.00	0.68	1.00	1.00	0.68	1.00	1.00	0.25	0.25	0.25
Clearance Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	2321	1553	2236	1599	2236	1599	2321	1553	2236	410	410	349
vis Ratio Prot	c0.28	0.19	0.19	0.19	0.19	0.19	0.14	0.14	0.14	c0.20	c0.20	c0.20
vis Ratio Perm	0.42	0.37	0.28	0.20	0.28	0.20	0.57	0.57	0.57	0.57	0.57	0.80
Uniform Delay, d1	7.7	0.0	6.8	0.0	34.5	34.5	34.5	34.5	34.5	34.5	34.5	37.1
Progression Factor	0.77	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	0.3	0.4	0.3	0.3	1.9	1.9	1.9	1.9	1.9	1.9	1.9	12.6
Delay (s)	6.3	0.4	7.1	0.3	36.5	36.5	36.5	36.5	36.5	36.5	36.5	49.7
Level of Service	A	A	A	A	A	A	A	A	A	D	D	D
Approach Delay (s)	4.1	4.8	4.8	4.8	4.8	4.8	4.8	4.8	4.8	4.3	4.3	4.3
Approach LOS	A	A	A	A	A	A	A	A	A	B	B	B

**Intersection Summary**

HCM Average Control Delay	14.8	HCM Level of Service	B
HCM Volume to Capacity ratio	0.52		
Actuated Cycle Length (s)	105.0	Sum of lost time (s)	8.0
Intersection Capacity Utilization	49.9%	ICU Level of Service	A
Analysis Period (min)	15		

HCM Unsignalized Intersection Capacity Analysis  
 4: 95th Avenue & Chevron-Holiday Inn/Project Driveway

Carl's Jr. TIS  
 2012 Existing (PM Peak)



Movement	WBL	WBR	NBL	NBT	NBR	SBL	SBT
Lane Configurations	37	142	643	Free	122	303	Free
Volume (veh/h)	0%	0%	0%	0%	0%	0%	0%
Stop	0.78	0.78	0.78	0.78	0.78	0.78	0.78
Grade	47	182	824	86	156	388	388
Hourly flow rate (vph)							
Pedestrians							
Lane Width (ft)							
Walking Speed (ft/s)							
Percent Blockage							
Right turn flare (veh)							
Median type							
Median storage (veh)							
Upstream signal (ft)							
pA, platoon unblocked	0.77	0.77	0.77	0.77	0.77	0.77	0.77
vC, conflicting volume	1569	867	867	910	910	910	910
vC1, stage 1 conf vol	701						
vC2, stage 2 conf vol	1589	677	677	732	732	732	732
vCu, unblocked vol	6.5	6.2	6.2	4.1	4.1	4.1	4.1
IC, single (s)	5.5						
IC, 2 stage (s)	3.6	3.3	3.3	2.2	2.2	2.2	2.2
IF (s)	82	48	48	77	77	77	77
p0 queue free %	285	349	349	667	667	667	667
ch capacity (veh/h)							
Direction, Lane #	WBL	WBR	NBL	NBT	NBR	SBL	SBT
Volume Total	47	182	910	156	388	388	388
Volume Left	47	0	0	156	0	0	0
Volume Right	0	182	86	0	0	0	0
cSH	265	349	1700	667	1700	1700	1700
Volume to Capacity	0.18	0.52	0.54	0.23	0.23	0.23	0.23
Queue Length 95th (ft)	16	72	0	23	0	0	0
Control Delay (s)	21.5	26.0	0.0	12.0	0.0	0.0	0.0
Lane LOS	C	D	D	B	B	B	B
Approach Delay (s)	25.1	0.0	0.0	3.5	3.5	3.5	3.5
Approach LOS	D						

**Intersection Summary**

Average Delay	4.5	ICU Level of Service	B
Intersection Capacity Utilization	56.0%		
Analysis Period (min)	15		

HCM Signalized Intersection Capacity Analysis  
 5: 95th Avenue & Commerce Circle

HCM Signalized Intersection Capacity Analysis  
 6: 95th Avenue & Ridder Rd

Carl's Jr. TIS  
 2012 Existing (PM Peak)

Carl's Jr. TIS  
 2012 Existing (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	110	2	35	11	1	20	15	580	14	5	330	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frbp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frbp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frt	0.95	1.00	0.86	1.00	0.86	1.00	1.00	0.95	1.00	0.95	1.00	1.00
Flt Protected	1703	1402	1530	1560	1595	1771	1802	1753				
Satd. Flow (perm)	1331	1402	1176	1560	892	1771	625	1753				
Peak-hour factor, PHF	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Adj. Flow (vph)	124	2	39	12	1	22	17	652	16	6	371	6
RTOR Reduction (vph)	0	32	0	0	18	0	0	1	0	0	1	0
Lane Group Flow (vph)	124	9	0	12	5	0	17	667	0	6	376	0
Confl. Peds. (#/hr)	6%	0%	17%	18%	100%	0%	13%	7%	0%	0%	8%	12%
Heavy Vehicles (%)	Perm	NA	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm
Turn Type	4	4	4	8	8	2	2	6	2	6	6	6
Protected Phases	8.4	8.4	8.4	8.4	8.4	28.6	28.6	28.6	28.6	28.6	28.6	28.6
Permitted Phases	8.4	8.4	8.4	8.4	8.4	28.6	28.6	28.6	28.6	28.6	28.6	28.6
Actuated Green, G (s)	0.19	0.19	0.19	0.19	0.19	0.64	0.64	0.64	0.64	0.64	0.64	0.64
Effective Green, g (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Actuated g/C Ratio	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Clearance Time (s)	248	262	220	291	567	1126	397	1174				
Vehicle Extension (s)	c0.09	0.01	0.01	0.00	0.02	0.38	0.01	0.21				
Lane Grp Cap (vph)	0.50	0.04	0.05	0.02	0.03	0.59	0.02	0.34				
vs Ratio Prot	16.4	15.0	15.0	14.9	3.0	4.8	3.0	3.8				
vs Ratio Perm	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00				
v/c Ratio	1.6	0.1	0.1	0.0	0.1	2.3	0.1	0.8				
Uniform Delay, d1	18.0	15.0	15.1	15.0	3.1	7.1	3.1	4.6				
Progression Factor	B	B	B	B	B	A	A	A				
Incremental Delay, d2	B	B	B	B	B	A	A	A				
Delay (s)	B	B	B	B	B	A	A	A				
Level of Service	B	B	B	B	B	A	A	A				
Approach Delay (s)	B	B	B	B	B	A	A	A				
Approach LOS	B	B	B	B	B	A	A	A				
<b>Intersection Summary</b>												
HCM Average Control Delay	7.8											
HCM Volume to Capacity ratio	0.57											
Actuated Cycle Length (s)	45.0											
Intersection Capacity Utilization	50.8%											
Analysis Period (min)	15											
c Critical Lane Group	A											

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	82	19	48	6	21	56	46	391	4	47	234	82
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frbp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frbp, ped/bikes	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	0.89	1.00	1.00	0.89	1.00	1.00	0.96	1.00	0.96	1.00	1.00
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00
Satd. Flow (perm)	1645	1538	1543	1599	1504	1821	1766	1710				
Peak-hour factor, PHF	1385	1538	1299	1559	872	1821						
Adj. Flow (vph)	91	21	53	7	23	62	51	434	4	52	260	91
RTOR Reduction (vph)	0	44	0	0	52	0	0	0	0	0	22	0
Lane Group Flow (vph)	91	30	0	7	33	0	51	438	0	52	329	0
Confl. Peds. (#/hr)	4	4	4	4	4	4	4	4	4	4	4	4
Heavy Vehicles (%)	9%	11%	10%	10%	17%	10%	5%	20%	4%	25%	2%	5%
Turn Type	Perm	NA	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm
Protected Phases	4	4	4	8	8	2	2	6	2	6	6	6
Permitted Phases	4	4	4	8	8	2	2	6	2	6	6	6
Actuated Green, G (s)	5.0	5.0	5.0	5.0	5.0	17.7	17.7	17.7	17.7	17.7	17.7	17.7
Effective Green, g (s)	5.0	5.0	5.0	5.0	5.0	17.7	17.7	17.7	17.7	17.7	17.7	17.7
Actuated g/C Ratio	0.16	0.16	0.16	0.16	0.16	0.58	0.58	0.58	0.58	0.58	0.58	0.58
Clearance Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	226	250	212	254	503	1050	539	966				
vs Ratio Prot	c0.07	0.02	0.01	0.01	0.06	c0.24	0.19	0.19				
vs Ratio Perm	0.40	0.12	0.03	0.03	0.10	0.42	0.10	0.33				
v/c Ratio	11.5	11.0	10.8	11.0	2.9	3.6	2.9	3.4				
Uniform Delay, d1	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00				
Progression Factor	1.2	0.2	0.1	0.2	0.1	0.3	0.1	0.2				
Incremental Delay, d2	12.7	11.2	10.9	11.2	3.0	3.9	3.0	3.6				
Delay (s)	B	B	B	B	B	A	A	A				
Level of Service	B	B	B	B	B	A	A	A				
Approach Delay (s)	B	B	B	B	B	A	A	A				
Approach LOS	B	B	B	B	B	A	A	A				
<b>Intersection Summary</b>												
HCM Average Control Delay	5.5											
HCM Volume to Capacity ratio	0.41											
Actuated Cycle Length (s)	30.7											
Intersection Capacity Utilization	45.4%											
Analysis Period (min)	15											
c Critical Lane Group	A											

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## HCM Analysis – Future

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HCM Signalized Intersection Capacity Analysis  
1: Boones Ferry Road & SW Day Rd

Carl's Jr. TIS  
Existing + Project (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	9	1	421	0	0	0	398	412	0	0	460	15
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)												
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt Protected	0.96	1.00	0.85	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Satd. Flow (prot)	1652	1553		1570	3505		1838					
Flt Permitted	0.77	1.00		0.95	1.00		1.00					
Satd. Flow (perm)	1328	1553		1570	3505		1838					
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Adj. Flow (vph)	10	1	453	0	0	428	443	0	0	495	16	
RTOR Reduction (vph)	0	0	72	0	0	0	0	0	0	0	1	0
Lane Group Flow (vph)	0	11	381	0	0	428	443	0	0	510	0	0
Confl. Peds. (#/hr)												
Heavy Vehicles (%)	11%	0%	4%	0%	0%	15%	3%	0%	0%	0%	3%	0%
Turn Type	Perm	NA	pt+ov	Perm	NA	Prot	NA	Prot	NA	Prot	NA	Prot
Protected Phases	8	8	8	1	4	1	6	1	6	1	5	2
Permitted Phases	8	6.9	48.9	39.0	89.1	46.1						
Actuated Green, G (s)	6.9	49.9		39.0	90.1	47.1						
Effective Green, g (s)	0.07	0.48		0.37	0.86	0.45						
Actuated g/C Ratio	4.0	4.0	5.0	4.0	5.0	3.0						
Clearance Time (s)	3.0	3.0	3.0	3.0	3.0	3.0						
Vehicle Extension (s)	87	738		583	3008	824						
Lane Grp Cap (vph)	c0.25			c0.27	0.13	c0.28						
v/s Ratio Prot	0.01	0.13	0.52	0.73	0.15	0.62						
v/c Ratio	46.2	19.2		28.5	1.2	22.1						
Uniform Delay, d1	1.00	1.00	1.00	0.77	0.59	1.00						
Progression Factor	0.7	0.6		7.6	0.1	3.5						
Incremental Delay, d2	46.9	19.8		29.6	0.8	25.6						
Delay (s)	D	B		C	A	C						
Level of Service	D	B		C	A	C						
Approach Delay (s)	20.4			0.0		25.6						
Approach LOS	C			A		B						

Intersection Summary			
HCM Average Control Delay	19.3	HCM Level of Service	B
HCM Volume to Capacity ratio	0.65		
Actuated Cycle Length (s)	105.0	Sum of lost time (s)	8.0
Intersection Capacity Utilization	60.3%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis  
2: Boones Ferry Road & 95th Avenue

Carl's Jr. TIS  
Existing + Project (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	166	5	736	45	5	5	326	700	15	5	730	116
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)												
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	0.88	1.00	1.00	1.00	1.00	0.97	0.95	1.00	1.00	0.95	1.00
Frb, ped/bikes	1.00	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt Protected	0.85	1.00	0.85	1.00	1.00	1.00	0.85	1.00	1.00	1.00	0.85	1.00
Satd. Flow (prot)	1708	2710	1804	1472		3127	3332				1805	3539
Flt Permitted	0.73	1.00	0.47	1.00		0.95	1.00				0.95	1.00
Satd. Flow (perm)	1299	2710	869	1472		3127	3332				1805	3539
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	173	5	767	47	5	5	340	729	16	5	760	121
RTOR Reduction (vph)	0	0	71	0	4	0	0	1	0	0	0	0
Lane Group Flow (vph)	0	178	696	47	6	0	340	744	0	5	760	71
Confl. Peds. (#/hr)	2		1	1	2							
Heavy Vehicles (%)	6%	0%	4%	0%	17%	20%	12%	8%	7%	0%	2%	10%
Turn Type	Perm	NA	pm+ov	Perm	NA	Prot	NA	Prot	NA	Prot	NA	Perm
Protected Phases	8	8	8	4	4	1	6	1	6	1	5	2
Permitted Phases	8	8	8	4	4	4						
Actuated Green, G (s)	19.6	49.9	19.6	19.6	19.6	30.3	72.1	72.1	1.3	43.1	43.1	43.1
Effective Green, g (s)	19.6	49.9	19.6	19.6	19.6	30.3	72.1	72.1	1.3	43.1	43.1	43.1
Actuated g/C Ratio	0.19	0.48	0.19	0.19	0.19	0.29	0.69	0.69	0.01	0.41	0.41	0.41
Clearance Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	242	1391	166	275	275	902	2288				22	1453
v/s Ratio Prot	c0.14			0.00		0.11	0.22				c0.21	
v/c Ratio	0.74	0.50	0.28	0.02	0.38	0.33	0.23	0.33	0.23	0.52	0.12	0.12
Uniform Delay, d1	40.3	19.0	36.7	34.9	29.8	6.6	51.4	23.2	19.2	23.2	19.2	19.2
Progression Factor	1.00	1.00	1.00	1.00	1.00	0.91	0.80	0.91	1.04	0.98	1.09	1.09
Incremental Delay, d2	11.0	0.3	0.9	0.0	1.1	0.3	4.4	0.3	4.4	0.3	0.1	0.1
Delay (s)	51.3	19.3	37.6	34.9	28.3	5.7	57.9	23.2	21.0	23.2	21.0	21.0
Level of Service	D	B	D	D	C	C	A	A	E	C	C	C
Approach Delay (s)	25.3			37.1		12.8			23.1			
Approach LOS	C			D		B			C			

Intersection Summary			
HCM Average Control Delay	20.3	HCM Level of Service	C
HCM Volume to Capacity ratio	0.55		
Actuated Cycle Length (s)	105.0	Sum of lost time (s)	8.0
Intersection Capacity Utilization	60.4%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis  
 3: I-5 SB On Ramp/I-5 SB Off Ramp & Boones Ferry Road/Sw Elligsen Rd Existing + Project (PM Peak)

Carl's Jr. TIS  
 Existing + Project (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4	4	4	4	4	4
Volume (vph)	0	953	568	0	598	310	0	0	0	450	0	443
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00	0.95	0.95	1.00	0.95	0.95	1.00
Flt	1.00	1.00	0.85	1.00	1.00	0.85	1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	3438	1553	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Satd. Flow (prot)	3438	1553	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Satd. Flow (perm)	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Peak-hour factor, PHF	0.993	0.993	0.993	0.993	0.993	0.993	0.993	0.993	0.993	0.993	0.993	0.993
Adj. Flow (vph)	0	993	581	0	623	323	0	0	0	469	0	461
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	993	581	0	623	323	0	0	0	234	0	235
Heavy Vehicles (%)	0%	5%	4%	0%	9%	1%	0%	0%	0%	4%	0%	15%
Turn Type	NA	Free	Free	NA	Free	Free	0%	0%	0%	Split	NA	Prot
Protected Phases	2	6	6	6	6	6	6	6	6	6	6	6
Permitted Phases	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free
Actuated Green, G (s)	69.1	105.0	69.1	105.0	69.1	105.0	69.1	105.0	69.1	105.0	69.1	105.0
Effective Green, g (s)	70.1	105.0	70.1	105.0	70.1	105.0	70.1	105.0	70.1	105.0	70.1	105.0
Actuated g/C Ratio	0.67	1.00	0.67	1.00	0.67	1.00	0.67	1.00	0.67	1.00	0.67	1.00
Clearance Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	2295	1553	2211	1599	422	422	422	422	422	422	422	422
vs Ratio Prot	0.29	0.19	0.19	0.14	0.14	0.14	0.14	0.14	0.14	0.14	0.14	0.21
vs Ratio Perm	0.37	0.37	0.28	0.20	0.20	0.20	0.55	0.55	0.55	0.55	0.55	0.82
Uniform Delay, d1	8.2	0.0	7.1	0.0	33.9	33.9	33.9	33.9	36.7	36.7	36.7	36.7
Progression Factor	0.74	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	0.5	0.6	0.3	0.3	1.6	1.6	1.6	1.6	13.4	13.4	13.4	13.4
Delay (s)	6.6	0.6	7.5	0.3	35.4	35.5	35.5	35.5	50.2	50.2	50.2	50.2
Level of Service	A	A	A	A	A	A	A	A	D	D	D	D
Approach Delay (s)	4.4	5.0	5.0	5.0	5.0	5.0	5.0	5.0	42.7	42.7	42.7	42.7
Approach LOS	A	A	A	A	A	A	A	A	B	B	B	B

Direction, Lane #	WB1	WB2	NB1	NB2	SB1	SB2
Volume Total	68	233	1049	321	252	252
Volume Left	68	0	0	195	0	0
Volume Right	0	233	119	0	0	0
CSH	37	244	1700	526	1700	1700
Volume to Capacity	1.83	0.96	0.62	0.37	0.15	0.15
Queue Length 95th (ft)	183	217	0	42	0	0
Control Delay (s)	627.8	89.9	0.0	12.1	0.0	0.0
Lane LOS	F	F	F	B	B	B
Approach Delay (s)	211.2	F	0.0	6.8	0.0	6.8
Approach LOS	F	F	0.0	6.8	0.0	6.8

Intersection Summary	
Average Delay	35.1
Intersection Capacity Utilization	69.7%
Analysis Period (min)	15
ICU Level of Service	C

HCM Signalized Intersection Capacity Analysis  
 4: 95th Avenue & Chevron-Holiday Inn/Project Driveway

Carl's Jr. TIS  
 Existing + Project (PM Peak)

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	4	4	4	4	4	4
Volume (veh/h)	53	162	725	93	152	295
Sign Control	Stop	Free	Free	Free	Free	Free
Grade	0%	0%	0%	0%	0%	0%
Peak-Hour Factor	0.78	0.78	0.78	0.78	0.78	0.78
Hourly flow rate (vph)	68	233	929	119	195	378
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)			None			None
Median type						
Median storage (veh)						
Upstream signal (ft)			543			580
pX, platoon unblocked	0.65	0.65		0.65		
vC, conflicting volume	1568	989		1049		
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1604	716		807		
tC, single (s)	7.0	6.9		4.2		
tC, 2 stage (s)						
tF (s)	3.6	3.3		2.2		
p0 queue free %	0	4		63		
cM capacity (veh/h)	37	244		526		

Direction, Lane #	WB1	WB2	NB1	NB2	SB1	SB2
Volume Total	68	233	1049	321	252	252
Volume Left	68	0	0	195	0	0
Volume Right	0	233	119	0	0	0
CSH	37	244	1700	526	1700	1700
Volume to Capacity	1.83	0.96	0.62	0.37	0.15	0.15
Queue Length 95th (ft)	183	217	0	42	0	0
Control Delay (s)	627.8	89.9	0.0	12.1	0.0	0.0
Lane LOS	F	F	F	B	B	B
Approach Delay (s)	211.2	F	0.0	6.8	0.0	6.8
Approach LOS	F	F	0.0	6.8	0.0	6.8

Intersection Summary	
Average Delay	35.1
Intersection Capacity Utilization	69.7%
Analysis Period (min)	15
ICU Level of Service	C

HCM Signalized Intersection Capacity Analysis  
5: 95th Avenue & Commerce Circle

Carl's Jr. TIS  
Existing + Project (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	230	2	35	11	1	20	35	568	14	5	338	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	0.95	1.00	0.86	1.00	0.86	1.00	0.95	1.00	0.95	1.00	0.95	1.00
Flt Protected	1703	1402	1530	1560	1560	1560	1995	1771	1803	1734	1754	1754
Satd. Flow (prot)	0.74	1.00	0.73	1.00	0.73	1.00	0.49	1.00	0.28	1.00	0.28	1.00
Flt Permitted	1331	1402	1176	1560	1560	1560	831	1771	527	1754	1754	1754
Satd. Flow (perm)	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Peak-hour factor, PHF	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Adj. Flow (vph)	258	2	39	12	1	22	39	638	16	6	380	6
RTOR Reduction (vph)	0	28	0	0	16	0	0	2	0	0	1	0
Lane Group Flow (vph)	258	13	0	12	7	0	39	652	2	6	385	0
Confl. Pnts. (#/hr)	5%	0%	17%	18%	100%	0%	13%	7%	0%	0%	8%	12%
Heavy Vehicles (%)	Perm	NA	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm
Turn Type	4	4	4	8	8	2	2	2	2	2	6	6
Protected Phases	4	4	4	8	8	2	2	2	2	2	6	6
Permitted Phases	13.0	13.0	13.0	13.0	13.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0
Actuated Green, G (s)	13.0	13.0	13.0	13.0	13.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0
Effective Green, g (s)	0.29	0.29	0.29	0.29	0.29	0.53	0.53	0.53	0.53	0.53	0.53	0.53
Actuated g/C Ratio	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Clearance Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Vehicle Extension (s)	385	405	340	451	443	945	281	935	281	935	281	935
Lane Grp Cap (vph)	c0.19	0.01	0.01	0.00	0.00	c0.37	0.01	0.22	0.01	0.22	0.01	0.22
v/s Ratio Prot	0.67	0.03	0.04	0.02	0.02	0.09	0.69	0.02	0.41	0.02	0.41	0.02
v/s Ratio Perm	14.1	11.5	11.5	11.4	11.4	5.1	7.8	5.0	6.3	5.0	6.3	5.0
Uniform Delay, d1	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Progression Factor	4.5	0.0	0.0	0.0	0.0	0.4	4.1	0.1	1.3	0.1	1.3	0.1
Incremental Delay, d2	18.6	11.5	11.5	11.4	11.4	5.5	11.9	5.1	7.6	5.1	7.6	5.1
Delay (s)	B	B	B	B	B	A	B	A	A	B	A	A
Level of Service	B	B	B	B	B	A	B	A	A	B	A	A
Approach Delay (s)	17.7	B	17.7	11.5	B	11.5	B	11.5	B	11.5	B	11.5
Approach LOS	B	B	B	B	B	B	B	B	B	B	B	B

Intersection Summary	
HCM Average Control Delay	11.7
HCM Volume to Capacity ratio	0.68
Actuated Cycle Length (s)	45.0
Intersection Capacity Utilization	58.8%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
6: 95th Avenue & Ridder Rd

Carl's Jr. TIS  
Existing + Project (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	84	19	48	6	21	58	46	395	4	49	238	84
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	0.98	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	1.00	0.89	1.00	0.89	1.00	1.00	1.00	1.00	1.00	1.00	0.96	1.00
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00
Satd. Flow (prot)	1645	1538	1543	1558	1558	1558	1604	1621	1766	1709	1766	1709
Flt Permitted	0.78	1.00	0.78	1.00	0.78	1.00	0.55	1.00	0.50	1.00	0.50	1.00
Satd. Flow (perm)	1358	1538	1274	1558	1558	1558	867	1821	926	1709	926	1709
Peak-hour factor, PHF	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Adj. Flow (vph)	93	21	53	7	23	64	51	439	4	54	264	93
RTOR Reduction (vph)	0	44	0	0	53	0	0	0	0	0	23	0
Lane Group Flow (vph)	93	30	0	7	34	0	51	443	0	54	334	0
Confl. Pnts. (#/hr)	4	4	4	4	4	4	4	4	4	4	4	4
Heavy Vehicles (%)	9%	11%	10%	17%	10%	5%	20%	4%	25%	2%	5%	12%
Turn Type	Perm	NA	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm
Protected Phases	4	4	4	8	8	2	2	2	2	2	6	6
Permitted Phases	5.1	5.1	5.1	5.1	5.1	17.7	17.7	17.7	17.7	17.7	17.7	17.7
Actuated Green, G (s)	5.1	5.1	5.1	5.1	5.1	17.7	17.7	17.7	17.7	17.7	17.7	17.7
Effective Green, g (s)	0.17	0.17	0.17	0.17	0.17	0.57	0.57	0.57	0.57	0.57	0.57	0.57
Actuated g/C Ratio	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Clearance Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Vehicle Extension (s)	225	255	211	258	258	498	1046	532	982	532	982	532
Lane Grp Cap (vph)	c0.07	0.02	0.02	0.01	0.01	c0.24	0.06	0.20	0.06	0.20	0.06	0.20
v/s Ratio Prot	0.41	0.12	0.12	0.03	0.13	0.10	0.42	0.10	0.34	0.10	0.34	0.10
v/s Ratio Perm	11.5	10.9	10.8	11.0	11.0	3.0	3.7	3.0	3.5	3.0	3.5	3.0
Uniform Delay, d1	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Progression Factor	1.2	0.2	0.2	0.1	0.2	0.1	0.3	0.1	0.2	0.1	0.2	0.1
Incremental Delay, d2	12.7	11.1	11.1	10.8	11.2	3.1	4.0	3.0	3.7	3.0	3.7	3.0
Delay (s)	B	B	B	B	B	A	A	A	A	A	A	A
Level of Service	B	B	B	B	B	A	A	A	A	A	A	A
Approach Delay (s)	12.0	B	12.0	11.2	B	11.2	3.9	3.6	3.6	3.6	3.6	3.6
Approach LOS	B	B	B	B	B	B	A	A	A	A	A	A

Intersection Summary	
HCM Average Control Delay	5.5
HCM Volume to Capacity ratio	0.42
Actuated Cycle Length (s)	30.8
Intersection Capacity Utilization	45.7%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
 1: Boones Ferry Road & SW Day Rd

Carl's Jr. TIS  
 Existing + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	9	1	4	0	0	0	397	427	0	0	467	15
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	1.00	0.85	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.96	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (prot)	1652	1553	1708	1570	3505	1838	1570	3505	1838	1570	3505	1838
Flt Permitted	0.77	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (perm)	1328	1553	1708	1570	3505	1838	1570	3505	1838	1570	3505	1838
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Adj. Flow (vph)	10	1	452	0	0	0	427	459	0	0	502	16
RTOR Reduction (vph)	0	0	70	0	0	0	0	0	0	0	1	0
Lane Group Flow (vph)	0	11	382	0	0	0	427	459	2	2	517	0
Confl. Pnts. (#/hr)	11%	0%	4%	0%	0%	0%	15%	3%	0%	0%	3%	0%
Heavy Vehicles (%)	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted
Turn Type	8	8	8	1	1	6	1	6	1	6	1	6
Protected Phases	8	8	8	1	1	6	1	6	1	6	1	6
Permitted Phases	8	8	8	1	1	6	1	6	1	6	1	6
Actuated Green, G (s)	6.9	49.9	39.0	89.1	46.1	46.1	39.0	89.1	46.1	46.1	39.0	89.1
Effective Green, g (s)	6.9	49.9	39.0	90.1	47.1	47.1	39.0	90.1	47.1	47.1	39.0	90.1
Actuated g/C Ratio	0.07	0.48	0.37	0.86	0.45	0.45	0.37	0.86	0.45	0.45	0.37	0.86
Clearance Time (s)	4.0	4.0	4.0	5.0	5.0	5.0	4.0	5.0	5.0	5.0	4.0	5.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	87	738	563	3008	824	824	563	3008	824	824	563	3008
v/s Ratio Prot	0.01	0.25	0.27	0.13	0.28	0.28	0.27	0.13	0.28	0.28	0.27	0.13
v/s Ratio Perm	0.13	0.52	0.73	0.15	0.63	0.63	0.73	0.15	0.63	0.63	0.73	0.15
v/c Ratio	46.2	19.2	28.5	1.2	22.2	22.2	28.5	1.2	22.2	22.2	28.5	1.2
Progression Factor	1.00	1.00	0.76	0.89	1.00	1.00	0.76	0.89	1.00	1.00	0.76	0.89
Incremental Delay, d2	0.7	0.6	7.5	0.1	3.6	3.6	7.5	0.1	3.6	3.6	7.5	0.1
Delay (s)	46.9	19.8	29.4	0.8	25.8	25.8	29.4	0.8	25.8	25.8	29.4	0.8
Level of Service	D	B	C	A	C	C	C	A	C	C	C	A
Approach Delay (s)	20.4	0.0	0.0	0.0	25.8	25.8	0.0	0.0	25.8	25.8	0.0	0.0
Approach LOS	C	A	A	A	B	B	A	A	B	B	A	A

Intersection Summary	19.1	HCM Level of Service	B
HCM Average Control Delay	19.1	HCM Level of Service	B
HCM Volume to Capacity ratio	0.65	Sum of lost time (s)	8.0
Actuated Cycle Length (s)	105.0	ICU Level of Service	B
Intersection Capacity Utilization	60.3%	Analysis Period (min)	15
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis  
 2: Boones Ferry Road & 95th Avenue

Carl's Jr. TIS  
 Existing + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	5	4	4	4	4	4	4	4	4	4	4
Volume (vph)	165	5	728	45	5	5	316	715	15	5	736	116
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	0.88	1.00	1.00	1.00	1.00	0.97	0.95	1.00	0.95	1.00	0.95
Flpb, ped/bikes	1.00	1.00	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	1.00	0.85	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.95	1.00	0.95	1.00	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1708	2710	1804	1472	3127	3333	1805	3539	1468	1805	3539	1468
Flt Permitted	0.73	1.00	0.47	1.00	0.95	1.00	0.95	1.00	1.00	0.95	1.00	1.00
Satd. Flow (perm)	1299	2710	891	1472	3127	3333	1805	3539	1468	1805	3539	1468
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	172	5	758	47	5	5	329	745	16	5	767	121
RTOR Reduction (vph)	0	0	69	0	4	0	0	1	0	0	0	49
Lane Group Flow (vph)	0	177	689	47	6	0	329	760	0	5	767	72
Confl. Pnts. (#/hr)	2	0	1	1	2	2	1	1	0	2	1	2
Heavy Vehicles (%)	6%	0%	4%	0%	0%	17%	12%	8%	7%	0%	2%	10%
Turn Type	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted	Permitted
Protected Phases	8	8	8	4	4	4	1	6	1	5	2	2
Permitted Phases	8	8	8	4	4	4	1	6	1	5	2	2
Actuated Green, G (s)	19.5	49.9	19.5	19.5	19.5	19.5	30.4	72.2	1.3	43.1	43.1	43.1
Effective Green, g (s)	19.5	49.9	19.5	19.5	19.5	19.5	30.4	72.2	1.3	43.1	43.1	43.1
Actuated g/C Ratio	0.19	0.48	0.19	0.19	0.19	0.19	0.29	0.69	0.01	0.41	0.41	0.41
Clearance Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	241	1391	165	273	905	292	22	1453	603	22	1453	603
v/s Ratio Prot	0.14	0.11	0.05	0.00	0.11	0.23	0.00	0.22	0.05	0.00	0.22	0.05
v/s Ratio Perm	0.73	0.50	0.28	0.02	0.36	0.33	0.23	0.53	0.12	0.23	0.53	0.12
v/c Ratio	40.3	18.9	36.8	35.0	29.6	6.6	51.4	23.3	19.2	51.4	23.3	19.2
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.80	1.03	0.98	1.08	1.08
Incremental Delay, d2	11.0	0.3	1.0	0.0	1.0	0.4	4.4	0.3	0.1	5.7	23.2	20.8
Delay (s)	51.3	19.2	37.7	35.0	28.0	5.7	57.2	23.2	20.8	57.2	23.2	20.8
Level of Service	D	B	D	C	C	C	E	C	C	E	C	C
Approach Delay (s)	25.3	0.0	37.2	0.0	12.4	12.4	0.0	0.0	23.1	0.0	23.1	0.0
Approach LOS	C	A	D	D	B	B	A	A	C	A	C	C

Intersection Summary	20.1	HCM Level of Service	C
HCM Average Control Delay	20.1	HCM Level of Service	C
HCM Volume to Capacity ratio	0.55	Sum of lost time (s)	8.0
Actuated Cycle Length (s)	105.0	ICU Level of Service	B
Intersection Capacity Utilization	60.3%	Analysis Period (min)	15
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis  
 3: I-5 SB On Ramp/I-5 SB Off Ramp & Boones Ferry Road/Sw Elligsen Rd Existing + Stage II (PM Peak)

Carl's Jr. TIS



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4	4	4	4	4	4
Volume (vph)	0	563	566	0	605	315	0	0	0	485	0	441
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00	0.95	0.95	1.00	0.95	1.00	0.95
Fit Protected	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Satd. Flow (prot)	3438	1563	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Fit Permitted	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Satd. Flow (perm)	3438	1563	3312	1599	1649	1649	1649	1649	1649	1649	1649	1649
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	0	593	579	0	630	328	0	0	0	516	0	459
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	163
Lane Group Flow (vph)	0	993	579	0	630	328	0	0	0	288	268	296
Heavy Vehicles (%)	0%	5%	4%	0%	9%	1%	0%	0%	0%	4%	0%	15%
Turn Type	NA	Free	Free	NA	Free	Free	NA	Free	Free	Split	NA	Prot
Protected Phases	2	6	6	6	6	6	6	6	6	6	6	6
Permitted Phases	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free
Actuated Green, G (s)	68.8	105.0	105.0	68.8	105.0	105.0	68.8	105.0	105.0	68.8	105.0	105.0
Effective Green, g (s)	69.8	105.0	105.0	69.8	105.0	105.0	69.8	105.0	105.0	69.8	105.0	105.0
Actuated g/C Ratio	0.66	1.00	1.00	0.66	1.00	1.00	0.66	1.00	1.00	0.66	1.00	1.00
Clearance Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	2285	1563	2202	1599	1649	1649	1649	1649	1649	1649	1649	1649
vis Ratio Prot	0.29	0.19	0.19	0.19	0.19	0.19	0.19	0.19	0.19	0.19	0.19	0.21
vis Ratio Perm	0.43	0.37	0.37	0.29	0.29	0.21	0.21	0.21	0.21	0.21	0.21	0.21
v/c Ratio	8.3	8.3	8.3	7.3	7.3	7.3	7.3	7.3	7.3	7.3	7.3	7.3
Uniform Delay, d1	0.74	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Progression Factor	0.5	0.6	0.6	0.3	0.3	0.3	0.3	0.3	0.3	0.3	0.3	0.3
Incremental Delay, d2	6.7	0.6	0.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6
Delay (s)	A	A	A	A	A	A	A	A	A	A	A	A
Level of Service	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s)	4.4	5.1	5.1	5.1	5.1	5.1	5.1	5.1	5.1	5.1	5.1	5.1
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	A

Intersection Summary	EB	WB	NB	SB
HCM Average Control Delay	15.2	15.1	15.1	15.1
HCM Volume to Capacity ratio	0.54	0.54	0.54	0.54
Actuated Cycle Length (s)	105.0	105.0	105.0	105.0
Intersection Capacity Utilization	50.7%	50.7%	50.7%	50.7%
Analysis Period (min)	15	15	15	15

c Critical Lane Group

HCM Unsignalized Intersection Capacity Analysis  
 4: 95th Avenue & Chevron-Holiday Inn/Project Driveway

Carl's Jr. TIS



Movement	WBL	WBR	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4
Volume (veh/h)	37	142	756	67	122	315	315
Stop	0%	0%	0%	0%	0%	0%	0%
Sign Control	Free	Free	Free	Free	Free	Free	Free
Grade	0%	0%	0%	0%	0%	0%	0%
Peak Hour Factor	0.78	0.78	0.78	0.78	0.78	0.78	0.78
Hourly flow rate (vph)	47	182	969	86	155	404	404
Pedestrians	0	0	0	0	0	0	0
Lane Width (ft)	12	12	12	12	12	12	12
Walking Speed (ft/s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Percent Blockage	0	0	0	0	0	0	0
Right turn flare (veh)	0	0	0	0	0	0	0
Median type	None	None	None	None	None	None	None
Median storage (veh)	0	0	0	0	0	0	0
Upstream signal (ft)	0.65	0.65	0.65	0.65	0.65	0.65	0.65
p/c, platoon unblocked	1527	1012	1012	1012	1012	1012	1012
v/c, conflicting volume	0.85	0.85	0.85	0.85	0.85	0.85	0.85
v/c1, stage 1 conf vol	0.85	0.85	0.85	0.85	0.85	0.85	0.85
v/c2, stage 2 conf vol	0.85	0.85	0.85	0.85	0.85	0.85	0.85
v/cu, unblocked vol	1542	745	745	745	745	745	745
IC, single (s)	7.0	6.9	6.9	6.9	6.9	6.9	6.9
IC, 2 stage (s)	3.6	3.3	3.3	3.3	3.3	3.3	3.3
IF (s)	0	0	0	0	0	0	0
p/c queue free %	0	21	21	21	21	21	21
pl queue free (veh/h)	45	232	232	232	232	232	232
Direction, Lane #	WB1	WB2	NB1	NB2	SB1	SB2	SB2
Volume Total	47	182	1055	291	269	269	269
Volume Left	47	0	0	155	0	0	0
Volume Right	0	182	86	0	0	0	0
CSH	45	232	1700	520	1700	1700	1700
Volume to Capacity	1.05	0.79	0.62	0.30	0.16	0.16	0.16
Queue Length 95th (ft)	109	143	0	31	0	0	0
Control Delay (s)	291.6	60.7	0.0	10.3	0.0	0.0	0.0
Lane LOS	F	F	F	B	B	B	B
Approach Delay (s)	108.4	0.0	0.0	5.3	0.0	0.0	0.0
Approach LOS	F	F	F	B	B	B	B

Intersection Summary	WB	NB	SB
Average Delay	15.1	15.1	15.1
Intersection Capacity Utilization	69.4%	69.4%	69.4%
Analysis Period (min)	15	15	15

ICU Level of Service C



HCM Signalized Intersection Capacity Analysis  
5: 95th Avenue & Commerce Circle

Carl's Jr. TIS  
Existing + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	230	2	35	11	1	20	35	573	14	5	342	5
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Fib, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ft	0.95	1.00	0.95	1.00	1.00	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Flt Protected	1703	1402	1530	1560	1955	1771	1805	1754				
Satd. Flow (prot)	0.74	1.00	0.73	1.00	0.49	1.00	0.27	1.00				
Flt Permitted	1331	1402	1175	1560	825	1771	519	1754				
Satd. Flow (perm)	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Peak-hour factor, PHF	258	2	39	12	1	22	39	644	16	6	384	6
Adj. Flow (vph)	0	28	0	0	16	0	0	2	0	0	1	0
RTOR Reduction (vph)	258	13	0	12	7	0	39	658	0	6	389	0
Lane Group Flow (vph)												
Confl. Peds. (#/hr)	5%	0%	17%	18%	100%	0%	13%	7%	0%	0%	8%	12%
Heavy Vehicles (%)	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA
Turn Type	4		8		2		2		6		6	
Protected Phases	4		8		2		2		6		6	
Permitted Phases	13.0	13.0	13.0	13.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0
Actuated Green, G (s)	0.29	0.29	0.29	0.29	0.53	0.53	0.53	0.53	0.53	0.53	0.53	0.53
Effective Green, g (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Actuated g/C Ratio	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Clearance Time (s)	385	405	340	451	440	945	277	935				
Vehicle Extension (s)	c0.19	0.01	0.01	0.00	c0.37	0.02	0.01	0.22				
Lane Grp Cap (vph)	0.67	0.03	0.04	0.02	0.09	0.70	0.02	0.42				
vs Ratio Prot	14.1	11.5	11.5	11.4	5.1	7.8	5.0	6.3				
v/s Ratio Perm	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00				
Uniform Delay, d1	4.5	0.0	0.0	0.0	0.4	4.2	0.1	1.4				
Progression Factor	18.6	11.5	11.5	11.4	5.5	12.0	5.1	7.7				
Incremental Delay, d2	B	B	B	B	A	B	A	A				
Delay (s)	17.7	B	B	B	11.5	B	11.7	7.6				
Level of Service	B	B	B	B	B	B	B	A				
Approach Delay (s)												
Approach LOS												

Intersection Summary	
HCM Average Control Delay	11.8
HCM Volume to Capacity ratio	0.69
Actuated Cycle Length (s)	45.0
Intersection Capacity Utilization	57.1%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
6: 95th Avenue & Ridder Rd

Carl's Jr. TIS  
Existing + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	82	19	48	6	21	58	46	402	4	50	243	82
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Total Lost time (s)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Fib, ped/bikes	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ft	1.00	0.89	1.00	1.00	0.89	1.00	1.00	1.00	1.00	0.95	1.00	1.00
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	1.00	1.00
Satd. Flow (prot)	1645	1538	1543	1558	1504	1821	1766	1712				
Flt Permitted	0.80	1.00	0.80	1.00	0.55	1.00	0.49	1.00				
Satd. Flow (perm)	1385	1538	1299	1558	864	1821	916	1712				
Peak-hour factor, PHF	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Adj. Flow (vph)	91	21	53	7	23	64	51	447	4	56	270	91
RTOR Reduction (vph)	0	44	0	0	54	0	0	0	0	0	22	0
Lane Group Flow (vph)	91	30	0	7	33	0	51	451	0	56	339	0
Confl. Peds. (#/hr)	4		10%	17%	10%	5%	20%	4%	25%	2%	5%	12%
Heavy Vehicles (%)	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA
Turn Type	4		8		2		2		6		6	
Protected Phases	4		8		2		2		6		6	
Permitted Phases	5.0	5.0	5.0	5.0	17.8	17.8	17.8	17.8	17.8	17.8	17.8	17.8
Actuated Green, G (s)	0.16	0.16	0.16	0.16	0.16	0.16	0.16	0.16	0.16	0.16	0.16	0.16
Effective Green, g (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Actuated g/C Ratio	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Clearance Time (s)	225	250	211	253	499	1052	529	969				
Vehicle Extension (s)	c0.07	0.02	0.02	0.02	0.06	0.20	0.06	0.20				
Lane Grp Cap (vph)	0.40	0.12	0.03	0.13	0.10	0.43	0.11	0.34				
vs Ratio Prot	11.5	11.0	10.9	11.0	2.9	3.6	2.9	3.4				
v/s Ratio Perm	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00				
Uniform Delay, d1	1.2	0.2	0.1	0.2	0.1	0.3	0.1	0.2				
Progression Factor	12.8	11.2	10.9	11.3	3.0	3.9	3.0	3.6				
Incremental Delay, d2	B	B	B	B	A	A	A	A				
Delay (s)	12.1	B	B	B	11.3	B	3.8	3.5				
Level of Service	B	B	B	B	B	B	A	A				
Approach Delay (s)												
Approach LOS												

Intersection Summary	
HCM Average Control Delay	5.5
HCM Volume to Capacity ratio	0.42
Actuated Cycle Length (s)	30.8
Intersection Capacity Utilization	45.9%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
 1: Boones Ferry Road & SW Day Rd

Carl's Jr. TIS  
 Existing + Project + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	9	1	422	0	0	0	399	431	0	0	471	15
Volume (vph)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Ideal Flow (vphpl)												
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frbp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt Protected	0.96	1.00	0.95	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Satd. Flow (prot)	1652	1553	1570	3505	1838	1838	1570	3505	1838	1838	1838	1838
Flt Permitted	0.77	1.00	0.95	1.00	1.00	1.00	0.95	1.00	1.00	1.00	1.00	1.00
Satd. Flow (perm)	1332	1553	1570	3505	1838	1838	1570	3505	1838	1838	1838	1838
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Adj. Flow (vph)	10	1	454	0	0	0	429	463	0	0	506	16
RTOR Reduction (vph)	0	0	68	0	0	0	0	0	0	0	1	0
Lane Group Flow (vph)	0	11	386	0	0	0	429	463	0	0	521	0
Confl. Pnts. (#/hr)												
Heavy Vehicles (%)	11%	0%	4%	0%	0%	0%	15%	3%	0%	0%	3%	0%
Turn Type	Perm	NA	pt-hov	Perm	Perm	Prot	NA	NA	6	Prot	NA	NA
Protected Phases	8	8	8	1	4	4	1	6	1	6	5	2
Permitted Phases	8	7.0	50.0	4	4	4	39.0	89.0	0	0	46.0	46.0
Actuated Green, G (s)		7.0	50.0				39.0	90.0			47.0	47.0
Effective Green, g (s)		0.07	0.48				0.37	0.86			0.45	0.45
Actuated g/C Ratio		4.0	5.0				4.0	5.0			5.0	5.0
Clearance Time (s)		3.0	3.0				3.0	3.0			3.0	3.0
Vehicle Extension (s)		89	740				583	3004			823	823
Lane Grp Cap (vph)		c0.25	c0.25				c0.27	0.13			c0.28	c0.28
v/s Ratio Prot		0.01	0.12				0.74	0.15			0.63	0.63
v/s Ratio Perm		46.1	19.2				28.5	1.2			22.4	22.4
Uniform Delay, d1		1.00	1.00				0.75	0.58			1.00	1.00
Progression Factor		0.6	0.7				7.7	0.1			3.7	3.7
Incremental Delay, d2		46.7	19.8				29.1	0.8			26.0	26.0
Delay (s)		D	B				C	A			C	C
Level of Service		20.5	B				0.0	A			26.0	C
Approach Delay (s)		C	C				14.4	B			26.0	C
Approach LOS		C	C				A	B			C	C

Intersection Summary	
HCM Average Control Delay	19.2
HCM Volume to Capacity ratio	0.65
Actuated Cycle Length (s)	105.0
Intersection Capacity Utilization	61.7%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
 2: Boones Ferry Road & 95th Avenue

Carl's Jr. TIS  
 Existing + Project + Stage II (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4	4	4	4	4	4
Volume (vph)	171	5	744	45	5	5	332	715	15	5	736	122
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	0.88	1.00	1.00	1.00	1.00	0.97	0.95	1.00	1.00	0.95	1.00
Frbp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt Protected	0.85	1.00	0.85	1.00	1.00	1.00	0.85	1.00	1.00	1.00	0.85	1.00
Satd. Flow (prot)	1708	2709	1804	1472	3127	3333	1805	3539	1468	1805	3539	1468
Flt Permitted	0.73	1.00	0.46	1.00	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00
Satd. Flow (perm)	1288	2709	876	1472	3127	3333	1805	3539	1468	1805	3539	1468
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	178	5	775	47	5	5	346	745	16	5	767	127
RTOR Reduction (vph)	0	0	69	0	4	0	0	1	0	0	0	0
Lane Group Flow (vph)	0	183	706	47	6	0	346	760	0	5	767	76
Confl. Pnts. (#/hr)	2	1	1	1	1	2	2	2	0	5	767	76
Heavy Vehicles (%)	6%	0%	4%	0%	17%	20%	12%	8%	7%	0%	2%	10%
Turn Type	Perm	NA	pm-hov	Perm	NA	Prot	NA	NA	6	Prot	NA	NA
Protected Phases	8	8	8	4	4	4	1	6	1	6	5	2
Permitted Phases	8	8	8	4	4	4	1	6	1	6	5	2
Actuated Green, G (s)	20.0	49.8	20.0	20.0	20.0	20.0	29.8	71.7	1.3	43.2	43.2	43.2
Effective Green, g (s)	20.0	49.8	20.0	20.0	20.0	20.0	29.8	71.7	1.3	43.2	43.2	43.2
Actuated g/C Ratio	0.19	0.47	0.19	0.19	0.19	0.19	0.28	0.68	0.01	0.41	0.41	0.41
Clearance Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	247	1388	167	280	887	2276	887	2276	22	1456	604	604
v/s Ratio Prot	c0.14	c0.14	0.12	0.05	0.00	0.11	0.23	0.00	0.00	c0.22	0.05	0.05
v/s Ratio Perm	0.74	0.51	0.28	0.02	0.39	0.33	0.23	0.53	0.23	0.53	0.13	0.13
Uniform Delay, d1	40.1	19.1	36.4	34.5	30.3	6.8	51.4	23.2	19.2	23.2	19.2	19.2
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.93	0.79	1.03	0.98	1.07	1.07
Incremental Delay, d2	11.3	0.3	0.9	0.0	1.2	0.4	4.4	0.3	0.1	4.4	0.3	0.1
Delay (s)	51.4	19.4	37.3	34.6	29.2	5.7	57.2	23.0	20.7	23.0	20.7	20.7
Level of Service	D	B	D	D	C	C	E	A	E	A	C	C
Approach Delay (s)	26.5	C	C	36.8	D	D	13.1	B	13.1	B	22.9	C
Approach LOS	C	C	C	D	D	D	B	B	B	B	C	C

Intersection Summary	
HCM Average Control Delay	20.4
HCM Volume to Capacity ratio	0.56
Actuated Cycle Length (s)	105.0
Intersection Capacity Utilization	60.9%
Analysis Period (min)	15
c Critical Lane Group	

HCM Signalized Intersection Capacity Analysis  
 3: I-5 SB On Ramp/I-5 SB Off Ramp & Boones Ferry Road/Sw Ellingsworth Project - Stage II (PM Peak)

Carl's Jr. TIS

Existing - Project + Stage II (PM Peak)

Movement	ESL	EST	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4	4	4	4	4	4
Volume (vph)	0	966	559	0	613	315	0	0	0	485	0	449
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00	0.95	0.95	1.00	0.95	0.95	1.00
Flt Protected	1.00	1.00	0.85	1.00	1.00	1.00	1.00	1.00	0.85	1.00	1.00	0.85
Satd. Flow (prot)	3438	1553	3312	1599	1649	1649	1649	1649	1404	1649	1649	1404
Flt Permitted	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95	1.00
Satd. Flow (perm)	3438	1553	3312	1599	1649	1649	1649	1649	1404	1649	1649	1404
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	0	1006	582	0	639	328	0	0	0	516	0	468
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	157
Lane Group Flow (vph)	0	1006	582	0	639	328	0	0	0	258	258	311
Heavy Vehicles (%)	0%	5%	4%	0%	9%	1%	0%	0%	0%	4%	0%	15%
Turn Type	NA	Free	Free	NA	Free	Free	Free	Free	Free	Split	NA	Prot
Protected Phases	2	Free	Free	6	Free	Free	Free	Free	Free	4	NA	4
Permitted Phases	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free
Actuated Green, G (s)	67.8	105.0	67.8	67.8	105.0	67.8	67.8	67.8	67.8	28.2	28.2	28.2
Effective Green, g (s)	68.8	105.0	68.8	68.8	105.0	68.8	68.8	68.8	68.8	28.2	28.2	28.2
Actuated g/C Ratio	0.66	1.00	0.66	0.66	1.00	0.66	0.66	0.66	0.66	0.27	0.27	0.27
Clearance Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	4.0	4.0	4.0	4.0	4.0	4.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	2253	1553	2170	1599	1599	1599	443	443	377	0.16	0.16	c0.22
vis Ratio Prot	c0.29	0.37	0.37	0.19	0.21	0.21	0.21	0.21	0.21	0.58	0.58	0.82
vis Ratio Perm	0.45	0.37	0.37	0.29	0.21	0.21	0.21	0.21	0.21	0.33	0.33	0.36
Uniform Delay, d1	8.8	0.0	0.0	7.7	0.0	0.0	33.3	33.3	36.1	1.00	1.00	1.00
Progression Factor	0.74	1.00	1.00	1.00	1.00	1.00	2.0	2.0	13.6	2.0	2.0	13.6
Incremental Delay, d2	0.5	0.6	0.6	0.3	0.3	0.3	35.2	35.2	49.7	0.0	0.0	0.0
Delay (s)	7.1	0.6	0.6	8.1	0.3	0.3	35.2	35.2	49.7	0.0	0.0	0.0
Level of Service	A	A	A	A	A	A	D	D	D	D	D	D
Approach Delay (s)	4.7	5.4	5.4	5.4	5.4	5.4	0.0	0.0	42.1	0.0	0.0	42.1
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	D

Intersection Summary	WBL	WBR	NBL	NBT	NBR	SBL	SBT
Volume Total	68	233	1065	326	262	68	233
Volume Left	0	0	0	195	0	0	0
Volume Right	0	233	119	0	0	0	0
CSH	33	236	1700	510	1700	33	236
Volume to Capacity	2.03	0.99	0.63	0.38	0.15	2.03	0.99
Queue Length 95th (ft)	191	230	0	44	0	191	230
Control Delay (s)	735.2	100.3	0.0	12.5	0.0	735.2	100.3
Lane LOS	F	F	B	B	B	F	F
Approach Delay (s)	243.5	0.0	0.0	6.9	0.0	243.5	0.0
Approach LOS	F	F	B	B	B	F	F

Intersection Summary	WBL	WBR	NBL	NBT	NBR	SBL	SBT
Average Delay	39.6	70.7%	70.7%	15	15	39.6	70.7%
Intersection Capacity Utilization	70.7%	70.7%	70.7%	15	15	70.7%	70.7%
Analysis Period (min)	15	15	15	15	15	15	15

HCM Signalized Intersection Capacity Analysis  
 4: 95th Avenue & Chevron-Holiday Inn/Project Driveway

Carl's Jr. TIS

Existing - Project + Stage II (PM Peak)

Movement	ESL	EST	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	4	4	4	4	4	4	4	4	4	4	4	4
Volume (veh/h)	53	182	738	93	152	307	0	0	0	0	0	0
Stop	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free	Free
Grade	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Peak-Hour Factor	0.78	0.78	0.78	0.78	0.78	0.78	0.78	0.78	0.78	0.78	0.78	0.78
Hourly flow rate (vph)	68	233	946	119	195	394	0	0	0	0	0	0
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Walking Speed (ft/s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5
Percent Blockage	0	0	0	0	0	0	0	0	0	0	0	0
Right turn flare (veh)	0	0	0	0	0	0	0	0	0	0	0	0
Median type	None	None	None	None	None	None	None	None	None	None	None	None
Median storage (veh)	0	0	0	0	0	0	0	0	0	0	0	0
Upstream signal (ft)	0	0	0	0	0	0	0	0	0	0	0	0
pX, platoon unblocked	0.64	0.64	0.64	0.64	0.64	0.64	0.64	0.64	0.64	0.64	0.64	0.64
vC, conflicting volume	1592	1006	1006	1592	1006	1006	1592	1006	1006	1592	1006	1006
vC1, stage 1 conf vol	0	0	0	0	0	0	0	0	0	0	0	0
vC2, stage 2 conf vol	0	0	0	0	0	0	0	0	0	0	0	0
vCu, unblocked vol	1645	725	725	1645	725	725	1645	725	725	1645	725	725
tC, single (s)	7.0	6.9	6.9	7.0	6.9	6.9	7.0	6.9	6.9	7.0	6.9	6.9
tC, 2 stage (s)	0	0	0	0	0	0	0	0	0	0	0	0
tF (s)	3.6	3.3	3.3	3.6	3.3	3.3	3.6	3.3	3.3	3.6	3.3	3.3
p0 queue free %	0	1	1	0	1	1	0	1	1	0	1	1
c/cv capacity (veh/h)	33	236	236	33	236	236	33	236	236	33	236	236

Direction, Lane #	WB1	WB2	NB1	NB2	SB1	SB2
Volume Total	68	233	1065	326	262	68
Volume Left	0	0	0	195	0	0
Volume Right	0	233	119	0	0	0
CSH	33	236	1700	510	1700	33
Volume to Capacity	2.03	0.99	0.63	0.38	0.15	2.03
Queue Length 95th (ft)	191	230	0	44	0	191
Control Delay (s)	735.2	100.3	0.0	12.5	0.0	735.2
Lane LOS	F	F	B	B	B	F
Approach Delay (s)	243.5	0.0	0.0	6.9	0.0	243.5
Approach LOS	F	F	B	B	B	F

Intersection Summary	WB1	WB2	NB1	NB2	SB1	SB2
Average Delay	39.6	70.7%	70.7%	15	15	39.6
Intersection Capacity Utilization	70.7%	70.7%	70.7%	15	15	70.7%
Analysis Period (min)	15	15	15	15	15	15

HCM Signalized Intersection Capacity Analysis  
5: 95th Avenue & Commerce Circle

Carl's Jr. TIS  
Existing + Project + Stage I (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	230	2	35	11	1	20	35	581	14	5	350	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00
Flt Protected	1703	1402	1530	1560	1595	1771	1805	1754	1805	1754	1805	1754
Satd. Flow (prot)	0.74	1.00	0.73	1.00	0.48	1.00	0.27	1.00	0.27	1.00	0.27	1.00
Flt Permitted	1331	1402	1176	1560	812	1771	506	1754	506	1754	506	1754
Satd. Flow (perm)	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Peak-hour factor, PHF	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Adj. Flow (vph)	258	2	39	12	1	22	39	653	16	6	393	6
RTOR Reduction (vph)	0	28	0	0	16	0	0	2	0	0	0	1
Lane Group Flow (vph)	258	13	0	12	7	0	39	667	0	6	398	0
Confl. Peds. (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Heavy Vehicles (%)	8%	0%	17%	18%	100%	0%	13%	7%	0%	0%	8%	12%
Turn Type	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA
Protected Phases	4		4		8		2		2		6	
Permitted Phases	13.0	13.0	13.0	13.0	13.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0
Actuated Green, G (s)	13.0	13.0	13.0	13.0	13.0	24.0	24.0	24.0	24.0	24.0	24.0	24.0
Effective Green, g (s)	0.28	0.29	0.29	0.29	0.29	0.53	0.53	0.53	0.53	0.53	0.53	0.53
Actuated g/C Ratio	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Clearance Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Vehicle Extension (s)	385	405	340	451	433	945	270	935	270	935	270	935
Lane Grp Cap (vph)	c0.19	0.01	0.01	0.00	0.00	c0.38	0.01	0.23	0.01	0.01	0.23	0.01
vs Ratio Prot	0.67	0.03	0.04	0.02	0.02	0.09	0.71	0.02	0.02	0.43	0.02	0.43
vs Ratio Perm	14.1	11.5	11.5	11.4	11.4	5.1	7.9	5.0	6.3	6.3	6.3	6.3
Uniform Delay, d1	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Progression Factor	4.5	0.0	0.0	0.0	0.0	0.4	4.4	0.2	1.4	0.2	1.4	0.2
Incremental Delay, d2	18.6	11.5	11.5	11.4	11.4	5.6	12.3	5.1	7.8	5.1	7.8	5.1
Delay (s)	B	B	B	B	B	A	B	A	B	A	B	A
Level of Service	B	B	B	B	B	A	B	A	B	A	B	A
Approach Delay (s)	17.7		17.7		11.5		11.9		11.9		7.7	
Approach LOS	B		B		B		B		B		A	
<b>Intersection Summary</b>												
HCM Average Control Delay	11.9											
HCM Volume to Capacity ratio	0.69											
Actuated Cycle Length (s)	45.0											
Intersection Capacity Utilization	57.5%											
Analysis Period (min)	15											
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis  
6: 95th Avenue & Ridder Rd

Carl's Jr. TIS  
Existing + Project + Stage I (PM Peak)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	84	19	48	6	21	60	46	406	4	52	247	84
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flpb, ped/bikes	0.99	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Flt	1.00	0.89	1.00	1.00	0.89	1.00	1.00	1.00	1.00	0.95	1.00	0.96
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00	0.95	1.00
Satd. Flow (prot)	1645	1538	1543	1556	1556	1504	1821	1766	1712	1766	1712	1712
Flt Permitted	0.78	1.00	0.78	1.00	0.54	1.00	0.49	1.00	0.49	1.00	0.49	1.00
Satd. Flow (perm)	1358	1538	1274	1556	859	1821	908	1712	859	1821	908	1712
Peak-hour factor, PHF	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Adj. Flow (vph)	93	21	53	7	23	67	51	451	4	58	274	93
RTOR Reduction (vph)	0	44	0	0	56	0	0	0	0	0	22	0
Lane Group Flow (vph)	93	30	0	7	34	0	51	455	0	58	345	0
Confl. Peds. (#/hr)	4		4		4		4		2	2		2
Heavy Vehicles (%)	9%	11%	10%	17%	10%	5%	20%	4%	25%	2%	5%	12%
Turn Type	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA	Perm	NA
Protected Phases	4		4		8		2		2		6	
Permitted Phases	5.1	5.1	5.1	5.1	5.1	17.8	17.8	17.8	17.8	17.8	17.8	17.8
Actuated Green, G (s)	5.1	5.1	5.1	5.1	5.1	17.8	17.8	17.8	17.8	17.8	17.8	17.8
Effective Green, g (s)	0.17	0.17	0.17	0.17	0.17	0.58	0.58	0.58	0.58	0.58	0.58	0.58
Actuated g/C Ratio	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Clearance Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Vehicle Extension (s)	224	254	210	257	257	495	1049	523	986	523	986	986
Lane Grp Cap (vph)	c0.07	0.42	0.12	0.03	0.13	0.10	0.43	0.11	0.35	0.11	0.35	0.35
vs Ratio Prot	11.6	11.0	10.8	11.0	10.8	3.0	3.7	3.0	3.5	3.0	3.5	3.5
vs Ratio Perm	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay, d1	1.2	0.2	0.1	0.2	0.1	0.3	0.3	0.1	0.3	0.1	0.3	0.3
Progression Factor	12.8	11.2	10.9	11.2	10.9	3.0	4.0	3.1	3.7	3.1	3.7	3.7
Incremental Delay, d2	B	B	B	B	B	A	A	A	A	A	A	A
Delay (s)	B	B	B	B	B	A	A	A	A	A	A	A
Level of Service	B	B	B	B	B	A	A	A	A	A	A	A
Approach Delay (s)	12.1		12.1		11.2		3.9		3.9		3.6	
Approach LOS	B		B		B		A		A		A	
<b>Intersection Summary</b>												
HCM Average Control Delay	5.5											
HCM Volume to Capacity ratio	0.43											
Actuated Cycle Length (s)	30.9											
Intersection Capacity Utilization	46.3%											
Analysis Period (min)	15											
c Critical Lane Group												



# ODOT Collision Data

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# **Trip Generation and Access Analysis Memorandum**

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## MEMORANDUM

**TO:** Mike Ward, City of Wilsonville

**FROM:** Scott Mansur, P.E., PTOE  
Brad Coy, P.E.

**DATE:** March 28, 2012

**SUBJECT:** Carl's Jr. Trip Generation and Access Analysis

P12023-001-000

This memorandum documents trip generation assumptions and an access evaluation for the proposed Carl's Jr. restaurant and retail space on the southeast corner of the Boones Ferry Road/95<sup>th</sup> Avenue intersection in Wilsonville, Oregon. The applicant has requested that an evaluation of driveway operations be performed as an initial stage to the proposed development site's traffic analysis due to the critical impact that site access has on the feasibility of the project site and the proposed land uses. Figure 1 shows the proposed project site and the study area for the access analysis, which includes the following two intersections:

- Boones Ferry Road/95<sup>th</sup> Avenue
- 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas Access

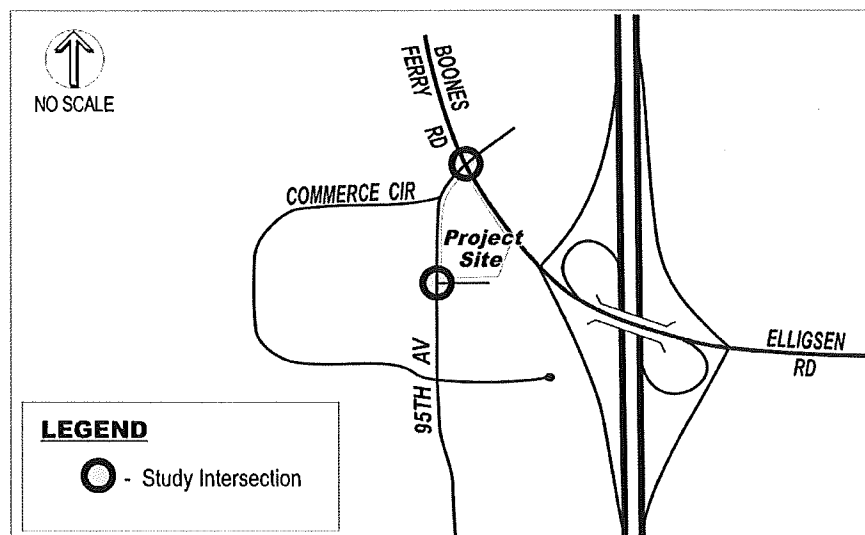


Figure 1: Project Site and Study Area

The sections of this memorandum document the proposed development, trip generation and distribution, existing traffic conditions, future traffic conditions, and access modifications.

## Proposed Development

Based on information provided by the project sponsor, the proposed development includes a 2,786 square-foot Carl's Jr. restaurant (including a drive-up window and indoor seating). In addition, the development includes a 3,375 square-foot retail business at the north end of the site. A preliminary site plan is provided in the appendix.

The development will access 95<sup>th</sup> Avenue using the driveway that serves the existing Chevron gas station and Holiday Inn. Portions of this driveway are located on the Holiday Inn, Chevron, and project site properties. Therefore, the applicant should coordinate with these adjacent property owners to determine what modifications should be made to the driveway at the time of site development to support safe and efficient access to all three sites.

## Trip Generation and Distribution

Trip generation and distribution were performed for the proposed project so that the site's driveway operations could be analyzed. Due to the proximity of the Boones Ferry Road/95<sup>th</sup> Avenue intersection and the impact that queues from this intersection have on the project driveway, it was also included in the analysis.

### *Trip Generation*

Trip generation is the method used to estimate the number of vehicles that are added to the site driveways and roadway network by the proposed development during a specified period (i.e., such as the p.m. peak hour). The trip rates used to estimate new p.m. peak hour project trips levels were obtained from the Institute of Transportation Engineers (ITE).<sup>1</sup>

- P.M. peak hour trip generation estimates were performed for the proposed 2,786 square-foot Carl's Jr. restaurant using the ITE rates for Fast Food with Drive-Thru (Land Use Code 934).
- P.M. peak hour trip generation estimates were performed for the proposed 3,375 square-foot retail building on the north end of the project site assuming the building develops as a Specialty Retail Center (ITE Land Use Code 814), which generally consists of a small strip shopping center containing a variety of retail shops that typically specialize in apparel, hard goods and services such a real estate and investment offices, dance studios, florists, and small restaurants. If more intensive uses are proposed for the strip mall than assumed by this traffic study, additional analysis will be needed.

A 10 percent internal trip rate was assumed to account for those customers who are visiting multiple land uses on the site (i.e., those who "trip chaining" by traveling between the Carl's Jr., the specialty retail, the Chevron gas station, and/or Holiday Inn). In addition, both of the proposed land uses attract pass-by trips, which were assumed to be drawn from 95<sup>th</sup> Avenue based on the existing directional (northbound/southbound) traffic volume split.

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<sup>1</sup> *Trip Generation, 8<sup>th</sup> Edition*, Institute of Transportation Engineers, 2008

Table 1 lists the project's trip generation assumptions. The total site is expected to generate 124 (62 in, 62 out) p.m. peak hour trips. Of these trips, 12 (6 in, 6 out) are expected to be internal trips and 52 (26 in, 26 out) are expected to be pass-by trips. Therefore, the site is expected to add 60 (30 in, 30 out) new p.m. peak hour trips to the study area roadway network.

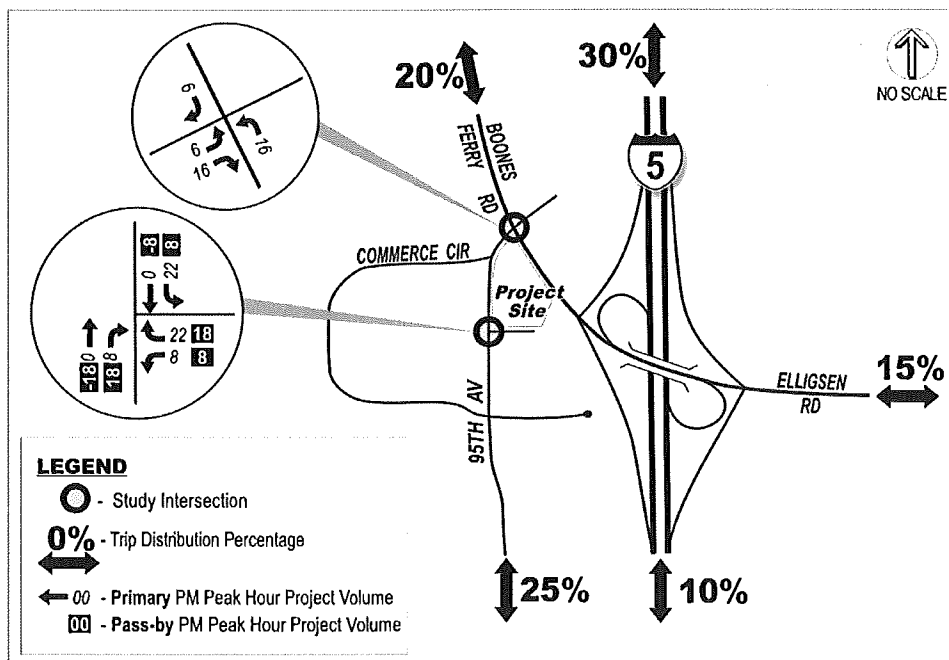
**Table 1: Carl's Jr. and Retail Center Trip Generation**

Land Use (ITE Code)	Size	P.M. Peak Hour Trip Generation Rate	P.M. Peak Hour Trips		
			In	Out	Total
Fast-Food Restaurant w/Drive-Through Window (934)	2.79 KSF <sup>a</sup>	33.84 trips/KSF <sup>a</sup>	49	45	94
Specialty Retail Center (814)	3.38 KSF <sup>a</sup>	2.71 trips/KSF <sup>a</sup>	13	17	30
<b>Total Trips</b>			<b>62</b>	<b>62</b>	<b>124</b>
<i>Internal Trips</i>			-6	-6	-12
<b>Driveway Trips</b>			<b>56</b>	<b>56</b>	<b>112</b>
<i>Pass-by Trips</i>			-26	-26	-52
<b>New Primary Trips</b>			<b>30</b>	<b>30</b>	<b>60</b>

<sup>a</sup> KSF = 1,000 square feet

*Trip Distribution*

The trip distribution for the new project traffic was generally based on existing traffic patterns in the study area and is shown in Figure 2. This figure also shows how the resulting project traffic volumes were routed onto the nearby roadway network based on the trip generation and distribution estimates as well as the pass-by trip assumptions.



**Figure 2: Project Traffic Volumes and Trip Distribution**

## Existing Traffic Conditions

Existing traffic conditions were evaluated at the project access and the Boones Ferry Road/95<sup>th</sup> Avenue intersection to identify any current safety and operational concerns and to provide a baseline for comparing the future conditions that may result from the proposed project. This section documents existing traffic volumes, existing intersection operations, and field observations.

### Traffic Volumes

Traffic volume turn movement counts were performed at the project driveway for the p.m. peak period (i.e., between 3:00 p.m. and 6:00 p.m.) on March 8, 2012. Recent historical traffic counts had been performed at the Boones Ferry Road/95<sup>th</sup> Avenue intersection on January 18, 2012. Balancing adjustments were performed at the intersections following typical traffic engineering practices. Figure 1 shows the highest hourly volumes assumed for the intersection analysis.

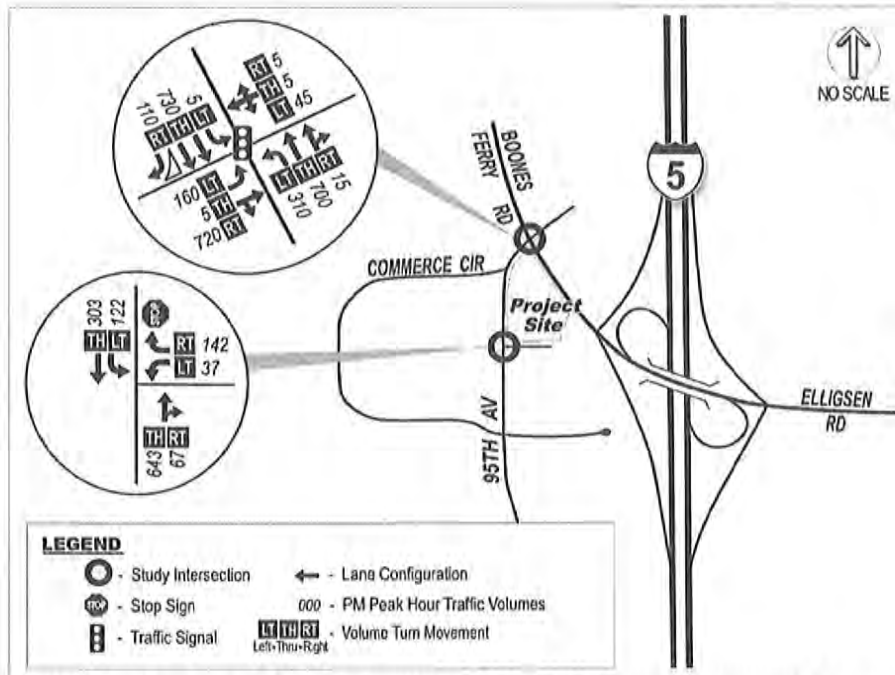


Figure 3: 2012 Existing Study Intersection Turn Movement Volumes (P.M. Peak Hour)

### Existing Intersection Operations

The existing p.m. peak hour intersection operations at the study intersections were determined based on the 2000 Highway Capacity Manual methodology.<sup>2</sup> The estimated average delay, level of service (LOS), and volume to capacity (v/c) ratio are shown in Table 2. The Boones Ferry Road/95<sup>th</sup> Avenue intersection does not currently meet the City of Wilsonville's level of service (LOS) "D" operating standard. However, an improvement project at this intersection is currently under contract and expected to be completed later in 2012.

<sup>2</sup> 2000 Highway Capacity Manual, Transportation Research Board, Washington DC, 2000.

**Table 2: 2012 Existing P.M. Peak Hour Intersection Operating Conditions**

Intersection	Operating Standard	P.M. Peak Hour			Meets Standards?
		Delay	LOS	V/C	
<b>Signalized</b>					
Boones Ferry Road/95 <sup>th</sup> Avenue	LOS D	59.5	<u>E</u>	0.74	No
<b>Unsignalized Two-Way Stop</b>					
95 <sup>th</sup> Avenue/Holiday Inn-Chevron Gas	N/A <sup>a</sup>	25.1	B/D	0.54	N/A <sup>a</sup>

Signalized Intersections:

LOS = Level of Service of Intersection  
V/C = Volume-to-Capacity Ratio of Intersection

Unsignalized Intersections:

LOS = Level of Service of Major Street/Minor Street  
V/C = Volume-to-Capacity Ratio of Worst Movement

**Bold Underlined** values do not meet standards.

<sup>a</sup> The City's LOS D standard does not apply to private driveways.

**Field Observations**

Field observations were performed during the p.m. peak hour to observe traffic operations and queuing in the vicinity of the project site.<sup>3</sup> During the visit, significant northbound queues were observed on 95<sup>th</sup> Avenue at the Boones Ferry Road intersection. Even though these queues extended past the Chevron/Holiday Inn driveway, many drivers on 95<sup>th</sup> Avenue were mindful of driveway traffic and stopped to allow entering and exiting maneuvers. Without the yielding by vehicles on 95<sup>th</sup> Avenue, delays could have been much higher for driveway traffic.

The full length of the northbound queues on 95<sup>th</sup> Avenue was approximately 1,500 feet (i.e., extending south past the 95<sup>th</sup> Avenue/Commerce Circle traffic signal). The current improvement project being constructed at the Boones Ferry Road/95<sup>th</sup> Avenue intersection is expected to address this northbound queuing issue.

Observations were also made of the shared Chevron gas station and Holiday Inn driveway, which is shown in Figure 4. One of the primary concerns was the improper use of the Holiday Inn driveway, which includes ingress and egress lanes that are separated by a raised median (see "1"). Multiple drivers were observed entering using the egress lane or exiting using the ingress lane. Another concern was how the driveway shifts (see "2"), which often resulted in drivers crossing over the stripped center-



**Figure 4: Existing Chevron Gas Station and Holiday Inn Driveway Concerns**

<sup>3</sup> Traffic operations at the site driveways were observed

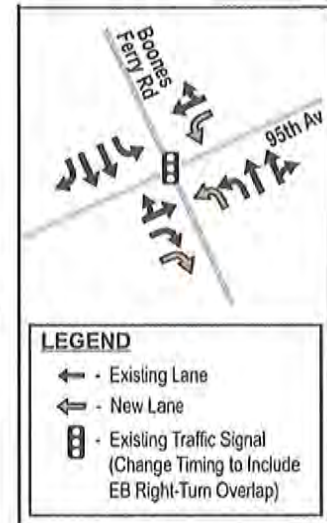


line, particularly the westbound left-turn drivers who were avoiding the right-turn queue. Significant queuing has also been observed during the height of the peak period, with exiting queues that back up on the driveway from 95<sup>th</sup> Avenue to the gas station pumps.

## Future Traffic Conditions

Future traffic analysis was performed for the project access to identify potential site access concerns. The future traffic analysis included traffic volume forecasts, future intersection operations, and traffic simulation.

The Boones Ferry Road/95<sup>th</sup> Avenue intersection was included in the analysis due to its proximity and the impact that queues from this intersection have on the project driveway. Because the Boones Ferry Road/95<sup>th</sup> Avenue improvement project is currently under contract and expected to be completed later in 2012, its completion was assumed for the future analysis. Figure 5 provides a schematic of the roadway geometry and traffic control improvements.

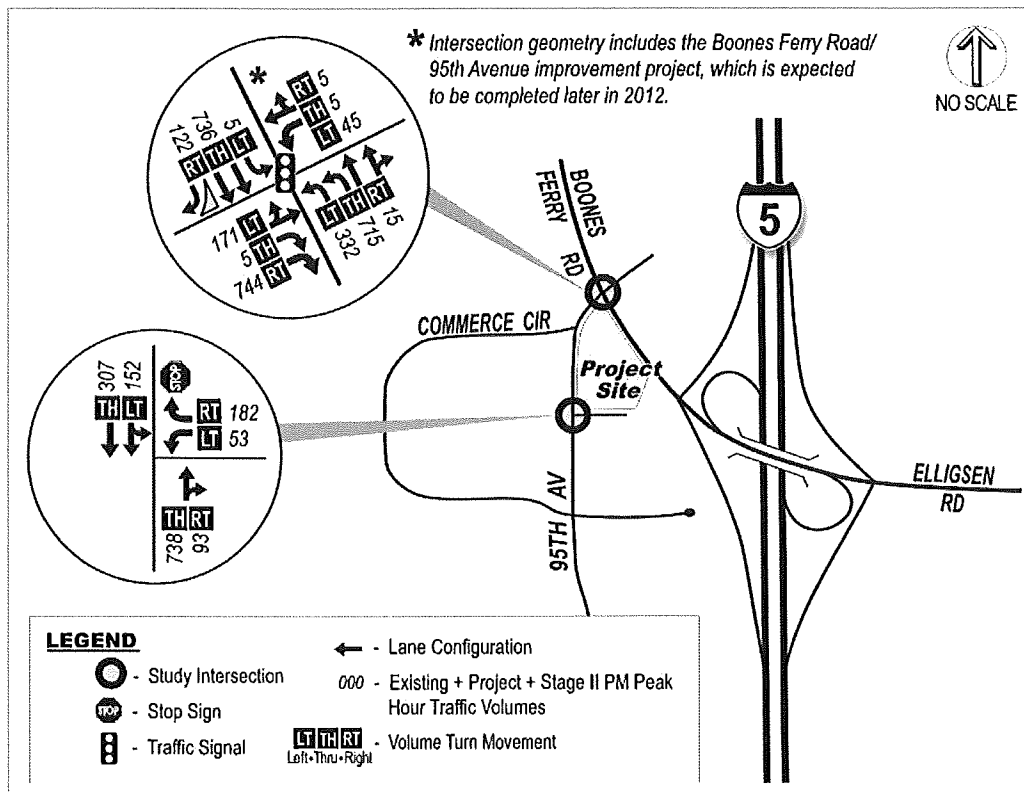


**Figure 5: Intersection Improvement Project**

## Traffic Volume Forecasts

Traffic volume forecasts were prepared for the study intersections the “Existing + Project + Stage II” traffic scenario, which includes the project trip generation volumes and traffic from other developments in the City of Wilsonville that have Stage II approval or are under construction. The proposed project traffic volumes are based on the trip generation and distribution estimates described previously in this memorandum. The Stage II traffic is estimated based on the list of currently approved Stage II developments, which was provided by City staff.<sup>4</sup> Figure 6 shows the “Existing + Project + Stage II” traffic volumes used for the analysis.

<sup>4</sup> Email from Blaise Edmonds, City of Wilsonville, March 8, 2012 (see appendix for Stage II list).



**Figure 6: Existing + Project + Stage II Traffic Volumes (P.M. Peak Hour)**

**Future Intersection Operations**

Future intersection operations analysis was performed for the study intersections to identify potential access operational concerns for the proposed project site. Table 3 lists the average delay, level of service (LOS), and volume to capacity (v/c) ratio under p.m. peak hour conditions for the “Existing + Project + Stage II” traffic scenario.

Due to the current improvement project, the Boones Ferry Road/95<sup>th</sup> Avenue intersection would meet the City’s operating standards. However, traffic operations at the project driveway are expected to worsen due to the additional driveway traffic being added by the project site. The operations analysis indicates that the vehicles exiting the project driveway would experience significant delay (greater than 50.0 seconds/vehicle). High delays are expected for both the left-turn and right-turn traffic exiting the site, but would be higher for the left-turns due to the additional crossing maneuver required. Because the City’s LOS D standard does not apply to private driveways, the City’s primary concern is ensuring that driveway operations do not negatively impact the City’s public streets or public safety. Long delays and queuing at the private access can lead to undesired driver behavior.

**Table 3: Future P.M. Peak Hour Intersection Operating Conditions**

Intersection	Operating Standard	Existing + Project + Stage II			Meets Standards?
		Delay	LOS	V/C	
<b>Signalized</b>					
Boones Ferry Road/95 <sup>th</sup> Avenue	LOS D	20.4	C	0.56	Yes
<b>Unsignalized Two-Way Stop</b>					
95 <sup>th</sup> Avenue/Holiday Inn-Chevron Gas	N/A <sup>a</sup>	>50.0	B/F	>1.0	N/A <sup>a</sup>

Signalized Intersections:

LOS = Level of Service of Intersection  
V/C = Volume-to-Capacity Ratio of Intersection

Unsignalized Intersections:

LOS = Level of Service of Major Street/Minor Street  
V/C = Volume-to-Capacity Ratio of Worst Movement

**Bold Underlined** values do not meet standards.

<sup>a</sup> The City's LOS D standard does not apply to private driveways; however, LOS D operations are preferred.

Due to the constraints of the project site, access alternatives that will resolve the minor street delay concerns (i.e., reduce delays below 50.0 seconds/vehicle) do not exist. However, additional delays can be avoided if sufficient storage is provided for both the left-turn and right-turn lanes exiting the driveway. Currently, there is only sufficient room for up to two left-turning vehicles to wait in the short left-turn lane. If either left-turn or right-turn queues exceed two vehicles, then the other movement is impeded. Additional queue storage would require reconfiguration of the eastern portion of the driveway that accesses the Chevron station.

**Traffic Simulation**

Traffic simulation was performed in SimTraffic™ to evaluate the northbound queuing on 95<sup>th</sup> Avenue and to confirm the delay at the project driveway. The simulations suggest that minimal queuing will occur on 95<sup>th</sup> Avenue under the "Existing + Project + Stage II" traffic scenario once the 95th Avenue/Boones Ferry Road intersection improvements are completed. These queues are not expected to extend back past the project driveway, which will allow free-flow movement on 95<sup>th</sup> Avenue adjacent to the project site.

On the driveway approach to 95<sup>th</sup> Avenue, the traffic simulation suggests that the average vehicle delay at the project driveway was between 45 and 50 seconds, which falls in the level of service E range. This only slightly differs from than the LOS F calculated from the operations analysis. Therefore, during the most congested periods, traffic leaving the project site can expect to experience longer delays than exist today even though they will not be blocked by long northbound queues. These higher delays are due primarily to the additional traffic from the proposed development.

## Driveway Modifications

As discussed in the Future Intersection Operations section of this memorandum, significant delay is expected at the project driveway. However, the City's LOS D standard does not apply to private driveways. Therefore, the City's primary concern is ensuring that driveway operations do not negatively impact the City's public streets or public safety.

One of the primary ways in which onsite queuing may contribute to off-site impacts is if long exiting queues on the driveway block traffic from turning left into the project site. Figure 7 illustrates how this blockage may result in queues that back up onto 95<sup>th</sup> Avenue.

In addition, Chevron and Holiday Inn have existing traffic issues that were documented previously in this memorandum. It is desirable

that all of the internal driveway issues should be resolved as part of the proposed development. This will require coordination with Chevron and Holiday Inn.



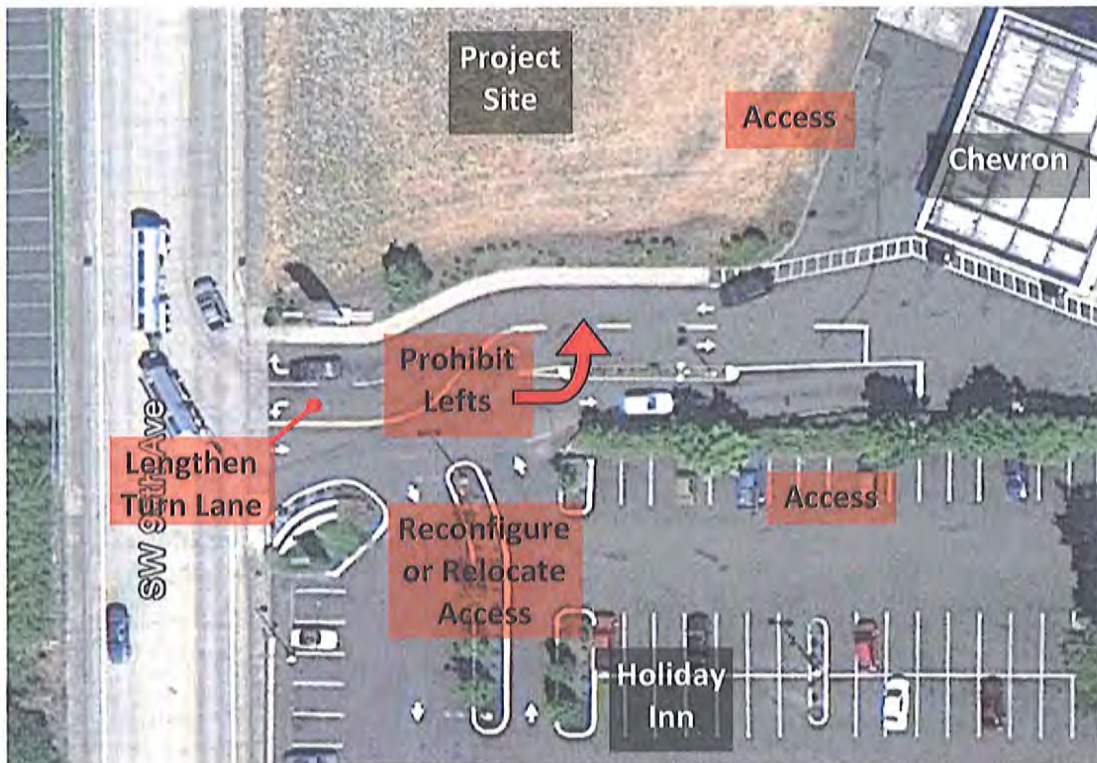
Figure 7: Internal Project Access Concern

Various driveway reconfiguration options were sketched on the applicant's site plan or an aerial photograph of the site. These are provided in the appendix and include the key elements shown in Figure 8 and listed below that should be considered by the applicant in coordination with Chevron and Holiday Inn:

- Move the project access near the Chevron fueling islands so that entering vehicles do not have to turn across the queued traffic leaving the site.
- Prohibit left turns along the main driveway throat. If an access is provided to the project site, then make it a right-in/right-out
- Reconfigure lane configurations so that additional queuing can be accommodated on the westbound left-turn lane exiting the site.

These options will not decrease the minor street delay. However, they will facilitate improved circulation and safety onsite, which will minimize impacts to the public street system. Prior to DKS moving forward with the full traffic impact study, coordination will be needed with the City and the applicant to determine a preferred driveway reconfiguration option to ensure that public street impacts are minimized.





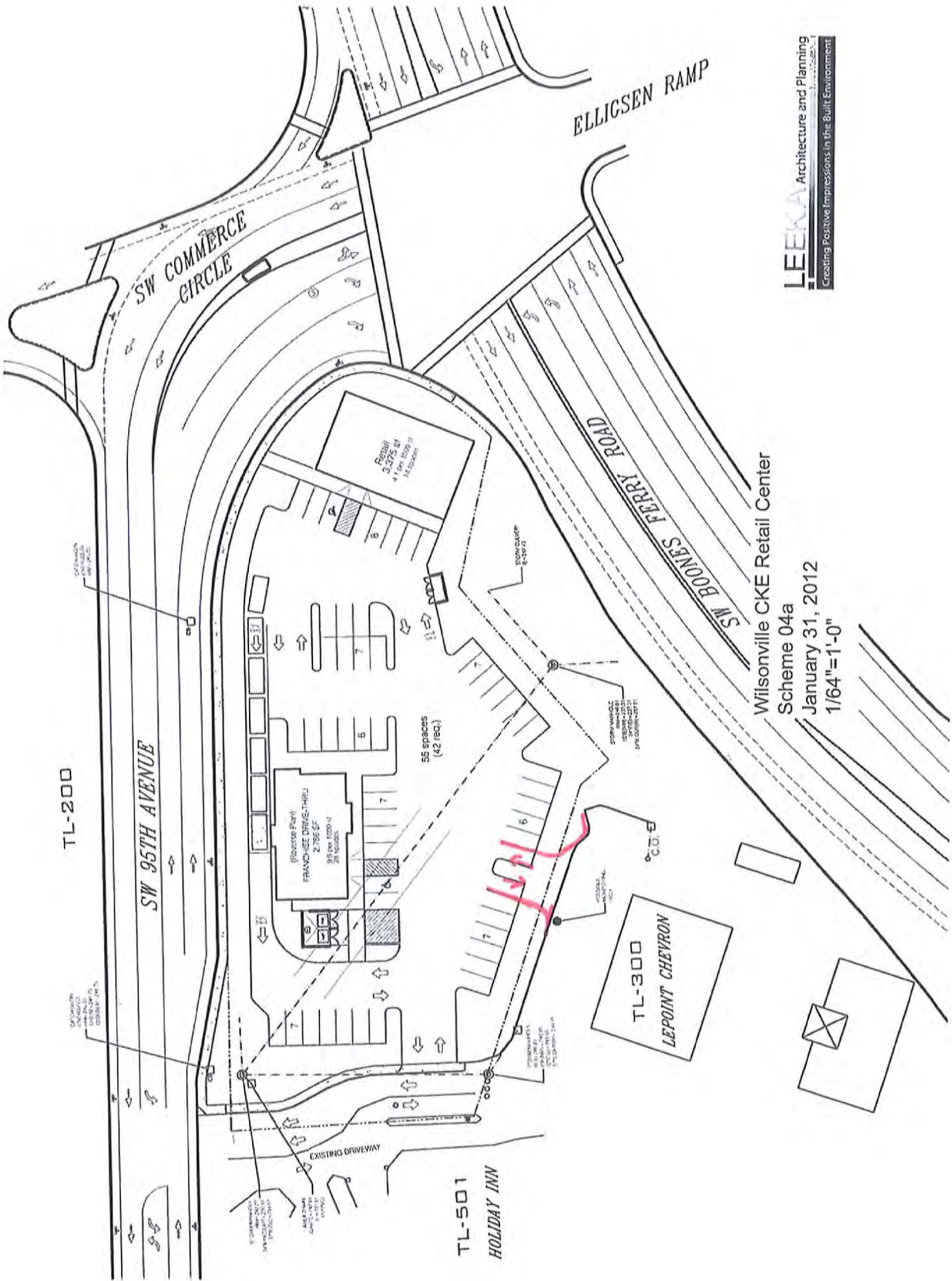
**Figure 8: Internal Driveway Reconfiguration Options**

## Truck Maneuvers

Another important access consideration is how truck maneuvers will be made at the driveway and the internal project access. Adequate turning radius should be provided on the northeast corner of the 95<sup>th</sup> Avenue/Holiday Inn-Chevron Gas driveway to allow right-turning trucks to stay within the northbound travel lane on 95<sup>th</sup> Avenue (instead of requiring them to either mount the curb or use an opposing southbound travel lane).

Understanding truck needs will also help inform the driveway reconfiguration options. Because it is unclear how truck maneuvers would be made based on the current site plan provided by the applicant, prior to DKS moving forward with a traffic impact study, a turn template should be provided that shows how delivery trucks are expected to enter, maneuver, and exit the project site (i.e., to and from the north leg of 95<sup>th</sup> Avenue and the project building).

Let us know if you have any questions or comments.



TL-200

SW 95TH AVENUE

SW COMMERCE  
CIRCLE

ELLIGSEN RAMP

SW BONES FERRY ROAD

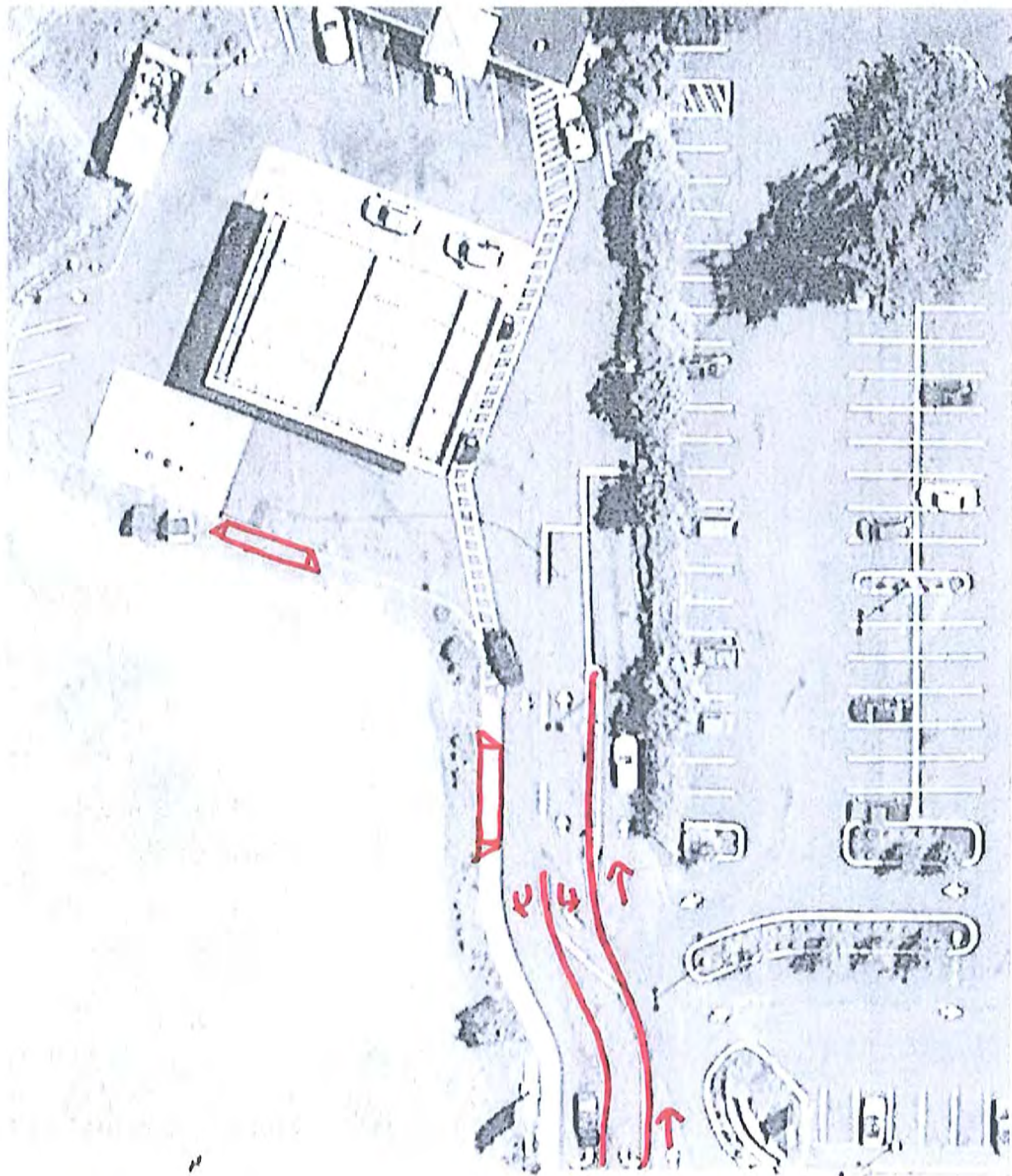
Wilsonville CKE Retail Center  
Scheme 04a  
January 31, 2012  
1/64"=1'-0"

TL-501  
HOLIDAY INN

TL-300  
LEPOINT CHEVRON

LEEKA Architecture and Planning  
Creating Positive Impressions in the Built Environment

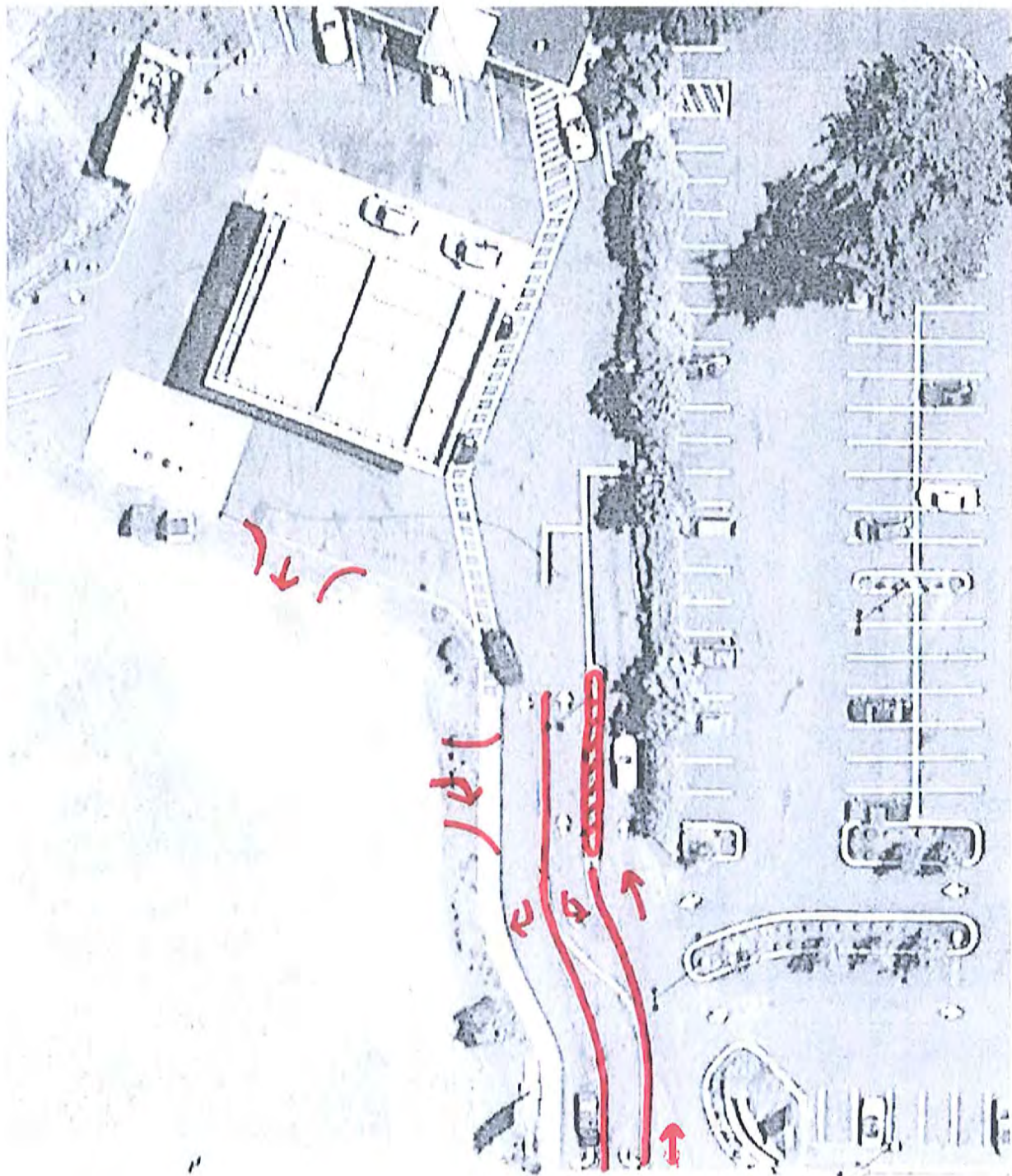




SW 95th Ave



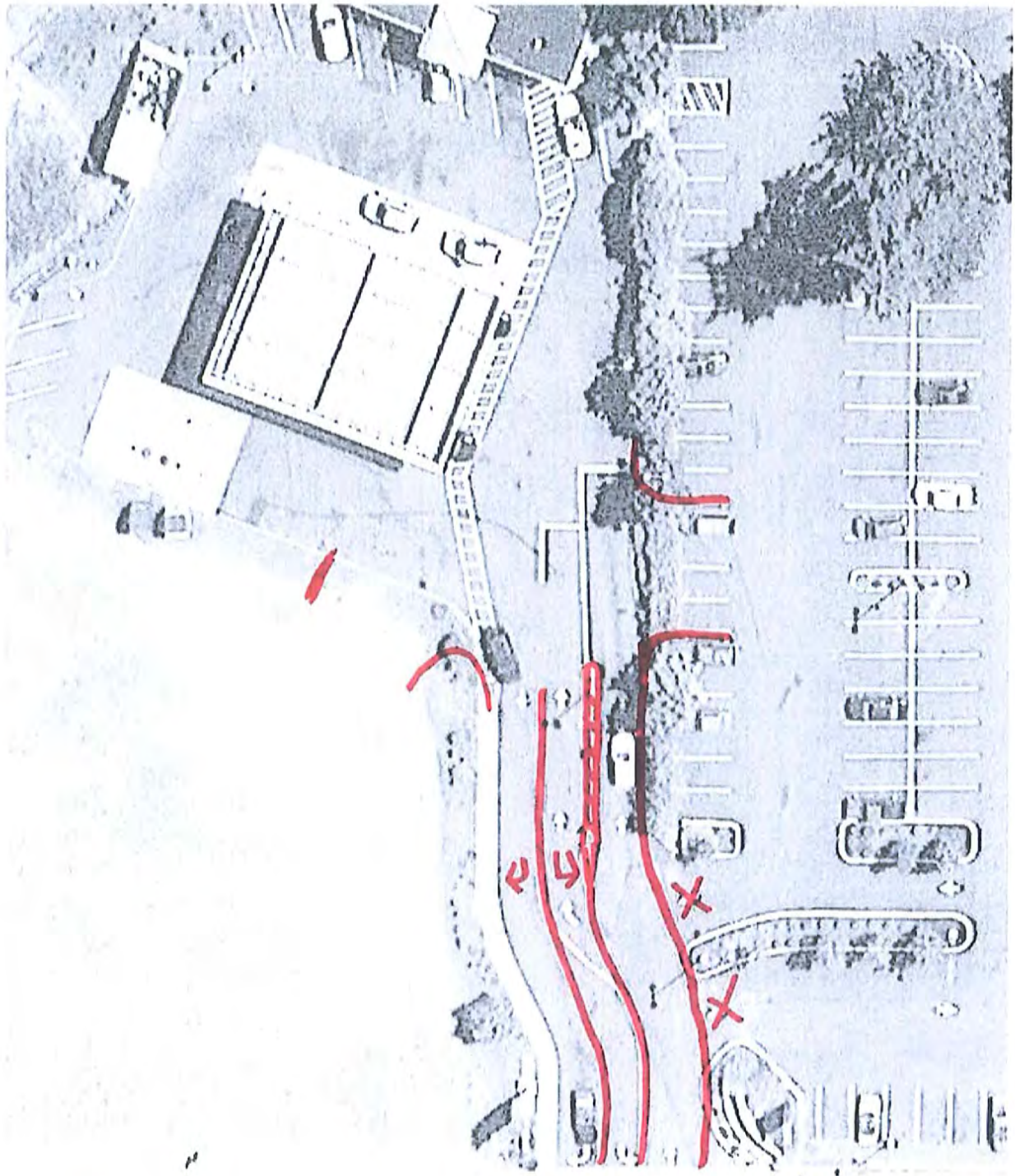




SW 95th Ave







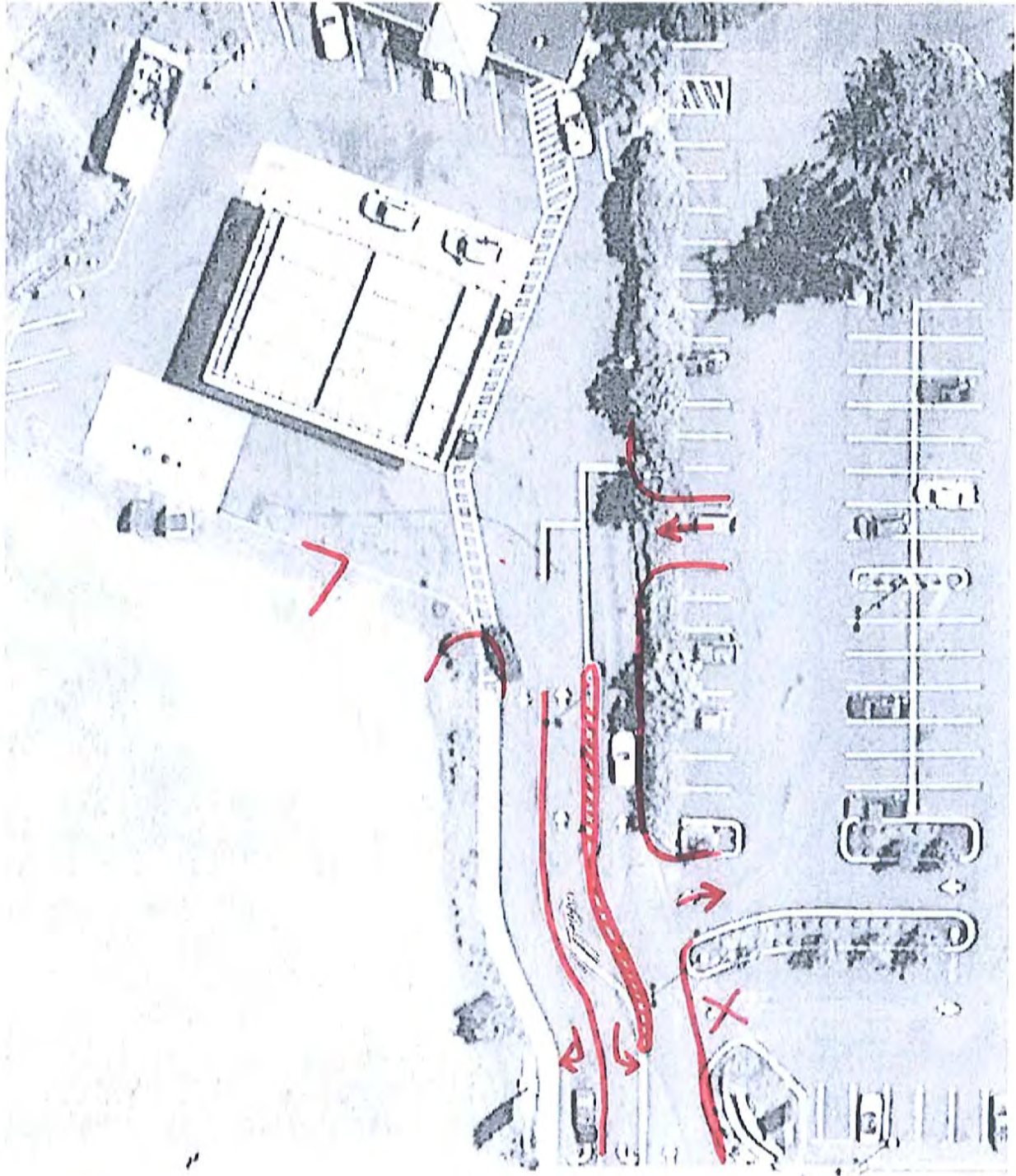
SW 95th Ave

(07)

(07)

(07)





SW 93th Ave



# ***Preliminary Storm Drainage Report***

***Carls Jr. Wilsonville  
-Boones Ferry Point Development-***

***City of Wilsonville, Oregon***

***December 31, 2012***

**Prepared for:**

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- Appendix A - Location Map, Existing Conditions Plan
- Appendix B - Soil Maps & Classification, Runoff Curve Numbers
- Appendix C - Preliminary Storm Water Design & Prelim. Plans
- Appendix D - On-site Basin Maps (Pre and Post), Utility plans, Basin calculations 2-25 (Pre & Post), Pre-developed Tc, Detention Pond Calculations, downstream basin map.
- Appendix E - On-site Geotechnical Report



## **STORM DRAINAGE REPORT**

City of Wilsonville, Washington County, OR  
MAP T3S R1E Section 2DB  
TAX LOTS 302

### **Purpose**

The purpose of this analysis is to:

- Describe existing and proposed site conditions.
- Provide data and analysis for the stormwater impacts due to the proposed Carls Jr. – Boones Ferry Pointe Development.
- Provide detention and water quality calculations for the proposed site.
- Provide upstream and downstream analysis for the site.
- Demonstrate that the proposed Boones Ferry Pointe does not significantly impact adjacent properties.

### **Introduction/Project Overview**

The proposed project will comprise development of Tax Lot 302 in Wilsonville, Oregon. As part of the work, a new Carls Jr restaurant will be constructed along with a small retail building. Necessary parking, utility, landscaping, lighting and shared access drive improvements will be constructed as needed to meet site and jurisdictional development requirements.

### **Existing Conditions**

The existing site is currently undeveloped. It is bounded to the north by NW Commerce Circle, to the northeast by Boones Ferry Road, to the southeast by an existing Chevron service station, to the south by an existing Holiday inn Parking lot, and lastly, to the west by SW 95<sup>th</sup> Avenue. The City is currently constructing frontage improvements along SW 95<sup>th</sup> Avenue therefore frontage improvements along this road will not be required as part of this project. There is an existing 24" storm line that collects stormwater from the north side of Boones Ferry and conveys it to the southwest corner of the site where it enters the existing conveyance system in 95<sup>th</sup> Avenue.

## Proposed Conditions

The proposed project will consist of two buildings and adjoined parking. The buildings will consist of a 2,900 sf Carl's Jr. Restaurant and a 3,150 sf retail building for which the tenant is currently unknown. The two buildings will share parking with one another, along with the existing Chevron service station to the east, and the shared access driveway to the south. A detention pond will be constructed at the southwest corner of the property. Stormwater treatment will be provided via a Contech Stormfilter cartridge catchbasin and manhole. The shared driveway directly south of the site that provided access to the new development, Chevron and Holiday in will be reconstructed. The existing exit from Holiday Inn will be closed and used for new parking stalls. The parking exit for Holiday Inn will be provided further east of the existing location and will align with the new exist location for the Carls Jr. – Boones Ferry Pointe location.

## Hydraulic Basin Analysis

Storm flows were calculated for both pre and post developed basins. Basin maps and summary basin calculations are included with this report as Appendix "D".

The Criteria for drainage basin calculations were based on requirements shown in the City of Wilsonville Public Works Standards 2006 – see Stormwater and Surface Water Standards.

The basin design criteria are as follows;

Design Criteria:

- Open channel flow
- 2-year, 24-hour event = 2.50 inches
- 5-year, 24-hour event = 3.10 inches
- 10-year, 24-hour event = 3.45 inches
- 25-year, 24-hour event = 3.90 inches
- 100-year, 24-hour event = 4.50 inches
- Conveyance system design storm = 25-year event
- Santa Barbara Urban Hydrograph methodology – SCS Type 1A



## Water Quality Design

### WATER QUALITY DESIGN:

As stated previously, on-site water quality will be provided via one Contech Stormfilter catchbasin (one cartridge) and one Contech Stormfilter manhole (3 cartridges). The Contech Stormfilter catchbasin will be located in the shared driveway and will be used to treat the additional impervious surface area created by the driveway build out. The Contech Stormfilter manhole will be used to treat the new post developed impervious site area for both the Carls Jr and retail site locations including parking areas.

### Water Quality Design:

The site impervious areas were calculated as follows:

**New Shared Driveway Impervious: 1,830 sf** - See Appendix "D" for basin maps.

Water Quality Calculations are as follows:

Water Quality Treatment Volume =  $(0.36") \times (1,830 \text{ sf}) \times (1 \text{ ft}/12") = 54.9 \text{ cu. ft.}$   
Design Discharge =  $(54.9 \text{ cu.ft.}) / (4 \text{ hrs.}) \times (3600 \text{ sec}/\text{hour}) = 0.004 \text{ cfs}$

**0.033 cfs = Contech WQ treatment flow per cartridge.**

**0.033 cfs > 0.004cfs** (one cartridge required for shared access driveway WQ treatment.

**New On-site Impervious: 40,340 sf** - See Appendix "D" for basin maps.

Water Quality Calculations are as follows:

Water Quality Treatment Volume =  $(0.36") \times (40,340 \text{ sf}) \times (1 \text{ ft}/12") = 1210.2 \text{ cu. ft.}$   
Design Discharge =  $(1210.2 \text{ cu.ft.}) / (4 \text{ hrs.}) \times (3600 \text{ sec}/\text{hour}) = 0.084 \text{ cfs}$

**0.033 cfs = Contech WQ treatment flow per cartridge.**

**0.099 cfs > 0.084cfs** (three cartridges required for on-site WQ treatment.

## Detention Design

### POST AND PRE-DEVELOPMENT FOR DETENTION POND:

The Pre and Post-developed site was broken down into drainage basins which are detailed on the enclosed Basin Maps – Exhibit “D”. The following table shows the Pre and Post-Developed runoff amounts calculated for the 2 Year, 10 Year, and 25 Year design storm events along with actual detention pond release rates for each. Rainfall intensities used to calculate runoff rates were taken from the City of Wilsonville Public Works Standards 2006 – see Stormwater and Surface Water Standards.

The following flows represent pre and post development runoff rates for the Carls Jr. Boones Ferry Pointe development.

<u>Storm Event</u>	<u>Pre-Development Run-off rate</u>	<u>Post-Development Run-off rate</u>	<u>Actual Detention Release Run-off rate</u>
2 Year-24 hour	0.14in.	0.58 cfs	0.14 cfs
10 Year-24 hour	0.31 in.	0.85 cfs	0.28 cfs
25 Year-24 hour	0.40 in.	0.98 cfs	0.40 cfs
100 Year-24 hour		1.16 cfs	Not Applicable

### **Computations and Routing - Detention:**

The Pre and Post-Development flows have been routed through the proposed detention pond. Detention and release rates have been designed to avoid any negative impacts to surrounding properties. The release rates have also been designed to meet the standards for detention as detailed in the City of Wilsonville Public Works Standards 2006 – see Stormwater and Surface Water Standards.

The Pond has been designed so that Post-Developed release rates do not exceed the Pre-Developed rates for the 2, 10, and 25 Year Storm events. See Exhibit “D” for stage-storage routing. The HydroCAD Modeling program was used to calculate detention volumes.

**Emergency Spillway Note:** an emergency detention pond overflow spillway is required to provide conveyance capacity for the 100 Year storm event per Wilsonville storm standards. The low point of the proposed detention pond will be at the southwest corner of the pond. The entire southwest corner of the pond is exposed in the event an overflow spillway is ever required. The overflow spillway will be located at the top of 25 Year detention storm event at Elevation 246.20.

**Basin summary:**

Post Developed Impervious  
0.93 Acres (includes new shared d/w area)  
-0.076 (existing impervious)  
0.85 Acres (used for det. calcs.)

Pre Developed Impervious  
0.076 Acres

Post Developed Pervious  
0.36 Acres

Pre Developed Pervious  
1.21 Acres

The following table details the required calculated detention volumes for the WQ, 2, 10, and 25 year storms:

<u>Storm Event</u>	<u>Detention Volume Provided</u>
2 Year-24 hour	2,003 cu.ft.
10 Year-24 hour	3,035 cu.ft.
25 Year-24 hour	3,285 cu.ft.

The proposed detention pond will have the following stage-storage available:

<u>Elevation</u>	<u>Area</u>	<u>Incremental Volume</u>	<u>Accumulated Volume</u>
243.00	329 SF		0
		756 cu.ft.	
244.00	1,183 SF		756 cu. ft.
		1,463 cu.ft.	
245.00	1,743 SF		1,463 cu.ft.
		2,066.5 cu.ft.	
246.00	2,390 SF		3,529.5 cu.ft.

Elev. 245.26 - 2 YR. detention  
Elev. 245.76 - 10 YR. detention  
Elev. 245.88 - 25 YR. detention

Volume used for flow dissipation and sediment storage (*not detention*)

See Appendix "D" for calculations



## Conveyance System Design and Analysis

Storm Drainage from The Boones Ferry Ponte development will be collected in inlet structures and storm drain laterals, where it will enter the storm pipe conveyance system.

Pipe conveyance for the project will be analyzed and designed to convey the peak 25-Year, 24-Hour storm event as part of the final construction drawing submittal.

Methodology: The site conveyance calculations will be performed using the SHUH - SCS Type 1-A unit hydrograph method.

## Upstream Analysis

There is currently a 24" storm line that carries water from the north side of Boones Ferry Road (just north of the proposed site) to the southwest corner of the site where it enters into the existing underground storm system. As part of the on-site utility work, this 24" CMP pipe will be removed and a new 24" pipe in a slightly revised alignment will take its place. None of the proposed on-site stormwater will enter into the new 24" storm line through the site. This line will function just as in the past.

## Downstream Analysis

Existing stormwater on-site is currently collected at the southwest corner of the site where it then enters into the existing storm system. The proposed stormwater will leave the site in exactly the same fashion as pre-developed conditions. Since on-site stormwater detention will be performed on-site, and extensive downstream analysis should not be necessary. The post developed storm flows for the 2-25 Year storm events will leave the site at pre-developed storm flow rates.

## Conclusions

- The conveyance system for the proposed Carls Jr./Boones Ferry Pointe site will be sized to convey the peak 25-Year, 24-Hour storm as per City of Wilsonville stormwater standards.
- Water Quality will be provided on-site per City of Wilsonville stormwater standards.
- Detention on-site as been provided to maintain existing downstream stormwater runoff characteristics as per City of Wilsonville stormwater standards.
- Stormwater runoff from the proposed site will not adversely impact the downstream basin area or adjacent neighboring properties.



## Hydrologic Methodology

Appendix B is the Soils Survey Information for the Boones Ferry Pointe property, which includes the Hydrologic Soils Classification for the site as found in The USDA Soil Conservation Services "Soil Survey of Washington County". The soil on-site is classified as Huberly Silt Loam - Hydrologic Soils Group D.

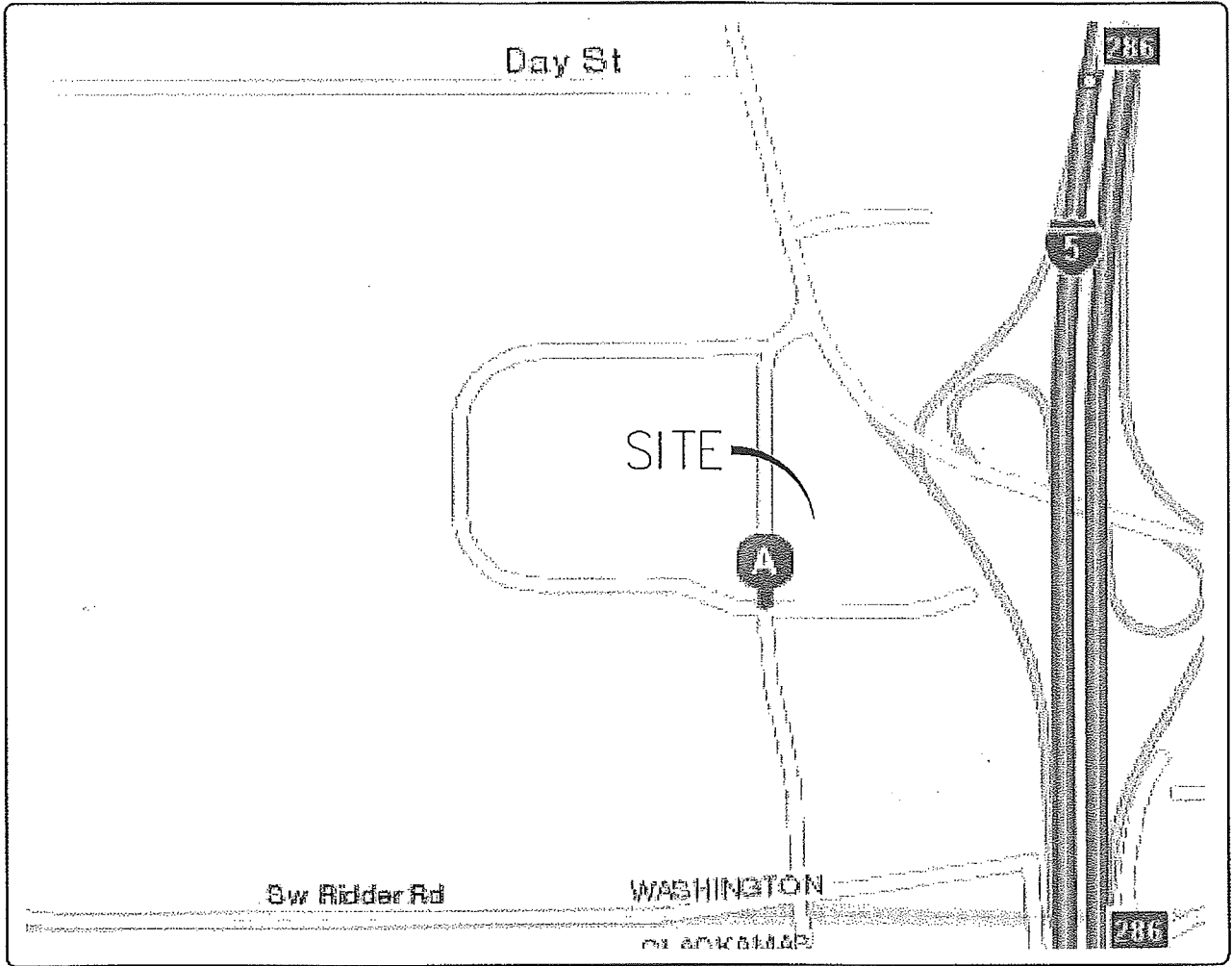
Appendix B is the SCS Runoff Curve Numbers as found in the Soil Conservation Services Manual "Urban Hydrology for Small Watersheds" (TR-55 Method). An SCS Curve Number of 77 was assumed for pre-developed on-site pervious surfaces based on the assumption of Brush in fair condition. An SCS Curve Number of 84 was assumed for all pre-developed on-site pervious surfaces based on the assumption of lawns in fair condition. Lastly, An SCS Curve Number of 98 was assumed for all impervious surfaces.

Appendix D The Santa Barbara Urban Hydrograph SCS-Type 1A Method was used to compute the tributary basins peak flows for the 2, 10 and 25-Year, 24-Hour, design storm event. Precipitation for the storm events was taken from City of Wilsonville stormwater design standards.

Appendix D Details the computer modeled Stage-Storage, Stage-Discharge and Orifice Design calculations for the detention pond. The detention pond was designed to release Post-Developed flows for the 2, 10, and 25 -Year storm events at Pre-Developed rates or less. These runoff rates were based on City of Wilsonville stormwater design standards.

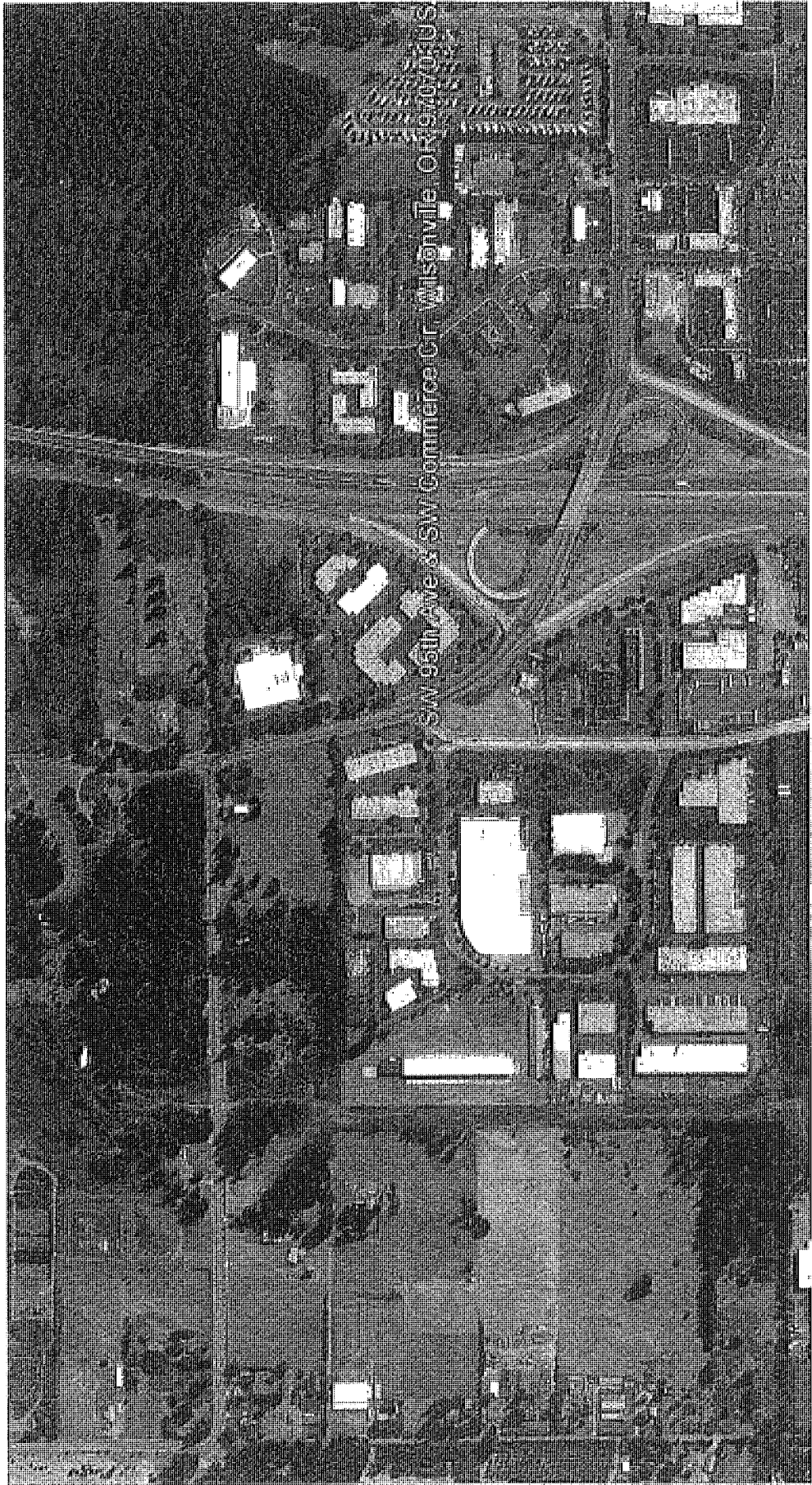
**Appendix A:**

Vicinity Map  
Existing Conditions Plan



VICINITY MAP  
NOT TO SCALE





**CURVE TABLE**

CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD
C1	135.83'	145.30'	53°29'12"	N26°54'00"E	130.95'
C2	18.43'	76.50'	39°56'40"	N73°33'56"E	18.06'
C3	34.69'	61.00'	32°34'45"	S49°48'42"E	34.22'
C4	28.05'	86.64'	1°51'38"	S27°49'35"E	28.04'

**LINE TABLE**

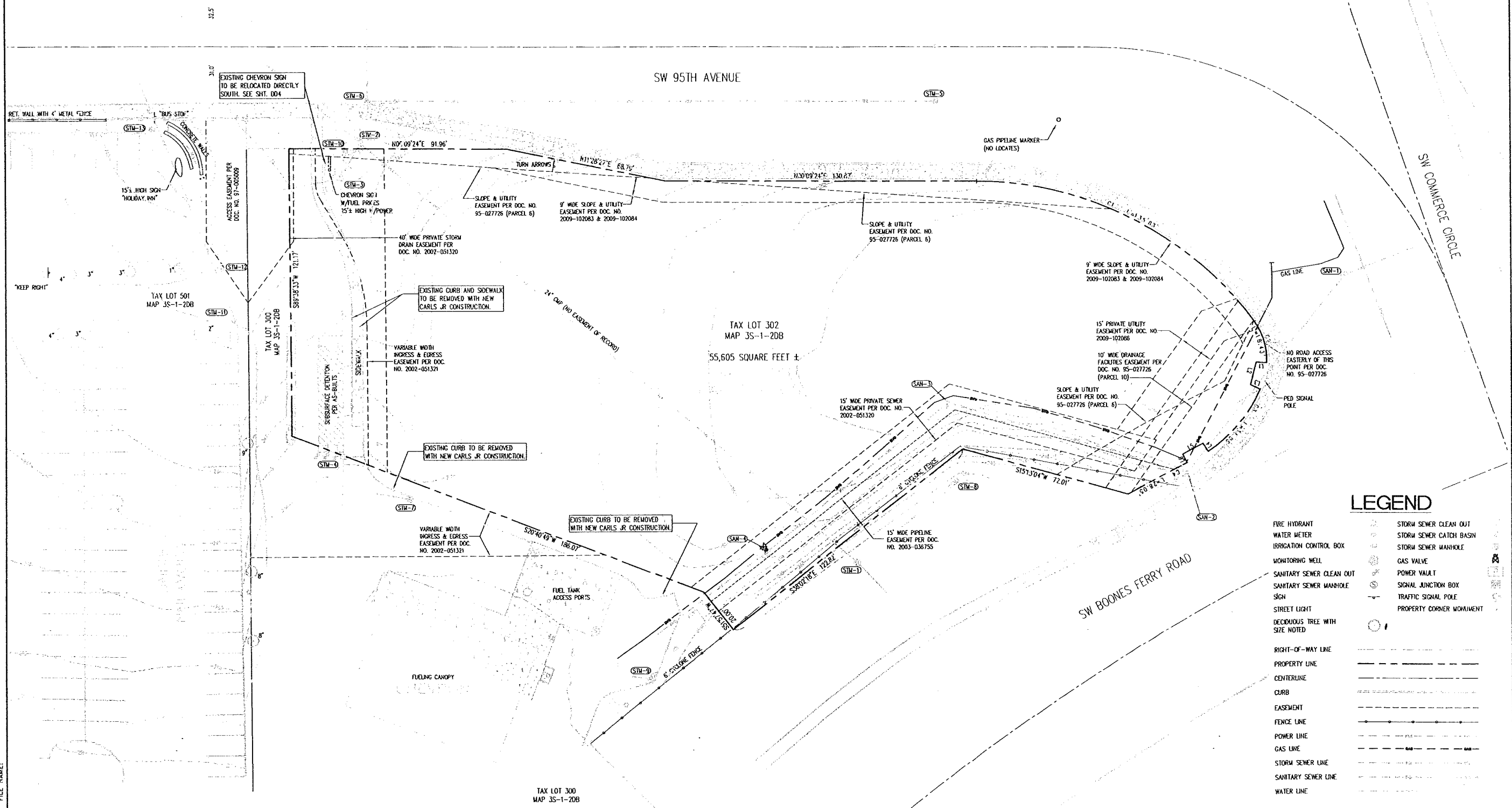
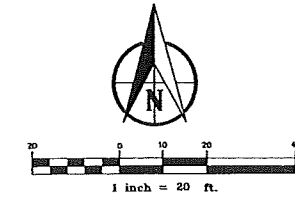
LINE	BEARING	LENGTH
L1	S03°28'26"W	4.50
L2	S75°40'24"E	9.87
L3	N23°53'53"E	4.52
L4	S56°28'43"W	4.04
L5	S28°07'38"E	9.52
L6	N63°26'08"E	4.00

**NOTES**

- FIELD WORK WAS COMPLETED DURING MARCH 2012. THERE IS CONSTRUCTION WORK SCHEDULED TO COMMENCE IN THE NEAR FUTURE ALONG THE STREETS ADJACENT TO THE PROPERTY SO CONDITIONS WITHIN THE RIGHT-OF-WAY WILL LIKELY CHANGE.
- ELEVATIONS ARE BASED ON WASHINGTON COUNTY BENCHMARK NO. 455. THE BENCHMARK IS LOCATED IN THE NORTHWEST CORNER OF THE BRIDGE CROSSING OVER INTERSTATE 5 WITH AN ELEVATION OF 281.55 (NOV 1929 DATUM).
- EASEMENT ARE PER PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY OF OREGON WITH AN EFFECTIVE DATE OF JANUARY 10, 2012 AND ORDER NO. 47251245586269-C150.
- ADDITIONAL FIELD WORK TO INCLUDE THE NORTH END OF THE PARKING LOT FOR THE HOLIDAY INN WAS COMPLETED ON MAY 29, 2012.

**STORM & SANITARY SEWER INFORMATION**

- |  |  |   |  |
|--|--|---|--|
| SAN-1 MANHOLE<br>RM 255.45'<br>12" IN (SE) 239.6'<br>12" OUT (N) 239.4'        | STM-1 MANHOLE<br>RM 246.88'<br>24" OUT (SW) 237.3'   | STM-6 CATCH BASIN<br>RM 245.44'<br>(HAS FILTER FABRIC, UNABLE TO MEAS. INVERT, NO AS-BUILT INFO AVAIL.) | STM-11 CATCH BASIN<br>RM 243.45'<br>WITH TRAP - OUT (W)                        |
| SAN-2 CLEANOUT<br>RM 43.3 AJOVE GRADE<br>E 244.4'                              | STM-2 MANHOLE<br>RM 244.70'<br>12" IN (SE) 233.4'<br>24" IN (NE) 232.6'<br>24" OUT (SW) 232.6'   | STM-7 CATCH BASIN<br>RM 249.55'<br>10" OUT (S) 246.6'<br>(HAS TRAP)                                     | STM-12 CATCH BASIN<br>RM 243.05'<br>WITH TRAP - OUT (S)                        |
| SAN-3 CLEANOUT<br>RM 4.3 AJOVE GRADE<br>E 245.8'                               | STM-3 MANHOLE<br>RM 243.52'<br>8" IN (S) 238.3'<br>8" IN (E) 238.3'<br>12" IN (E) 238.3'<br>12" OUT (NW) 238.3'<br>(W/ BURIED, UNABLE TO REMOVE LID) | STM-8 CATCH BASIN<br>RM 242.52'<br>16" CMP (SW) 242.5'  | STM-13 CATCH BASIN<br>RM 242.16'<br>WITH TRAP - OUT (H)                        |
| SAN-4 CLEANOUT<br>RM 4.3 AJOVE GRADE<br>E 246.8'                               | STM-4 MANHOLE<br>RM 248.88'<br>10" IN (N) 243.4' (STUB)<br>10" IN (E) 241.5'<br>12" OUT (W) 240.2'   | STM-9 CATCH BASIN<br>RM 248.94'<br>8" OUT (S) 247.0'  | STM-14 MANHOLE<br>RM 243.31'<br>12" IN (E) 236.9'<br>15" OUT (W) 236.8'        |
| * RECORD INFORMATION PER LAPORT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED) | STM-5 MANHOLE<br>RM 248.88'<br>10" IN (N) 243.4' (STUB)<br>10" IN (E) 241.5'<br>12" OUT (W) 240.2'   | STM-10 DITCH INLET<br>BOTTOM RM 242.87'<br>8" OUT (E) 240.0'  | * RECORD INFORMATION PER LAPORT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED) |



**LEGEND**

- |                                |                          |
|--------------------------------|--------------------------|
| FIRE HYDRANT                   | STORM SEWER CLEAN OUT    |
| WATER METER                    | STORM SEWER CATCH BASIN  |
| IRRIGATION CONTROL BOX         | STORM SEWER MANHOLE      |
| MONITORING WELL                | GAS VALVE                |
| SANITARY SEWER CLEAN OUT       | POWER VALVE              |
| SANITARY SEWER MANHOLE         | SIGNAL JUNCTION BOX      |
| SIGN                           | TRAFFIC SIGNAL POLE      |
| STREET LIGHT                   | PROPERTY CORNER MONUMENT |
| DECIDUOUS TREE WITH SIZE NOTED |                          |
| RIGHT-OF-WAY LINE              |                          |
| PROPERTY LINE                  |                          |
| CENTERLINE                     |                          |
| CURB                           |                          |
| EASEMENT                       |                          |
| FENCE LINE                     |                          |
| POWER LINE                     |                          |
| GAS LINE                       |                          |
| STORM SEWER LINE               |                          |
| SANITARY SEWER LINE            |                          |
| WATER LINE                     |                          |

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 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**EXISTING CONDITIONS PLAN**

REVISIONS


PRELIMINARY NOT FOR CONSTRUCTION

DATE: 08-xx-2012  
 BLDG. SUBMITTAL DATE: ①  
 ②  
 ③  
 BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

**DD1**

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

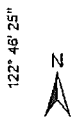
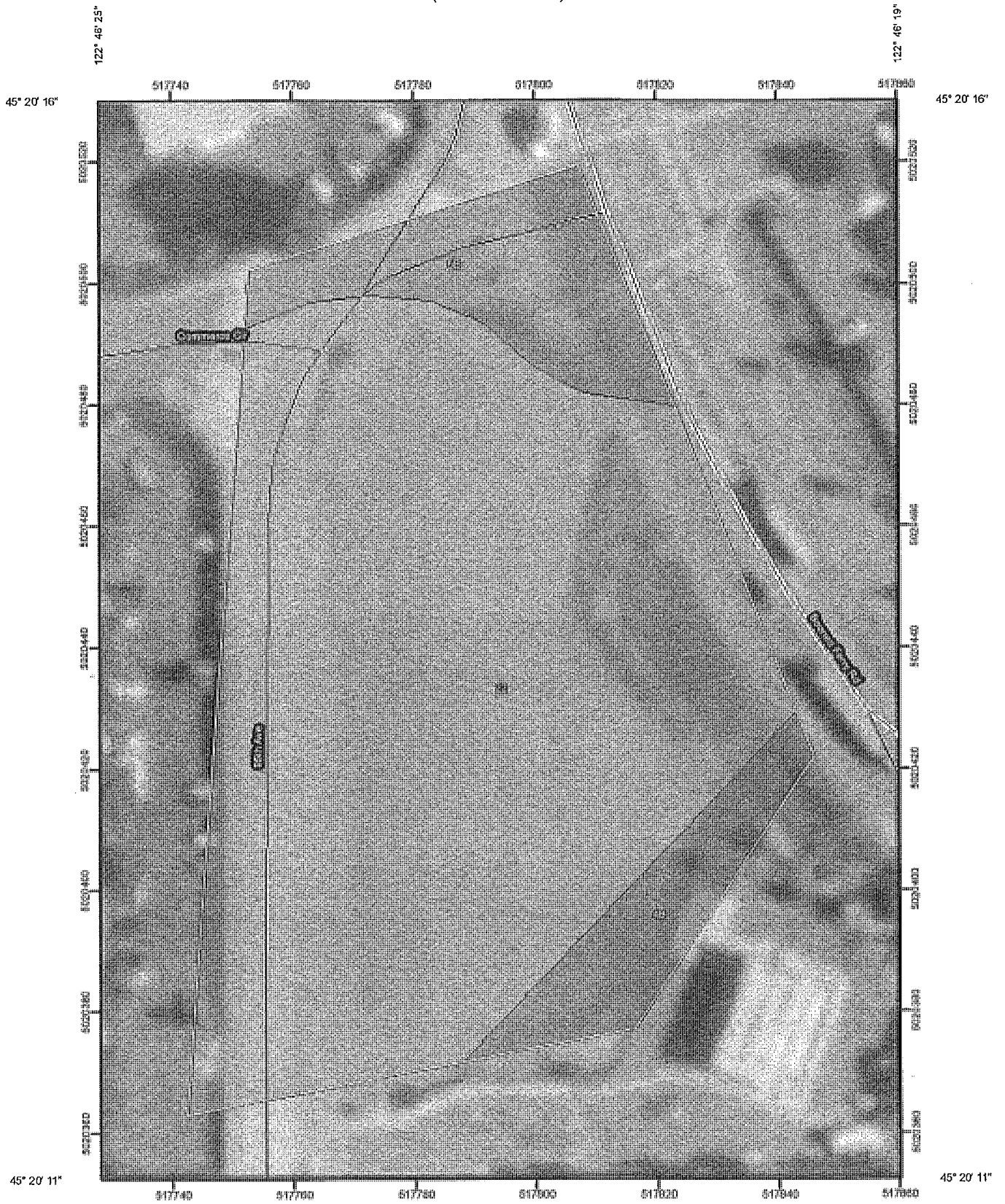
FILE NAME:

**Appendix B:**

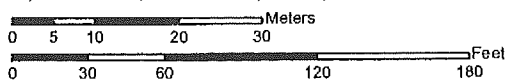
Soil Maps & Classification  
Runoff Curve Number Designation



Hydrologic Soil Group—Washington County, Oregon  
(Carls Jr Wilsonville)



Map Scale: 1:848 if printed on A size (8.5" x 11") sheet.



## Hydrologic Soil Group

Hydrologic Soil Group— Summary by Map Unit — Washington County, Oregon (OR067)				
Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
4B	Briedwell silt loam, 0 to 7 percent slopes	B	0.2	7.5%
5B	Briedwell stony silt loam, 0 to 7 percent slopes	B	0.3	10.9%
22	Huberly silt loam	D	2.3	81.6%
<b>Totals for Area of Interest</b>			<b>2.8</b>	<b>100.0%</b>

### Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

### Rating Options

*Aggregation Method:* Dominant Condition

## MAP LEGEND

Area of Interest (AOI)

Area of Interest (AOI)

Soils



Soil Map Units

Soil Ratings

A

A/D

B

B/D

C

C/D

D

Not rated or not available

Political Features

Cities

Water Features

Streams and Canals

Transportation

Rails

Interstate Highways

US Routes

Major Roads

Local Roads

## MAP INFORMATION

Map Scale: 1:848 if printed on A size (8.5" x 11") sheet.

The soil surveys that comprise your AOI were mapped at 1:20,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for accurate map measurements.

Source of Map: Natural Resources Conservation Service  
Web Soil Survey URL: <http://websoilsurvey.nrcs.usda.gov>  
Coordinate System: UTM Zone 10N NAD83

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Washington County, Oregon  
Survey Area Data: Version 9, Jul 15, 2010

Date(s) aerial images were photographed: 8/4/2005

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Table 2-2a Runoff curve numbers for urban areas <sup>1/</sup>

Cover description	Average percent impervious area <sup>2/</sup>	Curve numbers for hydrologic soil group			
		A	B	C	D
<i>Fully developed urban areas (vegetation established)</i>					
Open space (lawns, parks, golf courses, cemeteries, etc.) <sup>3/</sup> :					
Poor condition (grass cover < 50%) .....		68	79	86	89
Fair condition (grass cover 50% to 75%) .....		49	69	79	84
Good condition (grass cover > 75%) .....		39	61	74	80
Impervious areas:					
Paved parking lots, roofs, driveways, etc. (excluding right-of-way) .....		98	98	98	98
Streets and roads:					
Paved; curbs and storm sewers (excluding right-of-way) .....		98	98	98	98
Paved; open ditches (including right-of-way) .....		83	89	92	93
Gravel (including right-of-way) .....		76	85	89	91
Dirt (including right-of-way) .....		72	82	87	89
Western desert urban areas:					
Natural desert landscaping (pervious areas only) <sup>4/</sup> .....		63	77	85	88
Artificial desert landscaping (impervious weed barrier, desert shrub with 1- to 2-inch sand or gravel mulch and basin borders) .....		96	96	96	96
Urban districts:					
Commercial and business .....	85	89	92	94	95
Industrial .....	72	81	88	91	93
Residential districts by average lot size:					
1/8 acre or less (town houses) .....	65	77	85	90	92
1/4 acre .....	38	61	75	83	87
1/3 acre .....	30	57	72	81	86
1/2 acre .....	25	54	70	80	85
1 acre .....	20	51	68	79	84
2 acres .....	12	46	65	77	82
<i>Developing urban areas</i>					
Newly graded areas (pervious areas only, no vegetation) <sup>5/</sup> .....		77	86	91	94
Idle lands (CN's are determined using cover types similar to those in table 2-2c).					

POST DEVELOPED CURVE #

IMPERVIOUS

89

84

80

98

<sup>1/</sup> Average runoff condition, and  $I_{ii} = 0.2S$ .

<sup>2/</sup> The average percent impervious area shown was used to develop the composite CN's. Other assumptions are as follows: impervious areas are directly connected to the drainage system, impervious areas have a CN of 98, and pervious areas are considered equivalent to open space in good hydrologic condition. CN's for other combinations of conditions may be computed using figure 2-3 or 2-4.

<sup>3/</sup> CN's shown are equivalent to those of pasture. Composite CN's may be computed for other combinations of open space cover type.

<sup>4/</sup> Composite CN's for natural desert landscaping should be computed using figures 2-3 or 2-4 based on the impervious area percentage (CN = 98) and the pervious area CN. The pervious area CN's are assumed equivalent to desert shrub in poor hydrologic condition.

<sup>5/</sup> Composite CN's to use for the design of temporary measures during grading and construction should be computed using figure 2-3 or 2-4 based on the degree of development (impervious area percentage) and the CN's for the newly graded pervious areas.

**Table 2-2c** Runoff curve numbers for other agricultural lands <sup>1/</sup>

Cover description Cover type	Hydrologic condition	Curve numbers for hydrologic soil group			
		A	B	C	D
Pasture, grassland, or range—continuous forage for grazing. <sup>2/</sup>	Poor	68	79	86	89
	Fair	49	69	79	84
	Good	39	61	74	80
Meadow—continuous grass, protected from grazing and generally mowed for hay.	—	30	58	71	78
Brush—brush-weed-grass mixture with brush the major element. <sup>3/</sup>	Poor	48	67	77	83
	Fair	35	56	70	77
	Good	30 <sup>4/</sup>	48	65	73
Woods—grass combination (orchard or tree farm). <sup>5/</sup>	Poor	57	73	82	86
	Fair	43	65	76	82
	Good	32	58	72	79
Woods. <sup>6/</sup>	Poor	45	66	77	83
	Fair	36	60	73	79
	Good	30 <sup>4/</sup>	55	70	77
Farmsteads—buildings, lanes, driveways, and surrounding lots.	—	59	74	82	86

PREDEVELOPED CURVE #

83

77

<sup>1</sup> Average runoff condition, and  $I_a = 0.25$ .<sup>2</sup> *Poor*: <50% ground cover or heavily grazed with no mulch.*Fair*: 50 to 75% ground cover and not heavily grazed.*Good*: > 75% ground cover and lightly or only occasionally grazed.<sup>3</sup> *Poor*: <50% ground cover.*Fair*: 50 to 75% ground cover.*Good*: >75% ground cover.<sup>4</sup> Actual curve number is less than 30; use CN = 30 for runoff computations.<sup>5</sup> CN's shown were computed for areas with 50% woods and 50% grass (pasture) cover. Other combinations of conditions may be computed from the CN's for woods and pasture.<sup>6</sup> *Poor*: Forest litter, small trees, and brush are destroyed by heavy grazing or regular burning.*Fair*: Woods are grazed but not burned, and some forest litter covers the soil.*Good*: Woods are protected from grazing, and litter and brush adequately cover the soil.

**Appendix C:**

Prelim. Storm Design and Prelim.  
Plans

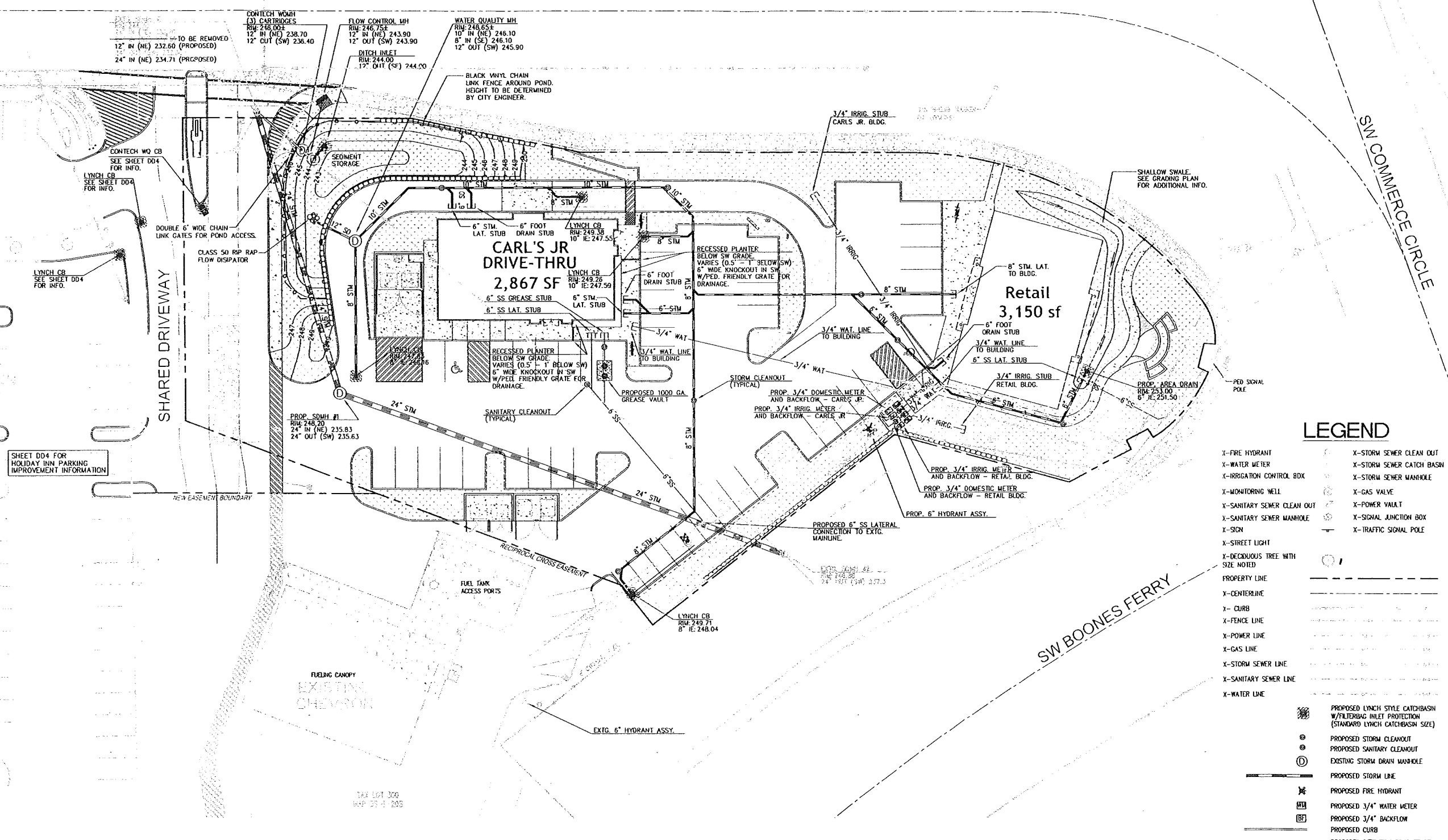




SW 95TH AVENUE

SW COMMERCE CIRCLE

SW BOONES FERRY



SHEET DD4 FOR HOLIDAY INN PARKING IMPROVEMENT INFORMATION

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

### LEGEND

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BDK
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STORM CLEANOUT
- PROPOSED SANITARY CLEANOUT
- EXISTING STORM DRAIN MANHOLE
- PROPOSED STORM LINE
- PROPOSED FIRE HYDRANT
- PROPOSED 3/4" WATER METER
- PROPOSED 3/4" BACKFLOW
- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED 6" SANITARY SEWER LATERAL
- PROPOSED 3/4" WATER LINE
- PROPOSED 3/4" IRRIGATION LINE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA
- PROPOSED BUILDING FOOTING DRAIN

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 WILSONVILLE, OREGON

**PRELIMINARY UTILITY PLAN**

REVISIONS


STAMP

**PRELIMINARY NOT FOR CONSTRUCTION**

DATE: 08-xx-2012

BLDG. SUBMITTAL DATE: ①

②

③

910 DATE:

CKE NO.:

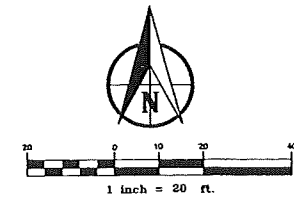
ARCH. NO.: 1199

SCALE:

SHEET

**DD2**

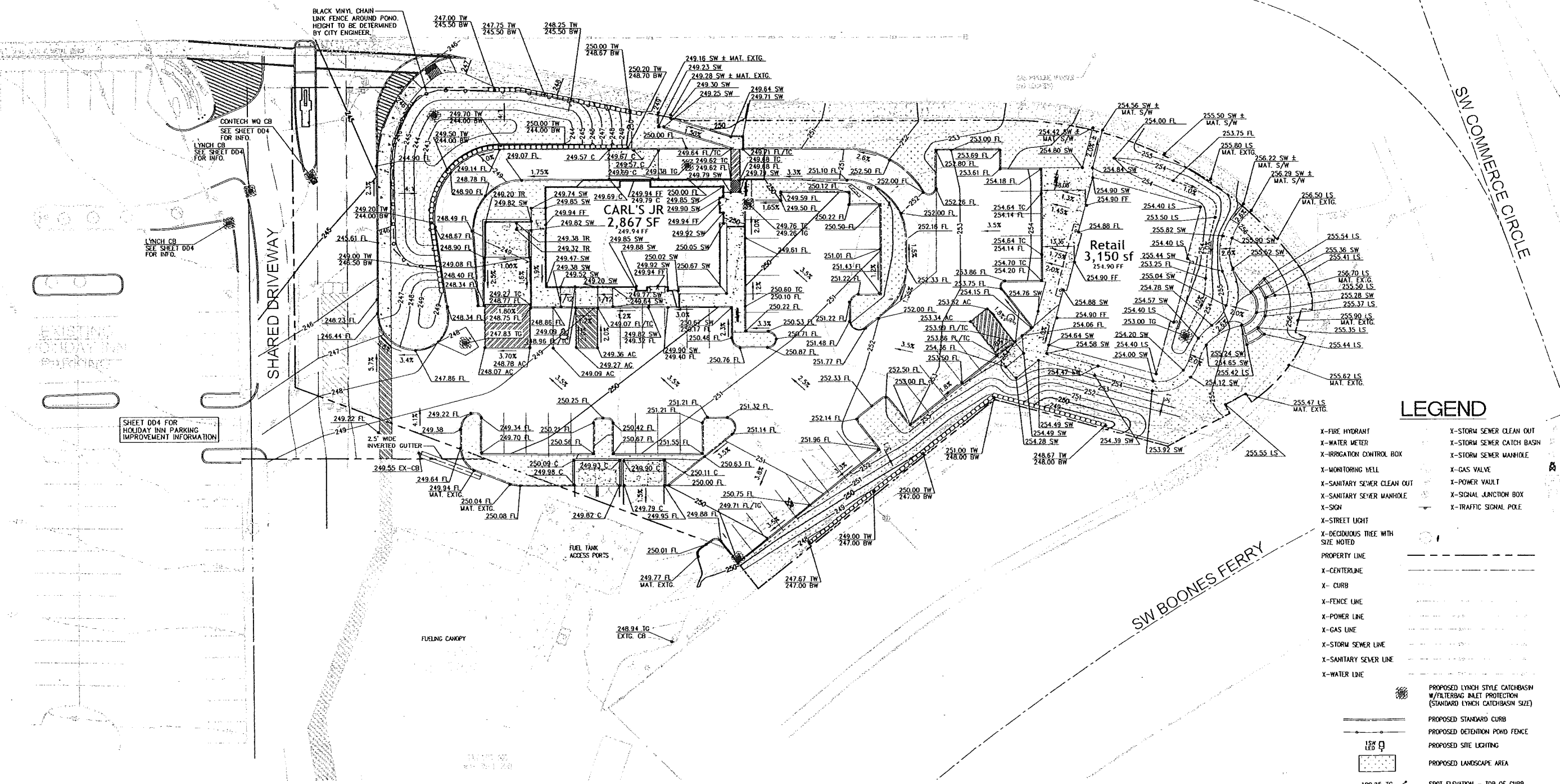
FILE NAME:



SW 95TH AVENUE

SW COMMERCE CIRCLE

SW BOONES FERRY



**LEGEND**

- X-FIRE HYDRANT
  - X-WATER METER
  - X-IRRIGATION CONTROL BOX
  - X-MONITORING WELL
  - X-SANITARY SEWER CLEAN OUT
  - X-SANITARY SEWER MANHOLE
  - X-SIGN
  - X-STREET LIGHT
  - X-DECIDUOUS TREE WITH SIZE NOTED
  - PROPERTY LINE
  - X-CENTERLINE
  - X-CURB
  - X-FENCE LINE
  - X-POWER LINE
  - X-GAS LINE
  - X-STORM SEWER LINE
  - X-SANITARY SEWER LINE
  - X-WATER LINE
  - X-STORM SEWER CLEAN OUT
  - X-STORM SEWER CATCH BASIN
  - X-STORM SEWER MANHOLE
  - X-GAS VALVE
  - X-POWER VAULT
  - X-SIGNAL JUNCTION BOX
  - X-TRAFFIC SIGNAL POLE
- 
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERING INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
  - PROPOSED STANDARD CURB
  - PROPOSED DETENTION POND FENCE
  - PROPOSED SITE LIGHTING
  - PROPOSED LANDSCAPE AREA
- 
- 189.25 TC
  - 188.75 FL
  - 188.16 TW
  - 184.81 BW
  - 189.50 FF
  - 187.01 TG
  - 189.08 C
  - 189.35 LS
- SPOT ELEVATION - TOP OF CURB
  - SPOT ELEVATION - FLOW LINE GUTTER
  - SPOT ELEVATION - TOP OF WALL
  - SPOT ELEVATION - BOTTOM OF WALL
  - SPOT ELEVATION - FINISH FLOOR
  - SPOT ELEVATION - TOP OF GRATE
  - SPOT ELEVATION - CONCRETE PAD
  - SPOT ELEVATION - LANDSCAPE

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

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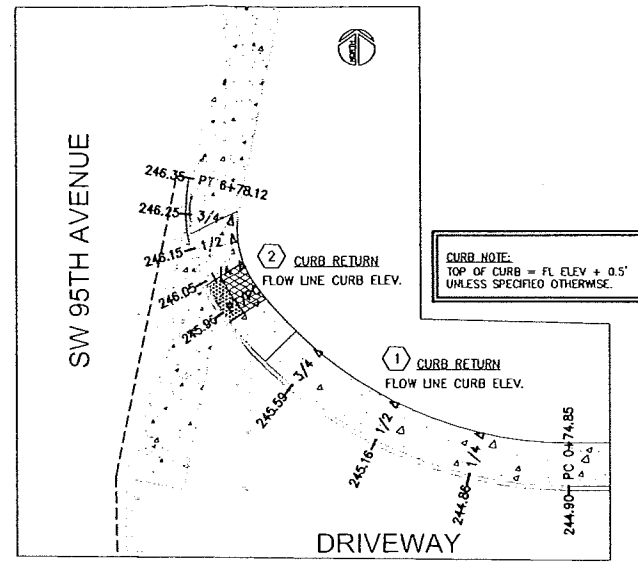
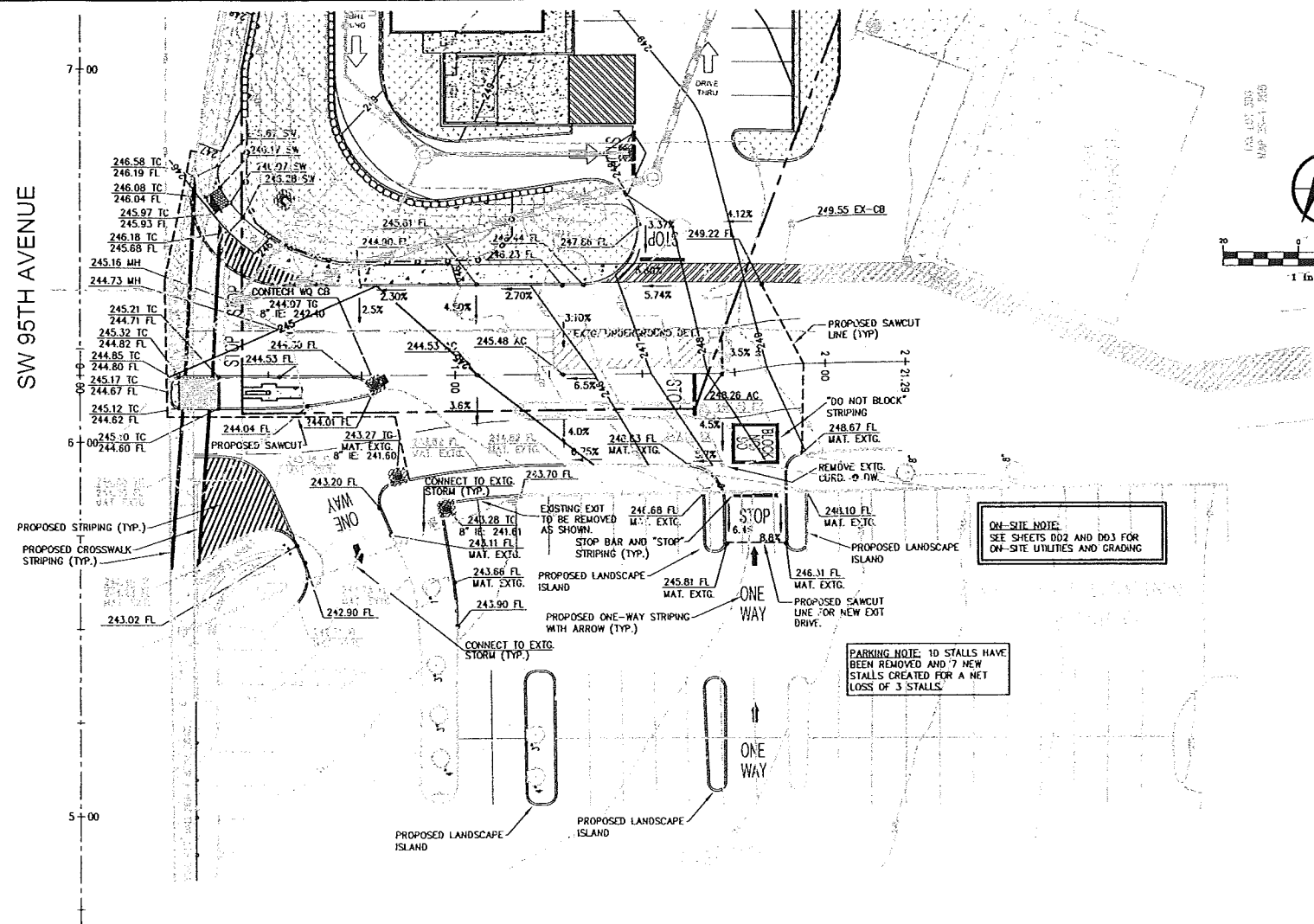
**PRELIMINARY GRADING PLAN**

REVISIONS



DATE: 08-2012  
 BLDG. SUBMITTAL: (1)  
 DATE: (2)  
 (3)  
 BID DATE: \_\_\_\_\_  
 CKE NO.: \_\_\_\_\_  
 ARCH. NO.: 1199  
 SCALE: \_\_\_\_\_  
 SHEET

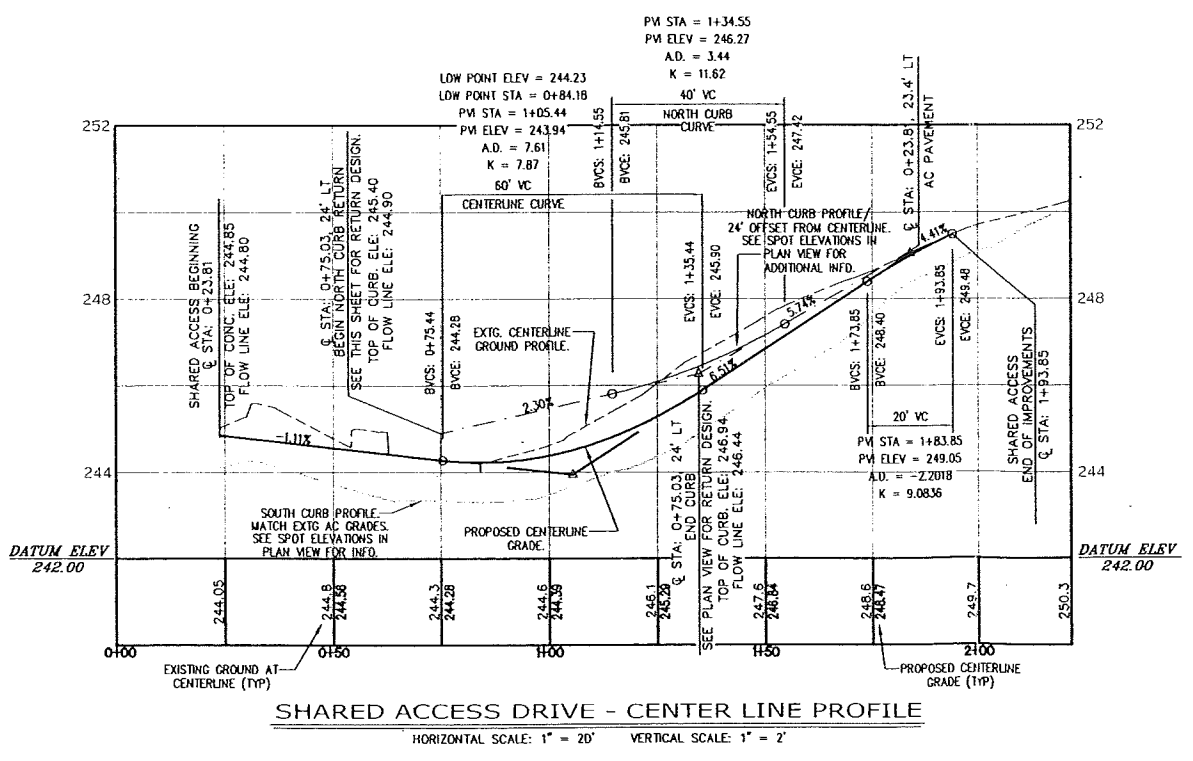
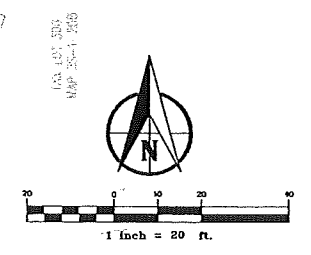
**DD3**



CURB RETURN NO. 1 DATA	
CURVE DATA	STREET STATION
Δ = 53°00'00"	P.C. = 0+74.65 24.00' LT
R = 50.00'	DRIVEWAY - EAST
T = 25.18'	P.T. = 0+34.68 44.29' LT
L = 46.65'	DRIVEWAY - WEST

CURB RETURN NO. 2 DATA	
CURVE DATA	STREET STATION
Δ = 48°00'00"	P.C. = 0+34.68 44.29' LT
R = 30.00'	DRIVEWAY - WEST
T = 8.91'	P.T. = 6+78.12 31.00' RT
L = 16.76'	NW 95TH AVENUE



**LEGEND**

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-EASEMENT
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STORM LINE
- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA

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**SHARED DRIVEWAY  
 PLAN AND PROFILE &  
 HOLIDAY INN PARKING  
 IMPROVEMENTS**

REVISIONS




DATE: 08-09-2012

BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③

CKE NO.:  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

**DD4**

FILE NAME:

**Appendix D:**

On-site Basin Maps  
Pre & Post Development Calcs (2 – 25 Yr)  
Pre-developed Tc  
Detention Calcs (2 - 25 Year)

**CURVE TABLE**

CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD
C1	135.83	145.50	53°29'12"	N26°54'00"E	130.95
C2	18.43	26.50	39°50'40"	N73°33'56"E	18.06
C3	34.69	61.00	32°34'45"	S49°48'42"E	34.22
C4	28.05	863.64	1°51'38"	S27°49'35"E	28.04

**LINE TABLE**

LINE	BEARING	LENGTH
L1	S01°28'26"W	4.50
L2	S75°40'24"E	9.87
L3	N23°53'53"E	4.52
L4	S56°28'43"W	4.04
L5	S28°07'38"E	9.52
L6	N63°26'08"E	4.00

**NOTES**

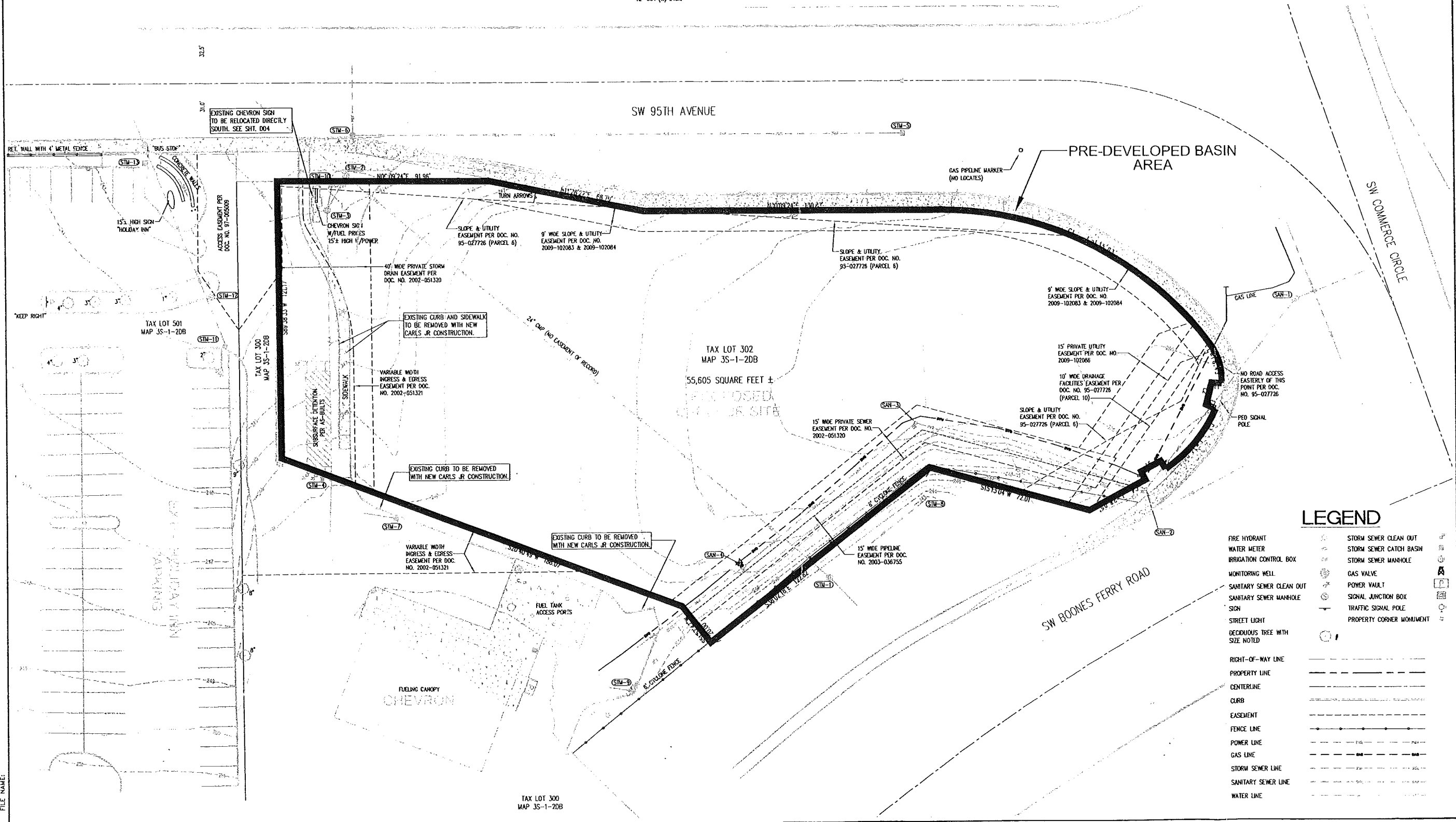
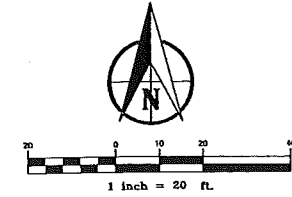
1. FIELD WORK WAS COMPLETED DURING MARCH 2012. THERE IS CONSTRUCTION WORK SCHEDULED TO COMMENCE IN THE NEAR FUTURE ALONG THE STREETS ADJACENT TO THE PROPERTY SO CONDITIONS WITHIN THE RIGHT-OF-WAY WILL LIKELY CHANGE.
2. ELEVATIONS ARE BASED ON WASHINGTON COUNTY BENCHMARK NO. 455. THE BENCHMARK IS LOCATED IN THE NORTHWEST CORNER OF THE BRIDGE CROSSING OVER INTERSTATE 5 WITH AN ELEVATION OF 281.55 (NVD 1929 DATUM).
3. EASEMENT ARE PER PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY OF OREGON WITH AN EFFECTIVE DATE OF JANUARY 10, 2012 AND ORDER NO. 47251249586204-0750.
4. ADDITIONAL FIELD WORK TO INCLUDE THE NORTH END OF THE PARKING LOT FOR THE HOLIDAY INN WAS COMPLETED ON MAY 29, 2012.

**STORM & SANITARY SEWER INFORMATION**

- SAN-1 MANHOLE RM 255.45' 12" IN (SE) 239.6' 12" OUT (N) 239.4'
- SAN-2 CLEAHOUT RM 4'± ABOVE GRADE IE 244.4'
- SAN-3 CLEAHOUT RM 4'± ABOVE GRADE IE 245.8'
- SAN-4 CLEAHOUT RM 4'± ABOVE GRADE IE 246.8'
- \* RECORD INFORMATION PER LAPORT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)
- STM-1 MANHOLE RM 248.88' 24" OUT (SW) 237.3'
- STM-2 MANHOLE RM 244.20' 12" IN (SE) 233.4' 24" IN (SE) 232.6' 24" OUT (SW) 232.6'
- STM-3 MANHOLE RM 243.57' 8" IN (S) 238.3' 8" IN (W) 238.3' 12" IN (E) 238.3' 12" OUT (NW) 238.3' (M/B BURIED, UNABLE TO REMOVE I/O)
- STM-4 MANHOLE RM 248.88' 8" IN (N) 243.4' (STUB) 10" IN (E) 241.5' 12" OUT (W) 240.2'
- STM-5 CATCH BASIN RM 245.44' (HAS FILTER FABRIC, UNABLE TO MEAS INVERT, NO AS-BUILT INFO AVAIL.)
- STM-7 CATCH BASIN RM 249.55' 10" OUT (S) 246.6' (HAS TRAP)
- STM-8 CURVERT 18" OMP (SW) 242.5'
- STM-9 CATCH BASIN RM 248.94' 8" OUT (S) 247.0'
- STM-10 DITCH INLET BOTTOM RM 242.87' 8" OUT (E) 240.0'
- STM-11 CATCH BASIN RM 243.45' WITH TRAP - OUT (W)
- STM-12 CATCH BASIN RM 243.05' WITH TRAP - OUT (S)
- STM-13 CATCH BASIN RM 242.15' WITH TRAP - OUT (N)
- STM-14 MANHOLE RM 243.31' 12" IN (E) 236.9' 15" OUT (W) 236.8'
- \* RECORD INFORMATION PER LAPORT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)

**PRE-DEVELOPED BASIN INFORMATION**

PRE-DEVELOPED IMPERVIOUS AREA: 0.076 ACRES  
 PRE-DEVELOPED PERVIOUS AREA: 1.21 ACRES  
 TOTAL SITE AREA: 1.28 ACRES



**LEGEND**

- FIRE HYDRANT
- WATER METER
- IRRIGATION CONTROL BOX
- MONITORING WELL
- SANITARY SEWER CLEAN OUT
- SANITARY SEWER MANHOLE
- SIGN
- STREET LIGHT
- DECADUOUS TREE WITH SIZE NOTED
- RIGHT-OF-WAY LINE
- PROPERTY LINE
- CENTERLINE
- CURB
- EASEMENT
- FENCE LINE
- POWER LINE
- GAS LINE
- STORM SEWER LINE
- SANITARY SEWER LINE
- WATER LINE
- STORM SEWER CLEAN OUT
- STORM SEWER CATCH BASIN
- STORM SEWER MANHOLE
- GAS VALVE
- POWER VAULT
- SIGNAL JUNCTION BOX
- TRAFFIC SIGNAL POLE
- PROPERTY CORNER MONUMENT

**KITTRIDGE ENGINEERS, LLC**  
 CLIENT  
 Wilsonville Deveco LLC  
 c/o Integrated Investment Partners  
 PO Box 916  
 Portland, OR 97207  
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PROJECT  
**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE  
**PRE-DEVELOPED BASIN PLAN**

REVISIONS


STAMP

DATE: 08-24-2012  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③

BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

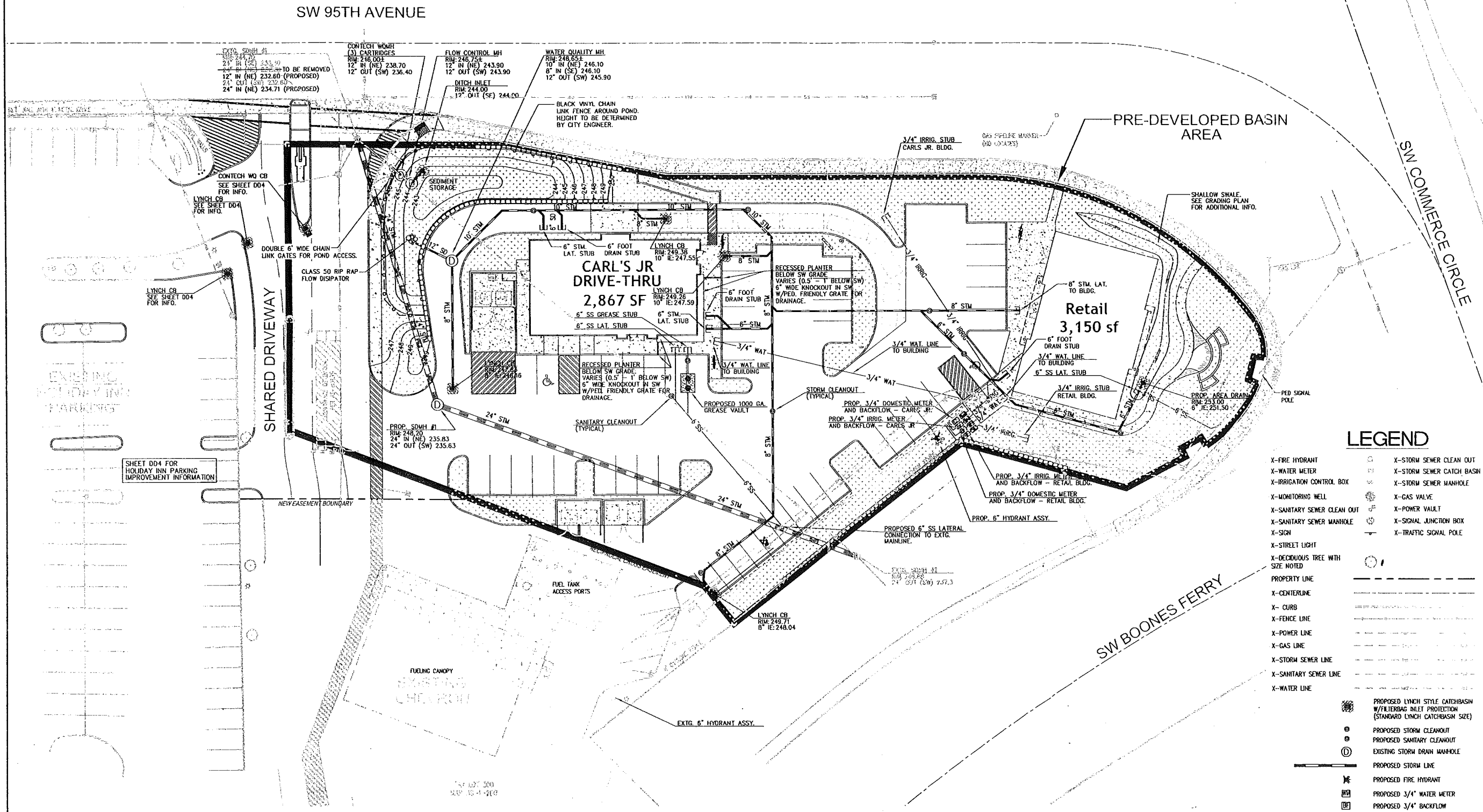
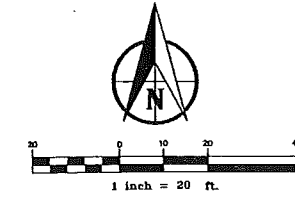
**DD1**

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

FILE NAME:

### POST-DEVELOPED BASIN INFORMATION

POST-DEVELOPED IMPERVIOUS AREA: 0.93 ACRES  
 PRE-DEVELOPED IMPERVIOUS AREA: 0.076 ACRES  
 "NET" POST-DEVELOPED IMPERVIOUS AREA: 0.085 ACRES  
 ("NET" AREA USED FOR DETENTION CALCS)  
 POST-DEVELOPED PERVIOUS AREA: 0.36 ACRES  
 TOTAL SITE AREA: 1.28 ACRES



### LEGEND

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STORM CLEANOUT
- PROPOSED SANITARY CLEANOUT
- EXISTING STORM DRAIN MANHOLE
- PROPOSED STORM LINE
- PROPOSED FIRE HYDRANT
- PROPOSED 3/4" WATER METER
- PROPOSED 3/4" BACKFLOW
- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED 6" SANITARY SEWER LATERAL
- PROPOSED 3/4" WATER LINE
- PROPOSED 3/4" IRRIGATION LINE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA
- PROPOSED BUILDING FOOTING DRAIN

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

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**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**PRELIMINARY UTILITY PLAN**

REVISIONS:

STAMP:

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER APPLICABLE REGULATING AGENCIES.

DATE: 08-xx-2012  
 BLDG. SUBMITTAL DATE:  
 ARCH. NO.: 1159  
 SCALE:  
 SHEET:

**DD2**

FILE NAME:

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14



**pond prelim #4 0815-2012 - Grade revised**

Type IA 24-hr 2 Year Rainfall=2.50"

Prepared by {enter your company name here}

Printed 9/6/2012

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Page 1

**Summary for Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

Runoff = 0.14 cfs @ 8.06 hrs, Volume= 0.089 af, Depth= 0.83"

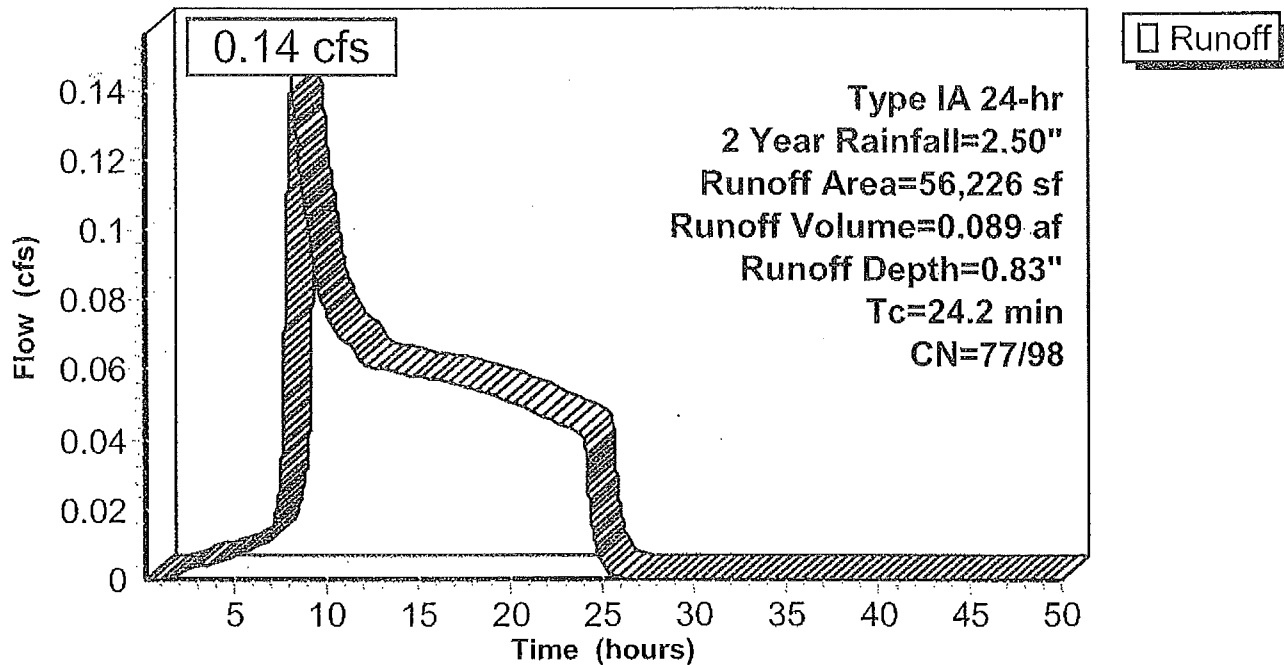
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
Type IA 24-hr 2 Year Rainfall=2.50"

	Area (sf)	CN	Description
*	52,920	77	PERVIOUS AREA
*	3,306	98	IMPERVIOUS AC
	56,226	78	Weighted Average
	52,920	77	94.12% Pervious Area
	3,306	98	5.88% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
24.2					Direct Entry, Calc'd time of Tc

**Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

**Hydrograph**



**Summary for Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

Runoff = 0.31 cfs @ 8.01 hrs, Volume= 0.161 af, Depth= 1.50"

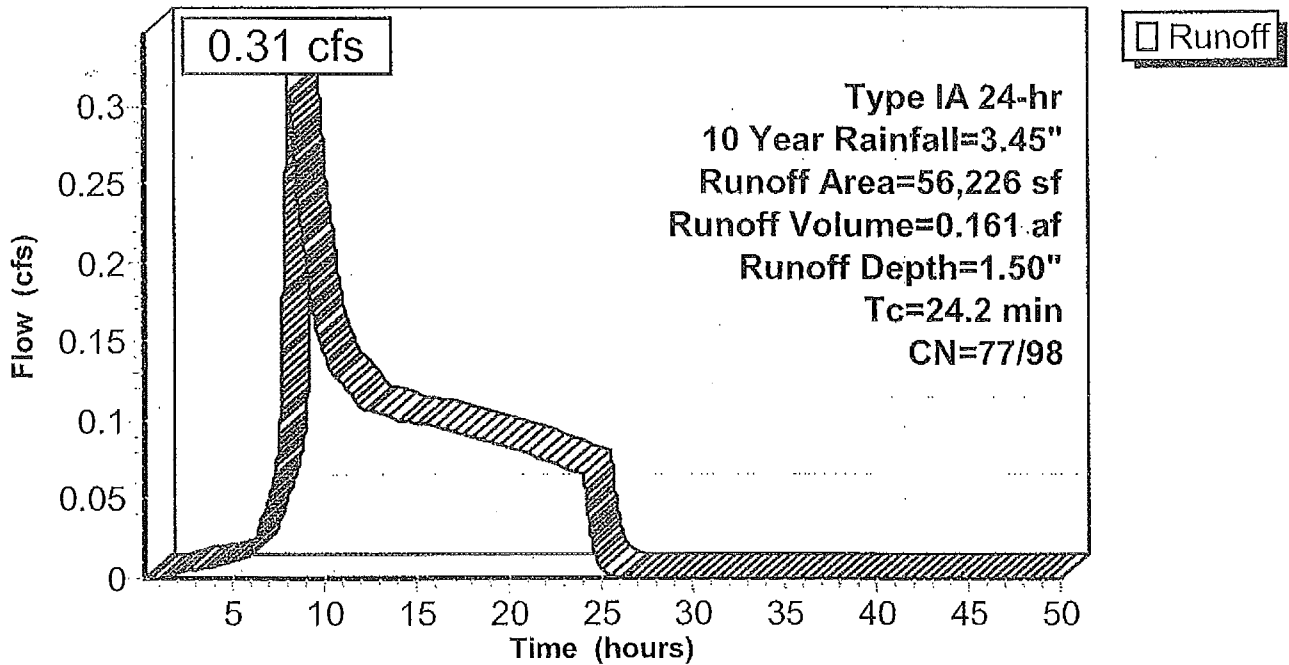
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Type IA 24-hr 10 Year Rainfall=3.45"

	Area (sf)	CN	Description
*	52,920	77	PERVIOUS AREA
*	3,306	98	IMPERVIOUS AC
	56,226	78	Weighted Average
	52,920	77	94.12% Pervious Area
	3,306	98	5.88% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
24.2					Direct Entry, Calc'd time of Tc

**Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

**Hydrograph**



**Summary for Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

Runoff = 0.40 cfs @ 8.01 hrs, Volume= 0.199 af, Depth= 1.85"

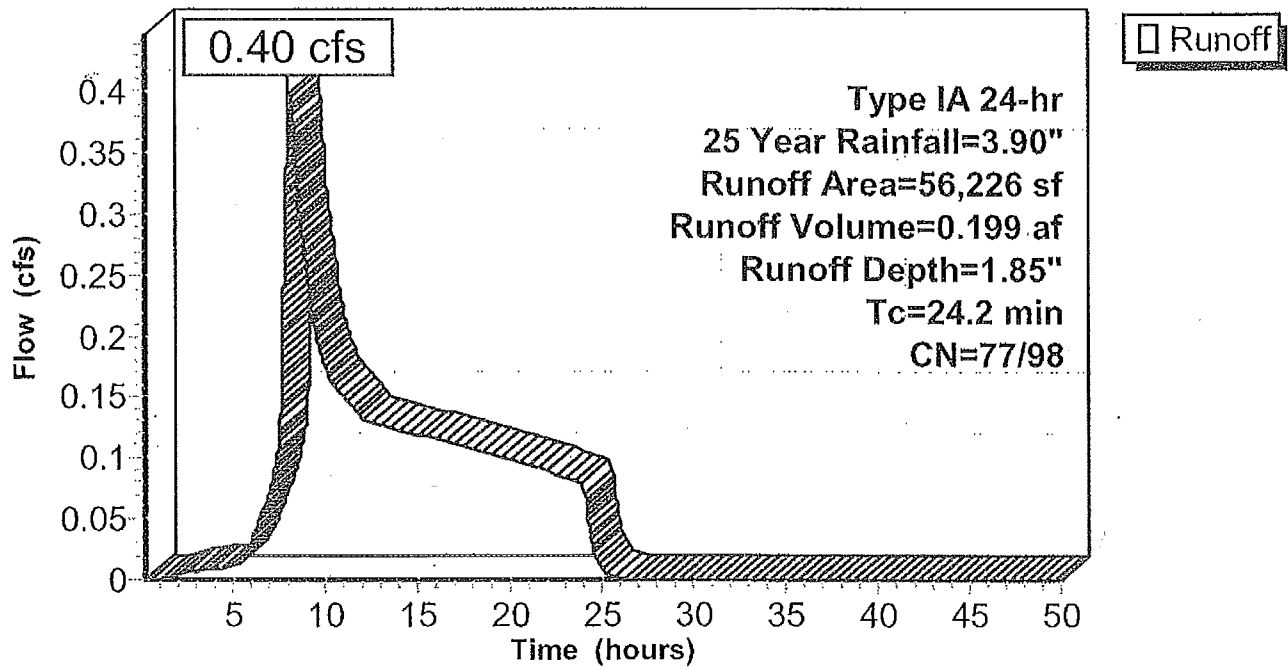
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
Type IA 24-hr 25 Year Rainfall=3.90"

	Area (sf)	CN	Description
*	52,920	77	PERVIOUS AREA
*	3,306	98	IMPERVIOUS AC
	56,226	78	Weighted Average
	52,920	77	94.12% Pervious Area
	3,306	98	5.88% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
24.2					Direct Entry, Calc'd time of Tc

**Subcatchment 7S: EXTG. CONDITIONS W/DW 77 curve**

**Hydrograph**



**pond prelim #4 0815-2012 - Grade revised**

Type IA 24-hr 2 Year Rainfall=2.50"

Prepared by {enter your company name here}

Printed 9/6/2012

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Page 1

**Summary for Subcatchment 3S: Post Dev. Site Area**

Runoff = 0.58 cfs @ 7.90 hrs, Volume= 0.195 af, Depth= 1.92"

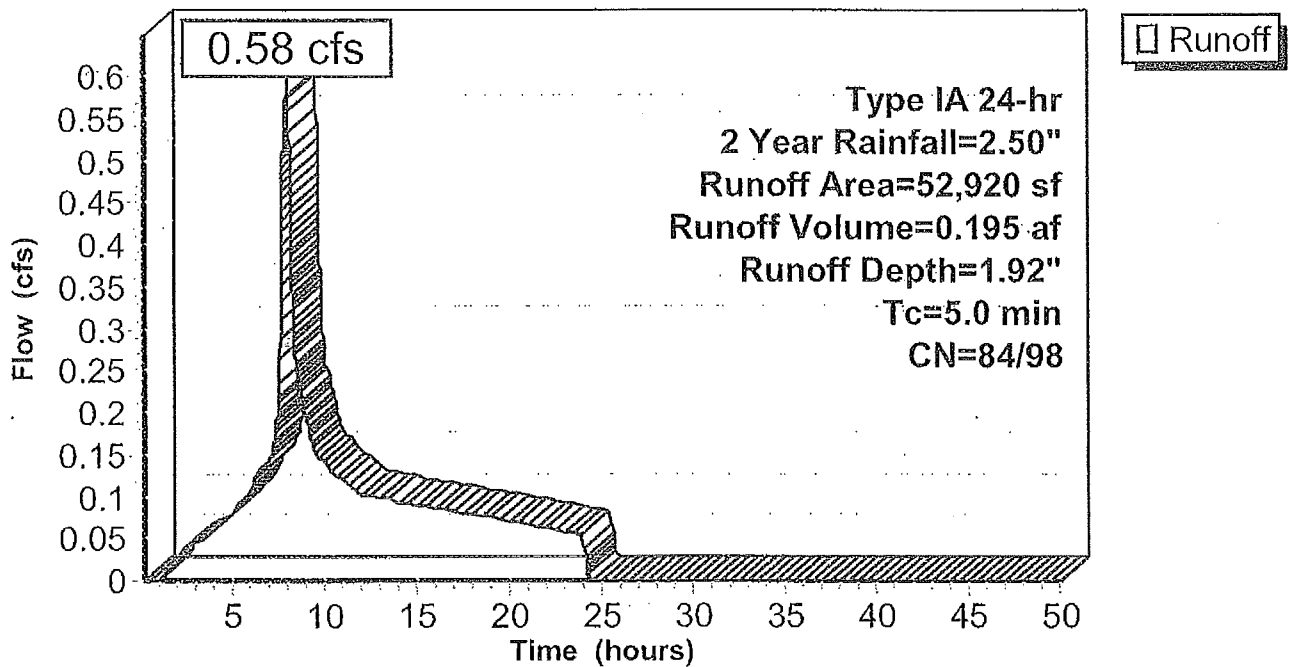
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
Type IA 24-hr 2 Year Rainfall=2.50"

	Area (sf)	CN	Description
*	15,886	84	PERVIOUS AREA
*	37,034	98	IMPERVIOUS AC
	52,920	94	Weighted Average
	15,886	84	30.02% Pervious Area
	37,034	98	69.98% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed Tc

**Subcatchment 3S: Post Dev. Site Area**

**Hydrograph**



**Summary for Subcatchment 3S: Post Dev. Site Area**

Runoff = 0.85 cfs @ 7.89 hrs, Volume= 0.285 af, Depth= 2.82"

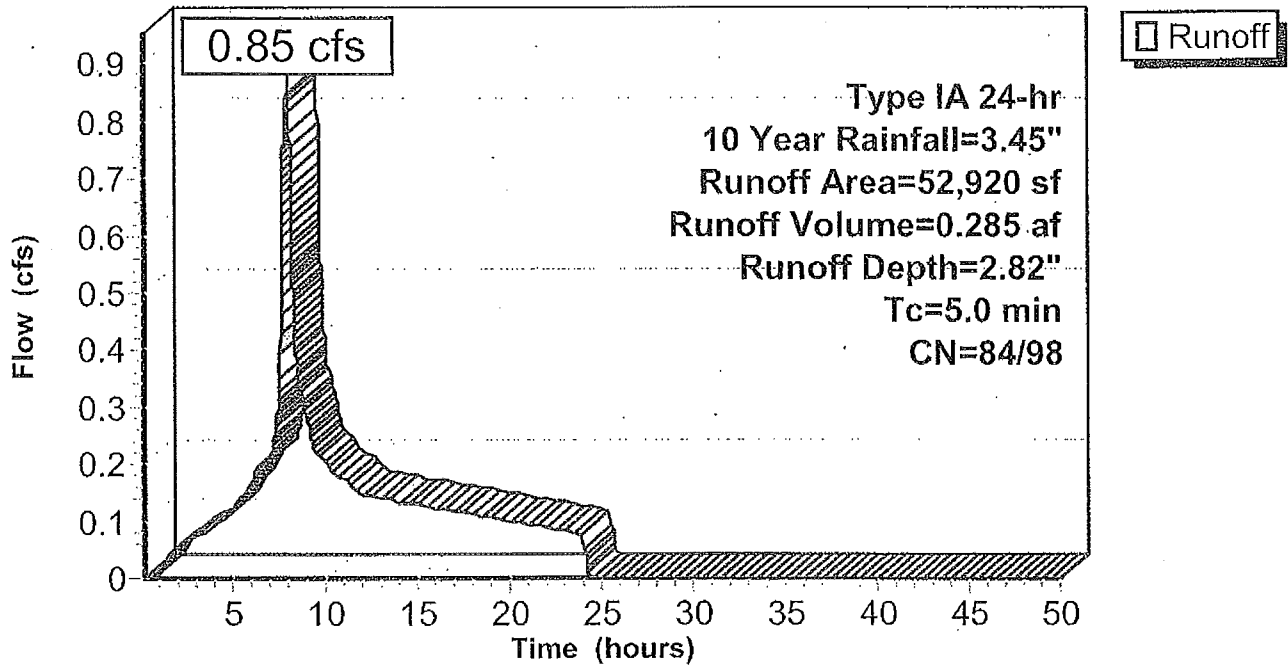
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Type IA 24-hr 10 Year Rainfall=3.45"

	Area (sf)	CN	Description
*	15,886	84	PERVIOUS AREA
*	37,034	98	IMPERVIOUS AC
	52,920	94	Weighted Average
	15,886	84	30.02% Pervious Area
	37,034	98	69.98% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed Tc

**Subcatchment 3S: Post Dev. Site Area**

**Hydrograph**



**Summary for Subcatchment 3S: Post Dev. Site Area**

Runoff = 0.98 cfs @ 7.89 hrs, Volume= 0.329 af, Depth= 3.25"

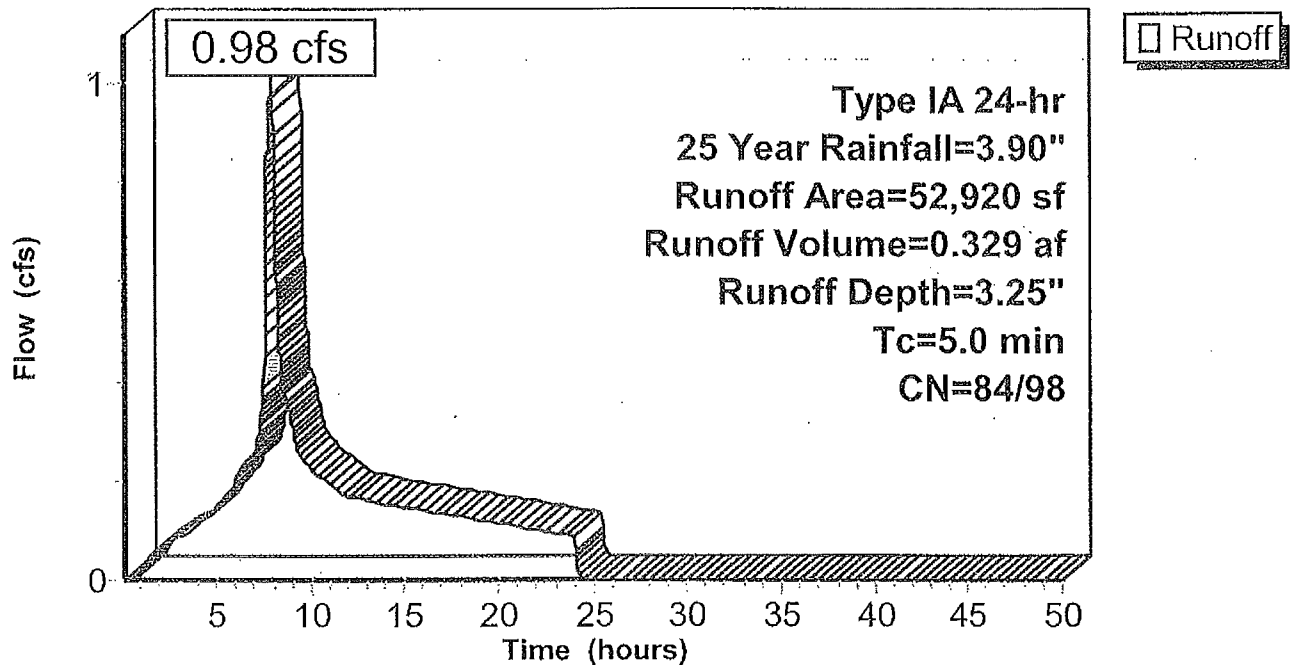
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Type IA 24-hr 25 Year Rainfall=3.90"

	Area (sf)	CN	Description
*	15,886	84	PERVIOUS AREA
*	37,034	98	IMPERVIOUS AC
	52,920	94	Weighted Average
	15,886	84	30.02% Pervious Area
	37,034	98	69.98% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed Tc

**Subcatchment 3S: Post Dev. Site Area**

**Hydrograph**





**Summary for Subcatchment 3S: Post Dev. Site Area**

Runoff = 1.16 cfs @ 7.89 hrs, Volume= 0.388 af, Depth= 3.83"

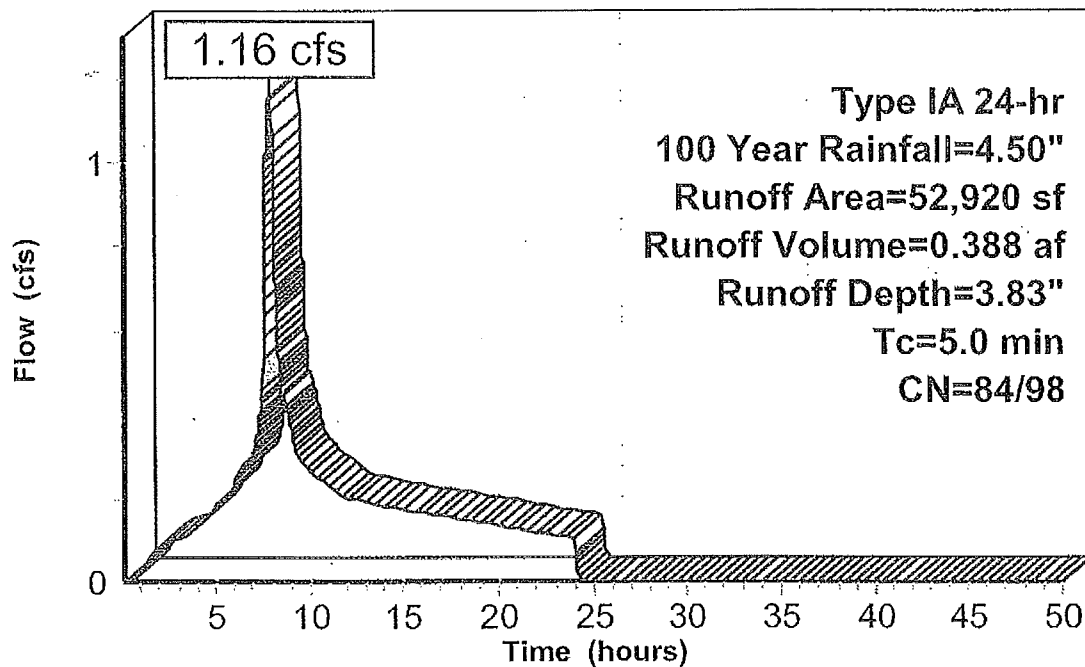
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
Type IA 24-hr 100 Year Rainfall=4.50"

	Area (sf)	CN	Description
*	15,886	84	PERVIOUS AREA
*	37,034	98	IMPERVIOUS AC
	52,920	94	Weighted Average
	15,886	84	30.02% Pervious Area
	37,034	98	69.98% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Assumed Tc

**Subcatchment 3S: Post Dev. Site Area**

**Hydrograph**



Runoff

Type IA 24-hr  
100 Year Rainfall=4.50"  
Runoff Area=52,920 sf  
Runoff Volume=0.388 af  
Runoff Depth=3.83"  
Tc=5.0 min  
CN=84/98

Ck

CPK 14:34 14-May-12

Project CARL.002  
CARLS JR - WILSONVILLE

TIME OF CONCENTRATION

Pre-developed Time of Concentration

2-year, 24-hour rainfall = 2.50"

	<i>flow type</i>	<i>description</i>	<i>coeff.</i>	<i>distance</i>	<i>fall</i>	<i>slope</i>	<i>t/c</i>
1	overland sheet	natural.range	n=0.13	300.0'	6.0'	2.00%	23.81'
2	shallow concentrated	bare.ground	K=13	81.0'	7.0'	8.64%	0.35'

total Time of Concentration = 24.16'

**CURVE TABLE**

CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD
C1	135.83'	145.50'	53°29'12"	N76°54'00"E	130.95'
C2	18.43'	28.50'	39°50'40"	N73°33'56"E	18.06'
C3	34.69'	81.00'	32°34'45"	S49°48'42"E	34.22'
C4	28.05'	88.64'	15°1'38"	S27°49'35"E	28.04'

**LINE TABLE**

LINE	BEARING	LENGTH
L1	S03°28'26"W	4.50
L2	S75°40'24"E	9.87
L3	N23°53'53"E	4.52
L4	S56°28'43"W	4.04
L5	S28°07'38"E	9.52
L6	N63°26'06"E	4.00

**NOTES**

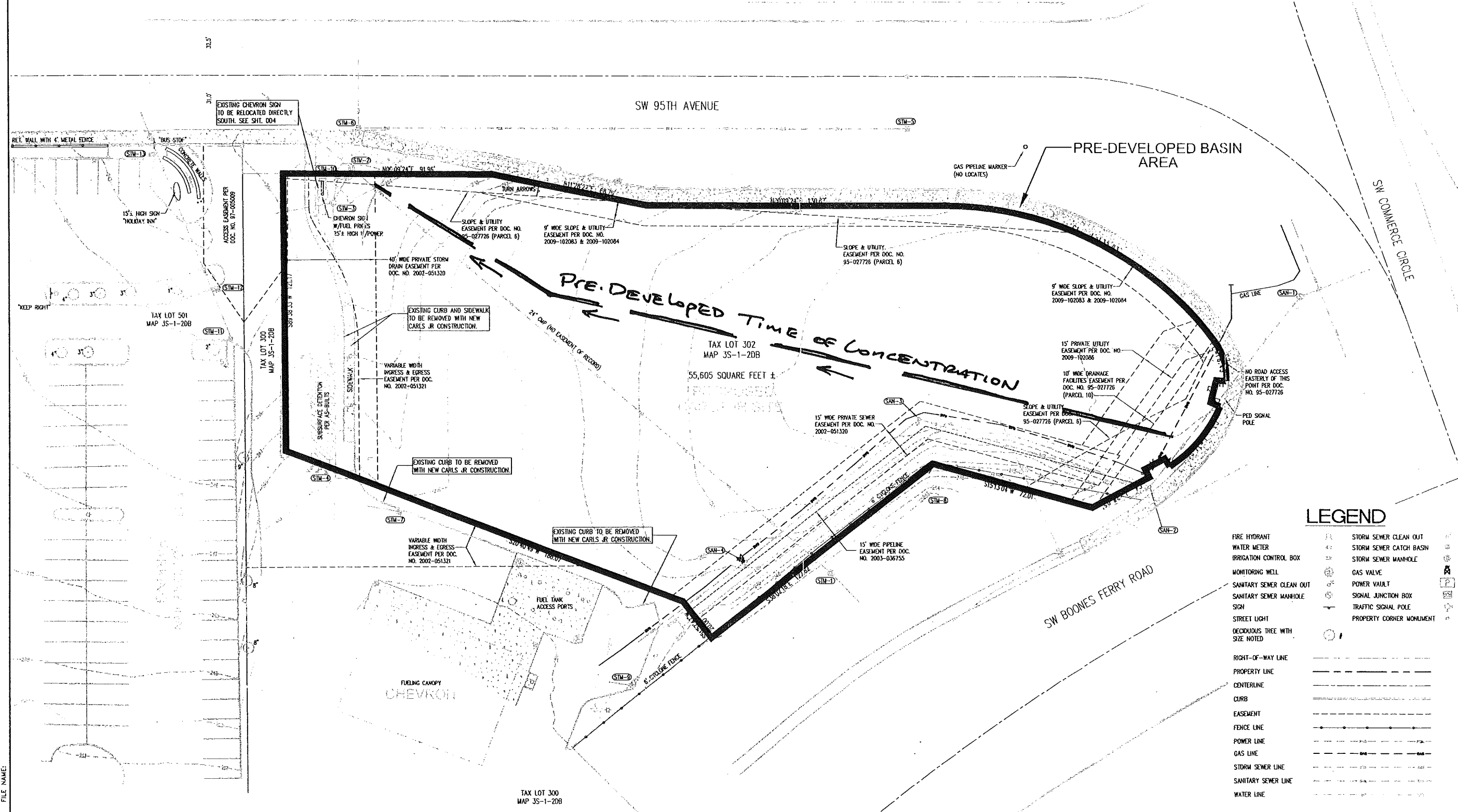
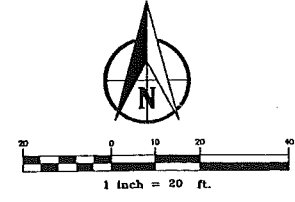
- FIELD WORK WAS COMPLETED DURING MARCH 2012. THERE IS CONSTRUCTION WORK SCHEDULED TO COMMENCE IN THE NEAR FUTURE ALONG THE STREETS ADJACENT TO THE PROPERTY SO CONDITIONS WITHIN THE RIGHT-OF-WAY WILL LIKELY CHANGE.
- ELEVATIONS ARE BASED ON WASHINGTON COUNTY BENCHMARK NO. 455. THE BENCHMARK IS LOCATED IN THE NORTHWEST CORNER OF THE BRIDGE CROSSING OVER INTERSTATE 5 WITH AN ELEVATION OF 281.55 (NGVD 1929 DATUM).
- EASEMENT ARE PER PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY OF OREGON WITH AN EFFECTIVE DATE OF JANUARY 10, 2012 AND ORDER NO. 47251249586204-C150.
- ADDITIONAL FIELD WORK TO INCLUDE THE NORTH END OF THE PARKING LOT FOR THE HOLIDAY INN WAS COMPLETED ON MAY 29, 2012.

**STORM & SANITARY SEWER INFORMATION**

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- SAN-2 CLEAROUT RM 4'± ABOVE GRADE E 244.4'
- SAN-3 CLEAROUT RM 4'± ABOVE GRADE E 245.8'
- SAN-4 CLEAROUT RM 4'± ABOVE GRADE E 246.8'
- \* RECORD INFORMATION PER LAPONT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)
- STM-1 MANHOLE RM 246.88' 24" OUT (SW) 237.3'
- STM-2 MANHOLE RM 244.70' 12" IN (SE) 233.4' 24" IN (NE) 232.6' 24" OUT (SW) 232.6'
- STM-3 MANHOLE RM 243.52' 8" IN (S) 238.3' 8" IN (E) 238.3' 12" OUT (NW) 238.3' (WH BARRED, UNABLE TO REMOVE LID)
- STM-4 MANHOLE RM 248.88' 8" IN (W) 243.4' (STUB?) 10" IN (E) 241.5' 12" OUT (W) 240.2'
- STM-5 CATCH BASIN RM 245.44' (HAS FILTER FABRIC, UNABLE TO WEAS. INVERT, NO AS-BUILT INFO AVAIL.)
- STM-7 CATCH BASIN RM 249.55' 10" OUT (S) 246.0' (HAS TRAP)
- STM-8 CULVERT 18" CWP (SW) 242.5'
- STM-9 CATCH BASIN RM 248.84' 12" IN (E) 236.8' 8" OUT (S) 247.0'
- STM-10 DITCH INLET BOTTOM RM 242.67' 8" OUT (E) 240.0'
- STM-11 CATCH BASIN RM 243.45' WITH TRAP - OUT (W)
- STM-12 CATCH BASIN RM 243.05' WITH TRAP - OUT (S)
- STM-13 CATCH BASIN RM 242.16' WITH TRAP - OUT (W)
- STM-14 MANHOLE RM 243.31' 12" IN (E) 236.8' 15" OUT (W) 236.8'
- \* RECORD INFORMATION PER LAPONT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)

**PRE-DEVELOPED BASIN INFORMATION**

PRE-DEVELOPED IMPERVIOUS AREA: 0.076 ACRES  
 PRE-DEVELOPED PERVIOUS AREA: 1.21 ACRES  
 TOTAL SITE AREA: 1.28 ACRES



**LEGEND**

- FIRE HYDRANT
- WATER METER
- IRRIGATION CONTROL BOX
- MONITORING WELL
- SANITARY SEWER CLEAN OUT
- SANITARY SEWER MANHOLE SIGN
- STREET LIGHT
- DECIDUOUS TREE WITH SIZE NOTED
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- GAS LINE
- STORM SEWER LINE
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- WATER LINE
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- STORM SEWER CATCH BASIN
- STORM SEWER MANHOLE
- GAS VALVE
- POWER VAULT
- SIGNAL JUNCTION BOX
- TRAFFIC SIGNAL POLE
- PROPERTY CORNER MONUMENT

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**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**PRE-DEVELOPED BASIN PLAN**

REVISIONS


STAMP

DATE: 08-xx-2012  
 BLDG. SUBMITTAL DATE: ①  
 BID DATE: ②  
 CKE NO.: ③  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

**DD1**

FILE NAME:

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

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**pond prelim #4 0815-2012 - Grade revised**

Type IA 24-hr 2 Year Rainfall=2.50"

Prepared by {enter your company name here}

Printed 9/6/2012

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Page 2

**Summary for Pond 8P: DETENTION POND (77)**

Inflow Area = 1.215 ac, 69.98% Impervious, Inflow Depth = 1.92" for 2 Year event  
 Inflow = 0.58 cfs @ 7.90 hrs, Volume= 0.195 af  
 Outflow = 0.14 cfs @ 9.83 hrs, Volume= 0.195 af, Atten= 75%, Lag= 115.6 min  
 Primary = 0.14 cfs @ 9.83 hrs, Volume= 0.195 af

Routing by Stor-Ind method, Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Peak Elev= 245.26' @ 9.83 hrs Surf.Area= 1,912 sf Storage= 2,003 cf

Plug-Flow detention time= 170.0 min calculated for 0.195 af (100% of inflow)  
 Center-of-Mass det. time= 170.2 min ( 868.1 - 697.9 )

Volume	Invert	Avail.Storage	Storage Description
#1	244.00'	3,530 cf	<b>Custom Stage Data (Prismatic)</b> Listed below
Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
244.00	1,183	0	0
245.00	1,743	1,463	1,463
246.00	2,390	2,067	3,530

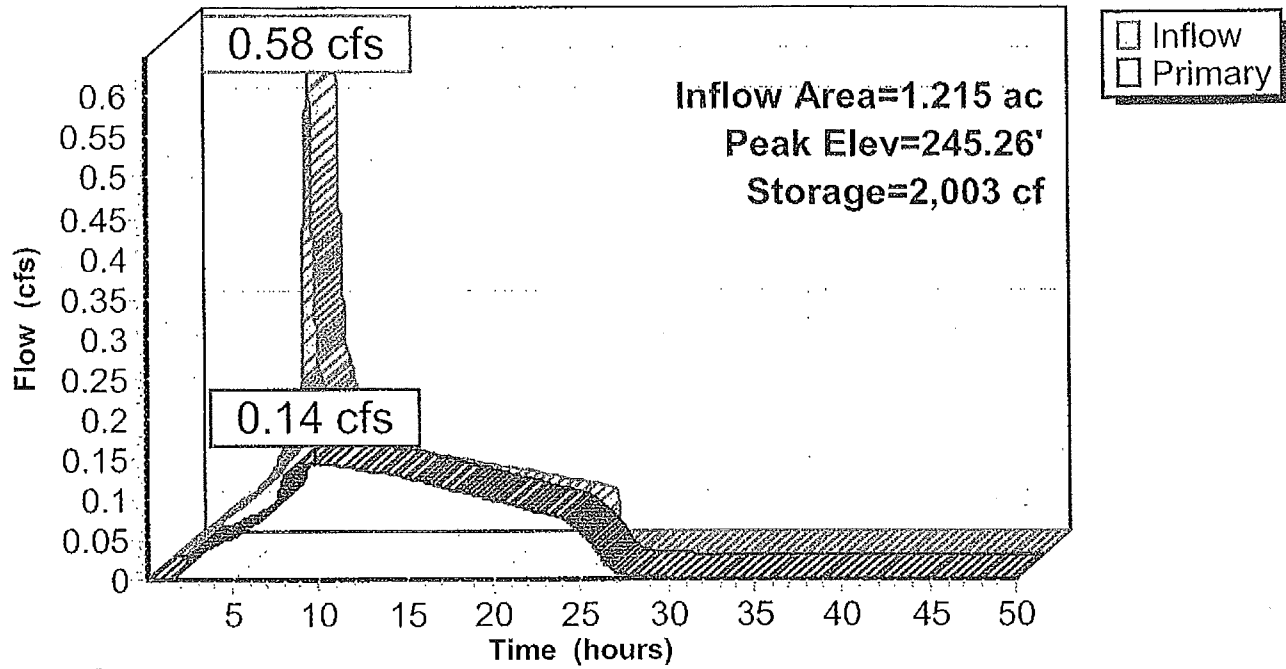
Device	Routing	Invert	Outlet Devices
#1	Primary	244.00'	<b>12.0" Round Culvert</b> L= 15.0' RCP, groove end projecting, Ke= 0.200 Inlet / Outlet Invert= 244.00' / 242.50' S= 0.1000 '/ Cc= 0.900 n= 0.013, Flow Area= 0.79 sf
#2	Device 1	242.50'	<b>2.2" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads
#3	Primary	245.55'	<b>5.0" Vert. Orifice/Grate</b> C= 0.600
#4	Primary	245.90'	<b>12.0" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads

**Primary OutFlow** Max=0.14 cfs @ 9.83 hrs HW=245.26' (Free Discharge)

- 1=Culvert (Passes 0.14 cfs of 4.12 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.14 cfs @ 5.41 fps)
- 3=Orifice/Grate ( Controls 0.00 cfs)
- 4=Orifice/Grate ( Controls 0.00 cfs)

### Pond 8P: DETENTION POND (77)

#### Hydrograph



**pond prelim #4 0815-2012 - Grade revised**

Type IA 24-hr 10 Year Rainfall=3.45"

Prepared by {enter your company name here}

Printed 9/6/2012

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**Summary for Pond 8P: DETENTION POND (77)**

Inflow Area = 1.215 ac, 69.98% Impervious, Inflow Depth = 2.82" for 10 Year event  
 Inflow = 0.85 cfs @ 7.89 hrs, Volume= 0.285 af  
 Outflow = 0.28 cfs @ 8.97 hrs, Volume= 0.285 af, Atten= 67%, Lag= 64.5 min  
 Primary = 0.28 cfs @ 8.97 hrs, Volume= 0.285 af

Routing by Stor-Ind method, Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Peak Elev= 245.76' @ 8.97 hrs Surf.Area= 2,235 sf Storage= 3,035 cf

Plug-Flow detention time= 208.7 min calculated for 0.285 af (100% of inflow)  
 Center-of-Mass det. time= 208.8 min ( 896.7 - 687.9 )

Volume	Invert	Avail.Storage	Storage Description
#1	244.00'	3,530 cf	<b>Custom Stage Data (Prismatic)</b> Listed below
Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
244.00	1,183	0	0
245.00	1,743	1,463	1,463
246.00	2,390	2,067	3,530

Device	Routing	Invert	Outlet Devices
#1	Primary	244.00'	<b>12.0" Round Culvert</b> L= 15.0' RCP, groove end projecting, Ke= 0.200 Inlet / Outlet Invert= 244.00' / 242.50' S= 0.1000 ' Cc= 0.900 n= 0.013, Flow Area= 0.79 sf
#2	Device 1	242.50'	<b>2.2" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads
#3	Primary	245.55'	<b>5.0" Vert. Orifice/Grate</b> C= 0.600
#4	Primary	245.90'	<b>12.0" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads

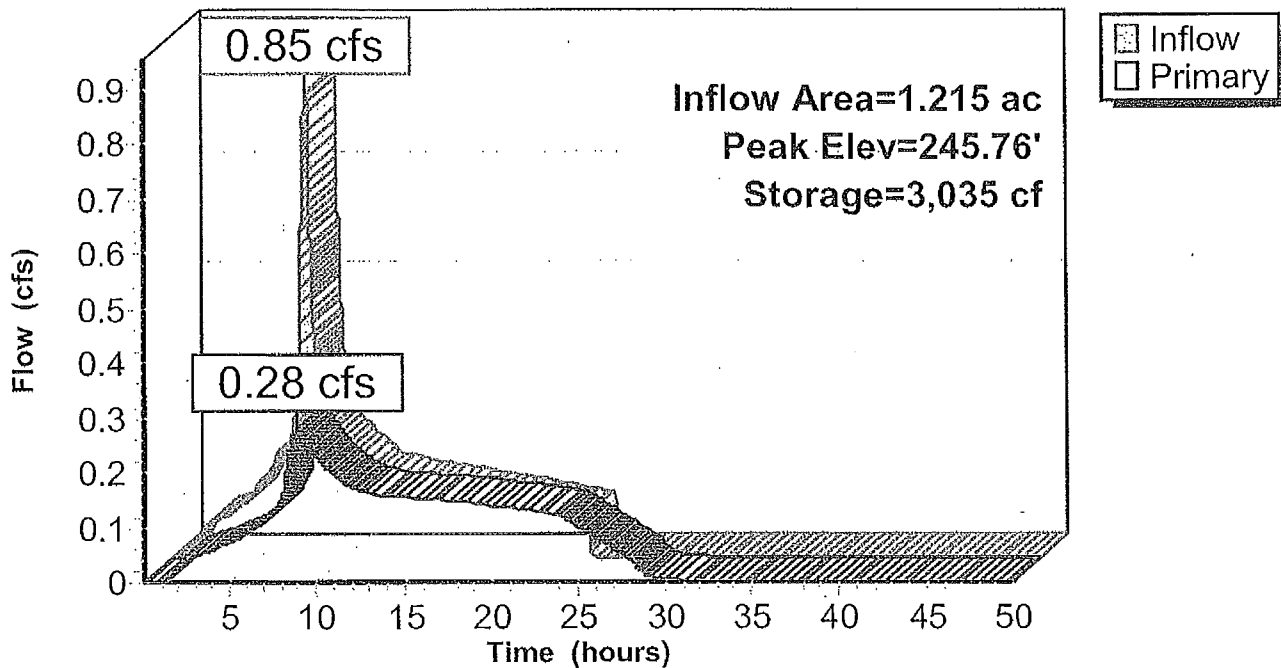
**Primary OutFlow** Max=0.28 cfs @ 8.97 hrs HW=245.76' (Free Discharge)

- 1=Culvert (Passes 0.17 cfs of 5.31 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.17 cfs @ 6.39 fps)
- 3=Orifice/Grate (Orifice Controls 0.11 cfs @ 1.56 fps)
- 4=Orifice/Grate ( Controls 0.00 cfs)



### Pond 8P: DETENTION POND (77)

#### Hydrograph



**pond prelim #4 0815-2012 - Grade revised**

Type IA 24-hr 25 Year Rainfall=3.90"

Prepared by {enter your company name here}

Printed 9/6/2012

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**Summary for Pond 8P: DETENTION POND (77)**

Inflow Area = 1.215 ac, 69.98% Impervious, Inflow Depth = 3.25" for 25 Year event  
 Inflow = 0.98 cfs @ 7.89 hrs, Volume= 0.329 af  
 Outflow = 0.40 cfs @ 8.44 hrs, Volume= 0.329 af, Atten= 59%, Lag= 33.0 min  
 Primary = 0.40 cfs @ 8.44 hrs, Volume= 0.329 af

Routing by Stor-Ind method, Time Span= 0.10-50.00 hrs, dt= 0.01 hrs  
 Peak Elev= 245.88' @ 8.44 hrs Surf.Area= 2,313 sf Storage= 3,285 cf

Plug-Flow detention time= 205.0 min calculated for 0.329 af (100% of inflow)  
 Center-of-Mass det. time= 205.2 min ( 889.4 - 684.3 )

Volume	Invert	Avail.Storage	Storage Description
#1	244.00'	3,530 cf	<b>Custom Stage Data (Prismatic)</b> Listed below
Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
244.00	1,183	0	0
245.00	1,743	1,463	1,463
246.00	2,390	2,067	3,530

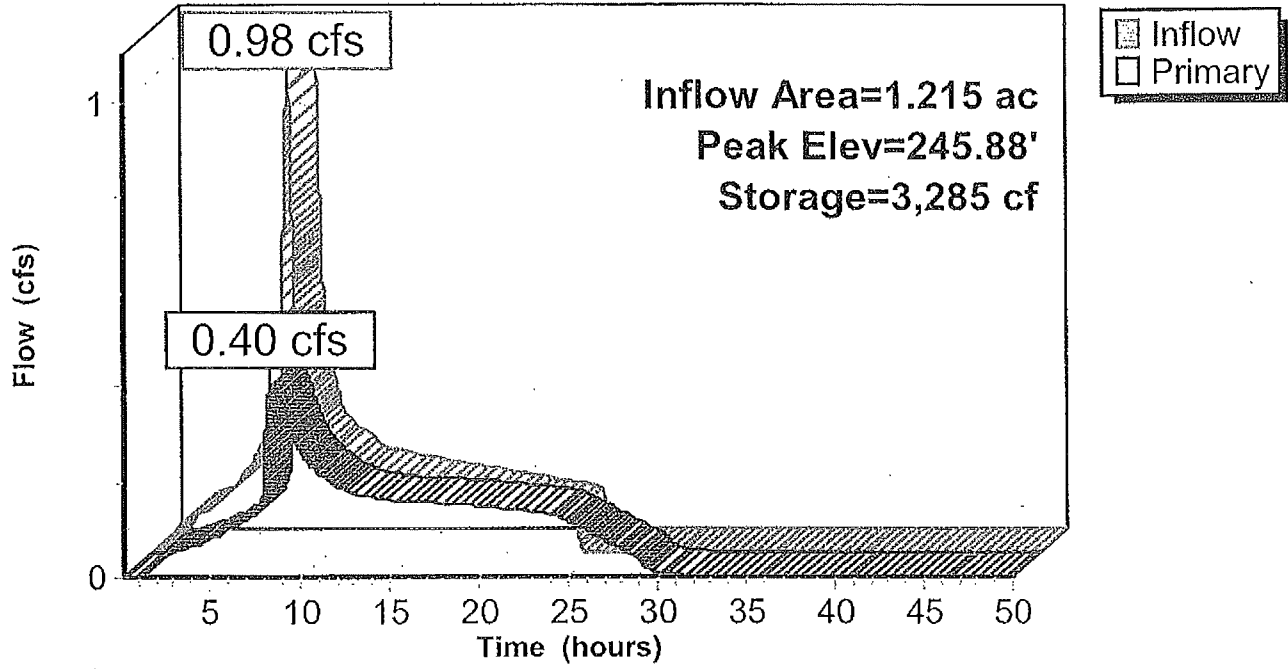
Device	Routing	Invert	Outlet Devices
#1	Primary	244.00'	<b>12.0" Round Culvert</b> L= 15.0' RCP, groove end projecting, Ke= 0.200 Inlet / Outlet Invert= 244.00' / 242.50' S= 0.1000 1' Cc= 0.900 n= 0.013, Flow Area= 0.79 sf
#2	Device 1	242.50'	<b>2.2" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads
#3	Primary	245.55'	<b>5.0" Vert. Orifice/Grate</b> C= 0.600
#4	Primary	245.90'	<b>12.0" Horiz. Orifice/Grate</b> C= 0.600 Limited to weir flow at low heads

**Primary OutFlow** Max=0.40 cfs @ 8.44 hrs HW=245.88' (Free Discharge)

- 1=Culvert (Passes 0.17 cfs of 5.56 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.17 cfs @ 6.60 fps)
- 3=Orifice/Grate (Orifice Controls 0.23 cfs @ 1.96 fps)
- 4=Orifice/Grate ( Controls 0.00 cfs)

**Pond 8P: DETENTION POND (77)**

**Hydrograph**



**Appendix E:**

Geotechnical Report



Real-World Geotechnical Solutions  
Investigation • Design • Construction Support

June 19, 2012  
Project No. 12-2608

Josh Veentjer  
c/o Sunnyside Devco, LLC.  
P.O. Box 916  
Portland, Oregon 97207  
Via email: [josh@iipcorp.com](mailto:josh@iipcorp.com)

**SUBJECT: GEOTECHNICAL ENGINEERING REPORT  
NEW RESTAURANT AND RETAIL STRUCTURES  
SW 95<sup>TH</sup> AVENUE AND SW BOONES FERRY ROAD  
WILSONVILLE, OREGON**

This report presents the preliminary results of a geotechnical engineering study conducted by GeoPacific Engineering, Inc. (GeoPacific) for the above-referenced project. The purpose of our investigation was to evaluate subsurface conditions at the site and to provide geotechnical recommendations for site development. This geotechnical study was performed in accordance with GeoPacific Proposal No. P-4177, dated May 23, 2012, and your subsequent authorization of our proposal and *General Conditions for Geotechnical Services*.

#### **SITE DESCRIPTION AND PROPOSED DEVELOPMENT**

The subject site is located southeast of the intersection of SW 95<sup>th</sup> Avenue and SW Boones Ferry Road in the City of Wilsonville, Washington County, Oregon. Topography at the site is flat to gently sloping. The property is currently unimproved.

Regional topographic mapping (Figure 1) indicates an intermittent stream once flowed through the site. Fill has since been placed at the site – our engineer recalls providing some observation and density testing of some of this fill at the north end of the site.

Based on the preliminary site plans provided, the proposed development consists of the construction of a retail structure, new restaurant building with drive through, parking lots, water quality facility, and associated underground utilities. The preliminary grading plan indicates maximum cuts and fills will be on the order of about 5 feet or less.

#### **REGIONAL AND LOCAL GEOLOGIC SETTING**

Regionally, the subject site lies within the Willamette Valley/Puget Sound lowland, a broad structural depression situated between the Coast Range on the west and the Cascade Range on the east. A series of discontinuous faults subdivide the Willamette Valley into a mosaic of

fault-bounded, structural blocks (Yeats et al., 1996). Uplifted structural blocks form bedrock highlands, while down-warped structural blocks form sedimentary basins.

Regional geologic mapping indicates the site is underlain by the Quaternary age (last 1.6 million years) Willamette Formation, a catastrophic flood deposit associated with repeated glacial outburst flooding of the Willamette Valley (Yeats et al., 1996). The last of these outburst floods occurred about 10,000 years ago. These deposits typically consist of horizontally layered, micaceous, silt to coarse sand forming poorly-defined to distinct beds less than 3 feet thick. Regional studies indicate that the thickness of the Willamette Formation in the vicinity of the subject site is approximately 45 feet (Madin, 1990).

The Willamette Formation is underlain by the Columbia River Basalt Formation (Yeats et al., 1996). The Miocene aged (about 14.5 to 16.5 million years ago) Columbia River Basalts are a thick sequence of lava flows which form the crystalline basement of the Tualatin Valley. The basalts are composed of dense, finely crystalline rock that is commonly fractured along blocky and columnar vertical joints. Individual basalt flow units typically range from 25 to 125 feet thick and interflow zones are typically vesicular, scoriaceous, brecciated, and sometimes include sedimentary rocks.

## **REGIONAL SEISMIC SETTING**

At least three major fault zones capable of generating damaging earthquakes are thought to exist in the vicinity of the subject site. These include the Portland Hills Fault Zone, the Gales Creek-Newberg-Mt. Angel Structural Zone, and the Cascadia Subduction Zone.

### **Portland Hills Fault Zone**

The Portland Hills Fault Zone is a series of NW-trending faults that include the central Portland Hills Fault, the western Oatfield Fault, and the eastern East Bank Fault. These faults occur in a northwest-trending zone that varies in width between 3.5 and 5.0 miles. The combined three faults vertically displace the Columbia River Basalt by 1,130 feet and appear to control thickness changes in late Pleistocene (approx. 780,000 years) sediment (Madin, 1990). The Portland Hills Fault occurs along the Willamette River at the base of the Portland Hills, and is about 10 miles northeast of the site. The Oatfield Fault occurs along the western side of the Portland Hills, and is about 8 miles northeast of the site. The accuracy of the fault mapping is stated to be within 500 meters (Wong, et al., 2000). No historical seismicity is correlated with the mapped portion of the Portland Hills Fault Zone, but in 1991 a M3.5 earthquake occurred on a NW-trending shear plane located 1.3 miles east of the fault (Yelin, 1992). Although there is no definitive evidence of recent activity, the Portland Hills Fault Zone is assumed to be potentially active (Geomatrix Consultants, 1995).

### **Gales Creek-Newberg-Mt. Angel Structural Zone**

The Gales Creek-Newberg-Mt. Angel Structural Zone is a 50-mile-long zone of discontinuous, NW-trending faults that lies about 11 miles southwest of the subject site. These faults are recognized in the subsurface by vertical separation of the Columbia River Basalt and offset seismic reflectors in the overlying basin sediment (Yeats et al., 1996; Werner et al., 1992). A geologic reconnaissance and photogeologic analysis study conducted for the Scoggins Dam site in the Tualatin Basin revealed no evidence of deformed geomorphic surfaces along the structural zone (Unruh et al., 1994). No seismicity has been recorded on the Gales Creek Fault (the fault



closest to the subject site); however, these faults are considered to be potentially active because they may connect with the seismically active Mount Angel Fault and the rupture plane of the 1993 M5.6 Scotts Mills earthquake (Werner et al. 1992; Geomatrix Consultants, 1995).

### **Cascadia Subduction Zone**

The Cascadia Subduction Zone is a 680-mile-long zone of active tectonic convergence where oceanic crust of the Juan de Fuca Plate is subducting beneath the North American continent at a rate of 4 cm per year (Goldfinger et al., 1996). A growing body of geologic evidence suggests that prehistoric subduction zone earthquakes have occurred (Atwater, 1992; Carver, 1992; Peterson et al., 1993; Geomatrix Consultants, 1995). This evidence includes: (1) buried tidal marshes recording episodic, sudden subsidence along the coast of northern California, Oregon, and Washington, (2) burial of subsided tidal marshes by tsunami wave deposits, (3) paleoliquefaction features, and (4) geodetic uplift patterns on the Oregon coast. Radiocarbon dates on buried tidal marshes indicate a recurrence interval for major subduction zone earthquakes of 250 to 650 years with the last event occurring 300 years ago (Atwater, 1992; Carver, 1992; Peterson et al., 1993; Geomatrix Consultants, 1995). The inferred seismogenic portion of the plate interface lies approximately along the Oregon Coast at depths of between 20 and 40 kilometers below the surface.

### **SUBSURFACE CONDITIONS**

Our site-specific exploration for this report was conducted on May 25, 2012. A total of four exploratory test pits were excavated to depths of 10 to 14.5 feet at the approximate locations shown on Figure 2. Test pit locations were located in the field by pacing or taping distances from apparent property corners and other site features shown on the plans provided. As such, the locations of the explorations should be considered approximate.

A GeoPacific geologist continuously monitored the field exploration program and logged the test pits. Soils observed in the explorations were classified in general accordance with the Unified Soil Classification System. During exploration, our geologist also noted geotechnical conditions such as soil consistency, moisture and groundwater conditions. Logs of test pits are attached to this report. The following report sections are based on the exploration program and summarize subsurface conditions encountered at the site.

**Undocumented Fill:** Undocumented fill was encountered directly at the ground surface in test pits TP-1 through TP-4. The fill generally consisted of medium stiff to stiff clayey SILT (ML) with subangular rock and asphalt fragments. The fill encountered in test pits TP-2 and TP-4 was underlain by stiff to very stiff fill at a depth of 3.5 to 4 feet. This fill consisted of clayey SILT (ML) to silty CLAY (CL) with trace gravel and inorganic debris (asphalt and concrete). In test pits, the fill extended to a depth of 8.5 to 11.5 feet. Other areas of undocumented fill may be present outside our test pit locations.

**Residual Soil** – Residual soil derived from in place decomposition of the underlying Columbia River Basalt Formation was encountered beneath the fill in test pits TP-1 through TP-4. These soils generally consisted of stiff, light reddish brown to gray clayey SILT (ML) to silty CLAY (CL) with strong orange and gray mottling and trace weathered basalt fragments. In test pits, the residual soil extended beyond the maximum depth of exploration (14.5 feet).

### Soil Moisture and Groundwater

On May 25, 2012, groundwater was encountered in test pit TP-1 at a depth of 14 feet. Groundwater seepage was encountered in test pit TP-3 at a depth of 3 feet and discharge was visually estimated at 1 gallon per minute. Soils encountered were moist. Experience has shown that temporary storm related perched groundwater within surface soils often occur over native deposits such as those beneath the site, particularly during the wet season. It is anticipated that groundwater conditions will vary depending on the season, local subsurface conditions, changes in site utilization, and other factors.

### **INFILTRATION TESTING**

Soil infiltration testing was performed using the pushed pipe infiltration method in test pit TP-1. Soil in test pit TP-1 was pre-saturated for a period of over 2 hours. The water level was measured to the nearest tenth of an inch every half hour with reference to the ground surface. Table 1 presents the results of our falling head infiltration test.

**Table 1. Summary of Infiltration Test Results**

Test Pit	Depth (feet)	Soil Type	Infiltration Rate(in/hr)	Hydraulic Head Range (inches)
TP-1	12	Clayey SILT (ML) to Silty CLAY (CL)	0.6	50-52

### **CONCLUSIONS AND RECOMMENDATIONS**

Our investigation indicates that the proposed development is geotechnically feasible, provided that the recommendations of this report are incorporated into the design and construction phases of the project. Medium stiff fill soils were encountered at the ground surface over much of the site. This poorly compacted material was likely placed after the placement of compacted fill at the north end of the site as recalled by our engineer. We recommend overexcavation of these poorly compacted soils and replacement with compacted structural fill to a depth 8 feet in the southern structure area, 4 feet for the northern retail structure, and 1 foot in parking lot, slab subgrade, and driveway areas. The deeper overexcavation and replacement may be limited to a trench that is one foot wider than footings on each side provided it is backfilled with compacted granular fill.

#### Site Preparation

Due to the presence of medium stiff fill encountered over much of the site, GeoPacific recommends overexcavation of fill soils and replacement with structural fill placed as described in the following Engineered Fill Section. The depth of overexcavation for the southern building footings is 8 feet and 4 feet for the northern retail structure footings. Horizontal limits should extend at least one foot on either side of proposed footings. The recommended overexcavation for parking lots, driveway and slab areas is 1 foot.

Existing buried structures, if encountered, should be demolished and any cavities structurally backfilled. Organic-rich root zones should then be stripped from construction areas of the site or where engineered fill is to be placed. The final depth of soil removal will be determined on the basis of a site inspection after the stripping/ excavation has been performed. Stripped topsoil should preferably be removed from the site. Any remaining topsoil should be stockpiled only in designated areas and stripping operations should be observed and documented by the geotechnical engineer or his representative.

Remaining undocumented fills and any subsurface structures (dry wells, basements, driveway and landscaping fill, old utility lines, septic leach fields, etc.) should be removed and the excavations backfilled with engineered fill. Explorations indicate that 8.5 to 11.5 feet of undocumented fill is present at the site. We anticipate that other areas of undocumented fill likely exist outside our explorations.

### **Engineered Fill**

All grading for the proposed development should be performed as engineered grading in accordance with the applicable building code at time of construction with the exceptions and additions noted herein. Proper test frequency and earthwork documentation usually requires daily observation and testing during stripping, rough grading, and placement of engineered fill. Imported fill material must be approved by the geotechnical engineer prior to being imported to the site. Oversize material greater than 6 inches in size should not be used within 3 feet of foundation footings, and material greater than 12 inches in diameter should not be used in engineered fill.

Engineered fill should be compacted in horizontal lifts not exceeding 8 inches using standard compaction equipment. We recommend that engineered fill be compacted to at least 95% of the maximum dry density determined by ASTM D698 (Standard Proctor) or equivalent. Field density testing should conform to ASTM D2922 and D3017, or D1556. All engineered fill should be observed and tested by the project geotechnical engineer or his representative. Typically, one density test is performed for at least every 2 vertical feet of fill placed or every 500 yd<sup>3</sup>, whichever requires more testing. Because testing is performed on an on-call basis, we recommend that the earthwork contractor be held contractually responsible for test scheduling and frequency.

Site earthwork will be impacted by soil moisture. Earthwork in wet weather would likely require extensive use of cement or lime treatment, or other special measures, at considerable additional cost compared to earthwork performed under dry-weather conditions.

### **Foundations**

Based on our understanding of the proposed project and the results of our exploration program, and assuming our recommendations for site preparation are followed, engineered fill is anticipated to be encountered at or near the foundation level of the proposed structures. These soils are generally very stiff to dense and should provide adequate support of the structural loads.

Shallow, conventional isolated or continuous spread footings may be used to support the proposed structure provided they are founded on competent native soils or engineered fill. We recommend a maximum allowable bearing pressure of 2,000 pounds per square foot (psf) for designing the footings. The recommended maximum allowable bearing pressure may be

increased by a factor of 1.33 for short term transient conditions such as wind and seismic loading. All footings should be founded at least 12 inches below the lowest adjacent finished grade. Minimum footing widths should be determined by the project engineer/architect in accordance with applicable design codes.

Assuming construction is accomplished as recommended herein, and for the foundation loads anticipated, we estimate total settlement of spread foundations of less than about 1 inch and differential settlement between two adjacent load-bearing components supported on competent soil of less than about  $\frac{3}{4}$  inch. We anticipate that the majority of the estimated settlement will occur during construction, as loads are applied.

Wind, earthquakes, and unbalanced earth loads will subject the proposed structure to lateral forces. Lateral forces on a structure will be resisted by a combination of sliding resistance of its base or footing on the underlying soil and passive earth pressure against the buried portions of the structure. For use in design, a coefficient of friction of 0.5 may be assumed along the interface between the base of the footing and subgrade soils. Passive earth pressure for buried portions of structures may be calculated using an equivalent fluid weight of 390 pounds per cubic foot (pcf), assuming footings are cast against dense, natural soils or engineered fill. The recommended coefficient of friction and passive earth pressure values do not include a safety factor. The upper 12 inches of soil should be neglected in passive pressure computations unless it is protected by pavement or slabs on grade.

Footing excavations should be trimmed neat and the bottom of the excavation should be carefully prepared. Loose, wet or otherwise softened soil should be removed from the footing excavation prior to placing reinforcing steel bars. GeoPacific should observe foundation excavations prior to placement of reinforcing steel and formwork, to verify that an appropriate bearing stratum has been reached and that the actual exposed soils are suitable to support the planned foundation loads.

The above foundation recommendations are for dry weather conditions. Due to the high moisture sensitivity of engineered fill and native soils, construction during wet weather is likely to require overexcavation of footings and backfill with compacted, crushed aggregate. As a result of this condition, we recommend foundation excavations be observed to verify subgrade strength.

### **Concrete Slabs-on-Grade**

Preparation of areas beneath concrete slab-on-grade floors should be performed as recommended in the *Site Preparation* section. Care should be taken during excavation for foundations and floor slabs, to avoid disturbing subgrade soils. If subgrade soils have been adversely impacted by wet weather or otherwise disturbed, the surficial soils should be scarified to a minimum depth of 8 inches, moisture conditioned to within about 3 percent of optimum moisture content, and compacted to engineered fill specifications. Alternatively, disturbed soils may be removed and the removal zone backfilled with additional crushed rock.

For evaluation of the concrete slab-on-grade floors using the beam on elastic foundation method, a modulus of subgrade reaction of 150 kcf (87 pci) should be assumed for the medium stiff native silt soils anticipated at subgrade depth. This value assumes the concrete slab system is designed and constructed as recommended herein, with a minimum thickness of crushed rock of 8 inches beneath the slab.

Interior slab-on-grade floors should be provided with an adequate moisture break. The capillary break material should consist of ODOT Open-Graded Aggregate specified in Table 02630-2 in the Oregon Standard Specifications for Construction 2008 edition. The minimum recommended thickness of capillary break materials on re-compacted soil subgrade is 8 inches. The total thickness of crushed aggregate will be dependent on the subgrade conditions at the time of construction, and should be verified visually by proof-rolling. Under-slab aggregate should be compacted to at least 90% of its maximum dry density as determined by ASTM D1557 or equivalent.

In areas where moisture will be detrimental to floor coverings or equipment inside the proposed structure, appropriate vapor barrier and damp-proofing measures should be implemented. A commonly applied vapor barrier system consists of a 10-mil polyethylene vapor barrier placed directly over the capillary break material. With this type of system, an approximately 2-inch thick layer of sand is often placed over the vapor barrier to protect it from damage, to aid in curing of the concrete, and also to help prevent cement from bleeding down into the underlying capillary break materials. Other damp/vapor barrier systems may also be feasible. Appropriate design professionals should be consulted regarding vapor barrier and damp proofing systems, ventilation, building material selection and mold prevention issues, which are outside GeoPacific's area of expertise.

### **Wet Weather Earthwork**

Soils underlying the site are likely to be moisture sensitive and may be difficult to handle or traverse with construction equipment during periods of wet weather. Earthwork is typically most economical when performed under dry weather conditions. Earthwork performed during the wet-weather season will probably require expensive measures such as cement treatment or imported granular material to compact fill to the recommended engineering specifications. If earthwork is to be performed or fill is to be placed in wet weather or under wet conditions when soil moisture content is difficult to control, the following recommendations should be incorporated into the contract specifications.

- Earthwork should be performed in small areas to minimize exposure to wet weather. Excavation or the removal of unsuitable soils should be followed promptly by the placement and compaction of clean engineered fill. The size and type of construction equipment used may have to be limited to prevent soil disturbance. Under some circumstances, it may be necessary to excavate soils with a backhoe to minimize subgrade disturbance caused by equipment traffic;
- The ground surface within the construction area should be graded to promote run-off of surface water and to prevent the ponding of water;
- Material used as engineered fill should consist of clean, granular soil containing less than 5 percent fines. The fines should be non-plastic. Alternatively, cement treatment of on-site soils may be performed to facilitate wet weather placement;
- The ground surface within the construction area should be sealed by a smooth drum vibratory roller, or equivalent, and under no circumstances should be left uncompacted and exposed to moisture. Soils which become too wet for compaction should be removed and replaced with clean granular materials;
- Excavation and placement of fill should be observed by the geotechnical engineer to verify that all unsuitable materials are removed and suitable compaction and site drainage is achieved; and

➤ Bales of straw and/or geotextile silt fences should be strategically located to control erosion.

If cement or lime treatment is used to facilitate wet weather construction, GeoPacific should be contacted to provide additional recommendations and field monitoring.

### **Excavating Conditions and Utility Trenches**

We anticipate that on-site soils can be excavated using conventional heavy equipment such as scrapers and trackhoes. All temporary cuts in excess of 4 feet in height should be sloped in accordance with U.S. Occupational Safety and Health Administration (OSHA) regulations (29 CFR Part 1926), or be shored. The existing native soils classify as Type B Soil and temporary excavation side slope inclinations as steep as 1H:1V may be assumed for planning purposes. This cut slope inclination is applicable to excavations above the water table only. Maintenance of safe working conditions, including temporary excavation stability, is the responsibility of the contractor. Actual slope inclinations at the time of construction should be determined based on safety requirements and actual soil and groundwater conditions.

Saturated soils and groundwater may be encountered in utility trenches, particularly during the wet season. We anticipate that dewatering systems consisting of ditches, sumps and pumps would be adequate for control of perched groundwater. Regardless of the dewatering system used, it should be installed and operated such that in-place soils are prevented from being removed along with the groundwater.

Vibrations created by traffic and construction equipment may cause some caving and raveling of excavation walls. In such an event, lateral support for the excavation walls should be provided by the contractor to prevent loss of ground support and possible distress to existing or previously constructed structural improvements.

PVC pipe should be installed in accordance with the procedures specified in ASTM D2321. We recommend that trench backfill be compacted to at least 95% of the maximum dry density obtained by Standard Proctor ASTM D698 or equivalent. Initial backfill lift thickness for a ¾"-0 crushed aggregate base may need to be as great as 4 feet to reduce the risk of flattening underlying flexible pipe. Subsequent lift thickness should not exceed 1 foot. If imported granular fill material is used, then the lifts for large vibrating plate-compaction equipment (e.g. hoe compactor attachments) may be up to 2 feet, provided that proper compaction is being achieved and each lift is tested. Use of large vibrating compaction equipment should be carefully monitored near existing structures and improvements due to the potential for vibration-induced damage.

Adequate density testing should be performed during construction to verify that the recommended relative compaction is achieved. Typically, one density test is taken for every 4 vertical feet of backfill on each 200-lineal-foot section of trench.

### **Erosion Control Considerations**

During our field exploration program, we did not observe soil types that would be considered highly susceptible to erosion. In our opinion, the primary concern regarding erosion potential will occur during construction, in areas that have been stripped of vegetation. Erosion at the site during construction can be minimized by implementing the project erosion control plan, which



should include judicious use of straw bales and silt fences. If used, these erosion control devices should be in place and remain in place throughout site preparation and construction.

Erosion and sedimentation of exposed soils can also be minimized by quickly re-vegetating exposed areas of soil, and by staging construction such that large areas of the project site are not denuded and exposed at the same time. Areas of exposed soil requiring immediate and/or temporary protection against exposure should be covered with either mulch or erosion control netting/blankets. Areas of exposed soil requiring permanent stabilization should be seeded with an approved grass seed mixture, or hydroseeded with an approved seed-mulch-fertilizer mixture.

### Footing and Roof Drains

Experience has shown that temporary perched groundwater conditions often occur over fine-grained native deposits such as those beneath the site, particularly during the wet season. To minimize the fluctuation of soil moisture content near structural foundations, we recommend that the structure be constructed with perimeter footing drains. The outside edge of perimeter footings should be provided with a drainage system consisting of 3-inch minimum diameter perforated plastic pipe embedded in a minimum of 1 ft<sup>3</sup> per lineal foot of clean, free-draining sand and gravel or 1 1/4" - 1/4" drain rock. The drain pipe and surrounding drain rock should be wrapped in non-woven geotextile (Mirafi 140N, or approved equivalent) to minimize the potential for clogging and/or ground loss due to piping. Water collected from the footing drains should be directed into the local storm drain system or other suitable outlet. A minimum 0.5 percent fall should be maintained throughout the drain and non-perforated pipe outlet. The footing drains should include clean-outs to allow periodic maintenance and inspection.

Down spouts and roof drains should collect roof water in a system separate from the footing drains in order to reduce the potential for clogging. Roof drain water should be directed to an appropriate discharge point well away from structural foundations. Grades should be sloped downward and away from buildings to reduce the potential for ponded water near structures.

### Pavement Design

For design purposes, we used an estimated resilient modulus of 9,000 for compacted native soil. Table 2 presents our recommended minimum pavement section for dry weather construction.

**Table 2 - Recommended Minimum Dry-Weather Pavement Section**

Material Layer	Parking Stalls	Private Driveways	Compaction Standard
Asphaltic Concrete (AC)	2.5 in.	3 in.	92%/ 92% of Rice Density AASHTO T-209
Crushed Aggregate Base 3/4"-0 (leveling course)	2 in.	2 in.	95% of Modified Proctor AASHTO T-180
Crushed Aggregate Base 1 1/2"-0	6 in.	8 in.	95% of Modified Proctor AASHTO T-180
Subgrade	12 in.	12 in.	90% of Standard Proctor AASHTO T-99 or equivalent

Any pockets of organic debris or loose fill encountered during ripping or tilling should be removed and replaced with engineered fill (see *Site Preparation* Section). In order to verify subgrade strength, we recommend proof-rolling directly on subgrade with a loaded dump truck during dry weather and on top of base course in wet weather. Soft areas that pump, rut, or weave should be stabilized prior to paving. If pavement areas are to be constructed during wet weather, the subgrade and construction plan should be reviewed by the project geotechnical engineer at the time of construction so that condition specific recommendations can be provided. The moisture sensitive subgrade soils make the site a difficult wet weather construction project.

During placement of pavement section materials, density testing should be performed to verify compliance with project specifications. Generally, one subgrade, one base course, and one asphalt compaction test is performed for every 100 to 200 linear feet of paving.

**Seismic Design**

Structures should be designed to resist earthquake loading in accordance with the methodology described in the 2006 International Building Code (IBC) with applicable 2007 Oregon Structural Specialty Code (OSSC) revisions. We recommend Site Class D be used for design per the OSSC, Table 1613.5.2. Design values determined for the site using the USGS (United States Geological Survey) *Earthquake Ground Motion Parameters* utility are summarized below.

**Table 3. Recommended Earthquake Ground Motion Parameters (2006 IBC / 2007 OSSC)**

Parameter	Value
Location (Lat, Long), degrees	45.337, -122.773
Mapped Spectral Acceleration Values (MCE, Site Class B):	
Short Period, $S_s$	0.886 g
1.0 Sec Period, $S_1$	0.325 g
Soil Factors for Site Class D:	
$F_a$	1.145
$F_v$	1.750
$SD_s = 2/3 \times F_a \times S_s$	0.677 g
$SD_1 = 2/3 \times F_v \times S_1$	0.379 g

Soil liquefaction is a phenomenon wherein saturated soil deposits temporarily lose strength and behave as a liquid in response to earthquake shaking. Soil liquefaction is generally limited to loose, granular soils located below the water table. Following development, on-site soils will consist predominantly of compacted engineered fill soils which are not considered susceptible to liquefaction. Therefore, it is our opinion that special design or construction measures are not required to mitigate the effects of liquefaction.

### UNCERTAINTIES AND LIMITATIONS

We have prepared this report for the owner and their consultants for use in design of this project only. This report should be provided in its entirety to prospective contractors for bidding and estimating purposes; however, the conclusions and interpretations presented in this report should not be construed as a warranty of the subsurface conditions. Experience has shown that soil and groundwater conditions can vary significantly over small distances. Inconsistent conditions can occur between explorations that may not be detected by a geotechnical study. If, during future site operations, subsurface conditions are encountered which vary appreciably from those described herein, GeoPacific should be notified for review of the recommendations of this report, and revision of such if necessary.

Sufficient geotechnical monitoring, testing and consultation should be provided during construction to confirm that the conditions encountered are consistent with those indicated by explorations. The checklist attached to this report outlines recommended geotechnical observations and testing for the project. Recommendations for design changes will be provided should conditions revealed during construction differ from those anticipated, and to verify that the geotechnical aspects of construction comply with the contract plans and specifications.

Within the limitations of scope, schedule and budget, GeoPacific attempted to execute these services in accordance with generally accepted professional principles and practices in the fields of geotechnical engineering and engineering geology at the time the report was prepared. No warranty, expressed or implied, is made. The scope of our work did not include environmental assessments or evaluations regarding the presence or absence of wetlands or hazardous or toxic substances in the soil, surface water, or groundwater at this site.

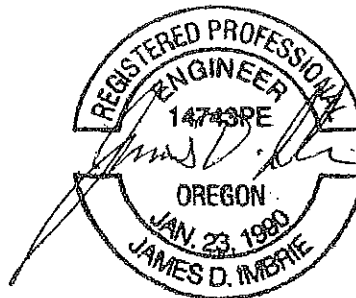
We appreciate this opportunity to be of service.

Sincerely,

GEOPACIFIC ENGINEERING, INC.



Beth K. Rapp, G.I.T.  
Project Geologist



EXPIRES: 06/30/2013

James D. Imbrie, G.E., C.E.G.  
Principal Geotechnical Engineer

- Attachments: References  
Checklist of Recommended Geotechnical Testing and Observation  
Figure 1 – Vicinity Map  
Figure 2 – Site and Exploration Plan  
Test Pit Logs (TP-1 – TP-4)

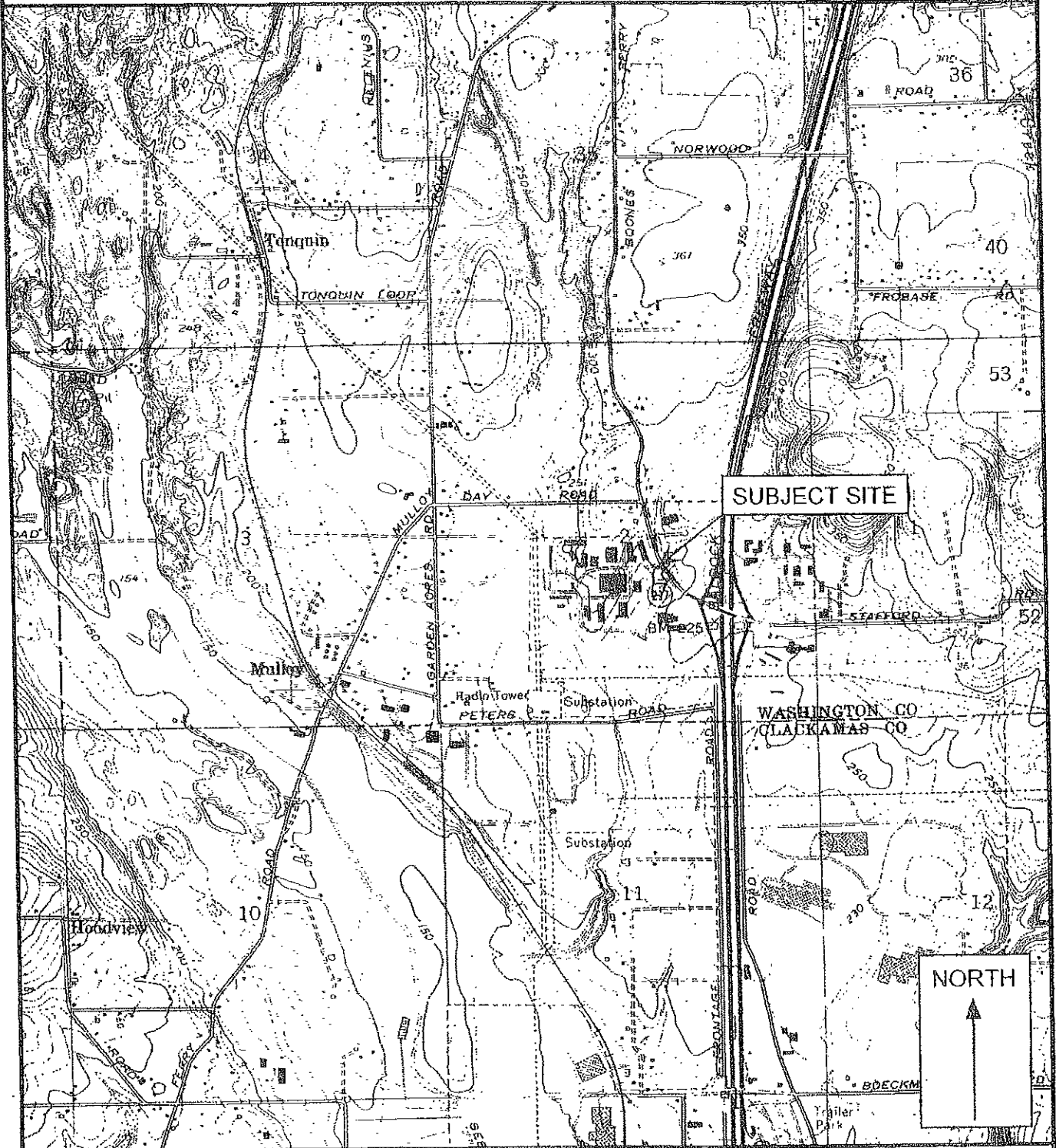
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- Yeats, R.S., Graven, E.P., Werner, K.S., Goldfinger, C., and Popowski, T., 1996, Tectonics of the Willamette Valley, Oregon: in *Assessing earthquake hazards and reducing risk in the Pacific Northwest*, v. 1: U.S. Geological Survey Professional Paper 1560, P. 183-222, 5 plates, scale 1:100,000.
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14835 SW 72nd Avenue  
 Portland, Oregon 97224  
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### VICINITY MAP



**Legend**

Approximate Scale 1 in = 2,000 ft

Date: 06/05/12

Drawn by: EKR

Base map: U.S. Geological Survey 7.5 minute Topographic Map Series, Sherwood, Oregon Quadrangle, 1961 (Photorevised 1985).

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

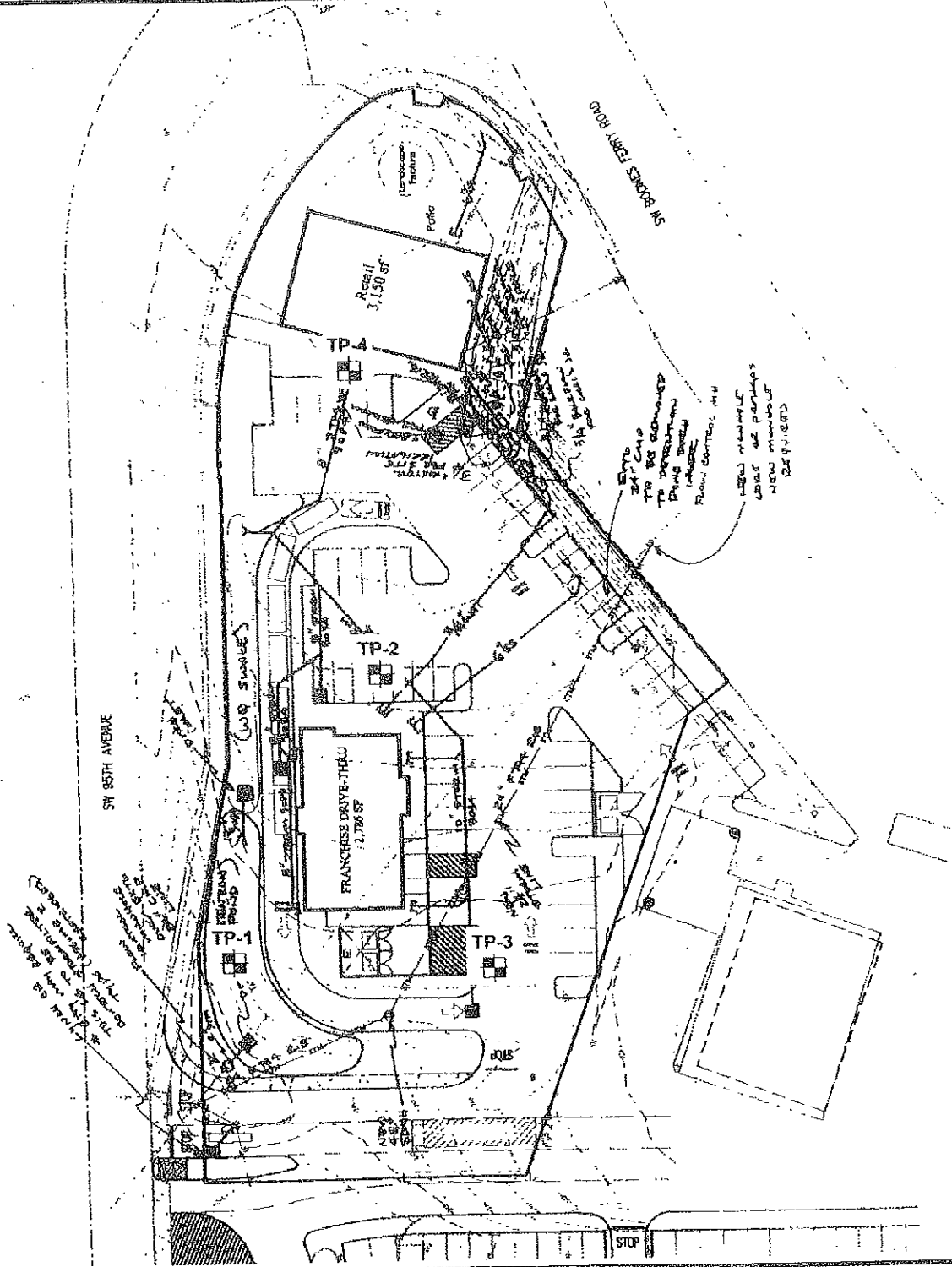
Project No. 12-2608

FIGURE 1




14835 SW 72nd Avenue  
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# SITE PLAN AND EXPLORATION LOCATIONS



**Legend**

TP-1  
 Test Pit Designation and Approximate Location

0 60'  
 APPROXIMATE SCALE 1"=60'

Date: 06/05/12  
 Drawn by: EKR

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

Project No. 12-2608

FIGURE 2





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# TEST PIT LOG

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

Project No. 12-2608

Test Pit No. TP-1

Depth (ft)	Pocket Penetrometer (tons/ft <sup>2</sup> )	Sample Type	In-Situ Dry Density (lb/ft <sup>3</sup> )	Moisture Content (%)	Water Bearing Zone	Material Description
1	1.0					
2	1.5					Medium stiff to stiff, clayey SILT (ML), with sand and gravel, brown, trace asphalt fragments, trace organics, subtle to strong orange and gray mottling, moist (Fill)
3	1.5					
4	1.5					
5						
6						
7						
8						
9						
10						
11						
12						Stiff, clayey SILT (ML) to silty CLAY (CL), trace weathered basalt, grayish brown, micaceous, orange and gray mottling, moist (Residual Soil)
13						
14						
15						Test Pit Terminated at 14.5 Feet.
16						
17						Note: Groundwater encountered at 14 feet.

**LEGEND**



Bag Sample



Bucket Sample



Shelby Tube Sample



Seepage



Water Bearing Zone



Water Level at Abandonment

Date Excavated: 5/25/2012

Logged By: B. Rapp

Surface Elevation:



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# TEST PIT LOG

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

Project No. 12-2608

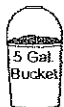
Test Pit No. **TP-2**

Depth (ft)	Pocket Penetrometer (tons/ft <sup>2</sup> )	Sample Type	In-Situ Dry Density (lb/ft <sup>3</sup> )	Moisture Content (%)	Water Bearing Zone	Material Description
1	2.0					Medium stiff to very stiff, clayey SILT (ML), with subangular gravel, brown, trace fine roots, strong orange and gray mottling, rock up to 9 inches in diameter, moist (Fill)
2	1.0					
3	1.0					
4	3.0					Stiff to very stiff, clayey SILT (ML) to silty CLAY (CL), with gravel, brown, trace inorganic debris (asphalt and concrete), trace wood fragments, strong orange and gray mottling, rock up to 12 inches in diameter, moist (Fill)
5						
6						
7						
8						
9						Stiff, clayey SILT (ML) to silty CLAY (CL), trace weathered basalt, light reddish brown, micaceous, subtle orange and gray mottling, moist (Residual Soil)
10						
11						
12						Test Pit Terminated at 12 Feet.
13						Note: No seepage or groundwater encountered.
14						
15						
16						
17						

**LEGEND**



100 to 1,000 g



5 Gal Bucket



Shaly Tube Sample



Seepage



Water Bearing Zone



Water Level at Abandonment

Date Excavated: 5/25/2012

Logged By: B. Rapp

Surface Elevation:




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# TEST PIT LOG

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

Project No. 12-2608

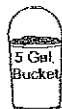
Test Pit No. **TP-3**

Depth (ft)	Pocket Penetrometer (tons/ft <sup>2</sup> )	Sample Type	In-Situ Dry Density (lb/ft <sup>3</sup> )	Moisture Content (%)	Water Bearing Zone	Material Description
1	1.0					Medium stiff to very stiff, clayey SILT (ML), with subangular gravel, brown, trace fine roots, strong orange and gray mottling, trace asphalt fragments, moderately organic zone at 4 to 5 feet, moist (Fill)
2	2.0					
3	1.5					
4	2.0					
5						
6						
7						
8						
9						Stiff, clayey SILT (ML) to silty CLAY (CL), trace weathered basalt, light reddish brown, micaceous, subtle orange and gray mottling, moist (Residual Soil)
10						Test Pit Terminated at 10 Feet.
11						
12						Note: Groundwater seepage encountered at 3 feet. Discharge visually estimated at 1 gallon per minute.
13						
14						
15						
16						
17						

**LEGEND**



Bag Sample



5 Gal. Bucket Sample



Shelby Tube Sample



Seepage



Water Bearing Zone



Water Level at Abandonment

Date Excavated: 5/25/2012

Logged By: B. Rapp

Surface Elevation:



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# TEST PIT LOG

Project: SW 95th Avenue and Boones Ferry Road  
 Wilsonville, Oregon

Project No. 12-2608

Test Pit No. **TP-4**

Depth (ft)	Pocket Penetrometer (tons/ft <sup>2</sup> )	Sample Type	In-Situ Dry Density (lb/ft <sup>3</sup> )	Moisture Content (%)	Water Bearing Zone	Material Description
1	1.0					Medium stiff, clayey SILT (ML), with subangular gravel, brown, trace fine roots, strong orange and gray mottling, moist (Fill)
2	1.0					
3	1.0					
4	3.5					Stiff to very stiff, clayey SILT (ML) to silty CLAY (CL), with gravel, brown, trace inorganic debris (asphalt and concrete), trace organics, strong orange and gray mottling, moist (Fill)
5						
6						
7						
8						
9						
10						Stiff to very stiff, clayey SILT (ML) to silty CLAY (CL), trace weathered basalt, light reddish brown, subtle orange and gray mottling, moist (Residual Soil)
11						
12						
13						
14						Test Pit Terminated at 13.5 Feet.
15						
16						Note: No seepage or groundwater encountered.
17						

**LEGEND**



100 to 1,000 g  
Bag Sample



5 Gal Bucket Sample



Shelby Tube Sample



Seepage



Water Bearing Zone



Water Level at Abandonment

Date Excavated: 5/25/2012

Logged By: B. Rapp

Surface Elevation:

LaPoint Business Group, LLC  
25410 Southwest 95th Avenue,  
Wilsonville, OR 97070

Holiday Inn  
25425 Southwest 95th Avenue  
Wilsonville, OR 97070

Riverwood Business Center, LLC.  
15585 SW Tualatin Sherwood Rd.  
Sherwood, OR 97140

TKG Investments, LLC.  
9120 SW PIONEER CT STE B  
WILSONVILLE , OR , 97070

EGHDAMI, CYRUS & MEHRNAZ  
10085 Southwest Commerce  
Circle  
Wilsonville, OR 97070

AGC Center, LLC.  
9450 Southwest Commerce Circle  
Wilsonville, OR 97070

Commerce Center South, LLC.  
166 KINGS HWY N  
WESTPORT , CT , 06880

NW1/4 SE1/4 SECTION 2 T3S RIW W.M.

WASHINGTON COUNTY OREGON

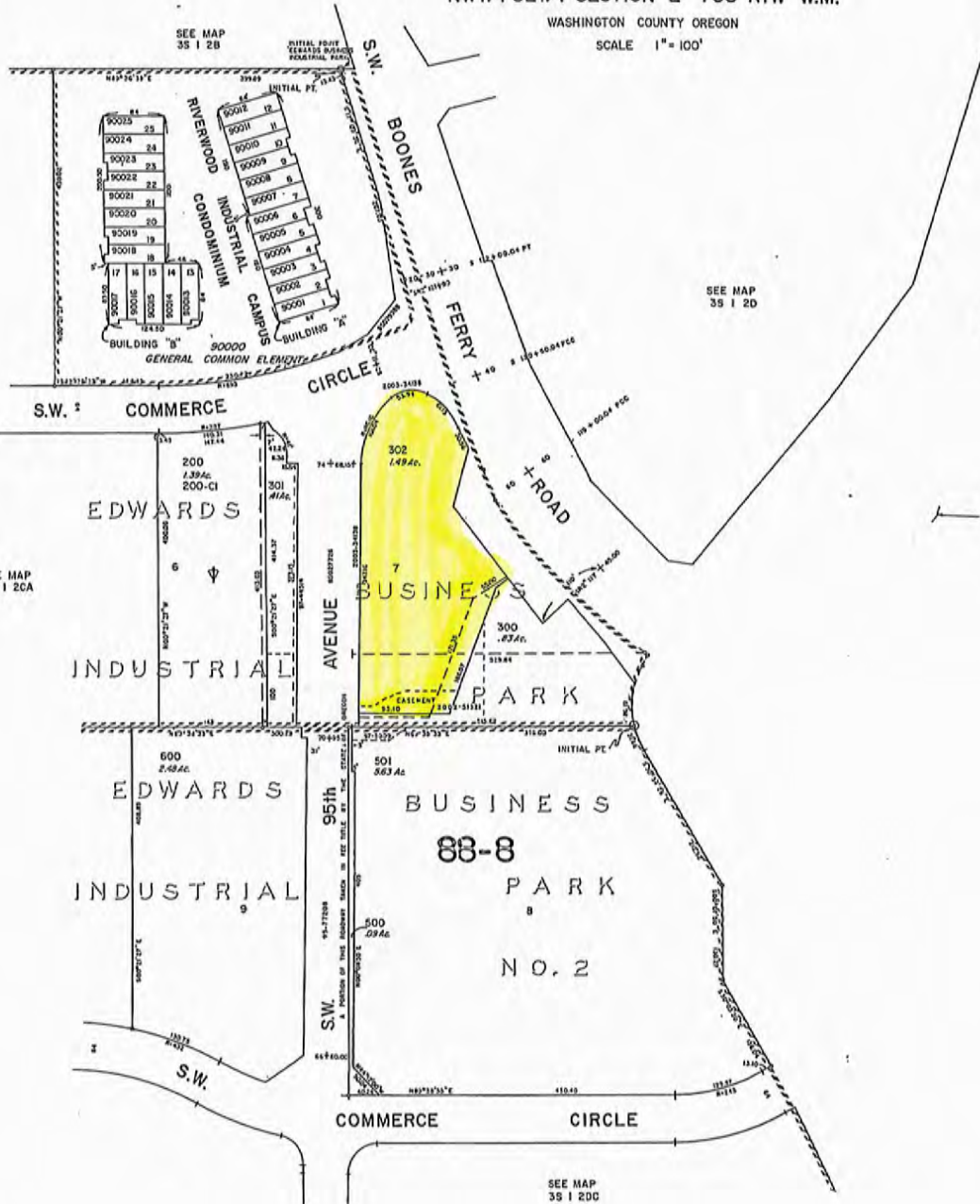
SCALE 1"=100'

3S 1 2DB

CANCELLED TAX LOTS  
105,103,303,

SEE MAP  
3S 1 2B

INITIAL FOUR  
TERRACE SURVEY  
FOR AGRICULTURAL PLOTS



SEE MAP  
3S 1 2D



FREWAY



SEE MAP  
3S 1 2D

+ L 403+74.56 P.C.S

BALDOCK

+ S 85+18.45 P.O.T  
+ S 415+16.27 P.O.S

R.H.

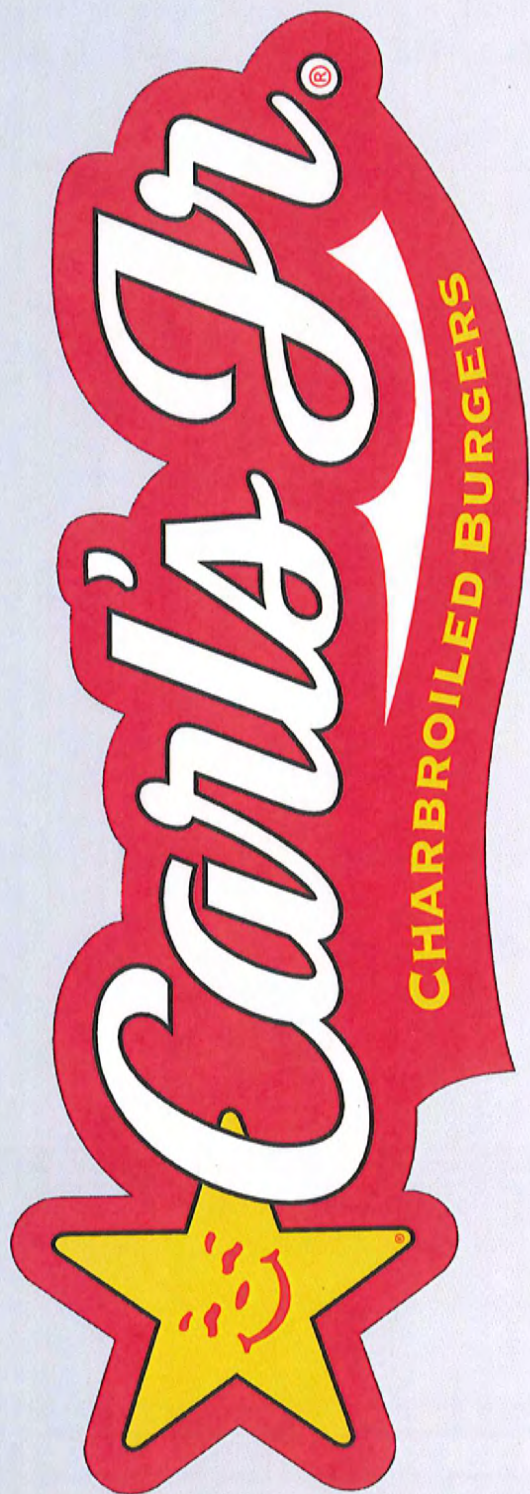
FOR ASSESSMENT PURPOSES ONLY.  
DO NOT RELY ON FOR ANY OTHER USE.

WILSONVILLE  
3S 1 2DB

SEE MAP  
3S 1 2DC

3/2/2011 10:03:39





SW 95TH AVENUE & SW BOONES FERRY ROAD,  
WILSONVILLE, OR 97070



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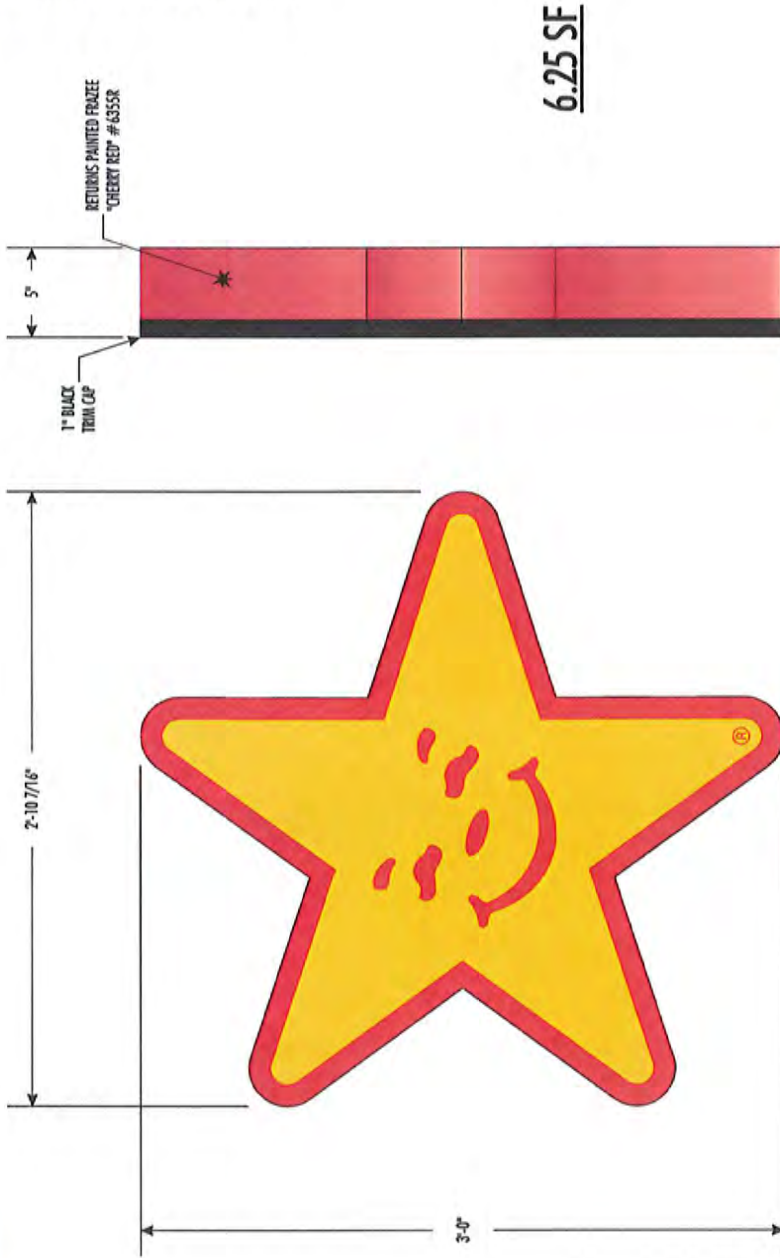
**A**

**NEW 36" ILL. CARL'S JR STAR SIGN**

SCALE: NTS

ORDER PART # CKE36STAR

ITEM	DESCRIPTION
A	3M 3630-73 RED VINYL
B	3M 3630-015 YELLOW VINYL
C	TRIM CAP - BLACK
D	RETURNS - FRAZEE "CHERRY RED" #655SR
E	
F	
MATERIAL LIST	
1	FACE - YELLOW ACRYLIC # 4072
2	ALUMINUM
3	
4	
5	



**FRONT VIEW**

**SIDE VIEW**

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Revision	D





**B**

**NEW 42" ILL. CARL'S JR CLOUD SIGN**  
SCALE: NTS

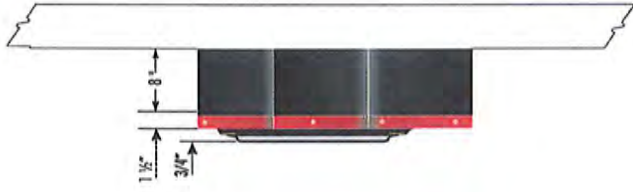
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**SIDE VIEW**

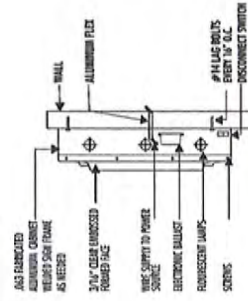


**FRONT VIEW**



**SIDE VIEW**

**30 SF**



**SECTION VIEW**

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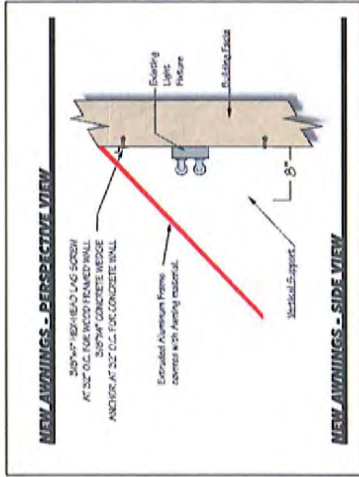
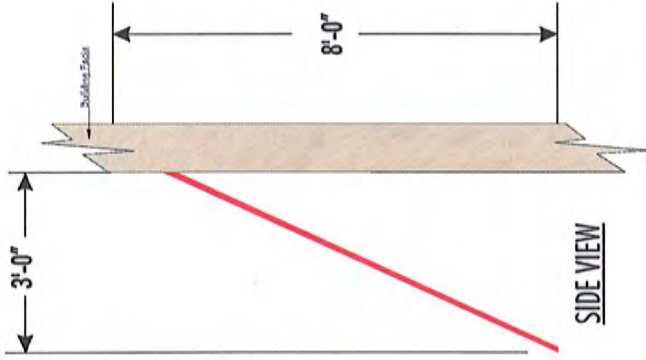
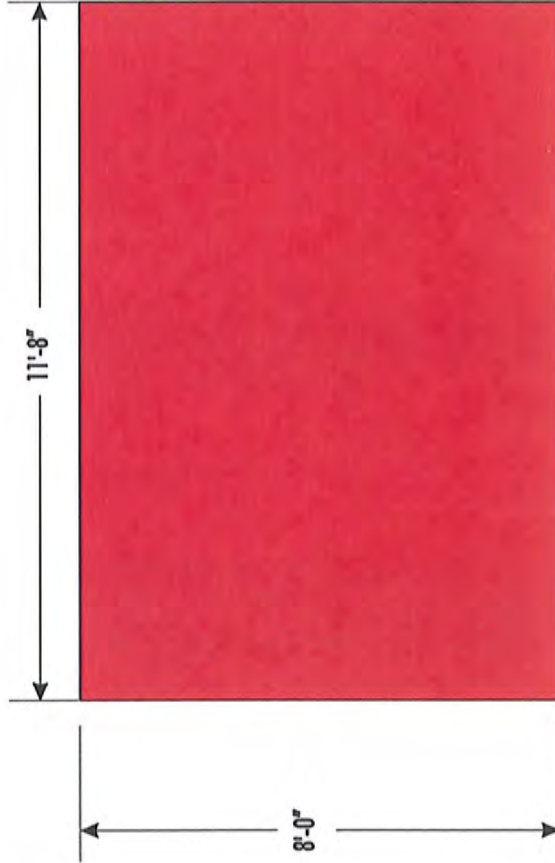
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Revision	D



D

**ILLUMINATED VINYL TRANSLUCENT AWNING (RED)**

ORDER PART # CKE4AWN-001



**CARL'S JR. - NEW STRAIGHT AWNING (S)**  
SCALE: NTS

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Tel: 951-742-8354 • Fax: 951-742-7443  
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Revision	D

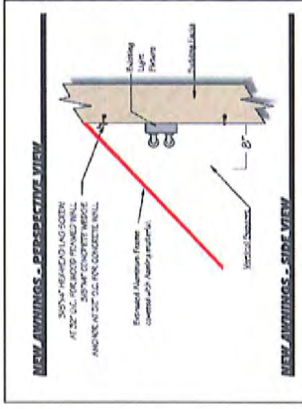
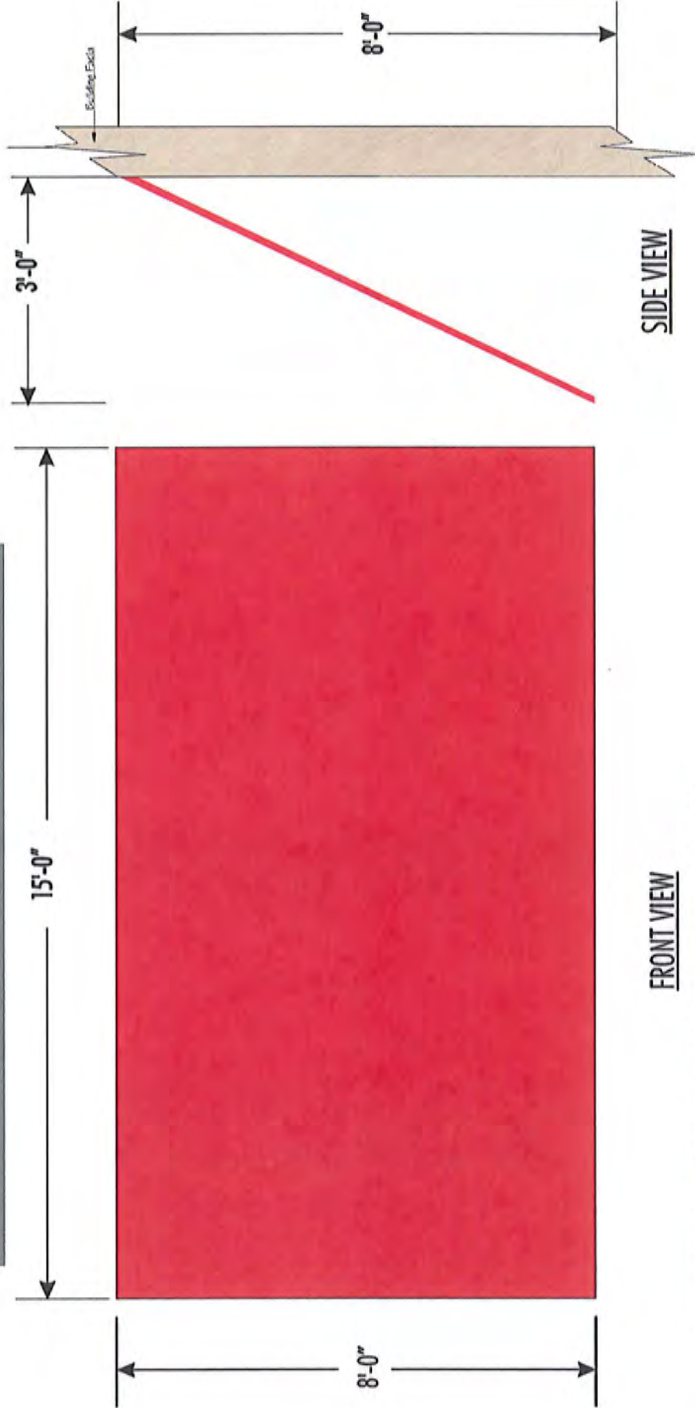






# ILLUMINATED VINYL TRANSLUCENT AWNING (RED)

ORDER PART # CKE4AWN-003



FRONT VIEW

SIDE VIEW

CARL'S JR. - NEW STRAIGHT AWNING (S)

SCALE: NTS



6111 Phoenix Blvd., Newport, CA 95070  
800.433.4243 • Fax 925.560.7143  
Website: www.signresource.com

CARL'S JR. - WILSONVILLE, OR.

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D

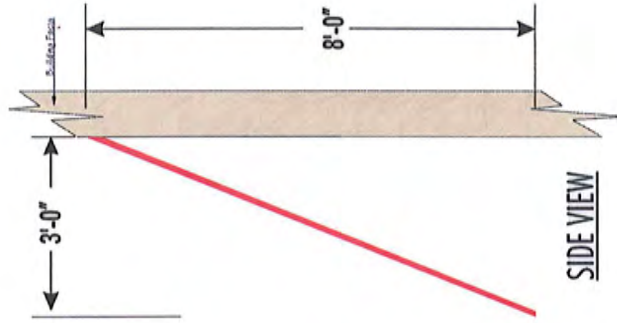
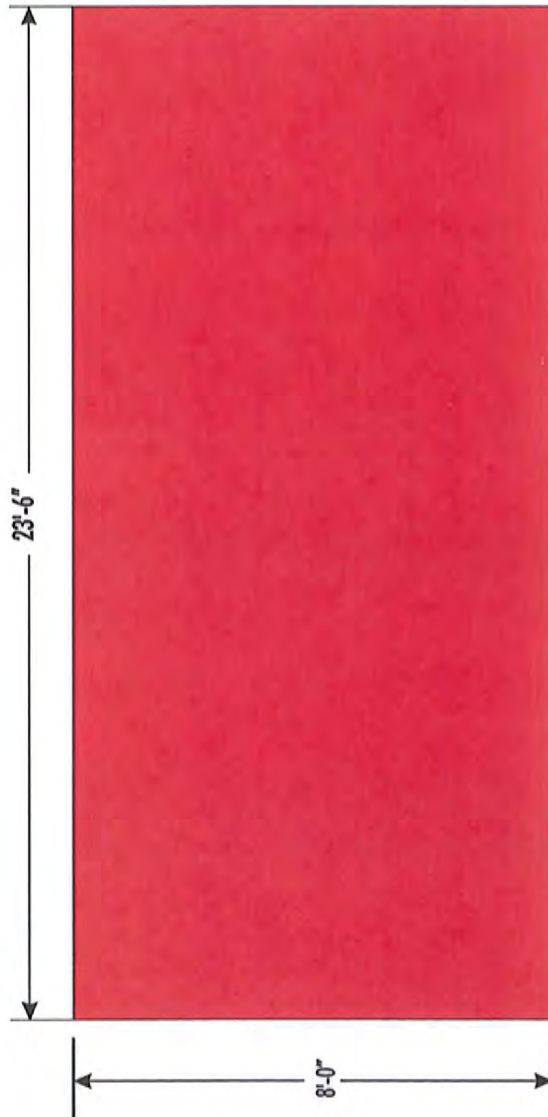
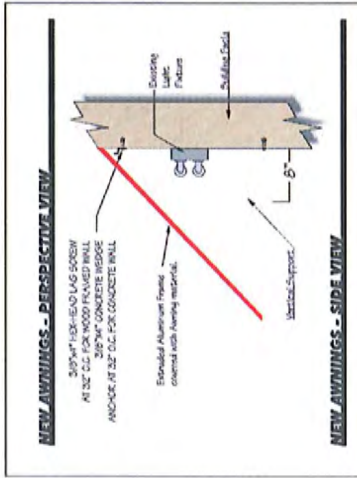






# ILLUMINATED VINYL TRANSLUCENT AWNING (RED)

ORDER PART # CKE4AWN-002



CARL'S JR. - NEW STRAIGHT AWNING (S)  
SCALE: NTS

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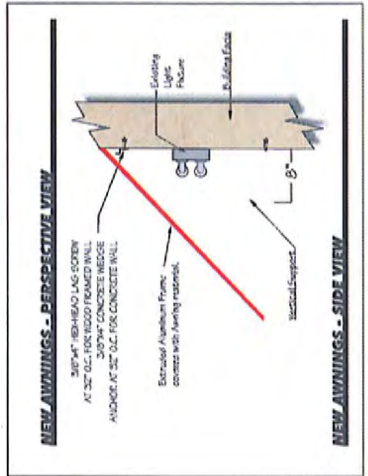
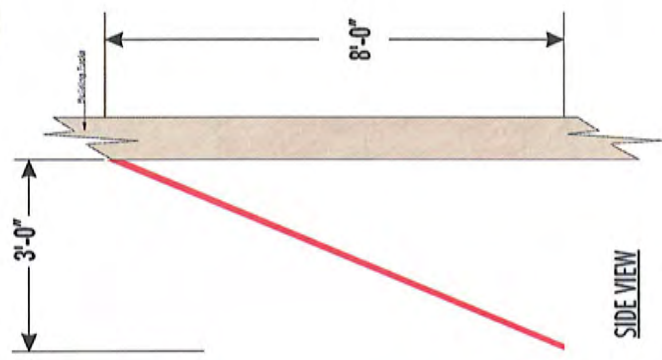
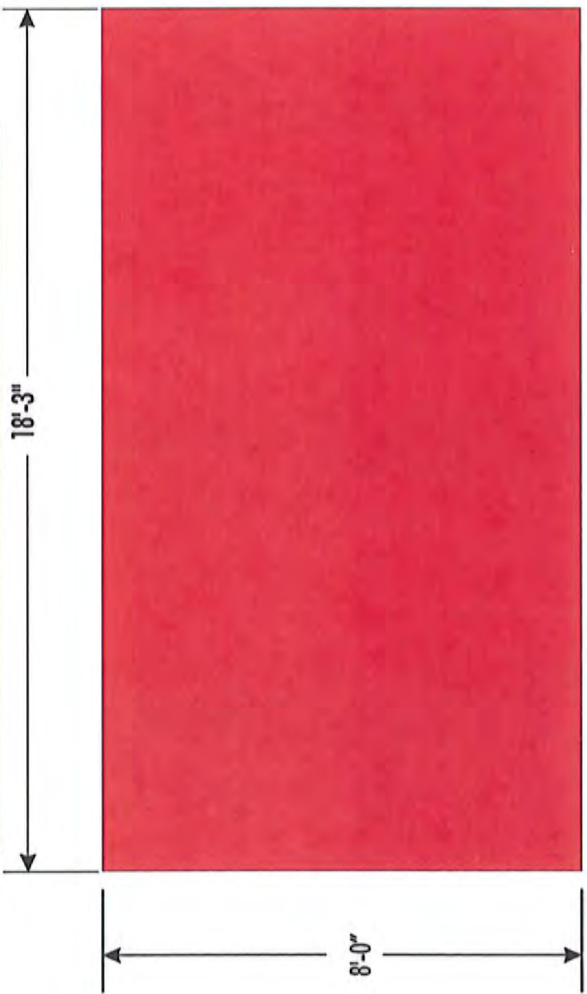
Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D



**G**

**ILLUMINATED VINYL TRANSLUCENT AWNING (RED)**

**ORDER PART # CKE4AWN-004**



**CARL'S JR. - NEW STRAIGHT AWNING (S)**  
SCALE: NTS

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Revision	D

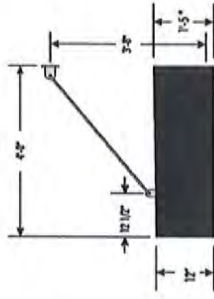
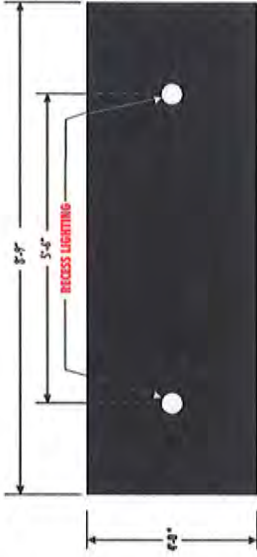






# ILLUMINATED METAL CANOPY

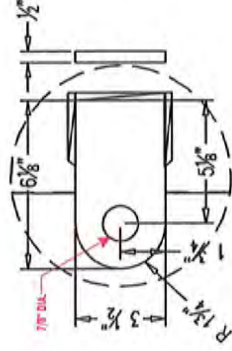
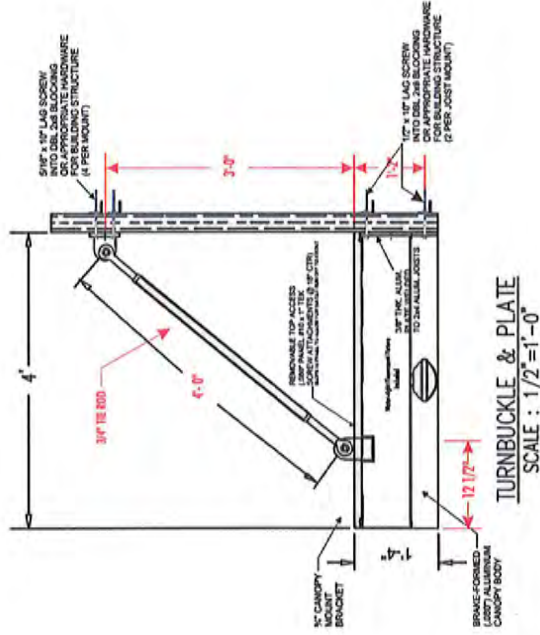
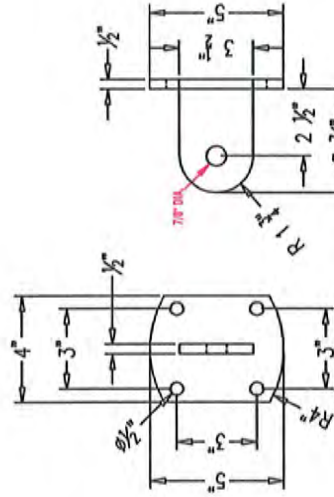
ORDER PART # CKE48-12-105



FRONT VIEW

BOTTOM VIEW

SIDE VIEW



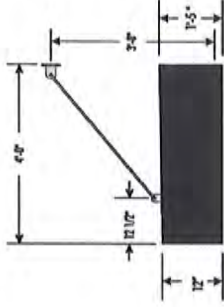
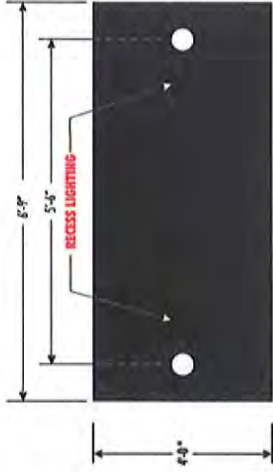
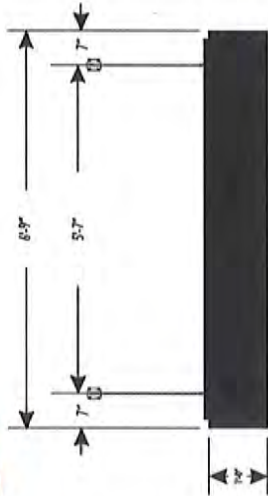
Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	10





# ILLUMINATED METAL CANOPY

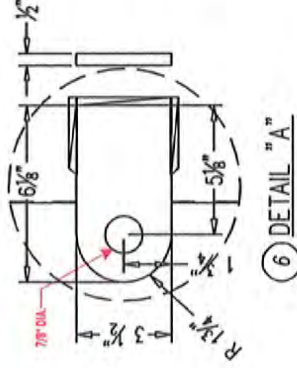
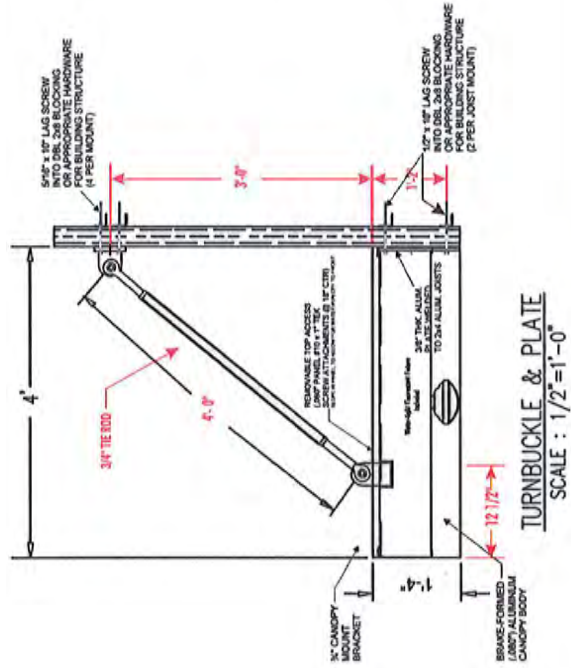
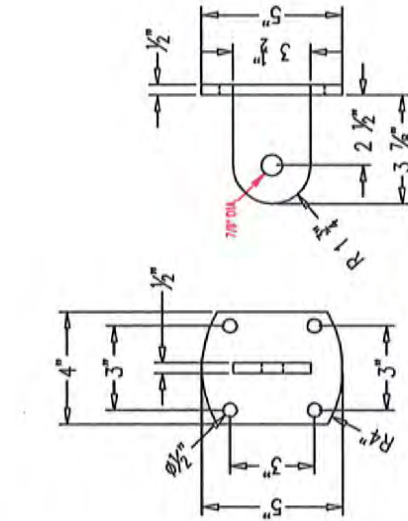
ORDER PART # CKE48-12-81



FRONT VIEW

BOTTOM VIEW

SIDE VIEW



Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D



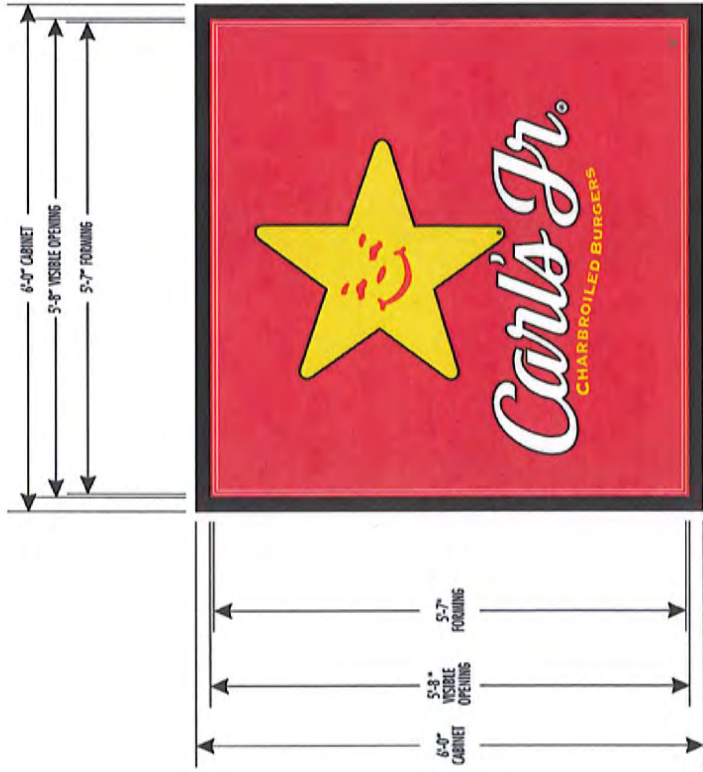






# ILLUMINATED PYLON SIGN

ORDER PART # CKEZ0042



**SignResource**  
IDENTITY GROUP  
6135 Dorrance Blvd., Hayward, CA 90270  
800.433.4283 • Fax 925.560.7143  
Website: www.signresource.com

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Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D



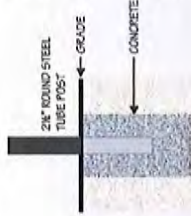




# ILLUMINATED DIRECTIONAL SIGNS

## Specifications:

- CABINETS:** FABRICATED ALUMINUM 9" DEEP, WITH 1/2" RETAINERS AND RETURNS PAINTED BLACK.
- FACES:** WHITE FLEXILEXAN WITH FIRST SURFACE APPLIED CUTTER RESISTANT TRANSLUCENT VINYL GRAPHICS AS ILLUSTRATED.
- POST:** 2 1/2" ROUND STEEL TUBE POST PAINTED BLACK.
- ILLUMINATION:** INTERNAL T12 OHMO FLOURESCENT LAMPS.
- SWITCH:** TAMPER RESISTANT SWITCH: UL APPROVED 15A-120V/77A LEVITON #10KEL WITH KEYED WATERPROOF COPEX.



6 SF

ORDER PART # CKEZ203D00-001



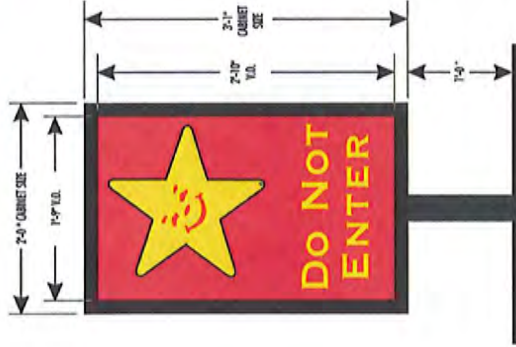
FRONT VIEW



OPPOSITE VIEW



SIDE VIEW



FRONT VIEW



OPPOSITE VIEW

6 SF

ORDER PART # CKEZ203D00-002

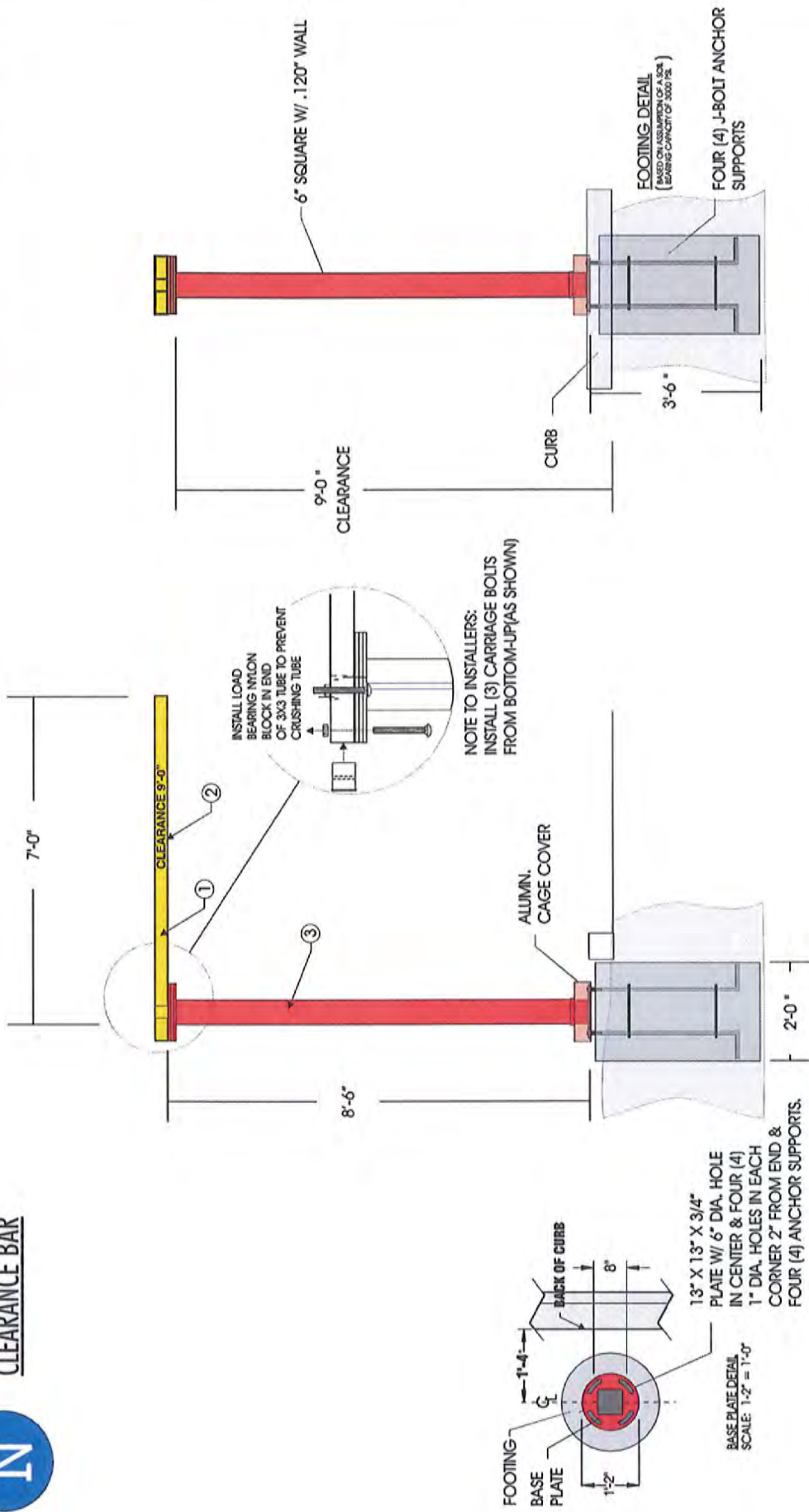
Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D





# CLEARANCE BAR

ORDER PART # CKECLRBAR



**SIDE VIEW**

**FRONT VIEW**

**SignResource**  
 IDENTITY GROUP  
 6135 Osbeck Blvd., Hayward, CA 94578  
 510-433-4283 • Fax 510-560-7143  
 Website: www.signresource.com

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Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	ID



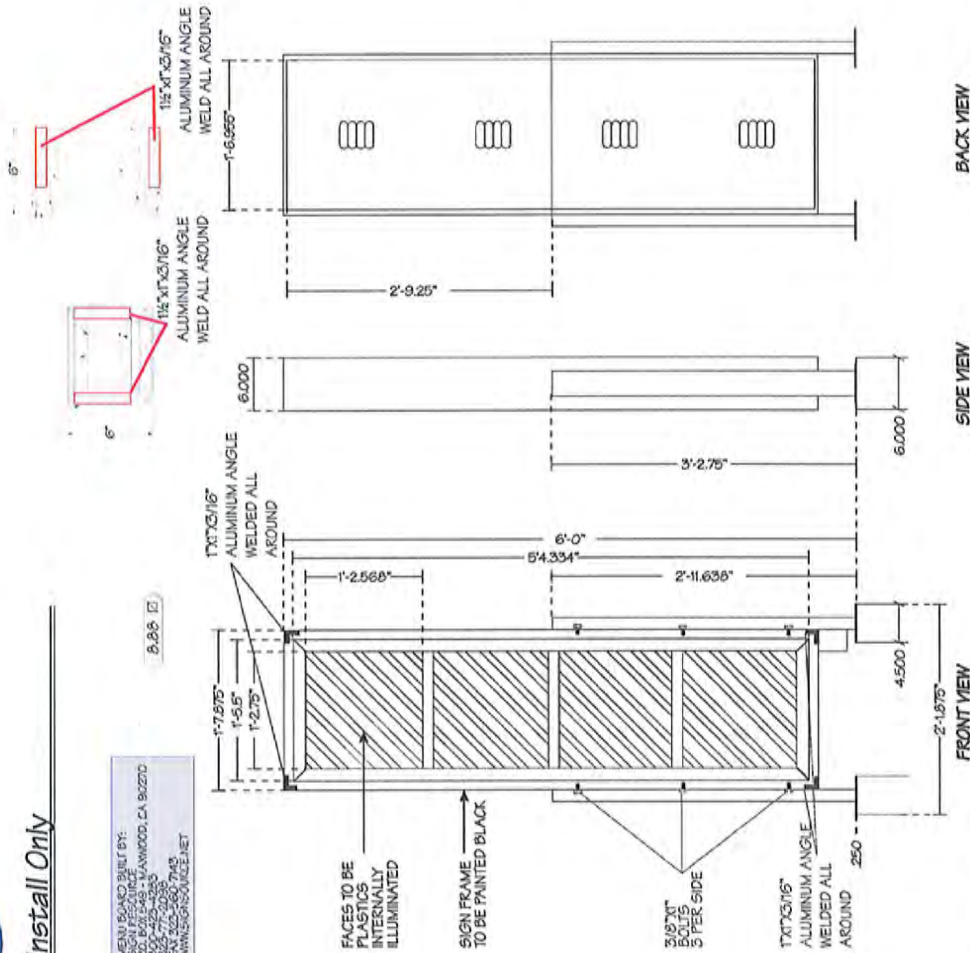
# 1

## DRIVE-THRU PREVIEW BOARD

Install Only

AGENT BEASCO BUILT BY:  
30 BAY LANE • WAINWOOD, CA 91270  
909-772-0333  
909-772-0335  
FAX 909-260-7443  
WWW.SIGNRESOURCE.NET

8.8.8.12



## PREVIEW MENU BOARD

SCALE: NTS

**SignResource**  
IDENTITY GROUP

6135 Plumas Blvd., Merced, CA 95370  
TEL 443-4283 • FAX 933-560-7143  
Website: www.signresource.com

CARLIS JR - WILSONVILLE, OR.

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D

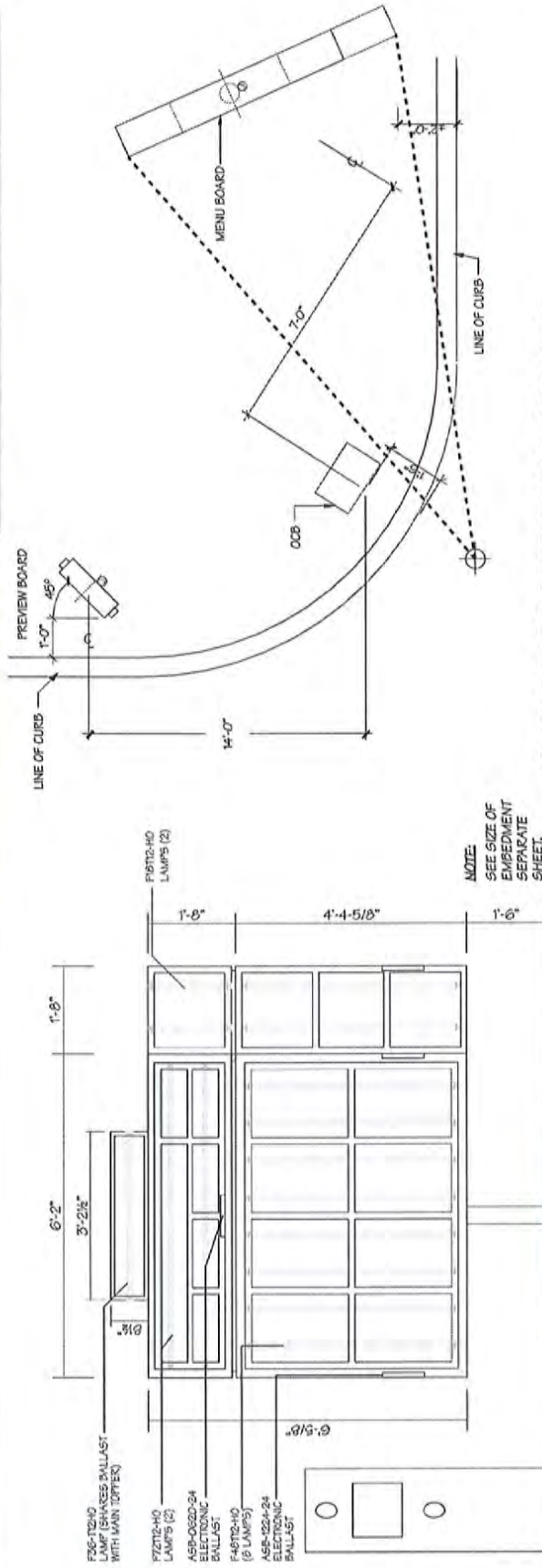




## 2 DRIVE-THRU MENU BOARD

Install Only

NOTE: ALL WORK TO COMPLY WITH 2007 CALIFORNIA ELECTRICAL CODE (CEC)



ELEVATION

See Engineering for details

### MENU BOARD LAYOUT AT DRIVE THRU

SCALE: NTS

RAISE TOP OF ALL CONCRETE FOOTINGS BASE PLATES TO 1 INCH ABOVE ADJACENT CURBS. COUPLE BOLT EXTENSIONS TO EXISTING ANCHOR BOLTS TO ACCOMMODATE RAISED BASE PLATE MOUNTING HEIGHT. FORM AND TROWEL FINISH CONCRETE FOOTING EXTENSION. APPLIES TO DRIVE-THRU MENU BOARD, OCB, DIRECTIONAL SIGNS, AND DRIVE-THROUGH CLEARANCE SIGN. TOP OF (NEW) PYLON CONCRETE BASE SHOULD BE FORMED, TROWEL FINISHED TO ABOVE ADJACENT CURB.

### MENU BOARD SPECIFICATIONS

SINGLE FACED ILLUMINATED DRIVE-THRU MENU BOARD CABINET IS OF EXTRUDED ALUMINUM CONSTRUCTION WASHED AND PRIMED WITH ETCHING PRIMER AND PAINTED BLACK. DOORS ARE TEMPERED GLASS PANELS WITH EXTRUDED ALUMINUM FRAMES MOUNTED ON PIANO HINGES. ALL DOORS TO LOCK WITH A UNIVERSAL KEY. COPY PANELS ARE WHITE 1/8" ACRYLIC WITH PLASTIC "H" TRACK FOR 20 SLATS. TRANSLITE PANELS ARE WHITE 1/8" ACRYLIC WITH 1/8" CLEAR OVERLAY.

### ELECTRICAL SPECIFICATIONS

ONE (1) 20 AMP BREAKER. TOTAL OF 7 AMPS IN SIGN, 120 VOLT.

### EXTERIOR MENU BOARD

SCALE: NTS

SignResource  
IDENTITY GROUP

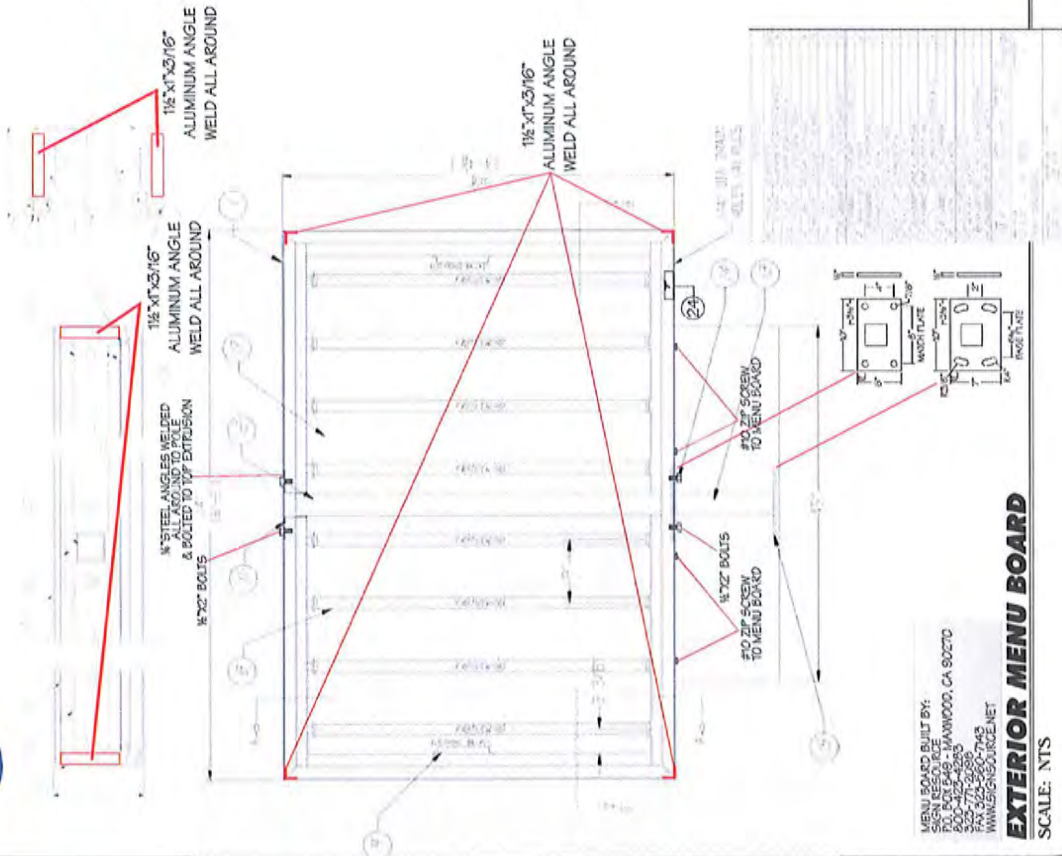
611E Disney Blvd., Newport, CA 95070  
800.433.4283 • Fax 925.560.7143  
Website: www.signresource.com

CARLS JR. - WILSONVILLE, OR.

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D

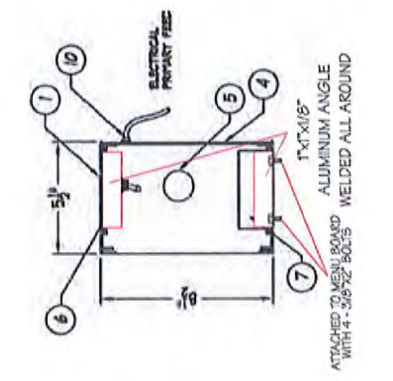
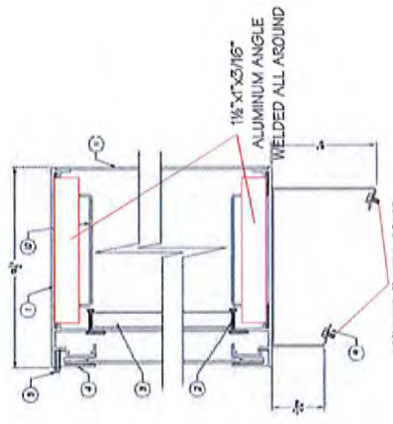
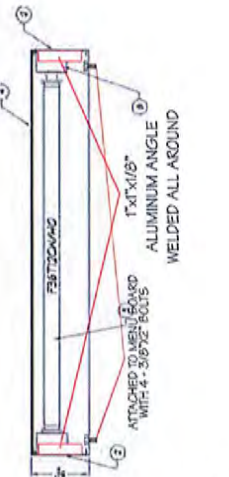
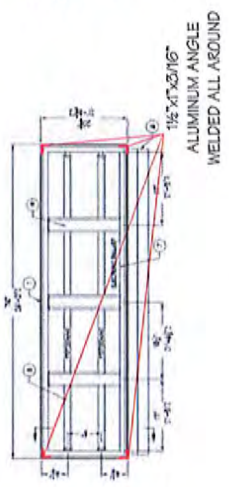
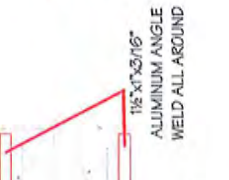


2



MENU BOARD BUILT BY:  
SIGN RESOURCE  
6135 District Blvd., Marywood, CA 90270  
Tel: 48283 • Fax: 3450643  
WWW.SIGNRESOURCE.NET

**EXTERIOR MENU BOARD**  
SCALE: NTS



PARTS LIST

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PARTS LIST

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PARTS LIST

1	1\"/>
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**Sign Resource**  
IDENTITY GROUP  
6135 District Blvd., Marywood, CA 90270  
Tel: 48283 • Fax: 3450643  
Website: www.signresource.com

**CARL'S JR. - WILSONVILLE, OR.**

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D

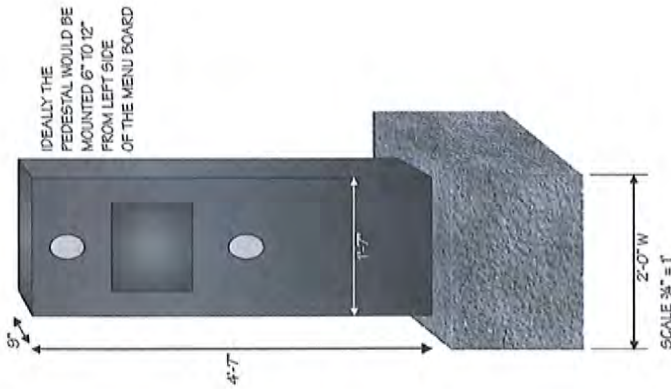




# 3

## DRIVE-THRU ORDER CONFIRMATION BOARD

Install Only



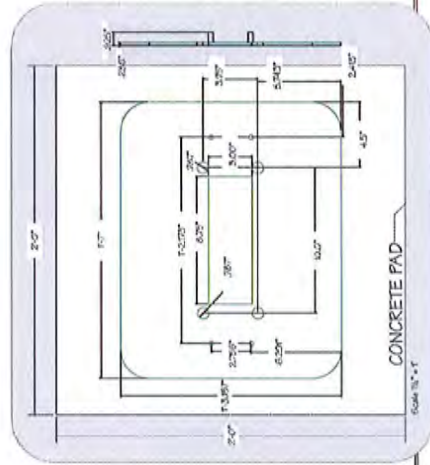
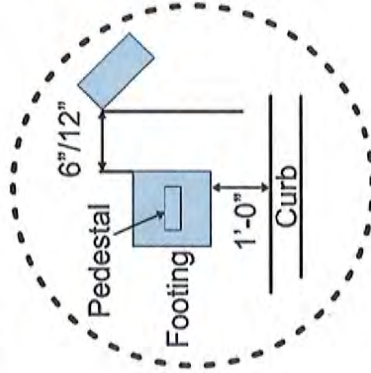
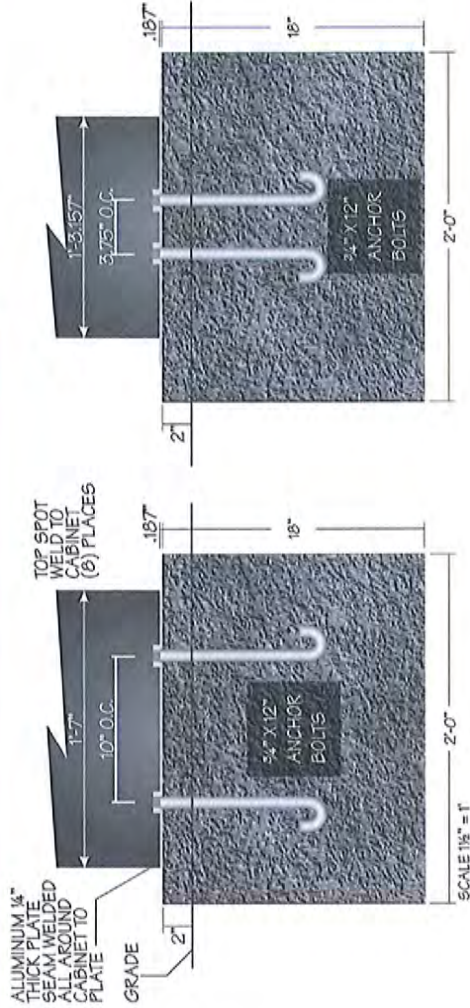
ORDER CONFIRMATION BOARD BUILT BY:  
 HyperActive Technologies  
 2500 Industrial Drive  
 Philadelphia, PA 19120  
 Phone: 412.522.5000 Fax: 412.522.5921

### ORDER CONFIRMATION BOARD

SCALE: NTS

ORDER CONFIRMATION BOARD FOR ILLUSTRATION PURPOSE ONLY TO BE INSTALLED BY OTHERS

SEE ENGINEERING FOR FOOTING DETAILS



NOTE: ALL WORK TO COMPLY WITH 2007 CALIFORNIA ELECTRICAL CODE (CEC)

**SignResource**  
 IDENTITY GROUP  
 6131 Phoenix Blvd., Newark, CA 94770  
 800.433.4283 • Fax 925.560.3742  
 Website: www.signresource.com

CARL'S JR. - WILSONVILLE, OR.

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D



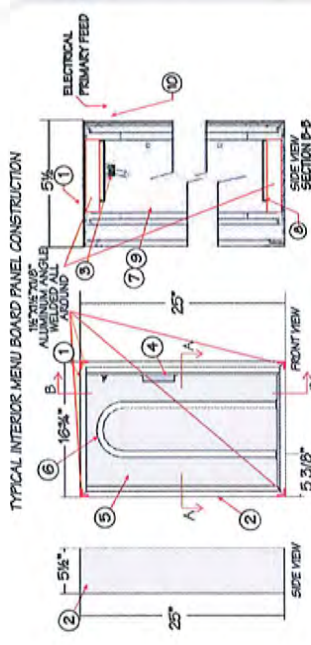
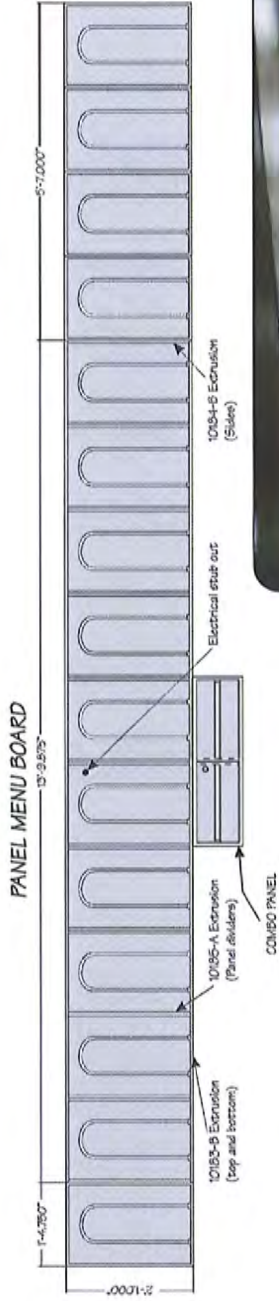


# 4 INTERIOR MENU-BOARD

*Install Only*

MENU BOARD BUILT BY:  
 SIGN RESOURCE  
 6135 DIXIE BLVD., MAYWOOD, CA 94720  
 925-433-4243  
 925-433-4243  
 FAX 925-560-7143  
 WWW.SIGNRESOURCE.NET

**ALL MANUFACTURING BY OTHERS  
 INSTALL ONLY**



PARTS LIST	
1	10183-B EXTRUSION (TOP AND BOTTOM)
2	10185-A EXTRUSION (PANEL DIVIDERS)
3	10084-B EXTRUSION (SLATS)
4	10183-B EXTRUSION (TOP AND BOTTOM)
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6	10084-B EXTRUSION (SLATS)
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188	10185-A EXTRUSION (PANEL DIVIDERS)
189	10084-B EXTRUSION (SLATS)
190	10183-B EXTRUSION (TOP AND BOTTOM)
191	10185-A EXTRUSION (PANEL DIVIDERS)
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295	10183-B EXTRUSION (TOP AND BOTTOM)
296	10185-A EXTRUSION (PANEL DIVIDERS)
297	10084-B EXTRUSION (SLATS)
298	10183-B EXTRUSION (TOP AND BOTTOM)
299	10185-A EXTRUSION (PANEL DIVIDERS)
300	10084-B EXTRUSION (SLATS)

**INTERIOR MENU BOARD**  
 SCALE: NTS

**Construction:**

- All aluminum extrusion construction, .0900 thick 6063-T5 alloy.
- All exposed surfaces of cabinets have a 5 step paint process including sanding, chemical degreasing,
- etching primer, polyurethane undercoat and polyurethane paint.
- All cabinets are manufactured to UL specifications and bear their label.

**Installation:**

- 14" Z (supplied) is mounted to wall. Brackets mounted to rear of cabinet; hang menu from Z bar.
- #14 Lag Bolts x3" Penetration every 16" O.C. (if dry wall & wood studs)
- 1/4" X3" Toggle Bolts every 16" O.C. (if tile wall).

**Electrical:**

- 120 volts, 60 Hz
- 2.37 amps
- Hardware electrical connection by GC
- Replacement lamps: F90 32 TB/741
- Replacement ballasts: Advance REL-4P32-5C Electronic

NOTE: ALL WORK TO COMPLY WITH 2007 CALIFORNIA ELECTRICAL CODE (CEC)

**SignResource**  
 IDENTITY GROUP  
 6135 Dixie Blvd., Maywood, CA 94720  
 925-433-4243 • Fax 925-560-7143  
 Website: www.signresource.com

**CARL'S JR. - WILSONVILLE, OR.**

Date Created	1/10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D

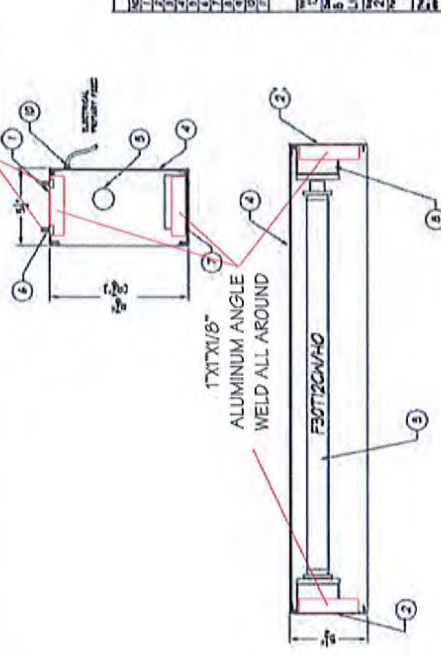
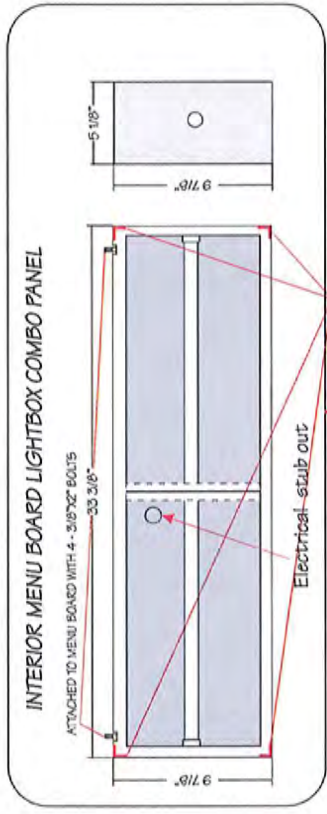


# 5

## INTERIOR MENU-BOARD LIGHT BOX COMBO PANEL

Install Only

**ALL MANUFACTURING BY OTHERS INSTALL ONLY**



PARTS LIST		QTY	DESCRIPTION	UNIT
1	1	1	1\"/>	



MENU BOARD BUILT BY:  
 SIGN RESOURCE  
 6131 Phoenix Blvd., Merced, CA 95270  
 800.433.4243 • Fax 520.560.3143  
 WWW.SIGNRESOURCE.NET

## INTERIOR MENU BOARD LIGHTBOX COMBO PANEL

SCALE: NTS

- Construction:**
- All aluminum extrusion construction, .0900 thick 6063-T5 alloy.
  - All exposed surfaces of cabinets have a 5 step paint process including sanding, chemical degreasing, etching primer, polyurethane undercoat and polyurethane paint.
  - All cabinets are manufactured to UL specifications and bear their label.
- Installation:**
- This cabinet bolts to the bottom of the Carl's 10/11 panel interior menu, Item #C00056080. Using 3/8" x 1/2" bolts into nuts pre-mounted at bottom of 10/11 menu board by manufacturer.
- Electrical:**
- 120 volts, 60 Hz
  - 40 amps
  - Hardware electrical connection by GC
  - Replacement lamps: F25T12/CIW/GO
  - Replacement ballasts: LC-25/040-H2 (1 each)
  - Replacement starter: F5-25
  - Provide separate circuit for all signs

NOTE: ALL WORK TO COMPLY WITH 2007 CALIFORNIA ELECTRICAL CODE (CEC)

**SignResource**  
 IDENTITY GROUP  
 6131 Phoenix Blvd., Merced, CA 95270  
 800.433.4243 • Fax 520.560.3143  
 Website: www.signresource.com

**CARL'S JR. - WILSONVILLE, OR.**

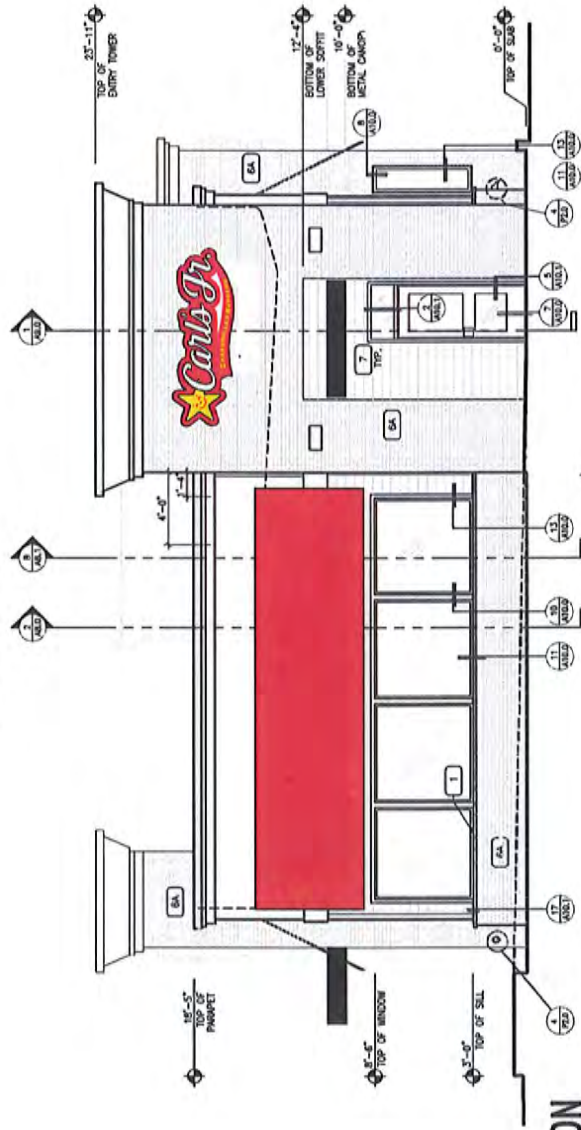
Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D



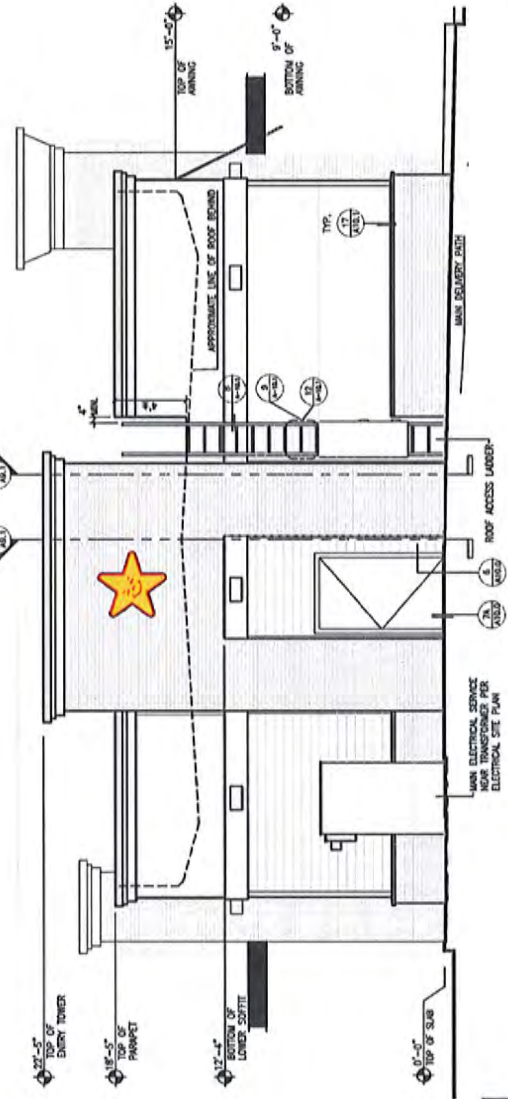




# ELEVATIONS



**NORTH ELEVATION**



**SOUTH ELEVATION**

**SignResource**  
 IDENTITY GROUP  
 6115 Dickses Blvd - Myrtlewood, CA 95070  
 925.433.4283 - Fax 925.560.7143  
 Website: www.signresource.com

**CARLS JR - WILSONVILLE, OR.**

Date Created	10/30/12
Address	SW 95TH AVENUE & COMMERCE CIRCLE, WILSONVILLE OR 97070
Revision	D









REVISION	HIST	DATE	BY	REASON
1		11/20/12	M.E.	INITIAL DOWNSHIP RELEASE
2		11/20/12	M.E.	REMOVED SITE OF HEIGHT TO 12' TORS

**DO NOT MANUFACTURE  
LAYOUT TO BE VERIFIED**

ITEM	DESCRIPTION
A	BACK
B	ALUMINUM
C	ALUMINUM
D	ALUMINUM
E	ALUMINUM
F	ALUMINUM
G	ALUMINUM
H	ALUMINUM
I	ALUMINUM
J	ALUMINUM
K	ALUMINUM
L	ALUMINUM
M	ALUMINUM
N	ALUMINUM
O	ALUMINUM
P	ALUMINUM
Q	ALUMINUM
R	ALUMINUM
S	ALUMINUM
T	ALUMINUM
U	ALUMINUM
V	ALUMINUM
W	ALUMINUM
X	ALUMINUM
Y	ALUMINUM
Z	ALUMINUM

FIRST SURFACE DECORATION

THIS SIGN IS INTENDED TO BE MOUNTED ON A WALL OR OTHER SURFACE. THE SIGN IS NOT TO BE MOUNTED ON A WALL OR OTHER SURFACE. THE SIGN IS NOT TO BE MOUNTED ON A WALL OR OTHER SURFACE. THE SIGN IS NOT TO BE MOUNTED ON A WALL OR OTHER SURFACE.

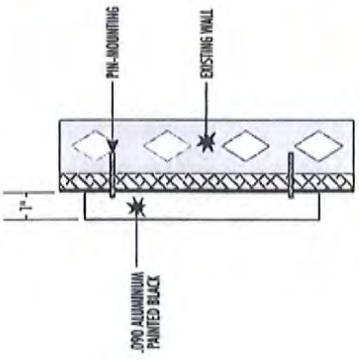
**GENERAL NOTES**

1. UNLESS OTHERWISE NOTED
2. FINISHES UNLESS NOTED
3. FINISHES UNLESS NOTED
4. FINISHES UNLESS NOTED
5. FINISHES UNLESS NOTED

**12" PIN MOUNTED LETTER SET**

DESIGNED BY: D. DANVISON  
DATE: 11/20/12  
PROJECT: BOONES FERRY POINTE

PROJECT NO: CCE00049  
SHEET NO: 1 OF 1



**TYPICAL SECTION DETAIL**  
NON ILLUMINATED CHANNEL LETTERS  
NOT TO SCALE



**FRONT VIEW**

**SIDE VIEW**









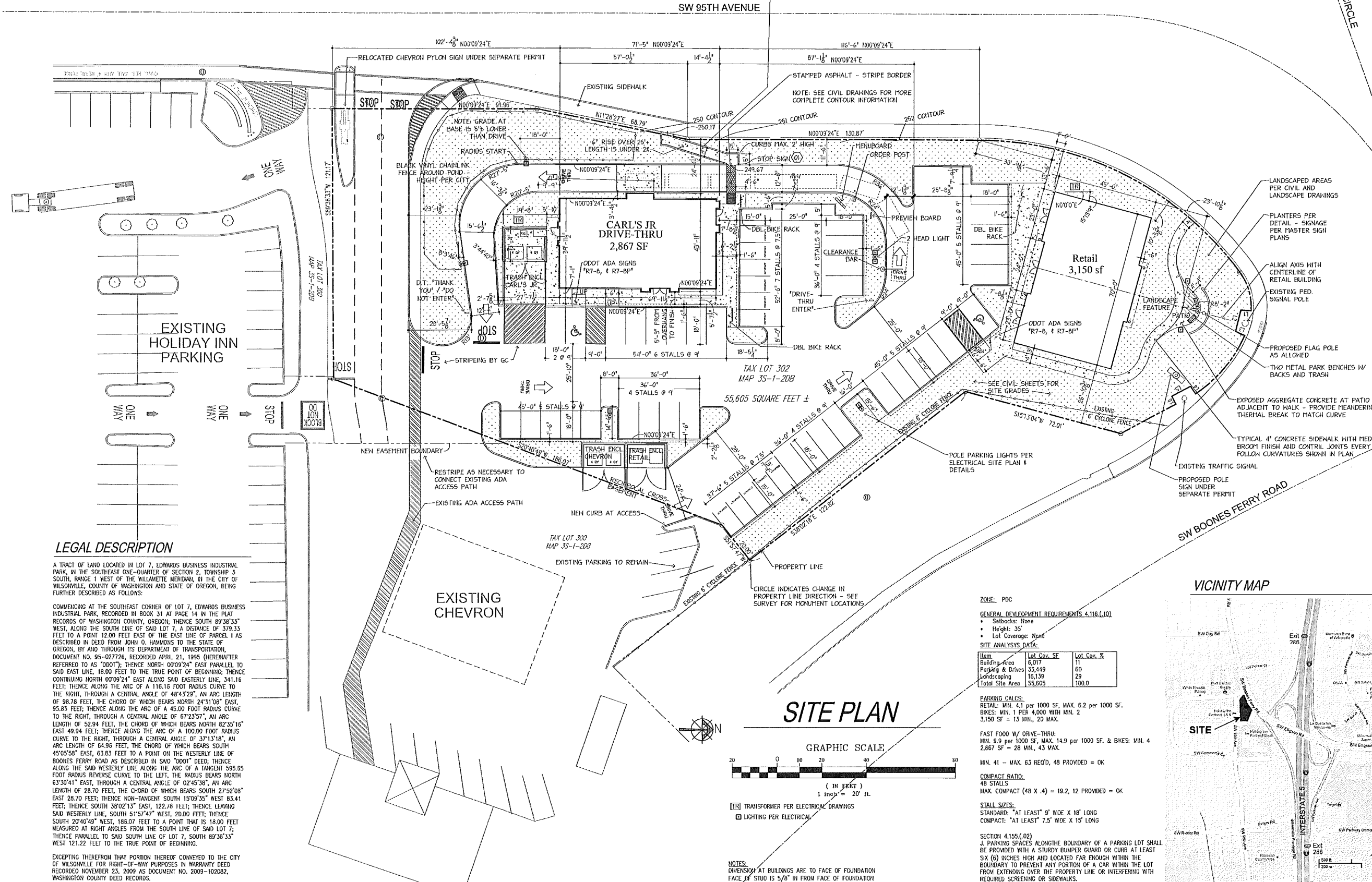




**SITE KEY NOTE:**

- (1) PEDESTRIAN CROSSING STOP SIGN TO BE CROSSING PANEL WITH FIXED BASE BY SAFETYSIGN.COM, CROSSING BLADE SIGN ITEM# Y496 / MUTCD# RI-8A

ADA PATH TO PUBLIC SIDEWALK ALONG EXISTING CONTOURS. MAX. 2% SLOPE ANY DIRECTION AND 5'x5' TURNING CLEARANCES AT ENDS - KEY SPOT ELEVATIONS SHOWN - REFER TO CIVIL FOR ADDITIONAL ELEV.



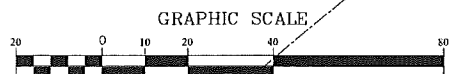
**LEGAL DESCRIPTION**

A TRACT OF LAND LOCATED IN LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, IN THE SOUTHEAST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, IN THE CITY OF WILSONVILLE, COUNTY OF WASHINGTON AND STATE OF OREGON, BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, RECORDED IN BOOK 31 AT PAGE 14 IN THE PLAT RECORDS OF WASHINGTON COUNTY, OREGON; THENCE SOUTH 89°38'33" WEST, ALONG THE SOUTH LINE OF SAID LOT 7, A DISTANCE OF 379.33 FEET TO A POINT 12.00 FEET EAST OF THE EAST LINE OF PARCEL 1 AS DESCRIBED IN DEED FROM JOHN O. HAMMONS TO THE STATE OF OREGON, BY AND THROUGH ITS DEPARTMENT OF TRANSPORTATION, DOCUMENT NO. 95-027726, RECORDED APRIL 21, 1995 (HEREINAFTER REFERRED TO AS "0001"); THENCE NORTH 00°09'24" EAST PARALLEL TO SAID EAST LINE, 18.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 00°09'24" EAST ALONG SAID EASTERLY LINE, 341.16 FEET; THENCE ALONG THE ARC OF A 116.16 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 48°43'29", AN ARC LENGTH OF 98.78 FEET, THE CHORD OF WHICH BEARS NORTH 24°31'08" EAST, 95.83 FEET; THENCE ALONG THE ARC OF A 45.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 67°23'57", AN ARC LENGTH OF 52.94 FEET, THE CHORD OF WHICH BEARS NORTH 82°35'16" EAST, 49.94 FEET; THENCE ALONG THE ARC OF A 100.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 37°13'18", AN ARC LENGTH OF 64.96 FEET, THE CHORD OF WHICH BEARS SOUTH 45°05'58" EAST, 63.83 FEET TO A POINT ON THE WESTERLY LINE OF BOONES FERRY ROAD AS DESCRIBED IN SAID "0001" DEED; THENCE ALONG THE SAID WESTERLY LINE ALONG THE ARC OF A TANGENT 595.55 FOOT RADIUS REVERSE CURVE TO THE LEFT, THE RADIUS BEARS NORTH 63°30'41" EAST, THROUGH A CENTRAL ANGLE OF 02°45'38", AN ARC LENGTH OF 28.70 FEET, THE CHORD OF WHICH BEARS SOUTH 27°52'08" EAST 28.70 FEET; THENCE NON-TANGENT SOUTH 15°09'35" WEST 63.41 FEET; THENCE SOUTH 35°02'13" EAST, 122.78 FEET; THENCE LEAVING SAID WESTERLY LINE, SOUTH 51°57'47" WEST, 20.00 FEET; THENCE SOUTH 27°40'42" WEST, 185.07 FEET TO A POINT THAT IS 18.00 FEET MEASURED AT RIGHT ANGLES FROM THE SOUTH LINE OF SAID LOT 7; THENCE PARALLEL TO SAID SOUTH LINE OF LOT 7, SOUTH 89°38'33" WEST 121.22 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION THEREOF CONVEYED TO THE CITY OF WILSONVILLE FOR RIGHT-OF-WAY PURPOSES IN WARRANTY DEED RECORDED NOVEMBER 23, 2009 AS DOCUMENT NO. 2009-102082, WASHINGTON COUNTY DEED RECORDS.

**SITE PLAN**



- (TR) TRANSFORMER PER ELECTRICAL DRAWINGS
- (L) LIGHTING PER ELECTRICAL

NOTES:  
DIMENSION AT BUILDINGS ARE TO FACE OF FOUNDATION  
FACE OF STUO IS 5/8" IN FROM FACE OF FOUNDATION

ZONE: PDC  
GENERAL DEVELOPMENT REQUIREMENTS 4.116 (1.0)

- Setbacks: None
- Height: 35'
- Lot Coverage: None

SITE ANALYSIS DATA:

Item	Lot Cov. SF	Lot Cov. %
Building Area	6,017	11
Parking & Drives	33,449	60
Landscaping	16,139	29
Total Site Area	55,605	100.0

PARKING CALCS:  
RETAIL: MIN. 4.1 per 1000 SF, MAX. 6.2 per 1000 SF.  
BIKES: MIN. 1 PER 4,000 WITH MIN. 2  
3,150 SF = 13 MIN., 20 MAX.

FAST FOOD W/ DRIVE-THRU:  
MIN. 9.9 per 1000 SF, MAX. 14.9 per 1000 SF. & BIKES: MIN. 4  
2,867 SF = 28 MIN., 43 MAX.

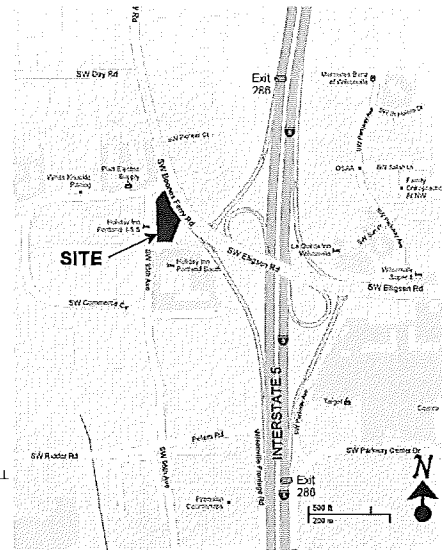
MIN. 41 - MAX. 63 REQ'D, 48 PROVIDED = OK

COMPACT RATIO:  
48 STALLS  
MAX. COMPACT (48 X 4) = 19.2, 12 PROVIDED = OK

STALL SIZES:  
STANDARD: "AT LEAST" 9' WIDE X 18' LONG  
COMPACT: "AT LEAST" 7.5' WIDE X 15' LONG

SECTION 4.155.(02)  
J. PARKING SPACES ALONG THE BOUNDARY OF A PARKING LOT SHALL BE PROVIDED WITH A STURDY BUMPER GUARD OR CURB AT LEAST SIX (6) INCHES HIGH AND LOCATED FAR ENOUGH WITHIN THE BOUNDARY TO PREVENT ANY PORTION OF A CAR WITHIN THE LOT FROM EXTENDING OVER THE PROPERTY LINE OR INTERFERING WITH REQUIRED SCREENING OR SIDEWALKS.

**VICINITY MAP**



**CB ANDERSON ARCHITECTS LLC**

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-782-2911  
Fax 782-5624

CLIENT:  
**Wilsonville Devco LLC**  
c/o Integrated Investment Partners  
90 Box 916  
Portland, OR 97207  
(503) 688-9400

PROJECT:  
**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE:  
**ARCHITECTURAL SITE PLAN**

REVISIONS

NO.	DESCRIPTION
1	
2	
3	
4	
5	



DATE: Jan-2-2013  
BLDG. SUBMITTAL DATE: 1  
DATE: 2  
DATE: 3  
BID DATE:  
CKE NO.:  
ARCH. NO.: 1199  
SCALE: AS SHOWN  
SHEET

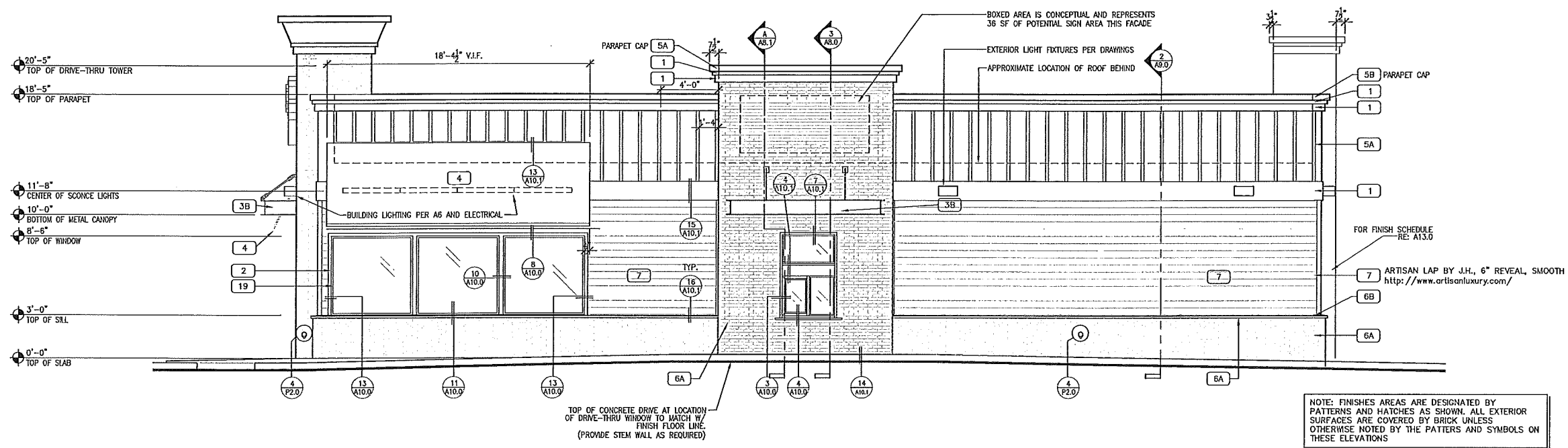
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KGK Foods Conversion Based on CKE Prototype Version 22-14

FILE NAME:

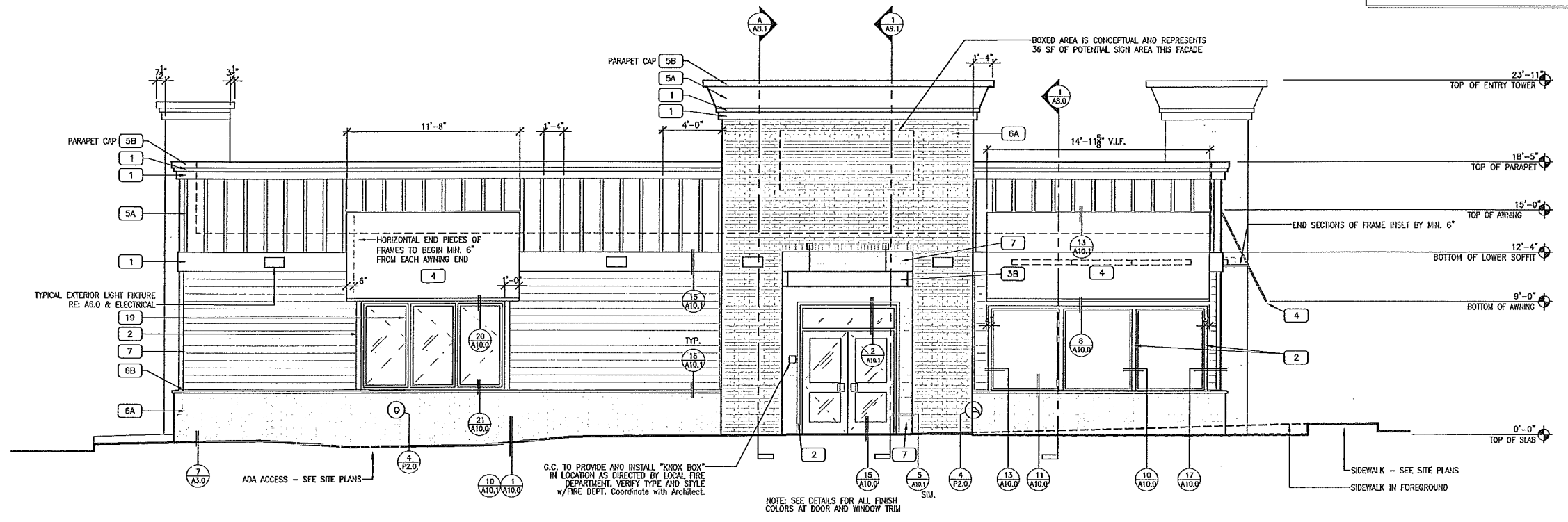






WEST ELEVATION

SCALE: 1/4"=1'-0" 1



EAST ELEVATION

SCALE: 1/4"=1'-0" 2

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**CB ANDERSON ARCHITECTS RLC**  
 7209 Greenwood Avenue N  
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 206-728-2911  
 Fax 782-5624

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**Wilsonville Devco LLC**  
 c/o Integrated Investment Partners  
 PO Box 916  
 Portland, OR 97207  
 (503) 688-5620

PROJECT  
**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE  
**EXTERIOR ELEVATIONS**

REVISIONS  
 Dec-28-2012

STAMP  
**REGISTERED ARCHITECT**  
 CRAIG B. ANDERSON  
 No. B-140  
 SEATTLE, WA  
 Exp 6-30-13  
 STATE OF OREGON

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER RELEVANT REGULATORY AGENCIES.  
 DATE: Dec-5-2012  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③  
 BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE: AS SHOWN  
 SHEET

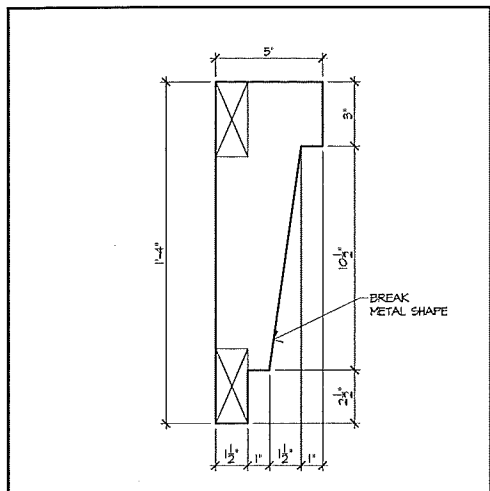
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FILE NAME

KGK Foods CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

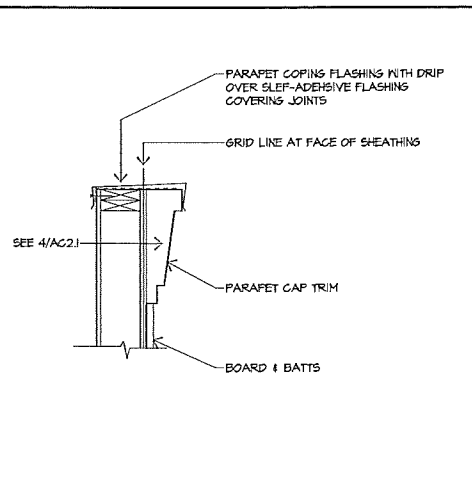






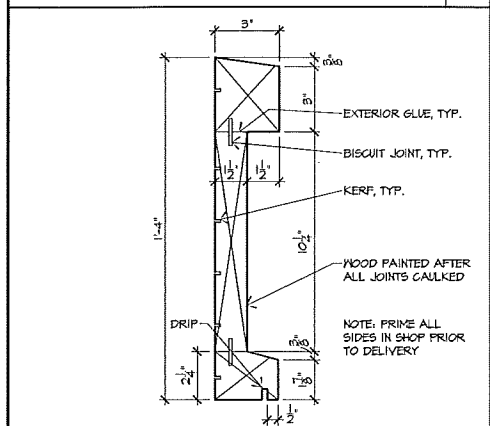
UPPER TRIM BAND

4



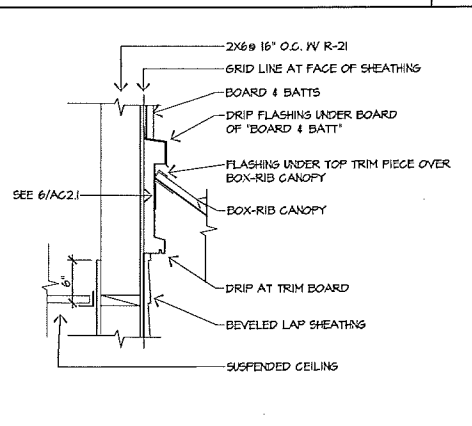
TOP OF BOX-RIB CANOPY

3



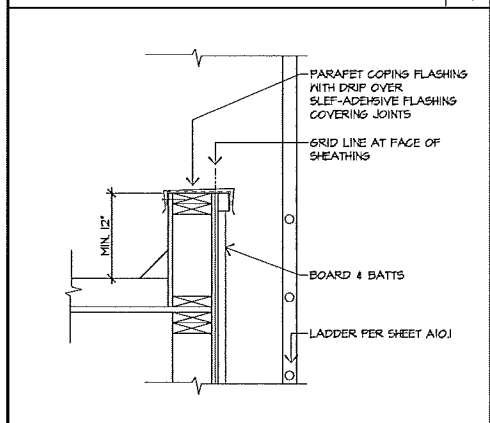
UPPER TRIM BAND

6



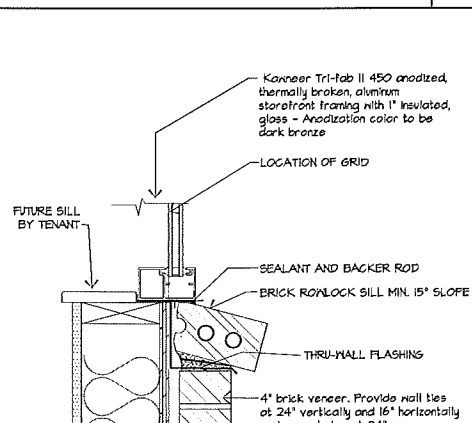
TOP OF BOX-RIB CANOPY

5



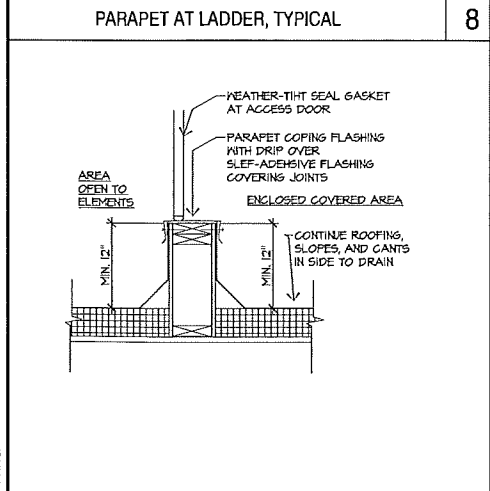
PARAPET AT LADDER, TYPICAL

8



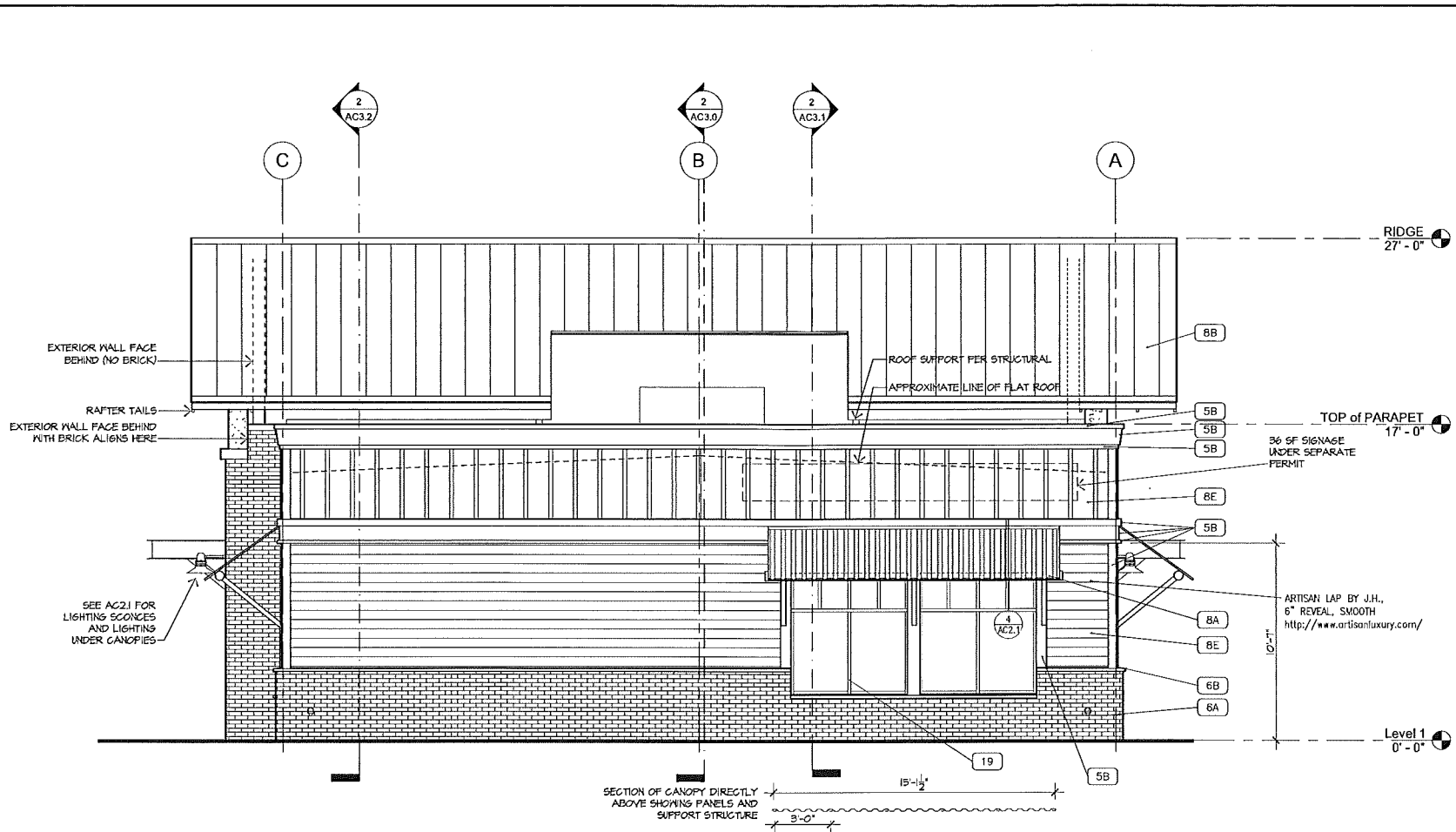
SILL AND BRICK WAINSCOT

7



CURB AT ROOF DOOR

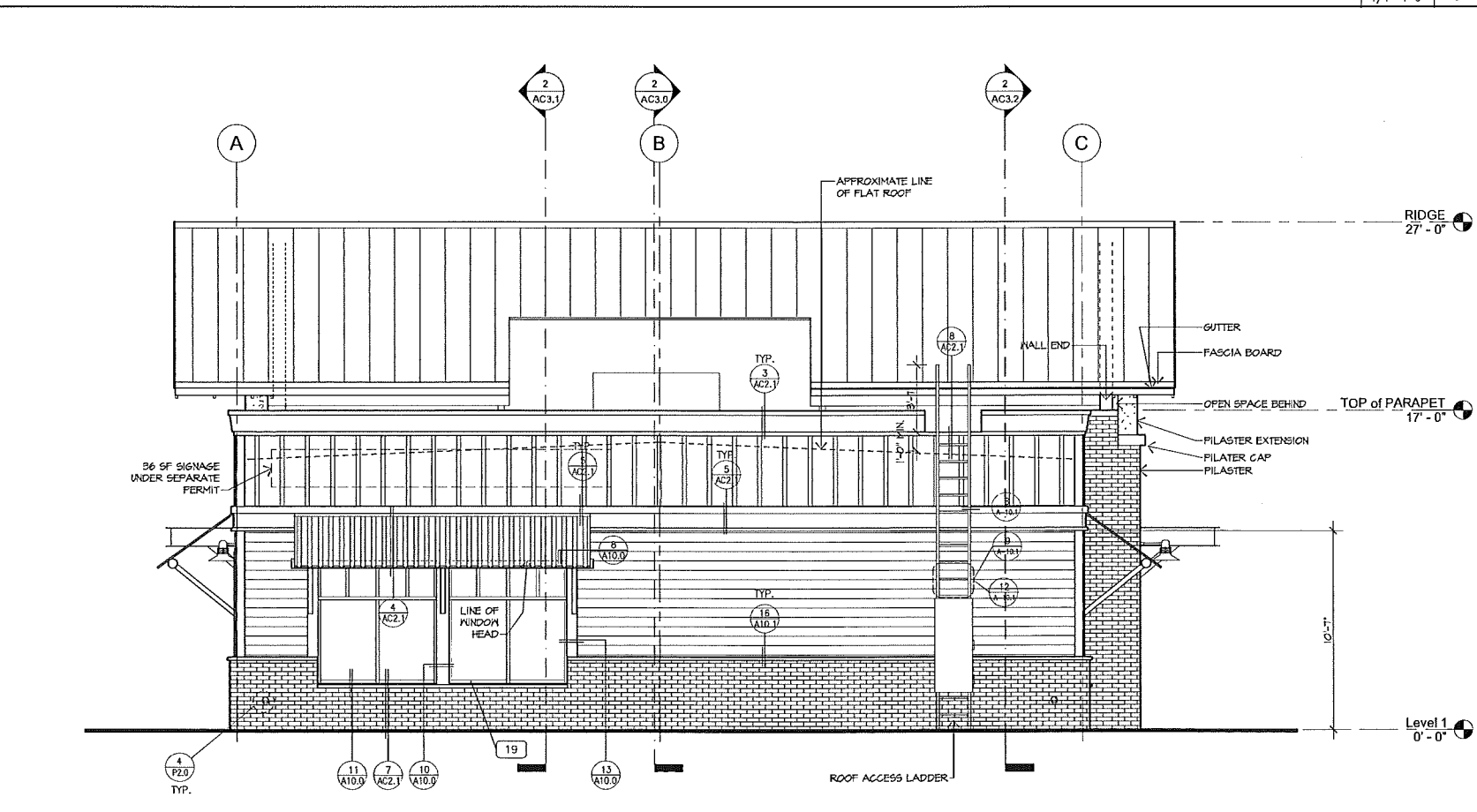
9



RETAIL BUILDING: WEST ELEVATION

SCALE: 1/4"=1'-0"

1



RETAIL BUILDING: EAST ELEVATION

SCALE: 1/4"=1'-0"

2

FILE NAME:

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CB ANDERSON ARCHITECTS PLLC

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Seattle, Washington 98103  
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Fax 782-5624

CLIENT

Wilsonville Devco LLC  
c/o Integrated Investment Partners  
10000 SW 9th Ave  
Portland, OR 97207  
(503) 488-9600

PROJECT

Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE

RETAIL BUILDING:  
ELEVATIONS  
(EAST & WEST)

REVISIONS


STAMP

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REGULATORY AGENCY

DATE: NOV-18-2012

BLDG. SUBMITTAL: ①

DATE: ②

③

BID DATE:

CKE NO.:

ARCH. NO.: 1159

SCALE:

SHEET

AC2.1



NO.	SIZE		TYPE	GLASS	FRAME	DETAILS				REMARKS
	APPROX. WIDTH	APPROX. HEIGHT				HEAD	JAMB	JAMB	SILL	
A	4'-0"	5'-6"	FIXED	1" INSULATED	ALUM.	8 A10.0	10 A10.0	13 A10.0	11 A10.0	* DUAL GLAZING
B	5'-3"	5'-6"	FIXED	1" INSULATED	ALUM.	8 A10.0	10 A10.0	13 A10.0	11 A10.0	* DUAL GLAZING
C	5'-6"	5'-6"	FIXED	1" INSULATED	ALUM.	8 A10.0	10 A10.0	13 A10.0	11 A10.0	* DUAL GLAZING
D	1'-6"	5'-6"	FIXED	FLOAT	ALUM.	8 A10.0	10 A10.0	13 A10.0	11 A10.0	* DUAL GLAZING
G	4'-0"	2'-4"	FIXED	1" TEMPERED	ALUM.	10 A10.0	10 A10.0	10 A10.0	2 A10.0	* DUAL GLAZING
H	2'-0"	2'-6"	SLDR	1" INSULATED	ALUM.	2 A10.0	3 A10.0	3 A10.0	4 A10.0	PROVIDE MANUAL SLIDING WINDOW SEE SPECS. REFER TO MECH. SHEETS FOR FLY FAN ABOVE (WINDY OPENING: 18"x24"=432 SQ.IN)
J	3'-4"	1'-8"	FIXED	1" INSULATED	ALUM.	8 A10.0	13 A10.0	11 A10.0		* DUAL GLAZING
K	6'-6"	1'-8"	FIXED	1" INSULATED	ALUM.	8 A10.0	13 A10.0	11 A10.0		* DUAL GLAZING
L	8'-0"	1'-2"	FIXED	1" TEMPERED	ALUM.	21 A10.0	21 A10.0	23 A10.0	21 A10.0	* DUAL GLAZING

NOTE: SIZES ARE APPROXIMATE. G.C. TO VERIFY w/FLOOR PLAN, ELEVATIONS, & FIELD MEASUREMENTS PRIOR TO FABRICATION.

WINDOW SCHEDULE

NO.	SIZE			DOOR TYPE	MATERIALS				DETAILS				REMARKS	
	WIDTH	HEIGHT	THICK.		FACE	CORE	GLASS	FINISH	FRAME	HEAD	JAMB	JAMB		LOUVER
1	3'-0"	7'-0"	1 3/4"	A	FULL GLASS	ALUM.	1/4"TEMP DUAL GLAZE	19	ALUM.	2	6		SEE SPEC.	* PANIC HARDWARE ON ALL PUBLIC EXIT DOORS INCLUDING TO PLAY AREA AND OUT. * NO LOCK OR LATCH ON PLAY ROOM EXIT GATE EXCEPT PANIC * BALANCE OF HARDWARE BY SUPPLIER * SIGN ABOVE DOOR TO READ: THIS DOOR TO REMAIN UNLOCKED WHEN BUILDING IS OCCUPIED. The sign shall be in letters 1" high on a contrasting background (2010 IRC 1008.1.9.3 sec. 2.2)
2	N/A													
3	N/A													
4	4'-0"	7'-0"	1 3/4"	B	FLUSH METAL	H.M.		8	HM	8	6	6	SEE SPEC.	PEEP HOLE, UNDERCUT 1"
5	N/A													
7M	3'-0"	6'-8"	1 3/4"	C	L/P	S.C. WOOD		25	HM	9	16	16	SEE SPEC.	SELF CLOSING, KICK PLATE, PUSH/PULL PLATE, UNDERCUT 1"
7W	3'-0"	6'-8"	1 3/4"	C	L/P	S.C. WOOD		25	HM	9	16	16	SEE SPEC.	SELF CLOSING, KICK PLATE, PUSH/PULL PLATE, UNDERCUT 1"
8	2'-3"	6'-8"	1 3/4"											
9	3'-0"	6'-8"	1 3/4"	D	L/P	S.C. WOOD		25	HM	9	16	16	SEE SPEC.	PEEP HOLE, UNDERCUT 1"

DOOR SCHEDULE

DOOR #1	6070 ALUMINUM EXTERIOR FROM DINING	DOOR #2	2068 P-LAM X HM LHR MEN FROM STORAGE
1EA	CYLINDER 20-05720-081 C123 VERIFY TYPE & FINISH WITH DR SUPPLIER BALANCE OF HARDWARE BY DR SUPPLIER	SCHLAGE	3EA BUTTS F179 4.5 X 4.5 652 STANLEY 1EA LOCKSET HD03PD RHO 626 SCHLAGE 1EA WALLSTOP W5497CCV 630 IVES 3EA SILENCERS SR84 GRAY GLYNN-JOHNSON
DOOR #4	4070 HM X HM LHR EXTERIOR FROM KITCHEN	DOOR #3	3068 P-LAM X HM LHR SCULLERY FROM OFFICE
1EA	CONTINUOUS HINGE CFH1 83HD ALUM PEMKO	3EA	BUTTS F179 4.5 X 4.5 652 STANLEY
1EA	ALARM MONITOR 4000 SEC. PROP. VON DUPRIN	1EA	LOCKSET HD03PD RHO 626 SCHLAGE
1EA	VIEWER (DOOR SCOPE) BLACK DU SSUNG	1EA	WALLSTOP W5497CCV 630 IVES
1EA	CLOSER 4111 SCUSH DEL RH ALUM LCN	3EA	SILENCERS SR84 GRAY GLYNN-JOHNSON
1EA	KICKPLATE KA050-124 X 45 630 TRINCO		
1EA	FLOORSTOP 1233 ALUM 626 TRIMCO		
1EA	THRESHOLD 299A 48" ALUM PEMKO		
1EA	SYEPEP 31SCH 48" ALUM PEMKO		
1SET	WEATHERSTRIP 303AV 48x4 ALUM PEMKO		
3EA	SILENCERS SR84 GRAY GLYNN-JOHNSON		
DOOR #7	3068 P-LAM X HM RH DINING TO WOMENS		
3EA	BUTTS F88179 4.5 X 4.5 652 STANLEY		
1EA	PUSH PLATE 1001-3.4 X 16 630 TRINCO		
1EA	PULL PLATE 1017-3.4 X 16 630 TRINCO		
1EA	CLOSER 1681DA RAMTG ALUM LCN		
1EA	KICKPLATE KA050 10 X 34 630 TRINCO		
1EA	WALLSTOP W5497CCV 630 IVES		
3EA	SILENCERS SR84 GRAY GLYNN-JOHNSON		
DOOR #7M	3068 P-LAM X HM LH DINING TO MEN		
3EA	BUTTS F88179 4.5 X 4.5 652 STANLEY		
1EA	PUSH PLATE 1001-3.4 X 16 630 TRINCO		
1EA	PULL PLATE 1017-3.4 X 16 630 TRINCO		
1EA	CLOSER 1681DA RAMTG ALUM LCN		
1EA	KICKPLATE KA050 10 X 34 630 TRINCO		
1EA	WALLSTOP W5497CCV 630 IVES		
3EA	SILENCERS SR84 GRAY GLYNN-JOHNSON		

DOOR HARDWARE SCHEDULE

A	ALUMINUM DOOR w/ INSULATION TEMP. GLASS SEE SPECS.	B	FLUSH HOLLOW METAL DOOR. SEE SPECS.	C	1 3/4" SOLID CORE WOOD DOOR w/ L/P EACH SIDE.	D	1 3/4" SOLID CORE WOOD DOOR w/ L/P EACH SIDE.
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DOOR TYPES

NO.	ROOM	FLOOR FINISH	BASE	*WALLS				*CEILING				REMARKS	
				MATERIALS	FINISH	MATERIAL	HEIGHT	FINISH					
101	ORDER												
102	DINING ROOM												
103	SERVING												
104	VESTIBULE												
105	MENS												
106	WOMENS												
107	CONDIMENT												
108	MANAGER OFFICE												
109	NOT USED												
110	LOCKERS/ EMPLOYEE												
111	SCULLERY												
112	DRIVE THROUGH												
113	KITCHEN												
114	DRY STORAGE												
115	COOLER												
116	FREEZER												

FINISH SCHEDULE

NO.	MATERIAL	MFG./COLOR	LUSTER/TYPE	NO.	MATERIAL	MFG./COLOR	LUSTER/TYPE
1	EXTERIOR: PAINT	SHERWIN WILLIAMS SW 6866 HEARTHROB	TRIM BANDS / SATIN	21	5/8" GYPSUM WALLBOARD	SEE SPECIFICATION RE: TO INT. ELEVATIONS FOR PAINT	LIGHT FOG SPRAY
2	EXTERIOR: PAINT	SHERWIN WILLIAMS SW 7020 BLACK FOX	WINDOW AND DOOR TRIM	22	BRICK VENEER-INTERIOR	CORONADO STONE PRODUCTS 2x8 RUSTIC USED BRICK - RUSTIC BLEND	LOGO ART TO BE APPLIED TO BRICK CORNER PIECES AS NEEDED
3	PAINT (INTERIOR)	SHERWIN WILLIAMS SW 6321 RED BAY		23	PLASTIC LAMINATE	FORMICA -- 3505-5B STORM SOLIDZ	
3B	EXTERIOR: PAINT	DURA-TECH 5000 MATTE BLACK	C-CHANNEL CANOPY	24	PLASTIC LAMINATE	FORMICA-FLINT CRYSTAL 3518-5B	MATTE FINISH
4	EXTERIOR: FABRIC AWNING	JOHN BOYLE & CO. INC. SIGNMASTER SUPREME	#627 SCARLET SUPREME	25	WAINSCOTING	ARBORITE-W 411-VL COPPER WOOD-BOIS OUVRE OR MARLITE-"COPPERWOOD" # PHL 172	USE MARLITE ALUM. TRIM- TOP, BOTTOM, JOINTS, & CORNERS. #M451, M460, M465, M470- MILL FINISH
5A	EXTERIOR: PAINT	SHERWIN WILLIAMS SW 6107 NOMADIC DESERT	BOARD AND BATT AND SLOPED PART OF TOWER TRIM	26	QUARRY TILE (KITCHEN) w/ 6" COVE BASE (3/8" RADIUS)	DAL OR EMSEY-RED FLASH (6"x6"x1/2") SEE SPECS.	GROUT - "LATCRETE" #D4 SABLE (III) USE ABRASIVE ON ALL TRAFFIC AREAS
5B	EXTERIOR: PAINT	SHERWIN WILLIAMS SW 6105 DIVINE WHITE	PARAPET CAP TRIM	27	PLASTIC LAMINATE	FORMICA STELLAR #689-5B (MATTE FINISH)	
6A	EXTERIOR: MASONRY VENEER	MUTUAL MATERIALS, "MUTUAL USED"	FULL BRICK VENEER	28	PAINT (INTERIOR)	SHERWIN WILLIAMS WEATHERED TEAK	
6B	EXTERIOR: MASONRY SILL	MUTUAL MATERIALS, "MUTUAL USED" STANDARD ROWLOCK SILL	FULL BRICK VENEER	29	METAL LAMINATE	CHEMETAL "CIRCLES" #406	
7	EXTERIOR: PAINT	SHERWIN WILLIAMS SW 6108 LATTE ON CERTAINTED ARTISAN SIDING	SATIN, BACK DOOR / ANY METAL TO BE SEMI-GL.	30	WALL TILE (R.R. WALL)	DAL TILE 6x8 GLOSS WHITE PLDZ POLARIS - RUNNING BOND PATTERN	GROUT- CUSTOM BDG PRODUCTS. Color "BLACK"
8	SEE AC2.0 FOR APPENDIX	OF RETAIL BUILDING FINISHES		30A	WALL TILE (R.R. WALL)	DAL TILE 2x6 GLOSS WHITE PLDZ POLARIS - RUNNING BOND PATTERN	2X6 BULLNOSE CAP TO MATCH R.R. WALL TILE.
9	PAINT (INTERIOR)	SHERWIN WILLIAMS P-1SW7038 TONY TAUPE EGGSHHELL FINISH	VESTIBULE ENTRY	31	DINING ROOM CHAIRS BY OWNER	MADISON GRAND SIARA #363499	"WHEAT"
10	PAINT (INTERIOR)	SHERWIN WILLIAMS, SW 6388 GOLDEN FLEECE	DINING ROOM WALLS	32	PLASTIC LAMINATE	FORMICA 909-58M "BLACK"	
11	PAINT (INTERIOR)	SHERWIN WILLIAMS, SW 6102 PORTABELLO	DINING ROOM TRIM AROUND WINDOWS	33A	DINING ROOM TILE (FIELD)	DAL Tile, AltaVista, Driftwood AV53 & Misty Rain AV52, 12x12, 50/50 mix	Dining Room Floor Grout - Custom Building Products #185 New Taupe
12	BATHROOM TILE (FIELD)	MISTY RAIN @ 90° AV52, 12x12	BATHROOM FLOOR GROUT, CUSTOM BUILDING PRODUCTS - #185 NEW TAUPE	33B	DINING ROOM TILE (BASE)	DAL Tile, Cove Base, S-36C9T, AV52, 6x12 Cove	
13	RUBBER BASE	BURKEMERCER 1/8" RUBBERMYTE WALL BASE	4" HIGH, Color "BLACK"	34	UPHOLSTERY BAR STOOL BY OWNER	EXPRESSIONS - BLACK EX277 ROQUE POPPY RUG8D	
14	GLASS BLOCK	PITTSBURGH CORNING GLASS BLOCK STANDARD PREMIER SERIES	DECORA 4 X 8 X 3-7/8	35	UPHOLSTERY BOOTHS	SEE K-7	
15	METAL LAMINATE	CHEMETAL NO. 406 "CIRCLES" BRUSHED ALUMINUM	(STAR AND TRIM)	36	UPHOLSTERY	SEE K-7	
16	PLASTIC LAMINATE	FORMICA # 839-58 STOP RED MATT FINISH		37	UPHOLSTERY	SEE K-7	
17	CEILING TILE	ARMSTRONG FINE FISSURED ADOBE	FRAME GRID SILVER SATIN 24" X 48"	38	UPHOLSTERY	SEE K-7	
18	CEILING TILE (KITCHEN)	ARMSTRONG VL RH90 FIREGUARD #87D NON-PERFORATED ICBO #5173 SEE SPECS. FOR ALTERNATE	24"x48" SUSPENDED T-BAR GRID SYSTEM- SEE SPECS.	39	WINDOW ROLLER SHADES (WHERE OCCURS)	FASCIA/ BOTTOM RAIL TO BE BLACK	MATERIAL TO BE PHIFER SHEARWEAVE BLACK
19	ALUMINUM FINISH	BLACK ANODIZED ALUMINUM OR METAL TO MATCH AEP SPAN DURA-TECH 5000 MATTE BLACK		40	PLAYGROUND VINYL WALL COVERING	MDC WALL COVERING MESA 1269 (TRAX 351)	(800) 621-4006
20	FRP PANELING	MARLITE P-151 LIGHT GREY	SEE SPECS. (KITCHEN)	41	SOLID SURFACE MATERIAL	CORIAN - COCOA BROWN (F)	
20A	TILE (R.R.)	DAL TILE PERMATONES 6421 (BLACK) 2 X 2	PLACE ONE FULL TILE ABOVE R/R PARTITIONS (APPRX. 78" A.F.F)				

INTERIOR AND EXTERIOR FINISH SCHEDULE (SEE SPEC BOOK FOR ALTERNATE MANUFACTURES)

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(503) 588-9600



**Boones Ferry Pointe**  
515 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

PROJECT

**SCHEDULE**  
 REVISIONS  
 STAMP  
 DATE: NOV-16-2012  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③  
 B/D DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE: AS SHOWN  
 SHEET

**A13.0**

FILE NAME:

**CURVE TABLE**

CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD
C1	135.83'	145.50'	53°29'12"	N26°34'00"E	130.95'
C2	18.43'	26.50'	39°50'40"	N73°33'56"E	18.06'
C3	34.69'	61.00'	32°34'45"	S49°48'42"E	34.22'
C4	28.05'	863.64'	1°51'38"	S27°49'35"E	28.04'

**LINE TABLE**

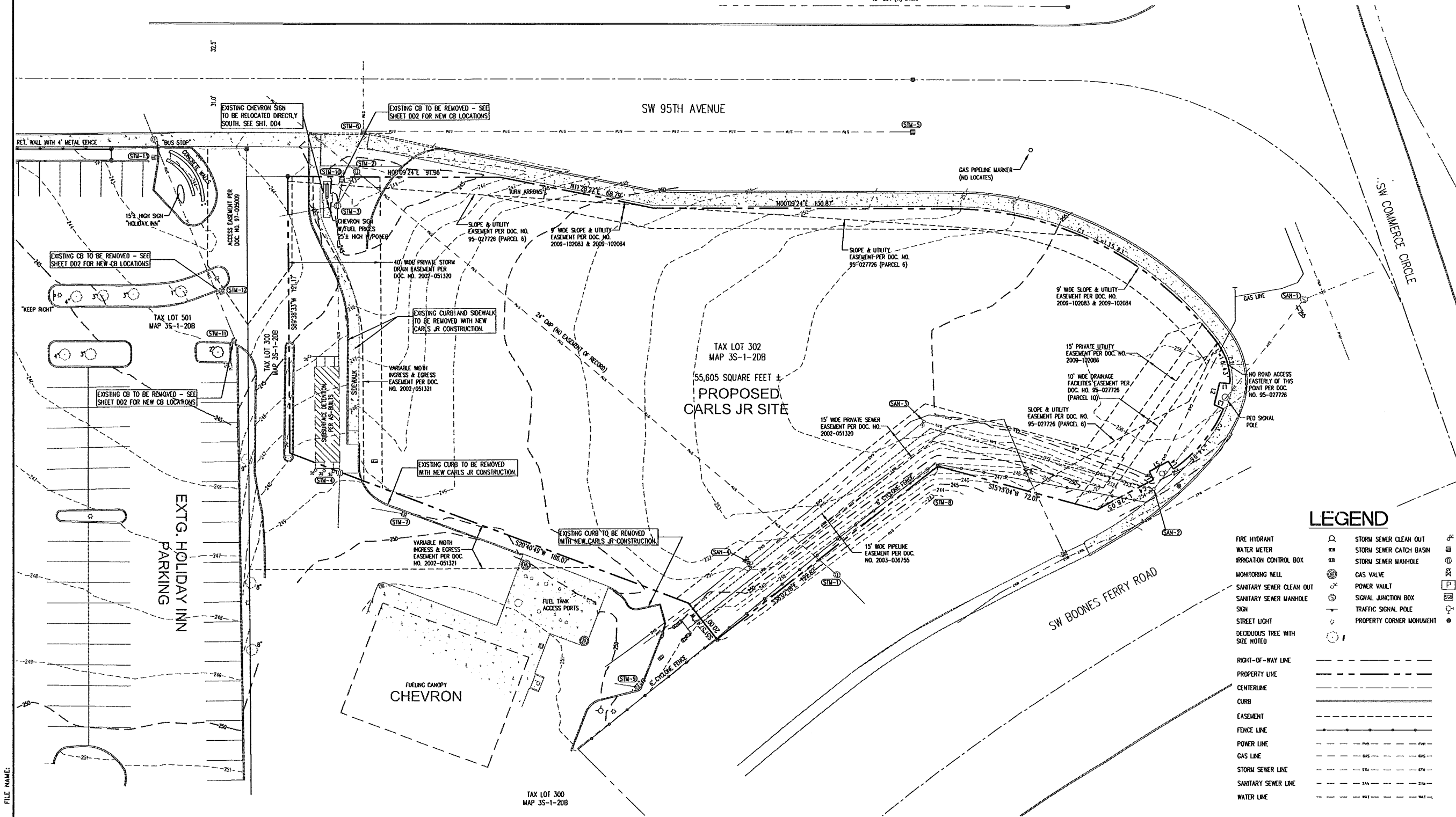
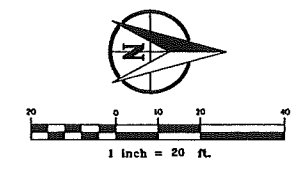
LINE	BEARING	LENGTH
L1	S03°28'26"W	4.50
L2	S75°40'24"E	9.87
L3	N23°33'53"E	4.52
L4	S56°28'43"W	4.04
L5	S28°07'38"E	9.52
L6	N63°26'08"E	4.00

**NOTES**

- FIELD WORK WAS COMPLETED DURING MARCH 2012. THERE IS CONSTRUCTION WORK SCHEDULED TO COMMENCE IN THE NEAR FUTURE ALONG THE STREETS ADJACENT TO THE PROPERTY SO CONDITIONS WITHIN THE RIGHT-OF-WAY WILL LIKELY CHANGE.
- ELEVATIONS ARE BASED ON WASHINGTON COUNTY BENCHMARK NO. 455. THE BENCHMARK IS LOCATED IN THE NORTHWEST CORNER OF THE BRIDGE CROSSING OVER INTERSTATE 5 WITH AN ELEVATION OF 281.55 (NGVD 1929 DATUM).
- EASEMENT ARE PER PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY OF OREGON WITH AN EFFECTIVE DATE OF JANUARY 10, 2012 AND ORDER NO. 472512495862904-CT50.
- ADDITIONAL FIELD WORK TO INCLUDE THE NORTH END OF THE PARKING LOT FOR THE HOLIDAY INN WAS COMPLETED ON MAY 29, 2012.

**STORM & SANITARY SEWER INFORMATION**

- |   |  |  |  |  |  |   |  |   |   |   |   |   |   |   |   |
|---|--|--|--|--|--|---|--|---|---|---|---|---|---|---|---|
| SAN-1 MANHOLE<br>RM 255.45'<br>12" IN (SE) 238.6'<br>12" OUT (N) 238.4'<br>E 244.4' | SAN-2 CLEAFOUT<br>RM 4 1/2 ABOVE GRADE<br>E 245.8' | SAN-3 CLEAFOUT<br>RM 4 1/2 ABOVE GRADE<br>E 245.8' | SAN-4 CLEAFOUT<br>RM 4 1/2 ABOVE GRADE<br>E 245.8' | STM-1 MANHOLE<br>RM 245.88'<br>24" OUT (SW) 237.3'<br>E 244.4' | STM-2 MANHOLE<br>RM 244.70'<br>12" IN (SE) 233.4'<br>24" IN (NE) 232.6'<br>24" OUT (SW) 232.6' | STM-3 MANHOLE<br>RM 243.52'<br>8" IN (S) 238.3'<br>8" IN (E) 238.3'<br>12" IN (W) 238.3'<br>(WH BURIED, UNABLE TO REMOVE LID) | STM-4 MANHOLE<br>RM 248.88'<br>8" IN (N) 243.4' (STUB?)<br>10" IN (E) 241.5'<br>12" OUT (W) 240.2' | STM-5 CATCH BASIN<br>RM 245.44'<br>(HAS FILTER FABRIC, UNABLE TO MEAS. INVERT, NO AS-BUILT INFO AVAIL.) | STM-6 CATCH BASIN<br>RM 249.55'<br>10" OUT (S) 246.6'<br>(HAS TRAP) | STM-7 CATCH BASIN<br>RM 248.94'<br>8" CMP (SW) 242.5'<br>STM-8 CATCH BASIN<br>RM 248.94'<br>8" OUT (S) 247.0' | STM-9 CATCH BASIN<br>RM 242.87'<br>BOTTOM RM 242.87'<br>8" OUT (E) 240.0' | STM-11 CATCH BASIN<br>RM 243.45'<br>WITH TRAP - OUT (W) | STM-12 CATCH BASIN<br>RM 243.05'<br>WITH TRAP - OUT (S) | STM-13 CATCH BASIN<br>RM 242.16'<br>WITH TRAP - OUT (W) | STM-14 MANHOLE<br>RM 243.31'<br>12" IN (E) 236.9'<br>15" OUT (N) 236.8' |
|---|--|--|--|--|--|---|--|---|---|---|---|---|---|---|---|
- \* RECORD INFORMATION PER LAPORT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)



**LEGEND**

- |                                |     |                          |   |
|--------------------------------|-----|--------------------------|---|
| FIRE HYDRANT                   | ⊙   | STORM SEWER CLEAN OUT    | ⊕ |
| WATER METER                    | ⊕   | STORM SEWER CATCH BASIN  | ⊕ |
| IRRIGATION CONTROL BOX         | ⊕   | STORM SEWER MANHOLE      | ⊕ |
| MONITORING WELL                | ⊕   | GAS VALVE                | ⊕ |
| SANITARY SEWER CLEAN OUT       | ⊕   | POWER VAULT              | ⊕ |
| SANITARY SEWER MANHOLE         | ⊕   | SIGNAL JUNCTION BOX      | ⊕ |
| SIGN                           | ⊕   | TRAFFIC SIGNAL POLE      | ⊕ |
| STREET LIGHT                   | ⊕   | PROPERTY CORNER MONUMENT | ⊕ |
| DECIDUOUS TREE WITH SIZE NOTED | ⊕   |                          |   |
| RIGHT-OF-WAY LINE              | --- |                          |   |
| PROPERTY LINE                  | --- |                          |   |
| CENTERLINE                     | --- |                          |   |
| CURB                           | --- |                          |   |
| EASEMENT                       | --- |                          |   |
| FENCE LINE                     | --- |                          |   |
| POWER LINE                     | --- |                          |   |
| GAS LINE                       | --- |                          |   |
| STORM SEWER LINE               | --- |                          |   |
| SANITARY SEWER LINE            | --- |                          |   |
| WATER LINE                     | --- |                          |   |

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14  
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**CLIENT**  
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 PO Box 916  
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**PROJECT**  
 Boones Ferry Pointe  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**DRAWING TITLE**  
 EXISTING CONDITIONS  
 PLAN

**REVISIONS**  
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**STAMP**

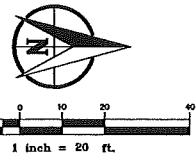
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**DATE:** 01-07-2013  
**BLDG. SUBMITTED DATE:** ①  
**DATE:** ②  
**DATE:** ③  
**REV. DATE:**  
**CKE NO.:**  
**ARCH. NO.:** 1199  
**SCALE:**  
**SHEET**

**DDI**

FILE NAME:

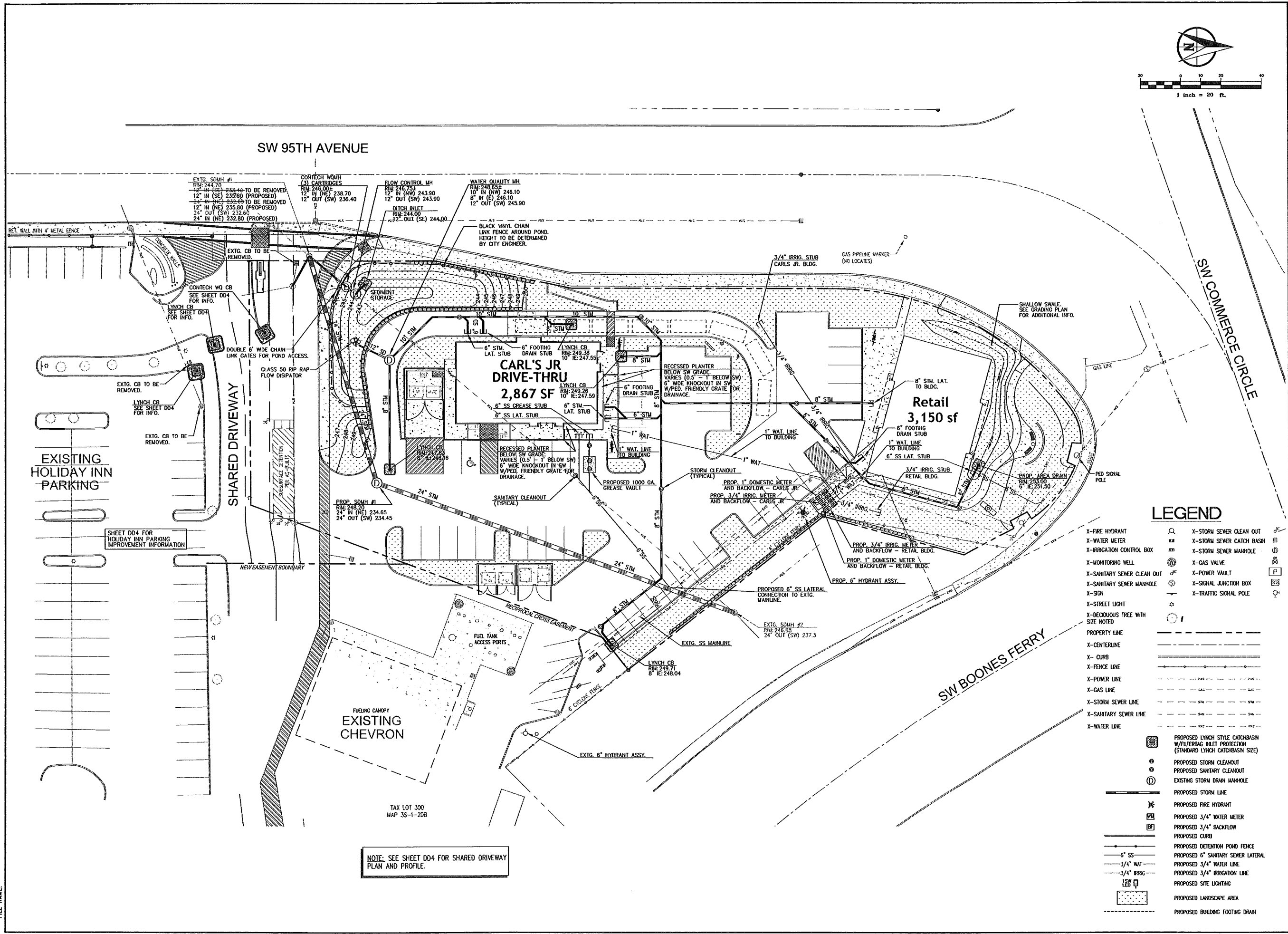




SW 95TH AVENUE

SW COMMERCE CIRCLE

SW BOONES FERRY



EXISTING HOLIDAY INN PARKING

SHARED DRIVEWAY

FUELING CANOPY  
EXISTING CHEVRON

TAX LOT 390  
MAP 35-1-20B

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

**LEGEND**

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STORM CLEANOUT
- PROPOSED SANITARY CLEANOUT
- EXISTING STORM DRAIN MANHOLE
- PROPOSED STORM LINE
- PROPOSED FIRE HYDRANT
- PROPOSED 3/4" WATER METER
- PROPOSED 3/4" BACKFLOW
- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED 6" SANITARY SEWER LATERAL
- PROPOSED 3/4" WATER LINE
- PROPOSED 3/4" IRRIGATION LINE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA
- PROPOSED BUILDING FOOTING DRAIN

**KITTED KITCHENS**  
6565 SW 207TH AVENUE  
ALOHHA, OR 97007  
TEL: (503) 708-3942

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**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**PRELIMINARY UTILITY PLAN**

REVISIONS



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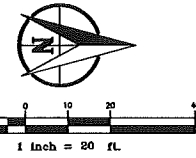
DATE: 01-07-2015  
BLDG. SUBMITTAL DATE: ①  
ARCH. NO.: 1199  
SCALE: SHEET

**DD2**

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

FILE NAME:

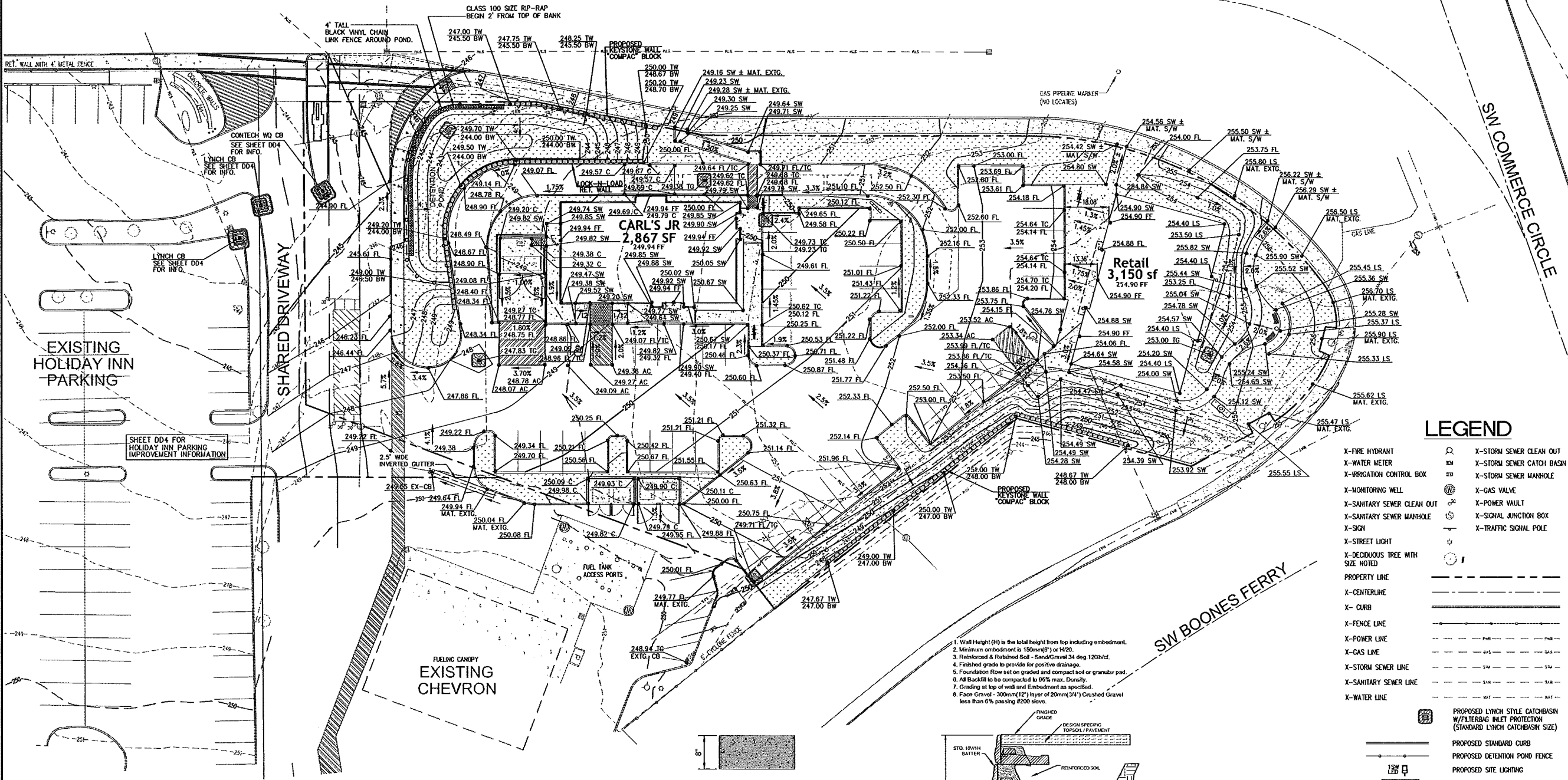




SW 95TH AVENUE

SW COMMERCE CIRCLE

SW BOONES FERRY



CARL'S JR  
2,867 SF

Retail  
3,150 sf

FUELING CANOPY  
EXISTING CHEVRON

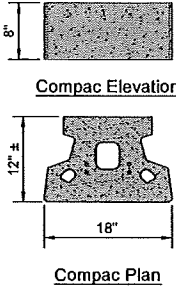
EXISTING HOLIDAY INN  
PARKING

SHARED DRIVEWAY

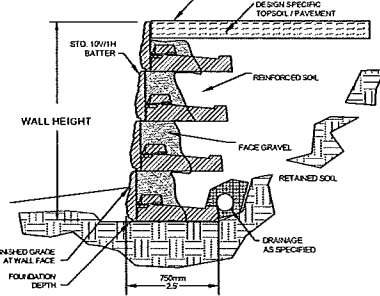
SHEET DD4 FOR  
HOLIDAY INN PARKING  
IMPROVEMENT INFORMATION

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY  
PLAN AND PROFILE.

1. Wall Height (H) is the total height from top including embedment.
2. Minimum embedment at 150mm(6") or H/20.
3. Reinforced & Rubbed Soil - Sand/Gravel 34 dog 1200vld.
4. Finished grade to provide for positive drainage.
5. Foundation Row set on graded and compact soil or granular pad.
6. All Backfill to be compacted to 95% max. Density.
7. Grading at top of wall and Embedment as specified.
8. Face Gravel - 200mm(8") layer of 20mm(3/4") Crushed Gravel less than 6% passing #200 sieve.



Compac Keystone Wall Unit  
NOT TO SCALE - WEST POND WALL



Lock-n-Load Wall Section  
NOT TO SCALE - EAST POND WALL

LEGEND

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STANDARD CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA
- 189.25 TC SPOT ELEVATION - TOP OF CURB
- 188.75 FL SPOT ELEVATION - FLOW LINE GUTTER
- 188.16 TW SPOT ELEVATION - TOP OF WALL
- 184.81 BW SPOT ELEVATION - BOTTOM OF WALL
- 189.50 FF SPOT ELEVATION - FINISH FLOOR
- 187.01 IG SPOT ELEVATION - TOP OF GRATE
- 189.08 C SPOT ELEVATION - CONCRETE PAD
- 189.35 LS SPOT ELEVATION - LANDSCAPE

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

**KITTED KEN ENGINEERS, LLC**  
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**Wilsonville Devco LLC**  
c/o Integrated Investment Partners  
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Portland, OR 97227  
(503) 488-9600

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**PRELIMINARY GRADING PLAN**

REVISIONS

STAMP  
REGISTERED PROFESSIONAL ENGINEER  
PRELIMINARY  
NOT FOR CONSTRUCTION  
C. P. KITTED  
REMARK: 12/20/13

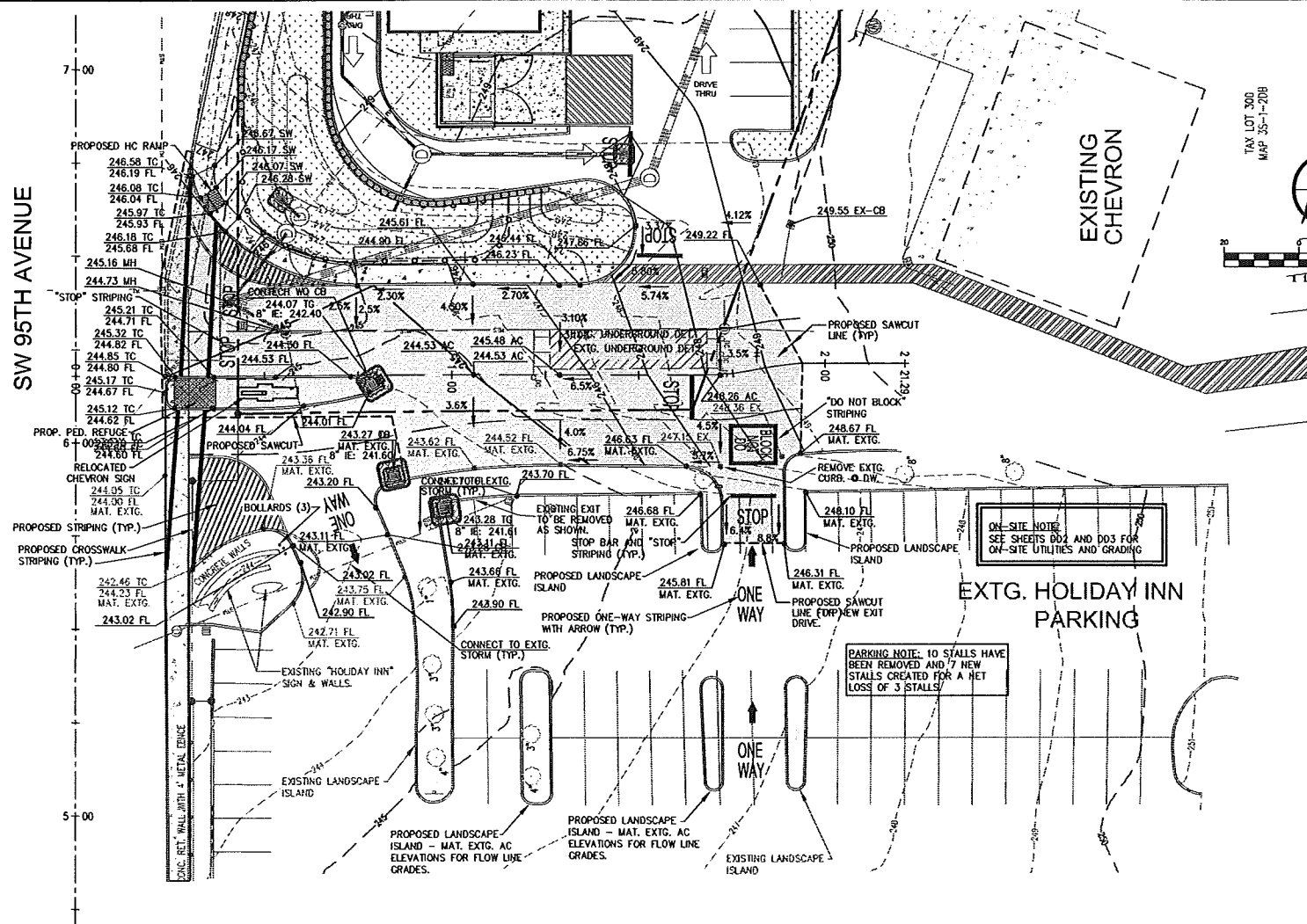
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BLDG. SUBMITTAL DATE: ①

BO DATE: ②

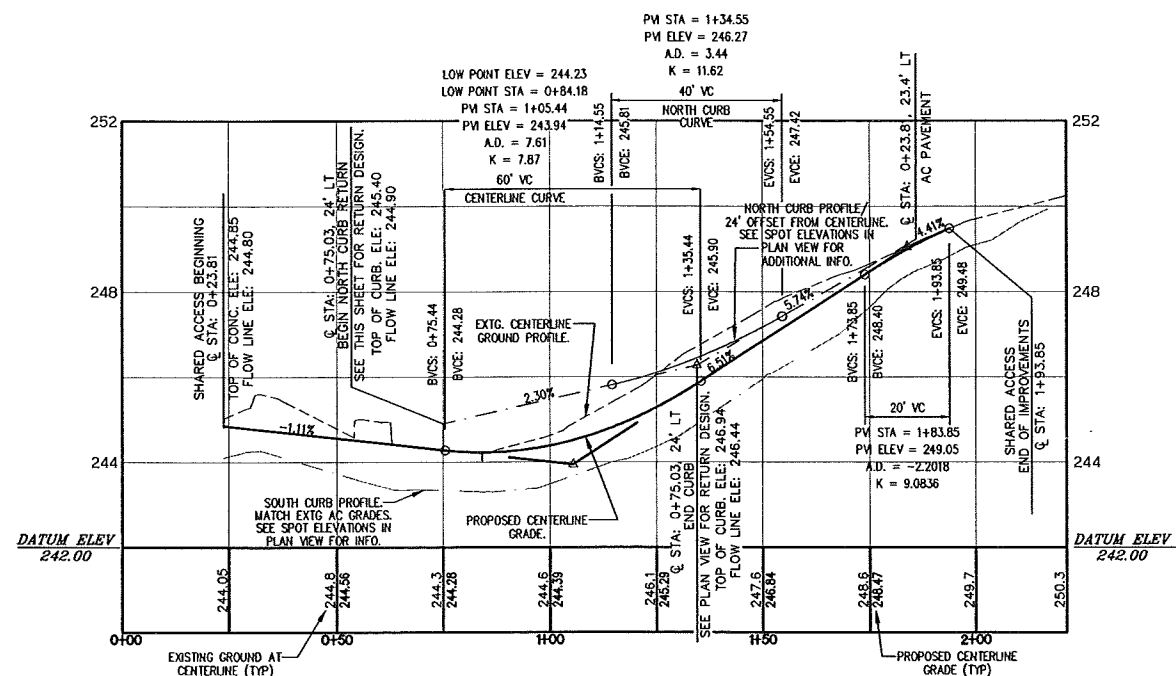
CKE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

**DD3**



SHARED ACCESS DRIVE - PLAN VIEW

HORIZONTAL SCALE: 1" = 20' VERTICAL SCALE: 1" = 2'

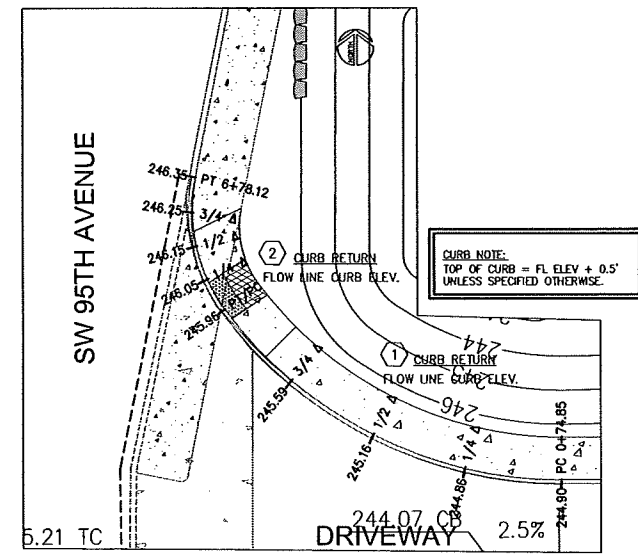
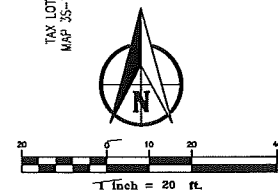


SHARED ACCESS DRIVE - CENTER LINE PROFILE

HORIZONTAL SCALE: 1" = 20' VERTICAL SCALE: 1" = 2'

CURB RETURN NO. 1 DATA	
CURVE DATA	STREET STATION
①	P.C. = 0+74.85
$\Delta = 53^{\circ}00'00''$	0+74.85 TO 2+00' LT DRIVEWAY - EAST
$R = 50.00'$	P.T. = 0+34.68
$T = 25.16'$	0+34.68 TO 4+29' LT DRIVEWAY - WEST
$L = 46.65'$	

CURB RETURN NO. 2 DATA	
CURVE DATA	STREET STATION
②	P.C. = 0+34.68
$\Delta = 48^{\circ}00'00''$	0+34.68 TO 4+29' LT DRIVEWAY - WEST
$R = 20.00'$	P.T. = 6+78.12
$T = 8.91'$	6+78.12 TO 9+00' RT SW 95TH AVENUE
$L = 16.76'$	



CURB RETURN DATA

SCALE: 1" = 10'

LEGEND

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-EASEMENT
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STORM LINE
- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA

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 (503) 688-9400

**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**SHARED DRIVEWAY AND PROFILE & PLAN AND PROFILE & HOLIDAY INN PARKING IMPROVEMENTS**

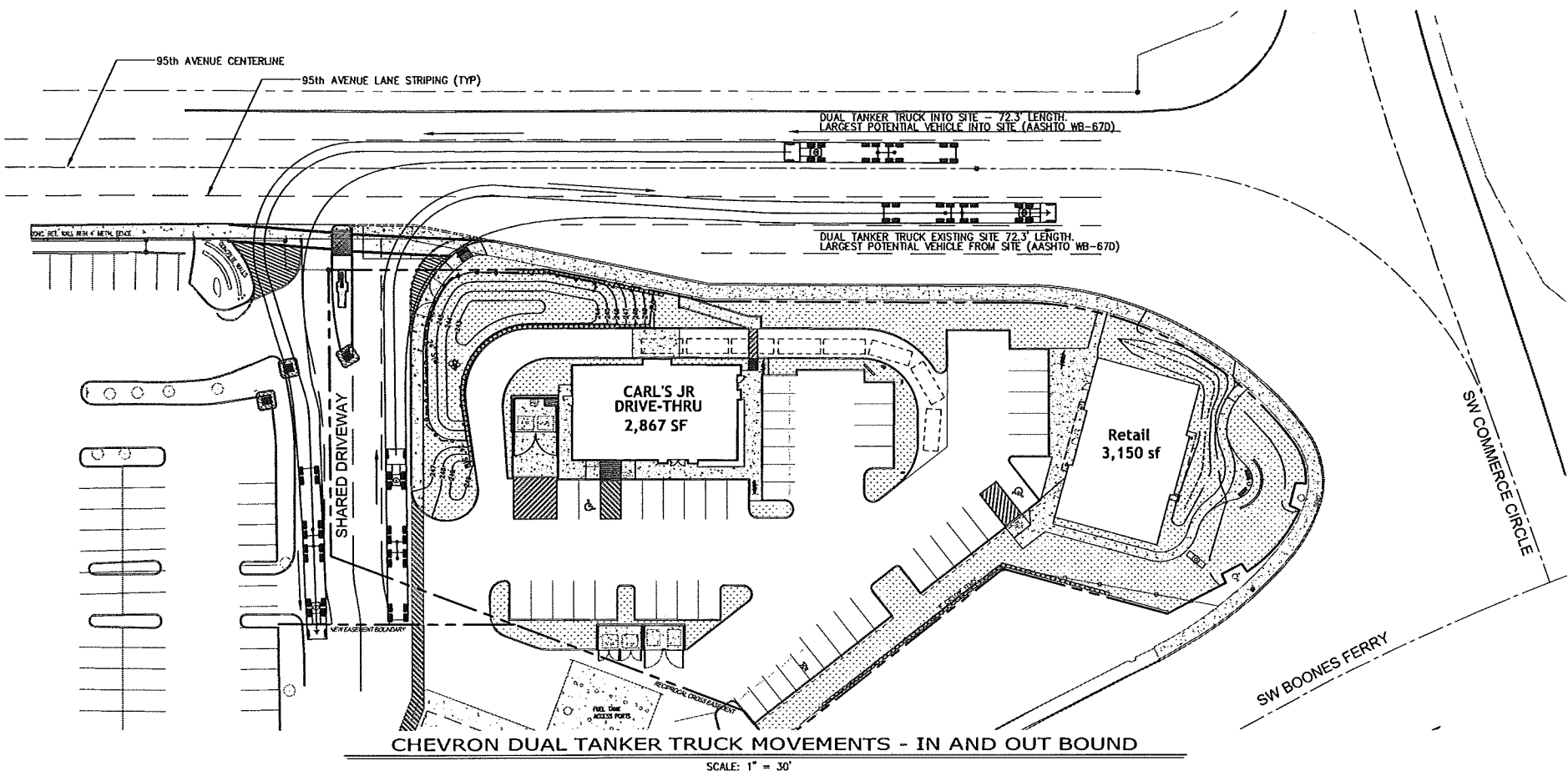
REVISIONS


STAMP

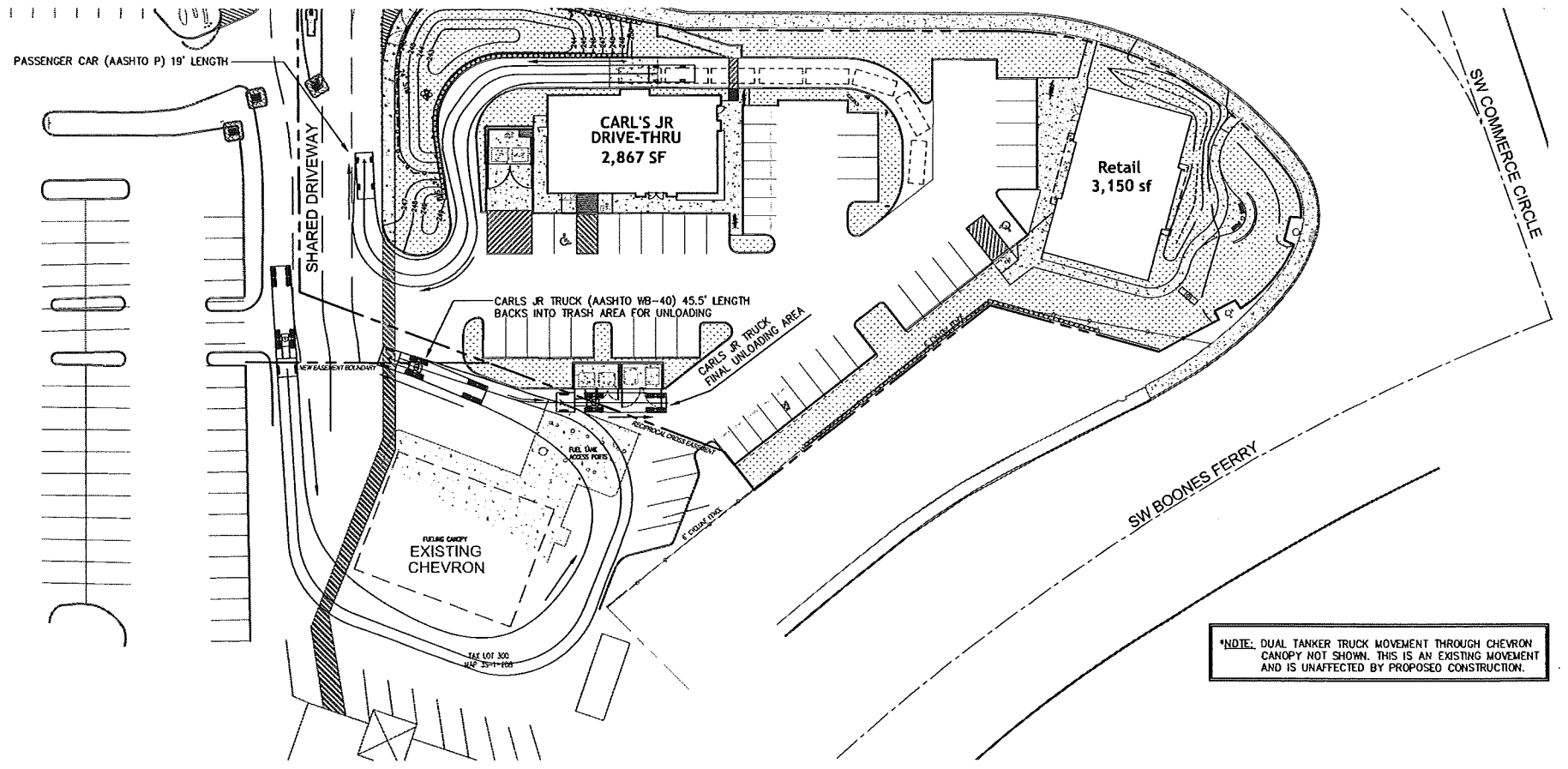
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 BLDG. SUBMITTAL DATE: ①  
 ②  
 ③  
 BID DATE: \_\_\_\_\_  
 CKE NO.: \_\_\_\_\_  
 ARCH. NO.: 1159  
 SCALE: \_\_\_\_\_  
 SHEET

DD4

FILE NAME:



**CHEVRON DUAL TANKER TRUCK MOVEMENTS - IN AND OUT BOUND**  
SCALE: 1" = 30'

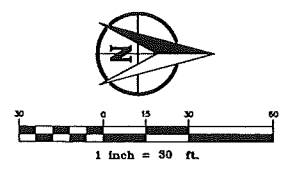


**PASSENGER CAR AND SERVICE TRUCK TURNING MOVEMENTS - CARLS JR.**  
SCALE: 1" = 30'

**LEGEND**

- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- PROPOSED STANDARD CURB
- PROPOSED LANDSCAPE AREA

\*NOTE: DUAL TANKER TRUCK MOVEMENT THROUGH CHEVRON CANOPY NOT SHOWN. THIS IS AN EXISTING MOVEMENT AND IS UNAFFECTED BY PROPOSED CONSTRUCTION.



FILE NAME:

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DRAWING TITLE  
**PROPOSED TRUCK TURNING MOVEMENTS**

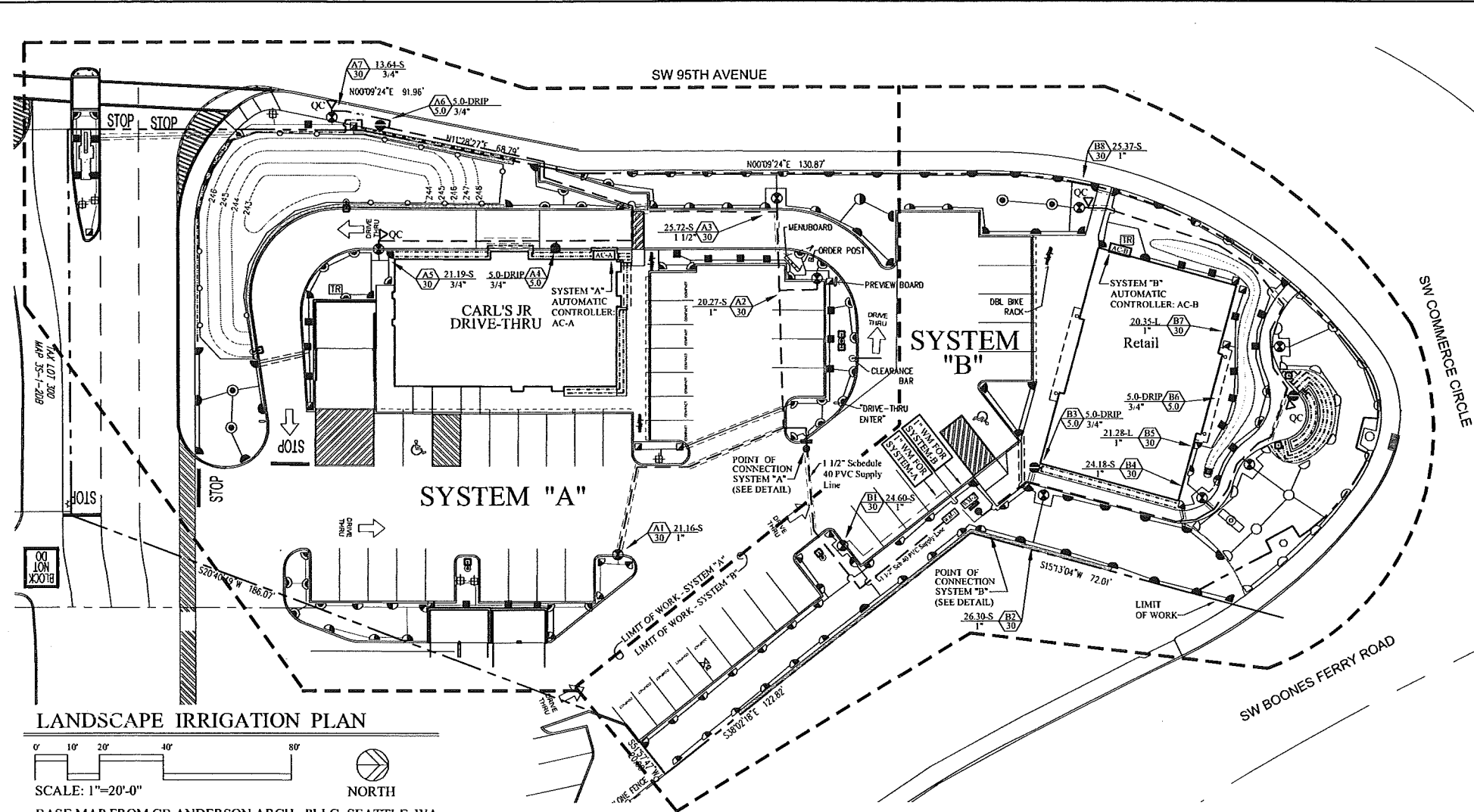
REVISIONS



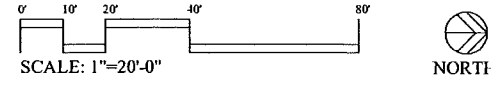
THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATORY AGENCIES.

DATE: 01-07-2013  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③  
 BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

**DD5**



**LANDSCAPE IRRIGATION PLAN**



SCALE: 1"=20'-0"  
 BASE MAP FROM CB ANDERSON ARCH., PLLC-SEATTLE, WA  
**IRRIGATION LINE AND SLEEVE SIZING**

CLASS 200 PVC LATERAL LINE		SCHEDULE 40 PVC MAIN LINE	
3/4" LINE	UP TO 10 GPM	3/4" VALVE	UP TO 17 GPM
1" LINE	BETWEEN 11-15 GPM	1" VALVE	BETWEEN 18-25 GPM
1 1/4" LINE	BETWEEN 16-25 GPM	1 1/2" VALVE	BETWEEN 26-41 GPM
1 1/2" LINE	BETWEEN 26-35 GPM	2" VALVE	OVER 65 GPM
2" LINE	BETWEEN 35-60 GPM		
2 1/2" LINE	OVER 61 GPM		

**SCHEDULE 40 PVC SLEEVING \* ALL MAINLINE SLEEVING TO BE MINIMUM 4"**

1 1/2" LINE	UP TO 10 GPM	3" SLEEVE	BETWEEN 26-35 GPM
2" LINE	BETWEEN 11-15 GPM	4" SLEEVE	BETWEEN 35-60 GPM
2 1/2" SLEEVE	BETWEEN 16-25 GPM	5" SLEEVE	OVER 61 GPM

**IRRIGATION NOTES**

- Irrigation design and layout assumes minimum 40 lbs. static water pressure 35 GPM at point of connection. Verify PSI and GPM, if discrepancy exists report to Landscape Architect and owner before proceeding.
- The Landscape Contractor shall inspect the site and verify existing conditions and dimensions prior to bidding and construction.
- Landscape Architect to have 48 hour notice to review pressure test for project. Provide owner with backflow certification for review and approval.
- Irrigation plan is diagrammatic. Adjust pipe, valves and irrigation heads according to field conditions and plant locations. Report any discrepancies to Landscape Architect.
- Coordinate irrigation points of connection and location of automatic controller with General Contractor. Provide two (2) 1" Water Meter by General Contractor. Coordinate all work with other trades involved.
- All valves will be placed in valve boxes in a manner which facilitates access for maintenance. Locate valves in shrub and ground cover beds unless approved by Landscape Architect. No valve boxes in lawn areas.
- All components of irrigation system shall be installed and adjusted to provide adequate coverage and no over spray onto buildings, windows, or paved areas. Contractor is responsible for providing a complete working irrigation system.
- Contractor is responsible for any damages to building and infrastructure during landscape construction.
- Bury control wires below mainline and tape to mainline every 10' o.c.
- Flush mainline before installing valves. Flush zone lines before installing nozzles. Test entire system before back filling trenches.
- Electric Valves and Backflow prevention assembly to be placed in below grade valve boxes and installed in shrub beds. Verify that valve boxes are not placed where plant materials are to be installed. Provide sleeving under all paved surfaces.
- Provide full coverage to all lawn and shrub beds. Install additional irrigation heads as necessary to achieve full coverage and water to all plantings.
- 120 AC Electrical power source at controller locations shall be provided by General Contractor. The Landscape Contractor shall make the final connection from the electrical source to the controller. All wiring per L & I Codes.
- All irrigation heads to be installed perpendicular to finish grade unless otherwise specified. Place all lawn heads in lawn areas, 4" from edge of paving or bedline. Place all shrub heads in shrub beds, min. 4" from bedline and min. 10" from all structures. CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS.
- Winterizing the system: The irrigation contractor shall install a manual drain valve assembly. Draining the mainline shall be accomplished by introducing compressed air into the system through the quick coupler valve at the point of connection.
- Demonstrate working system to building maintenance manager upon completion and acceptance.
- Locate all utilities before digging. Call 1-(800) 424-5555.
- Install irrigation system in accordance with all applicable codes and ordinances.
- Mount Rain Sensor near top of building. Place in approved location with Landscape Architect.
- From controller install one spare yellow wire to each valve box. All wire splices to be for wire connections: DBY-Direct Bury Splice by 3M.
- Contractor to provide reproducible PDF As-built drawings to Landscape Architect prior to close-out of job.
- Electrical control wires to be applied in control boxes only.
- Asphalt and/or concrete to be removed 2" from behind extruded curb or sidewalk in planter.
- Provide one (1) year warranty for parts, materials and workmanship upon final acceptance date from owner for work completed per Irrigation Plan.
- All mainlines to be buried min. 18" depth below grade; lateral lines to be buried min. 12" depth below grade.
- Sprinkler heads adjacent to head in parking, to be set on parking lot stripe.

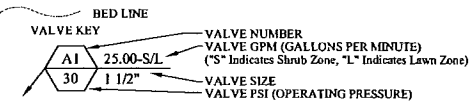
**IRRIGATION LEGEND**

SYMBOL	MANUFACTURER	PSI	RADIUS	GPM	LOCATION	SYMBOL	MANUFACTURER
OC	RAINBIRD 1804 W/15F-LA MPR PLASTIC NOZZLE	30	15'	3.70	LAWN	OC	RAINBIRD QUICK COUPLER VALVE 44RC, ASSUME 1" UNLESS OTHERWISE NOTED ON PLAN. PROVIDE HOSE SWIVEL. INSTALL PER DETAIL.
AC-A	RAIN BIRD 1804 W/15H-LA MPR PLASTIC NOZZLE	30	15'	1.85	LAWN	AC-A	SYSTEM "A" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.
AC-B	RAIN BIRD 1804 W/15Q-LA MPR PLASTIC NOZZLE	30	15'	0.93	LAWN	AC-B	SYSTEM "B" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.
1	RAIN BIRD 1804 W/12F MPR PLASTIC NOZZLE	30	12'	2.60	LAWN		GLEN HILTON MC MINI CLICK II RAIN SENSOR WITH BY-PASS SWITCH. INSTALL PER OWNER.
2	RAIN BIRD 1804 W/12H MPR PLASTIC NOZZLE	30	12'	1.30	LAWN		
3	RAIN BIRD 1804 W/12Q MPR PLASTIC NOZZLE	30	12'	0.65	LAWN		
4	RAIN BIRD 1804 W/10F MPR PLASTIC NOZZLE	30	10'	1.57	LAWN		
5	RAIN BIRD 1804 W/10H MPR PLASTIC NOZZLE	30	10'	0.78	LAWN		
6	RAIN BIRD 1804 W/10Q MPR PLASTIC NOZZLE	30	10'	0.39	LAWN		
7	RAIN BIRD 1804 W/95ST MPR PLASTIC NOZZLE	30	9x18"	1.73	LAWN		
8	RAIN BIRD 1804 W/155ST MPR PLASTIC NOZZLE	30	4x30"	1.21	LAWN		
9	RAIN BIRD 1804 W/15EST MPR PLASTIC NOZZLE	30	4x15"	0.61	LAWN		

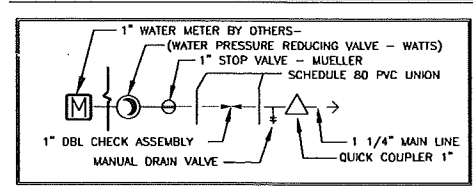
- NOTE:**  
 USE 12" HEIGHT POP-UP HEADS IN ALL OTHER LOCATIONS. CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS. USE 6" POP-UP HEADS ADJACENT TO HEAD IN PARKING STALLS.
- | SYMBOL | MANUFACTURER                              | PSI | RADIUS | GPM  | LOCATION |
|--------|---|-----|--------|------|----------|
| 10     | RAIN BIRD 1812 W/15F MPR PLASTIC NOZZLE   | 30  | 15'    | 3.70 | SHRUB    |
| 11     | RAIN BIRD 1812 W/15H MPR PLASTIC NOZZLE   | 30  | 15'    | 1.85 | SHRUB    |
| 12     | RAIN BIRD 1812 W/15Q MPR PLASTIC NOZZLE   | 30  | 15'    | 0.93 | SHRUB    |
| 13     | RAIN BIRD 1812 W/12F MPR PLASTIC NOZZLE   | 30  | 12'    | 2.60 | SHRUB    |
| 14     | RAIN BIRD 1812 W/12H MPR PLASTIC NOZZLE   | 30  | 12'    | 1.30 | SHRUB    |
| 15     | RAIN BIRD 1812 W/12Q MPR PLASTIC NOZZLE   | 30  | 12'    | 0.65 | SHRUB    |
| 16     | RAIN BIRD 1812 W/10F MPR PLASTIC NOZZLE   | 30  | 10'    | 1.57 | SHRUB    |
| 17     | RAIN BIRD 1812 W/10H MPR PLASTIC NOZZLE   | 30  | 10'    | 0.78 | SHRUB    |
| 18     | RAIN BIRD 1812 W/10Q MPR PLASTIC NOZZLE   | 30  | 10'    | 0.39 | SHRUB    |
| 19     | RAIN BIRD 1812 W/95ST MPR PLASTIC NOZZLE  | 30  | 9x18"  | 1.73 | SHRUB    |
| 20     | RAIN BIRD 1812 W/155ST MPR PLASTIC NOZZLE | 30  | 4x30"  | 1.21 | SHRUB    |
| 21     | RAIN BIRD 1812 W/15EST MPR PLASTIC NOZZLE | 30  | 4x15"  | 0.61 | SHRUB    |

- NOTE:**  
 USE 12" HEIGHT POP-UP HEADS IN ALL OTHER LOCATIONS. CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS. USE 6" POP-UP HEADS ADJACENT TO HEAD IN PARKING STALLS.
- DRIP LINE: MAXIMUM 18" WIDTH BETWEEN DRIP LINES**  
 RAIN BIRD DRIP LINE AND REQUIRED AIR/VACUUM RELIEF VALVES, IN-STEM FILTERS, DIFFUSER BUG CAPS AND OTHER REQUIRED EQUIPMENT TO PROVIDE COMPLETE COVERAGE OF PLANT MATERIAL.
- RAINBIRD PEB SERIES AUTOMATIC CONTROL VALVE, SIZE PER PLAN AND PROVIDE AMTEX CONTROL BOX & MIN. 2 CU. FT PEA GRAVEL.
- RAINBIRD PEB SERIES AUTOMATIC CONTROL VALVE, SIZE PER PLAN AND RAINBIRD DRIP LINE VALVE - PER MANUFACTURE SPEC. PROVIDE AMTEX CONTROL BOX & MIN. 2 CU. FT PEA GRAVEL.
- WATER PRESSURE REDUCING VALVE (IF NEEDED) - WATTS SERIES US, USB, SIZE PER P.O.C. DETAIL.
- MUELLER BRASS ISOLATION VALVE, SIZE PER P.O.C. DETAIL, INSTALL IN ROUND 6" AMTEX CONTROL BOX.
- FEBCO DBL CHECK VALVE, SIZE PER P.O.C. DETAIL, INSTALL IN LARGE AMTEX CONTROL BOX PER DETAIL.

- RAINBIRD QUICK COUPLER VALVE 44RC, ASSUME 1" UNLESS OTHERWISE NOTED ON PLAN. PROVIDE HOSE SWIVEL. INSTALL PER DETAIL.
- SYSTEM "A" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.
- SYSTEM "B" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.
- GLEN HILTON MC MINI CLICK II RAIN SENSOR WITH BY-PASS SWITCH. INSTALL PER OWNER.
- 1 1/4" MAIN SCHEDULE 40 PVC MAIN LINE, SIZE PER PLAN, MIN. DEPTH 18" BELOW FINISHED GRADE.
- 3/4" CLASS 200 PVC LATERAL LINE, SIZE PER PLAN, MIN. DEPTH 12" BELOW FINISHED GRADE.
- 2" SL SCHEDULE 40 PVC SLEEVING, SIZE PER PLAN, MIN. DEPTH 18" BELOW FINISHED GRADE FOR MAINLINE. MINIMUM DEPTH 12" BELOW FINISHED GRADE FOR LATERAL LINES.

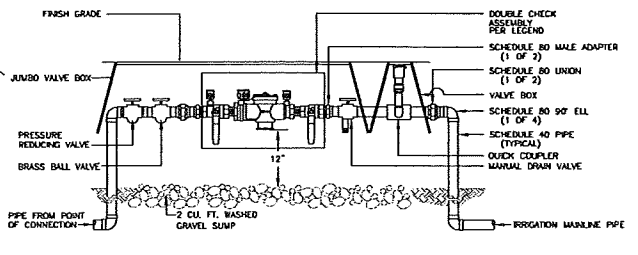


**SYSTEM "A" AND SYSTEM "B" - POINT OF CONNECTION DETAIL**

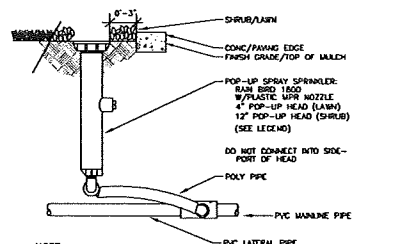


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 P.O. Box 1442 Maple Valley, Washington 98038  
 lauch@bethuneassociates.com www.bethuneassociates.com  
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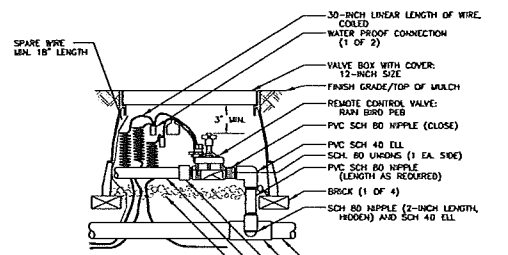
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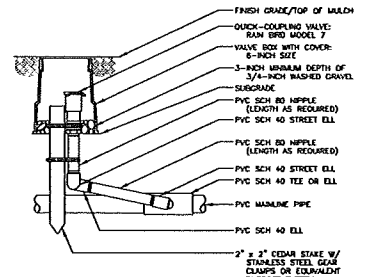
**DETAIL: BACKFLOW PREVENTER**  
 NO SCALE



**DETAIL: POP-UP SPRAY SPRINKLER**  
 NO SCALE



**DETAIL: REMOTE CONTROL VALVE**  
 NO SCALE



**DETAIL: QUICK-COUPLING VALVE**  
 NO SCALE

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**PROJECT**  
 Boones Ferry Pointe  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

**DRAWING TITLE**  
 LANDSCAPE IRRIGATION PLAN

**REVISIONS**

NO.	DATE	DESCRIPTION
1	11/09/2012	PER PLANNING, 01/02/2013

**STAMP**

DATE: 11/09/2012  
 SUBMITTAL: 1  
 DATE: 2  
 DATE: 3

REG. NO.:  
 MCH. NO.: 1199  
 SCALE:  
 SHEET: LANDSCAPE IRRIGATION PLAN  
**L 1.0**

FILE NAME:





SECTION 02800  
LANDSCAPE WORK

PART 1 - GENERAL

1.1 SUMMARY

- A. Section includes preparation of subsoil and topsoil; topsoil bedding; new or relocated trees, plants, seeded grass, sod lawn, ground cover, mulch, amendments, fertilizer and maintenance.
B. When specifications contained in this Section conflict with the Landscape Drawings, the Landscape Drawings take precedence.
C. Related Sections:
1. Section 02311 - Grading
2. Section 02320 - Backfill: Rough grading of site.
3. Section 02324 - Trenching: Rough grading over trench cut.
4. Section 02811 - Landscape Irrigation.

1.2 REFERENCES

- A. American National Standards Institute:
1. ANSI Z60.1 - Nursery Stock.
B. International Society of Arborists:
1. ISA - Certification Documentation for Tree Pruning Qualifications.
C. National Arborist Association:
1. NAA - Certification Documentation for Tree Pruning Qualifications.

1.3 DEFINITIONS

- A. Weeds: Any plant life not specified for a given area or scheduled.
B. Plants: Living trees, plants, grass, and ground cover specified in this Section and described in ANSI Z60.1.

1.4 SUBMITTALS

- A. Section 01330 - Submittal Procedures: Requirements for submittals.
B. Product Data: Submit list of plant material sources, data for amendments, fertilizer and other items required plant source list including nurseries and contact numbers 30 days in advance of contract.

1.5 CLOSE-OUT SUBMITTALS

- A. Section 01700 - Execution Requirements: Close-out procedures.
B. Operation and Maintenance Data: types, application frequency, and recommended coverage of fertilizers.

1.6 SUBSTITUTIONS

- A. No material substitution shall be made without the written approval of the Landscape Architect or the Owner's Representative.

1.7 QUALIFICATIONS

- A. Nursery: Company specializing in growing and cultivating the plants with five (5) years experience.
B. Installer: Company specializing in installing and planting the plants with five (5) years experience and approved by nursery. Installer must be a licensed and bonded landscaping contractor.
C. Tree Pruner: Company specializing in pruning trees with proof of ISA Arborist Certification or NAA Certified Tree Care Specialist.
D. Maintenance Services: Performed by the installer.

1.8 DELIVERY, STORAGE, AND HANDLING

- A. Section 01600 - Product Requirements: Product storage and handling requirements.
B. Deliver fertilizer in waterproof bags showing weight, chemical analysis, and name of manufacturer.
C. Protect and maintain plant life until planted.
D. Deliver plant life materials immediately prior to placement. Keep plants moist.
E. Plant material which has been damaged by reason of delivery, storage or handling will be rejected.

1.9 ENVIRONMENTAL REQUIREMENTS

- A. Do not install plant life when ambient temperatures may drop below 35 degrees F or rise above 90 degrees F.

1.10 COORDINATION

- A. Section 01300 - Administrative Requirements: Coordination and project conditions.
B. Install plant life after and coordinate with installation of underground irrigation system piping and watering heads specified in Section 02811.

1.11 GUARANTEE AND REPLACEMENT

- A. Section 01700 - Execution Requirements: Product warranties and product bonds.
B. Beginning at time of final acceptance, all materials and workmanship shall be guaranteed for a time period of one (1) year. During time of guarantee all plant material shall be in a healthy, thriving condition.
C. Provide one (1) year manufacturer's guarantee for trees, plants, seeded grass, sod, and ground cover.
D. Replace any seeded or sodded areas which fail to show vigorous growth after three (3) weeks of seeding. Fill and seed all areas which settle, as specified. At the end of the guarantee period, the seeded and sodded areas shall be a flourishing, vigorous, uniform, thick stand of specified grasses with no bare spots and no weeds. Remove and replace immediately and dead, diseased, dying or missing plant materials at no cost to the Owner during the guarantee period. Replacement plants shall be as specified and guaranteed until completion of the guarantee period.
E. Lawn that has been hydroseeded shall be allowed to become established. Landscape Contractor to cut straight edge between hydroseeded lawn and mulch planter. Remove all lawn over spray in mulch planter, dispose of debris off-site.

1.12 MAINTENANCE SERVICE

- A. Section 01700 - Execution Requirements: Maintenance service.
B. Beginning at the first installation of plant material, maintain plant life through final acceptance.
C. Continue maintenance until final acceptance.
D. Maintenance to include:
1. Cultivation and weeding plant beds and tree pits.
2. Applying herbicides for weed control. Remediate damage resulting from use of herbicides.
3. Remediate damage from use of insecticides.
4. Irrigating sufficient to saturate root system.
5. Pruning, including removal of dead or broken branches.
6. Disease control.
7. Maintaining wrapping, guys, turnbuckles and stakes. Adjust turnbuckles to keep guy wires tight. Repair or replace accessories when required.
8. Fertilizer.
9. Removal and replacement of dead or damaged plants.
10. Mowing and edging lawn.

PART 2 - PRODUCTS

2.1 TREES, PLANTS, AND GROUND COVER

- A. Planting Stock:
1. All plants shall be true to type and name in accordance with the latest edition of Standardized Plant Names, official code of the American Joint Committee on Horticulture Nomenclature, and each bundle or each plant shall be labeled properly.
2. All plants shall have a well-branched, vigorous and balanced root and exhibiting normal habit of growth and unless otherwise specified, shall be No 1 Grade conforming to "American Standard for Nursery Stock" of the American Association of Nurserymen (AAN). They shall be free from disease, injurious insects, insect eggs and larvae, mechanical wounds, broken branches, rot, decay or any other defect. Trees shall have reasonable straight trunks with well balanced tops and a single leader.
B. Trees, Plants, and Ground Cover:
1. Species and size identifiable in plant schedule, grown in climatic conditions similar to those in locality of the Work.

2.2 SOD LAWN

- A. Sod shall be supplied by local supplier per approval of Landscape Architect.
B. Sod shall be premium quality, uniform green color, free of all weeds, pests, poa annua, and disease. Sod shall be a minimum of twelve (12) months old, but not more than thirty (30) months old, and shall be delivered to the project the site within twenty-four (24) hours of cutting.

2.3 SOIL MATERIALS

- A. Topsoil: Imported topsoil from local supplier. Topsoil to have a 60/40 mix. Landscape Contractor to supply soil results and soil sample to Landscape Architect.
B. Topsoil: Fertile, agricultural soil, typical for locality, capable of sustaining vigorous plant growth, taken from drained site, free of subsoil, clay or impurities, plants, weeds and roots; minimum pH value of 5.4 and maximum pH value of 7.0; organic matter to exceed 1.5 percent, magnesium to exceed 100 units; phosphorus to exceed 150 units; potassium to exceed 20 units; soluble salts/conductivity not to exceed 900 ppm/0.9 mmhos/cm in soil.

SECTION 02811  
UNDERGROUND IRRIGATION SYSTEM

PART 1 - GENERAL

1.1 SUMMARY

- A. Section includes trenching, pipe and fittings, valves, sprinkler heads, accessories and controls for automatic landscape irrigation system.
B. Related Sections:
1. Section 02320 - Backfill: Backfilling utility structures.
2. Section 02324 - Trenching: Excavating and backfilling for irrigation piping.
3. Section 02800 - Landscape Work: Landscape plant and materials.
4. Section 16000 - Electrical Work - Bidder Design.

1.2 REFERENCES

- A. American Society for Testing and Materials:
1. ASTM D2235 - Standard Specification for Solvent Cement for Acrylonitrile-Butadiene-Styrene (ABS) Plastic Pipe and Fittings.
2. ASTM D2241 - Standard Specification for Poly (Vinyl Chloride) (PVC) Pressure-Rated Pipe (SDR Series).
3. ASTM D2282 - Standard Specification for Acrylonitrile-Butadiene-Styrene (ABS) Plastic Pipe (SDR-PR).
4. ASTM D2554 - Standard Specification for Solvent Cements for Poly (Vinyl Chloride) (PVC) Plastic Piping Systems.
B. National Electrical Manufacturers Association:
1. NEMA 250 - Enclosures for Electrical Equipment (1000 Volts Maximum).

1.3 SYSTEM DESCRIPTION

- A. Electronic solenoid or Hydraulic controlled underground automatic irrigation system, with low point or self pressure blowdown drain, as indicated.
B. Source Power: 120 volts AC, single phase.
C. Low Voltage Controls: 24 volts.

1.4 SUBMITTALS

- A. Section 01330 - Submittals Procedures:
B. Shop Drawings: Indicate piping layout to water source, location of sleeves under pavement, location and coverage of sprinkler heads, components, plant and landscaping features, site structures, schedule of outlets and fittings to be used.
C. Product Data: Submit manufacturer's technical data; and component and control system, and wiring diagrams.

1.5 CLOSEOUT SUBMITTALS

- A. Section 01700 - Execution Requirements: Closeout procedures.
B. Project Record Documents: Record actual locations of all concealed components, piping system and conduit.
C. Operation and Maintenance Data:
1. Provide instructions for operation and maintenance of system and controls, seasonal activation and shutdown, and manufacturer's parts catalog.
2. Provide schedule indicating length of time each valve is required to be open to provide a determined amount of water.

1.6 QUALIFICATIONS

- A. Manufacturer: Provide underground automatic irrigation as a complete unit produced by a single acceptable manufacturer, with minimum five (5) years experience, specializing in manufacturing products specified in this Section, including heads, valves, piping circuits, controls, and accessories.
B. Installer: Company specializing in performing Work of this Section with minimum five (5) years experience approved by manufacturer. Installer must be a licensed and bonded underground irrigation contractor.

1.7 FIELD MEASUREMENTS

- A. Verify pressure, flow and volume prior to installing irrigation system. If discrepancy, report to Landscape Architect.
B. Verify that field conditions and field measurements are as indicated.
C. Verify landscape plant material staking and layout.

1.8 COORDINATION

- A. Section 01300 - Administrative Requirements: Coordination and project coordination.
B. Coordinate the Work with site backfilling, landscape grading and delivery of plant life.

1.9 EXTRA MATERIALS

- A. Section 01700 - Execution Requirements: Spare parts and maintenance products.
1. Two (2) sprinkler heads of each type and size.
2. Two (2) valve keys for manual valves.
3. Two (2) valve box keys.
4. Two (2) wrenches for each type head core and for removing and installing each type head.

PART 2 - PRODUCTS

2.1 PIPE MATERIALS

- A. PVC Pipe: ASTM D2241; Schedule 40, size as required; solvent-weld sockets.
B. Fittings: Type and style of connection to match pipe.
C. Solvent Cement: ASTM D2564 for PVC pipe fittings.
D. Sleeve Material: PVC.

2.2 OUTLETS

- A. Manufacturers:
1. Rainbird Company.
2. Substitutions: Section 01600 - Product Requirements.
B. Outlets: Brass or bronze construction.
C. Rotary Type Sprinkler Head: Pop-up type without screens; fully adjustable for flow and pressure; size as indicated; with letter or symbol designating degree or arc and arrow indicating center of spray pattern.
D. Spray Type Sprinkler Heads: Fixed surface head: pop-up head with full circle, half circle, third circle, or quarter circle pattern.
E. Emitters: Adjustable outlet, non-clogging, with two (2) trickle tubes.

2.3 MANUAL VALVES

- A. Globe Valves: Bronze construction, non-rising stem.
B. Backflow Preventers: Bronze body construction, double check valve type, as required by governing Authority Having Jurisdiction.
C. Valve Box and Cover: AMTEK, 12 inch standard box with locking lid.

2.4 CONTROLS AND CONTROL VALVES

- A. Controller: Automatic controller, microprocessor solid state control with visible readout display and motor driven time for a multiple station system, programmable for seven (7) days in quarter (1/4) hour increments, with automatic start and shutdown.
B. Controller Housing: Weatherproof, watertight, with lockable access door.
C. Valves: Hydraulic or Electric Solenoid as indicated on Drawings, normally closed, including required fittings and accessories.
D. Wire Conductors: Color coded, copper conductor, direct burial type. Bundle grouped control wires together.

PART 3 - EXECUTION

3.1 EXAMINATION

- A. Section 01310 - Administrative Requirements: Coordination and project conditions.
B. Verify location of existing utilities.
C. Verify that required utilities are available, in proper location, and ready for use.

3.2 PREPARATION

- A. Comply with requirements of Uniform Plumbing Code.
B. Piping layout indicated is diagrammatic only. Route piping to avoid plants, ground cover, utilities, and structures.
C. Layout and stake locations of system components.
D. Review layout requirements with other affected Work. Coordinate locations of sleeves under paving to accommodate system.

3.3 TRENCHING

- A. Trench and backfill in accordance with Section 02320 and Section 2324.
B. Trench to accommodate grade changes and slope to drains. Excavate straight and true with bottoms sloped to low points.
C. Excavate trenches to provide 18 inches minimum coverage over top of installed piping, below finish grade.
D. Excavate trenches to a depth of 3 inches below invert of pipe, unless otherwise indicated.
E. All roots 4-inch diameter and greater that are cut shall be treated with an approved tree wound dressing.
F. Maintain trenches free of debris, material, or obstructions that may damage pipe.

3.4 SLEEVEING

- A. Install sleeves under all paved and surfaced areas, through walls, and as required to facilitate a smooth construction sequence. Extend sleeves as shown on Drawings. Coordinate with other trades as required.
B. Sleeves to be a minimum twice (2 x) the IPS diameter of insert pipe, and shall extend a minimum of 12 inches beyond edge of paving, walls, and walls. Plug ends of pipe to prevent soil from entering ends, if backfill is required prior to use.
C. Valve Box and Cover: AMTEK, 12 inch standard box with locking lid.

3.5 INSTALLATION

- A. Connect to utilities.
B. Set outlets and box covers at finish grade elevations.
C. Install control wiring in accordance with National Electric Code. Provide 10 inch expansion coil at each valve to which controls are connected, and at 100 foot intervals. Bury wire beside pipe. Mark valves with neoprene valve markers containing locking device. Set valve markers in 160 psi PVC pipe risers exiting from top of valve to finish grade.
D. After piping is installed, but before outlets are installed and backfilling commences, open valves and flush system with full head of water.
E. Install lawn heads at manufacturer's recommended heights. Install shrubbery heads at heights indicated. Located part-circle heads to maintain a minimum distance of 4 inches from walls and 2 inches from other boundaries, unless otherwise indicated.
F. Use dielectric fittings at connections where materials of dissimilar metal are joined.

3.6 FIELD QUALITY CONTROL

- A. Section 01400 - Quality Requirements: Testing and inspection services 01700 - Execution Requirements: Testing, adjusting, and balancing.
B. Prior to backfilling, test system for leakage for whole system to maintain 100 psi pressure for one (1) hour. Piping may be tested in sections to expedite Work. Coordinate with Landscape Architect w/48 hours notice.
C. System is acceptable if no leakage or loss of pressure occurs and system self drains during test period. Remove and repair piping, connections, valves which do not pass hydrostatic testing, at no cost to Owner.
D. Provide one (1) complete spring start-up and a full shutdown.

3.7 BACKFILLING

- A. Provide 18 inches minimum cover over top of piping. Trench and backfill to subgrade elevation as specified in Section 02324 - Trenching. Protect piping from displacement.

3.8 ADJUSTING

- A. Section 01700 - Execution Requirements: Testing, adjusting, and balancing.
B. Adjust control system to achieve time cycles required.
C. Set controller for water times suitable for the time of year and plan a written schedule on water, and place schedule adjacent to the controller.
D. Change or adjust head types for full water coverage as directed.

3.9 DEMONSTRATION AND TRAINING

- A. Instruct Owner's personnel in operation and maintenance of system, including adjusting of sprinkler heads. Use operation and maintenance material as basis demonstration.

END OF SECTION

FILE NAME:

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Landscape Architecture & Planning, ASLA
P.O. Box 1442 phone: (425) 432-9877
Maple Valley, Washington 98038 fax: (425) 432-9878
lauch@bethuneassociates.com www.bethuneassociates.com



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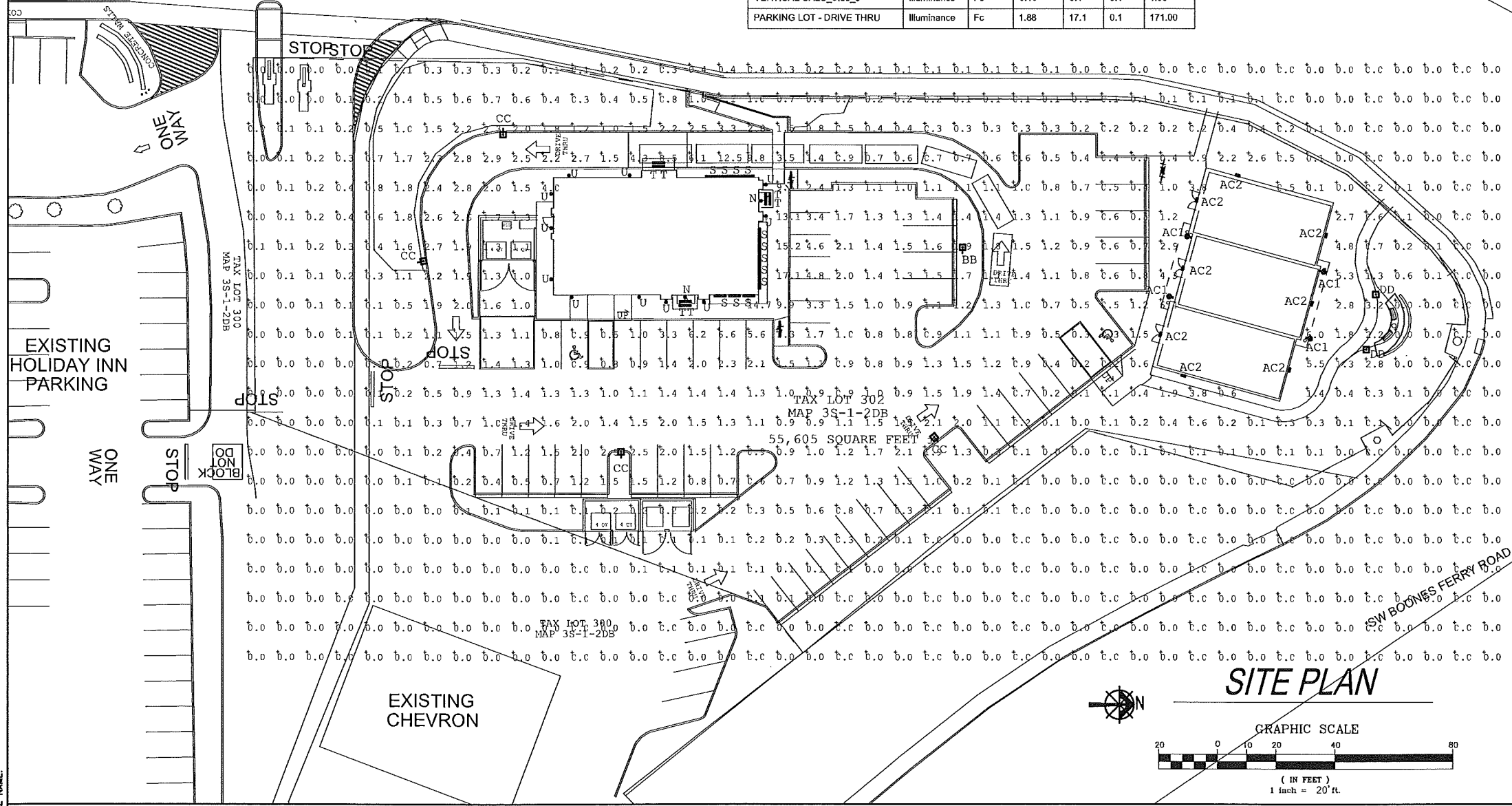
Vertical strip on the right side containing logos for CB Anderson Architects, Wilsonville Devco LLC, Boones Ferry Pointe, and a revision table with dates and descriptions.



Luminaire Schedule										
Symbol	Qty	TYPE	Mounting Height	Arrangement	proposed shielding	Total Lamp Lumens	LLF	CATALOG #	Proposed Wattage	Max. wattage per light zone L22
	2	DD	3.5 ft	SINGLE	SHELED	3800	0.600	BRL-36-50-MP	32	35
	2	N	10.0 ft	SINGLE	FULL SHELED	3200	0.700	1101F2642U	42	100
	6	T	10.0 ft	SINGLE	FULL SHELED	5800	0.750	FSRW-64	(2) 42	100
	12	S	10.0 ft	SINGLE	FULL SHELED	5800	0.750	SSF-232	(2) 42	100
	11	U	11.6 ft	SINGLE	FULL SHELED	6600	0.600	WB200-100PMH	100	100
	4	AC1	9 ft	SINGLE	FULL SHELED	2200	0.700	H91514-100MH	100	100
	8	AC2	9 ft	SINGLE	FULL SHELED	3200	0.700	TW5L-242TRF-WD	(2) 42	100
	4	CC	25 ft	SINGLE	FULL SHELED	N.A.	0.900	ATBA20LED70-R2	N.A.	100
	1	BB	25 ft	SINGLE	FULL SHELED	N.A.	0.900	ATBA20LED70-R2	N.A.	100

Calculation Summary						
Label	CalcType	Units	Avg	Max	Min	Max/Min
GENERAL AREA	Illuminance	Fc	0.71	17.1	0.0	N.A.
VERTICAL CALC_Side_1	Illuminance	Fc	0.09	0.5	0.0	N.A.
VERTICAL CALC_Side_10	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_11	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_12	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_13	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_14	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_15	Illuminance	Fc	0.03	0.1	0.0	N.A.
VERTICAL CALC_Side_16	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_17	Illuminance	Fc	0.00	0.0	0.0	N.A.
VERTICAL CALC_Side_18	Illuminance	Fc	0.07	0.1	0.0	N.A.
VERTICAL CALC_Side_19	Illuminance	Fc	0.02	0.1	0.0	N.A.
VERTICAL CALC_Side_2	Illuminance	Fc	0.62	2.2	0.0	N.A.
VERTICAL CALC_Side_20	Illuminance	Fc	0.14	0.2	0.1	2.00
VERTICAL CALC_Side_21	Illuminance	Fc	0.07	0.1	0.0	N.A.
VERTICAL CALC_Side_22	Illuminance	Fc	0.10	0.2	0.0	N.A.
VERTICAL CALC_Side_23	Illuminance	Fc	0.02	0.1	0.0	N.A.
VERTICAL CALC_Side_3	Illuminance	Fc	0.47	2.5	0.0	N.A.
VERTICAL CALC_Side_4	Illuminance	Fc	0.11	0.3	0.0	N.A.
VERTICAL CALC_Side_5	Illuminance	Fc	0.06	0.3	0.0	N.A.
VERTICAL CALC_Side_6	Illuminance	Fc	0.09	0.2	0.0	N.A.
VERTICAL CALC_Side_7	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_8	Illuminance	Fc	0.10	0.1	0.1	1.00
VERTICAL CALC_Side_9	Illuminance	Fc	0.10	0.1	0.1	1.00
PARKING LOT - DRIVE THRU	Illuminance	Fc	1.88	17.1	0.1	171.00

NOTE:  
EXTERIOR LIGHTING WILL BE CONTROLLED BY ASTRONOMICAL TIME CLOCK AND PHOTOCELL TO MET THE REQUIRED L22 LIGHTING REQUIREMENTS



PARKING LOT - DRIVE THRU  
Illuminance (Fc)  
Average = 1.88  
Maximum = 17.1  
Minimum = 0.1  
Max/Min Ratio = 171.00



CB ANDERSON ARCHITECTS LLC  
7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2911  
Fax 782-5624

CLIENT  
Wilsonville Devco LLC  
c/o Integrated Investment Partners  
9000 SW 84th  
Portland, OR 97207  
(503) 688-9600

PROJECT  
Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE  
PHOTOMETRIC SITE PLAN

REVISIONS  
△  
△  
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STAMP

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATING AGENCIES

DATE: December 5, 2012  
BLDG. SUBMITTAL DATE: ①  
DATE: ②  
BID DATE: ③  
CWE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

SE1.0

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14  
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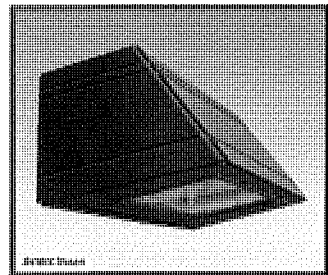
FILE NAME





# TRAPEZOIDAL WALL SCONCE

## TYPE AC-1



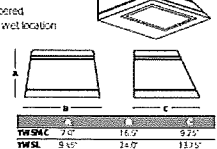
The TWS Series trapezoidal wall sconce offers unique styling and maximum versatility with two sizes, multiple light sources, and a variety of finishes. The downlight-only design features a maximum cutoff distribution with exceptional horizontal illumination.

### Fixture Specifications

#### FEATURES

- Durable cast aluminum housing\*
- Integrated design eliminates high angle brightness
- Luminaires finished in weatherproof powder coat paint
- Completely sealed, flat tempered glass lens suitable for use in wet location
- Ships complete with lamp

#### DIMENSIONS



#### ORDERING INFORMATION

SMPLX CATALOG NUMBER

TWSK	XXXXXX	WD	XX	XXX
Series	Mounting Source	Material	Finish	Voltage

Finish	Material	Mounting Source	Voltage
91 (Black)	93 (White)	95 (Dark Green)	96 (Galanvanized)
BR41 (Powder Coat Rust)	BR40 (Black Texture)	GN20 (Powder Coat Patina)	
29, 66, 82, 90, 92, 94, 97, 99, 100, 103, 104, 105, 110, 112, 113, 114, 115, 117, 118, 119, 120, 127, 128, 129, 133, 134, 135, 136, 98, 101, 102, 137, 138, 139, 140, 121, 122, 123, 124, 125, 126, 24, 44, 48, 49			



Performance Designed Lighting Products  
1665 Johnson Drive • Buffalo Grove, IL 60089 • Tel: (847) 544-4848 • Fax: (847) 275-0627 • Email: sales@searlight.com

#### ORDERING INFORMATION

Product Family	Fixture Height	Lamp	Lamp Type	Voltage	Color	Options
BRL-36-100-MP	36"	100W	MP	100-120V	Black	None
BRL-36-100-HPS	36"	100W	HPS	100-120V	Black	None
BRL-36-100-LED	36"	100W	LED	100-120V	Black	None

1. All MP lamps are T8 compatible with CFL or LED lamps. All HPS lamps are T8 compatible with LED lamps.
2. All CFL lamps are T8 compatible with CFL or LED lamps. All LED lamps are T8 compatible with CFL or LED lamps.
3. All lamps are T8 compatible with CFL or LED lamps.
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18. All lamps are T8 compatible with CFL or LED lamps.
19. All lamps are T8 compatible with CFL or LED lamps.
20. All lamps are T8 compatible with CFL or LED lamps.

COOPER Lighting  
Customer First Center 1121 Highway 74 South, Peachtree City, GA 30089 770 488 4500 FAX 770 488 4501

#### DESCRIPTION

The McGraw-Edison BSL/BRL Bollard Series, available in heights from 24" to 48", has clear, clean lines which blend with any architectural setting. Constructed of seamless, heavy-duty aluminum and finished with a tough polyester powder coat finish, the McGraw-Edison Bollard Series is guaranteed to resist all external contaminants. U.L. 150a listed and CSA certified for wet locations.

Bollards are designed for walkways, entrances, drives and other small area lighting applications where low mounting heights are desirable.

#### SPECIFICATION FEATURES

**Construction**  
TOP: Rugged, minimum 6/32" thick cast aluminum top cap secured via a concealed stainless steel allen screw with twist removal mechanism for lamp access. Flow through ventilation assure cool to the touch top. LOWER CASE: Cast Aluminum Lower Case provides sharp cutoff delivering no direct light above eye. Lower case secured to the shaft via tamper stainless steel rods and fasteners. LOWER HOUSING: Nominal 1/8" thick aluminum extruded housing. Bollard housing is secured to the base with threaded, counter-sunk screws for smooth, uncluttered appearance. BASE: Rugged cast aluminum. Completely concealed.

**Electrical**  
MID: High Power Factor ballast for 20'F starting. CFL Electronic Ballast for 0'F starting. Product is factory mounted to the base. Quick disconnects provided between lamp and electrical assembly. Metal Halide and High Pressure Sodium lamp sources up to 100W and up to 40V Compact Fluorescent sources.

**Optical**  
LAMP ENCLOSURE: One piece tempered glass with internal flutes for even dissemination of illumination. Decorative colored glass optional. Globe is fully gasketed via EPDM material. Socket is porcelain, medium base for HID Lamp sources and

### McGRAW-EDISON

Product #	Type
Project	DD
Comments	Date
Prepared by	

polycarbonate PBT GX24q 3/4 4 base for compact fluorescent lamps.

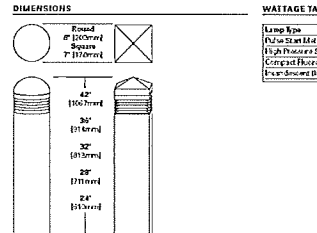
Mounting  
Base mounts onto foundation with three (3) 1/2" x 12 1/2" anchor bolts on a 4" dia. ball ends in centrally located 2 7/8" x 3 1/2" wide entrance opening provided.

Finish  
Premium fade and abrasion resistant, TGIC Polyester Powder Coat Finish. Standard colors are Black, Grey, Bronze, White, Dark Platinum and Graphite Metallic. Other finish colors available including all RAL matches.

#### BSL/BRL BOLLARD

26 - 100W  
Pulse Start Metal Halide  
High Pressure Sodium  
Compact Fluorescent  
Incandescent  
PATHWAY LUMINAIRE

#### DARK SKY COMPLIANT



Lamp Type	Footcandle @ 10'	Footcandle @ 20'	Footcandle @ 30'
100W MP	1.0	0.25	0.11
100W HPS	1.0	0.25	0.11
100W LED	1.0	0.25	0.11

COOPER Lighting  
Customer First Center 1121 Highway 74 South, Peachtree City, GA 30089 770 488 4500 FAX 770 488 4501

**H-91514-B**  
Mars Collection

Job Name:  
Type: TYPE AC-2  
Quantity:

**FINISH** - Offered in exceptional finishes, comprised of: polyester/polished powder coat, baking enamel liquid, raw metal, or galvanized finishes.

Standard Finishes are: 91 (Black), 93 (White), 95 (Dark Green), 96 (Galanvanized), BR41 (Powder Coat Rust), BR40 (Black Texture), GN20 (Powder Coat Patina).

Upgraded Finishes are: 29, 66, 82, 90, 92, 94, 97, 99, 100, 103, 104, 105, 110, 112, 113, 114, 115, 117, 118, 119, 120, 127, 128, 129, 133, 134, 135, 136, 98, 101, 102, 137, 138, 139, 140, 121, 122, 123, 124, 125, 126, 24, 44, 48, 49.

For interior finish of fixture refer to color chart on pages 344-348.

**MOUNTING** - Wall Mount.

**ACCESSORIES** - CAL (Clear Acrylic Layer), BAL (Blue Acrylic Layer), RAL (Red Acrylic Layer) and GNAL (Green Acrylic Layer) available.

**SOCKETS/LAMPS** - Available in:  
Incandescent  
- rated 100 watt max/120 volt, medium base.  
Compact Fluorescent (CFL)  
- rated 131W/2632 watt max/120/277 volt, GX240 base.  
Metal Halide (MH)  
- rated 35/50/70/100 watt max/120/208/240/277 volt, medium base, 4KV socket.  
High Pressure Sodium (HPS)  
- rated 50/70/100 watt max/120/277 volt, medium base.  
Light-Emitted Diode (LED)  
- rated 20w LED max. See LED specifications for complete part number.

**LENS OPTION** - CLR (Clear), RIB (Clear Ribbed), FR (Frosted), RD (Red), BL (Blue), GRN (Green), AM (Amber) and AMC (Amber Crackle) available.

**UL LISTED**

MADE IN THE U.S.A. Suitable for wet location.

**PHOTOMETRICS**

**Footcandle Table**  
Footcandle values are based on a 10' mounting height and are based on the footcandle values of each fixture's distribution curve in units of mounting height.

Mounting Height	Footcandle @ 10'	Footcandle @ 20'	Footcandle @ 30'
10'	1.0	0.25	0.11
20'	1.0	0.25	0.11
30'	1.0	0.25	0.11

**ANCHOR BOLT TEMPLATE (NOT TO SCALE)**

**ORDERING INFORMATION**

Product Family	Fixture Height	Lamp	Lamp Type	Voltage	Color	Options
BRL-36-100-MP	36"	100W	MP	100-120V	Black	None
BRL-36-100-HPS	36"	100W	HPS	100-120V	Black	None
BRL-36-100-LED	36"	100W	LED	100-120V	Black	None

COOPER Lighting  
Customer First Center 1121 Highway 74 South, Peachtree City, GA 30089 770 488 4500 FAX 770 488 4501

**Wilsonville Devco LLC**  
c/o Integrated Treatment Partners  
1000 NE Oregon Street  
Portland, OR 97207  
(503) 688-9600

**Boones Ferry Pointe**  
5095 SW 95th Ave & Commerce Circle  
Wilsonville, Oregon

**REVISIONS**

NO.	DATE	DESCRIPTION
1		
2		
3		

DATE: December 5, 2012  
ELOG SUBMITTAL DATE:  
BID DATE:  
CNE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

**SE1.3**

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-114

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ADDITIONAL SHEETS: 22-114-01, 22-114-02, 22-114-03, 22-114-04, 22-114-05, 22-114-06, 22-114-07, 22-114-08, 22-114-09, 22-114-10, 22-114-11, 22-114-12, 22-114-13, 22-114-14, 22-114-15, 22-114-16, 22-114-17, 22-114-18, 22-114-19, 22-114-20, 22-114-21, 22-114-22, 22-114-23, 22-114-24, 22-114-25, 22-114-26, 22-114-27, 22-114-28, 22-114-29, 22-114-30, 22-114-31, 22-114-32, 22-114-33, 22-114-34, 22-114-35, 22-114-36, 22-114-37, 22-114-38, 22-114-39, 22-114-40, 22-114-41, 22-114-42, 22-114-43, 22-114-44, 22-114-45, 22-114-46, 22-114-47, 22-114-48, 22-114-49, 22-114-50, 22-114-51, 22-114-52, 22-114-53, 22-114-54, 22-114-55, 22-114-56, 22-114-57, 22-114-58, 22-114-59, 22-114-60, 22-114-61, 22-114-62, 22-114-63, 22-114-64, 22-114-65, 22-114-66, 22-114-67, 22-114-68, 22-114-69, 22-114-70, 22-114-71, 22-114-72, 22-114-73, 22-114-74, 22-114-75, 22-114-76, 22-114-77, 22-114-78, 22-114-79, 22-114-80, 22-114-81, 22-114-82, 22-114-83, 22-114-84, 22-114-85, 22-114-86, 22-114-87, 22-114-88, 22-114-89, 22-114-90, 22-114-91, 22-114-92, 22-114-93, 22-114-94, 22-114-95, 22-114-96, 22-114-97, 22-114-98, 22-114-99, 22-114-100

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**CB ANDERSON ARCHITECTS PLLC**

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-782-2911  
Fax 782-5624

CLIENT

**Wilsonville Devco LLC**  
c/o Integrated Investment Partners  
PO Box 916  
Portland, OR 97207  
(503) 688-8600

PROJECT

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

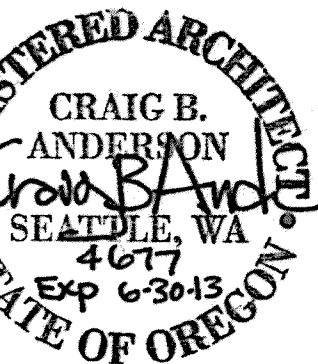
DRAWING TITLE

**ARCHITECTURAL SITE PLAN**

REVISIONS

- △
- △
- △
- △
- △

STAMP



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DATE: Jan-2-2013

BLDG. SUBMITTAL DATE: ①

DATE: ②

BID DATE: ③

CHE NO.:

ARCH. NO.: 1199

SCALE: AS SHOWN

SHEET

**A1.0**

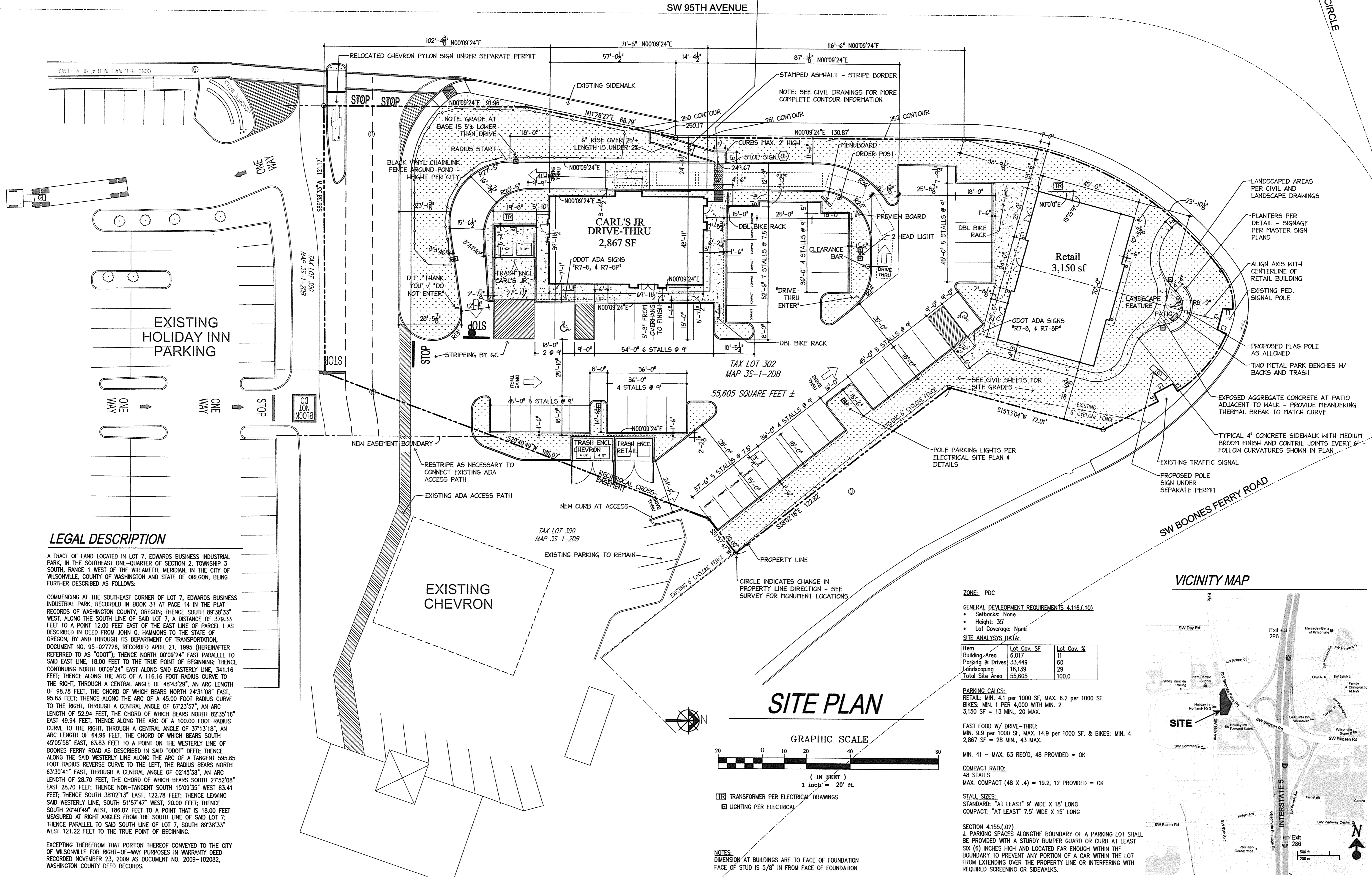
CGK Foods Conversion Based on CKE Prototype Version 22-14

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**SITE KEY NOTE:**

- ① PEDESTRIAN CROSSING STOP SIGN TO BE CROSSING PANEL WITH FIXED BASE BY SAFETY SIGN CO., CROSSING BLADE SIGN ITEM# Y4936 / MUTCD# RI-6A

ADA PATH TO PUBLIC SIDEWALK ALONG EXISTING CONTOURS W. MAX. 2% SLOPE ANY DIRECTION AND 5'X5' TURNING CLEARANCES AT ENDS - KEY SPOT ELEVATIONS SHOWN - REFER TO CIVIL FOR ADDITIONAL ELEV.



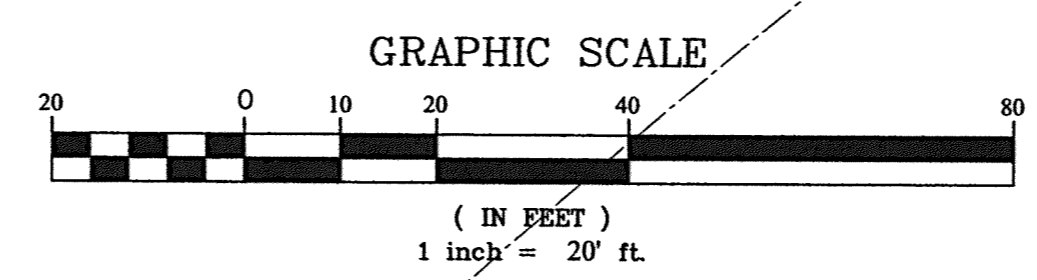
**LEGAL DESCRIPTION**

A TRACT OF LAND LOCATED IN LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, IN THE SOUTHEAST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, IN THE CITY OF WILSONVILLE, COUNTY OF WASHINGTON AND STATE OF OREGON, BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF LOT 7, EDWARDS BUSINESS INDUSTRIAL PARK, RECORDED IN BOOK 31 AT PAGE 14 IN THE PLAT RECORDS OF WASHINGTON COUNTY, OREGON; THENCE SOUTH 89°38'33" WEST, ALONG THE SOUTH LINE OF SAID LOT 7, A DISTANCE OF 379.33 FEET TO A POINT 12.00 FEET EAST OF THE EAST LINE OF PARCEL 1 AS DESCRIBED IN DEED FROM JOHN Q. HAMMONS TO THE STATE OF OREGON, BY AND THROUGH ITS DEPARTMENT OF TRANSPORTATION, DOCUMENT NO. 95-027726, RECORDED APRIL 21, 1995 (HEREINAFTER REFERRED TO AS "0001"); THENCE NORTH 00°09'24" EAST PARALLEL TO SAID EAST LINE, 18.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 00°09'24" EAST ALONG SAID EASTERLY LINE, 341.16 FEET; THENCE ALONG THE ARC OF A 116.16 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 48°43'29", AN ARC LENGTH OF 98.78 FEET, THE CHORD OF WHICH BEARS NORTH 24°31'08" EAST, 95.83 FEET; THENCE ALONG THE ARC OF A 45.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 67°23'57", AN ARC LENGTH OF 52.94 FEET, THE CHORD OF WHICH BEARS NORTH 82°35'16" EAST 49.94 FEET; THENCE ALONG THE ARC OF A 100.00 FOOT RADIUS CURVE TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 37°13'18", AN ARC LENGTH OF 64.96 FEET, THE CHORD OF WHICH BEARS SOUTH 45°05'58" EAST, 63.83 FEET TO A POINT ON THE WESTERLY LINE OF BOONES FERRY ROAD AS DESCRIBED IN SAID "0001" DEED; THENCE ALONG THE SAID WESTERLY LINE ALONG THE ARC OF A TANGENT 595.65 FOOT RADIUS REVERSE CURVE TO THE LEFT, THE RADIUS BEARS NORTH 63°30'41" EAST, THROUGH A CENTRAL ANGLE OF 02°45'39", AN ARC LENGTH OF 28.70 FEET, THE CHORD OF WHICH BEARS SOUTH 27°52'08" EAST 28.70 FEET; THENCE NON-TANGENT SOUTH 15°09'35" WEST 83.41 FEET; THENCE SOUTH 38°02'13" EAST, 122.78 FEET; THENCE LEAVING SAID WESTERLY LINE, SOUTH 51°57'47" WEST, 20.00 FEET; THENCE SOUTH 20°40'49" WEST, 186.07 FEET TO A POINT THAT IS 18.00 FEET MEASURED AT RIGHT ANGLES FROM THE SOUTH LINE OF SAID LOT 7; THENCE PARALLEL TO SAID SOUTH LINE OF LOT 7, SOUTH 89°38'33" WEST 121.22 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION THEREOF CONVEYED TO THE CITY OF WILSONVILLE FOR RIGHT-OF-WAY PURPOSES IN WARRANTY DEED RECORDED NOVEMBER 23, 2009 AS DOCUMENT NO. 2009-102082, WASHINGTON COUNTY DEED RECORDS.

**SITE PLAN**



TRANSFORMER PER ELECTRICAL DRAWINGS  
LIGHTING PER ELECTRICAL

NOTES:  
DIMENSION AT BUILDINGS ARE TO FACE OF FOUNDATION  
FACE OF STUD IS 5/8" IN FROM FACE OF FOUNDATION

ZONE: PDC

GENERAL DEVELOPMENT REQUIREMENTS 4.116.(10)

- Setbacks: None
- Height: 35'
- Lot Coverage: None

SITE ANALYSIS DATA:

Item	Lot Cov. SF	Lot Cov. %
Building Area	6,017	11
Parking & Drives	33,449	60
Landscaping	16,139	29
Total Site Area	55,605	100.0

PARKING CALCS:  
RETAIL: MIN. 4.1 per 1000 SF, MAX. 6.2 per 1000 SF.  
BIKES: MIN. 1 per 4,000 with MIN. 2, 3,150 SF = 13 MIN., 20 MAX.

FAST FOOD W/ DRIVE-THRU:  
MIN. 9.9 per 1000 SF, MAX. 14.9 per 1000 SF. & BIKES: MIN. 4, 2,867 SF = 28 MIN., 43 MAX.

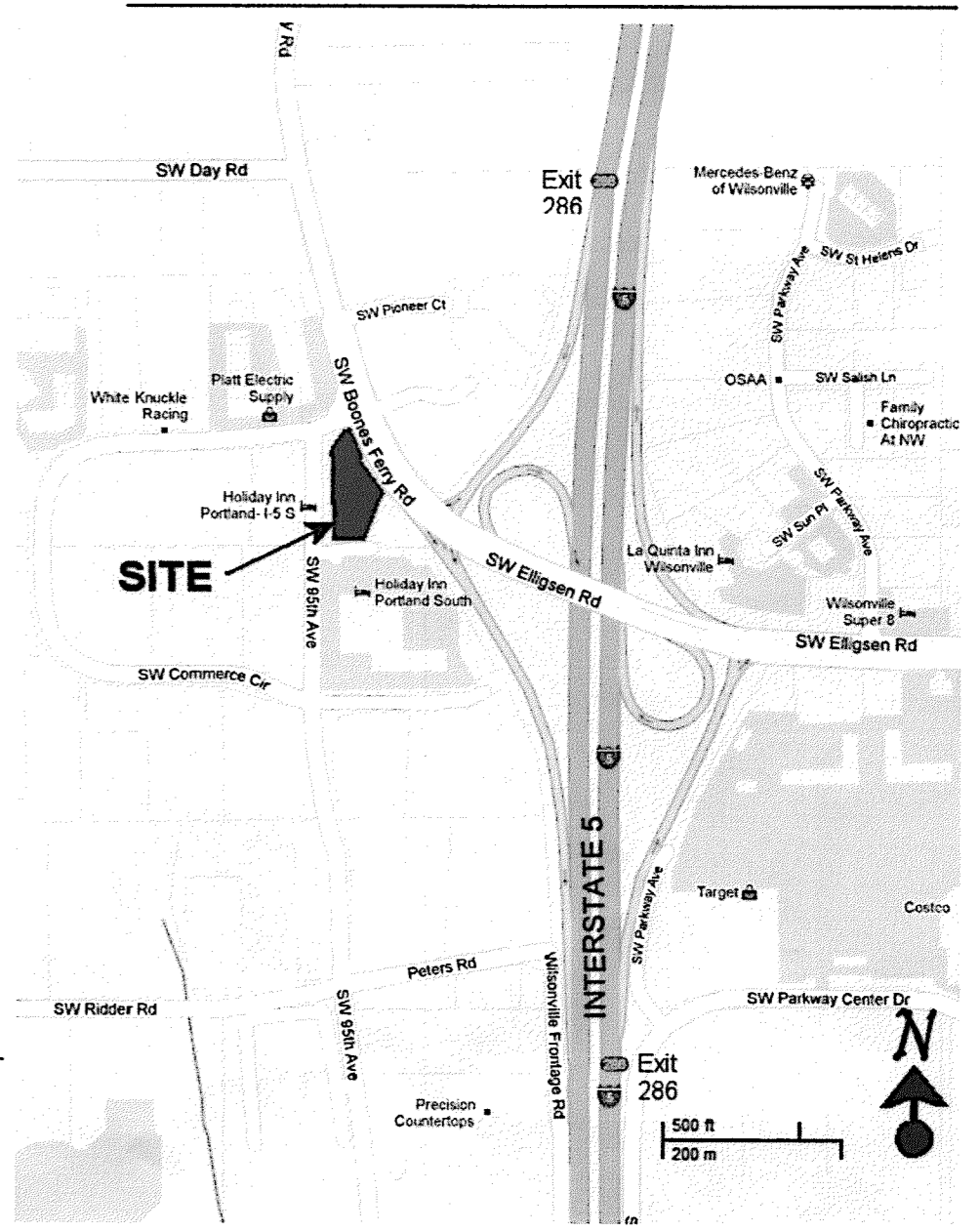
MIN. 41 - MAX. 63 REQ'D, 48 PROVIDED = OK

COMPACT RATIO:  
48 STALLS  
MAX. COMPACT (48 X 4) = 19.2, 12 PROVIDED = OK

STALL SIZES:  
STANDARD: "AT LEAST" 9' WIDE X 18' LONG  
COMPACT: "AT LEAST" 7.5' WIDE X 15' LONG

SECTION 4.155.(02)  
J. PARKING SPACES ALONG THE BOUNDARY OF A PARKING LOT SHALL BE PROVIDED WITH A STURDY BUMPER GUARD OR CURB AT LEAST SIX (6) INCHES HIGH AND LOCATED FAR ENOUGH WITHIN THE BOUNDARY TO PREVENT ANY PORTION OF A CAR WITHIN THE LOT FROM EXTENDING OVER THE PROPERTY LINE OR INTERFERING WITH REQUIRED SCREENING OR SIDEWALKS.

**VICINITY MAP**



FILE NAME:





CB ANDERSON ARCHITECTS PLLC

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-782-2911  
Fax 782-5624

CLIENT

Wilsonville Devco LLC  
c/o Integrated Investment Partners  
Portland, OR 97207  
(503) 688-9600

PROJECT

Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE

PLYWOOD BACKING PLAN

REVISIONS

△	
△	
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△	

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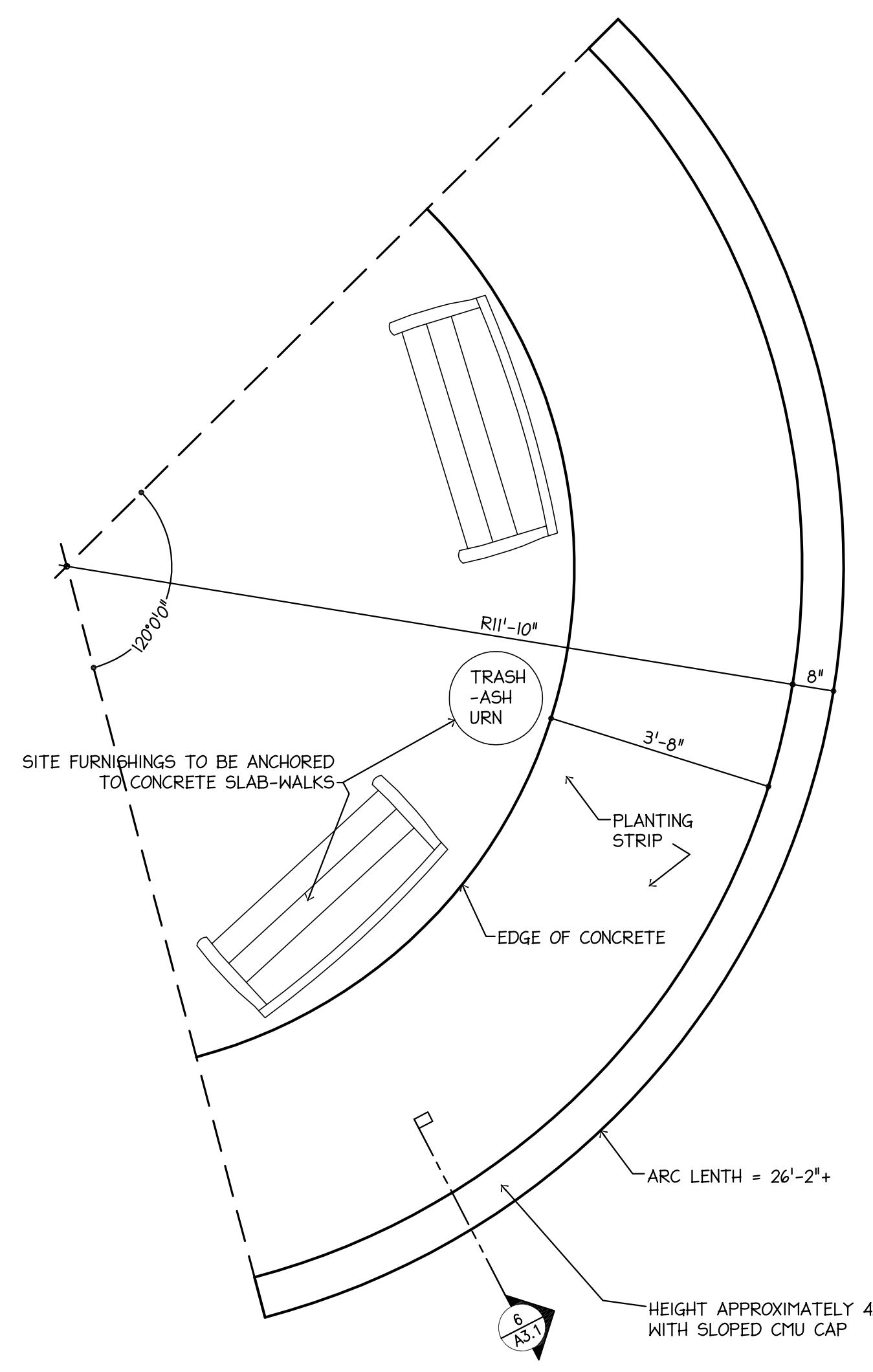
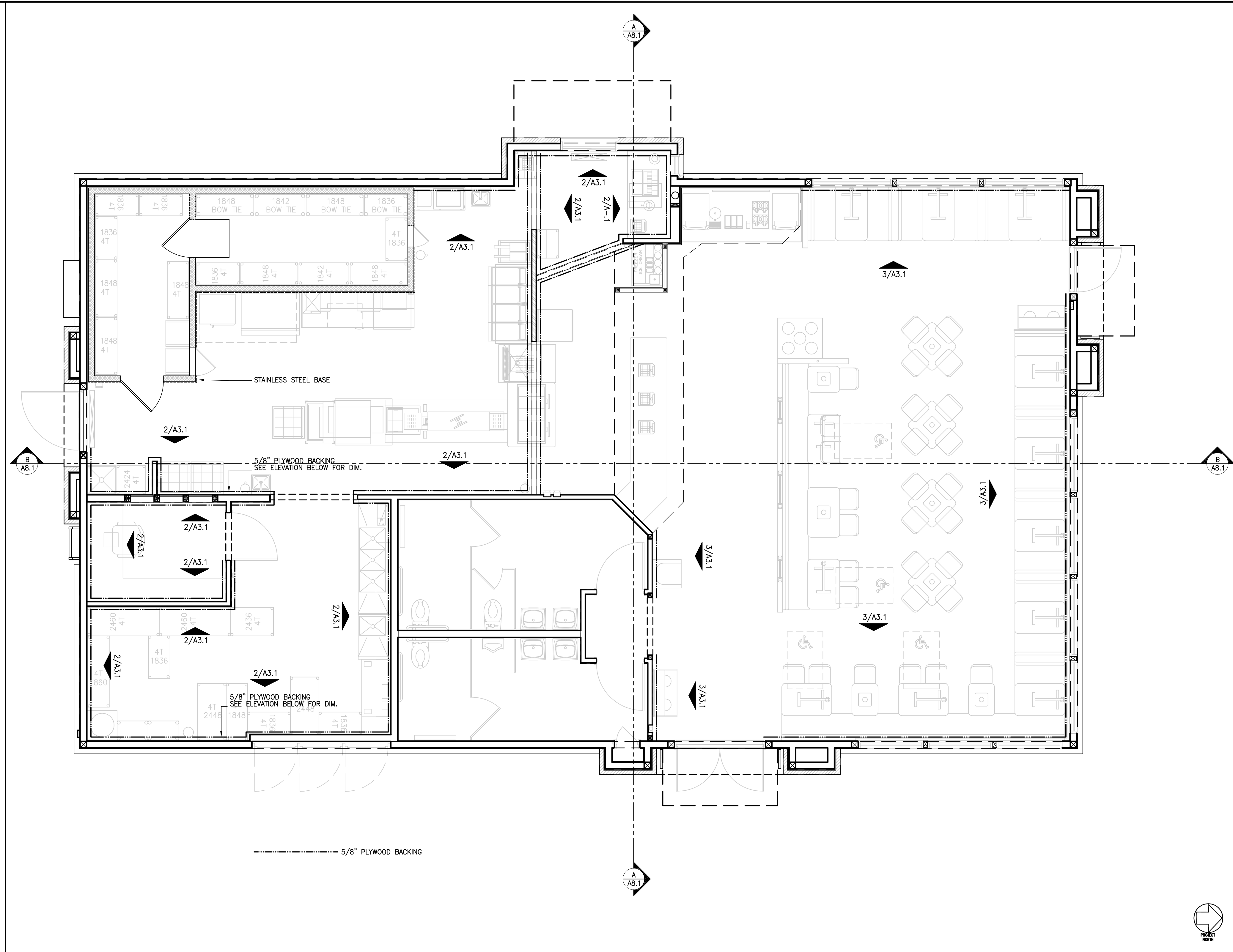
CKE NO.:

ARCH. NO.: 1199

SCALE: AS SHOWN

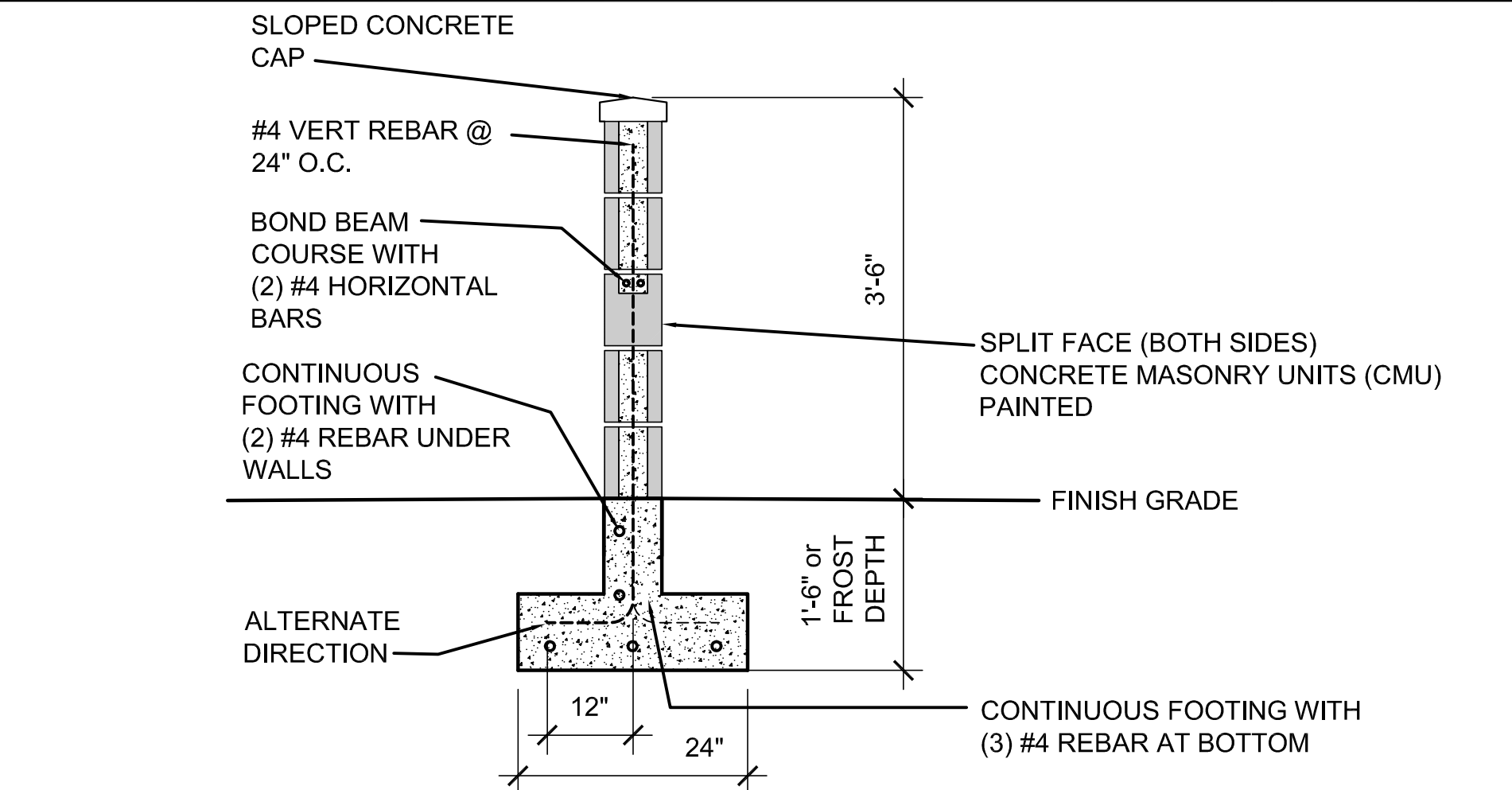
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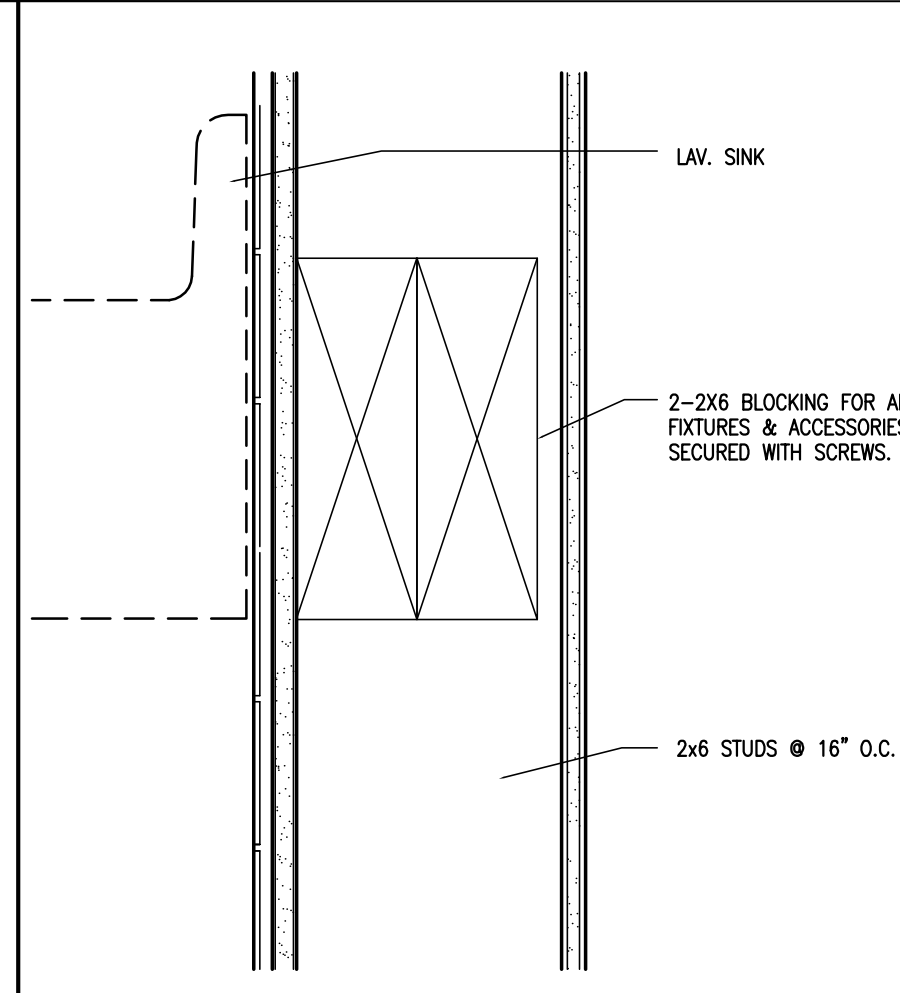


ENLARGED PLAZA WALL PLAN SCALE: 1/2"=1'-0" 5

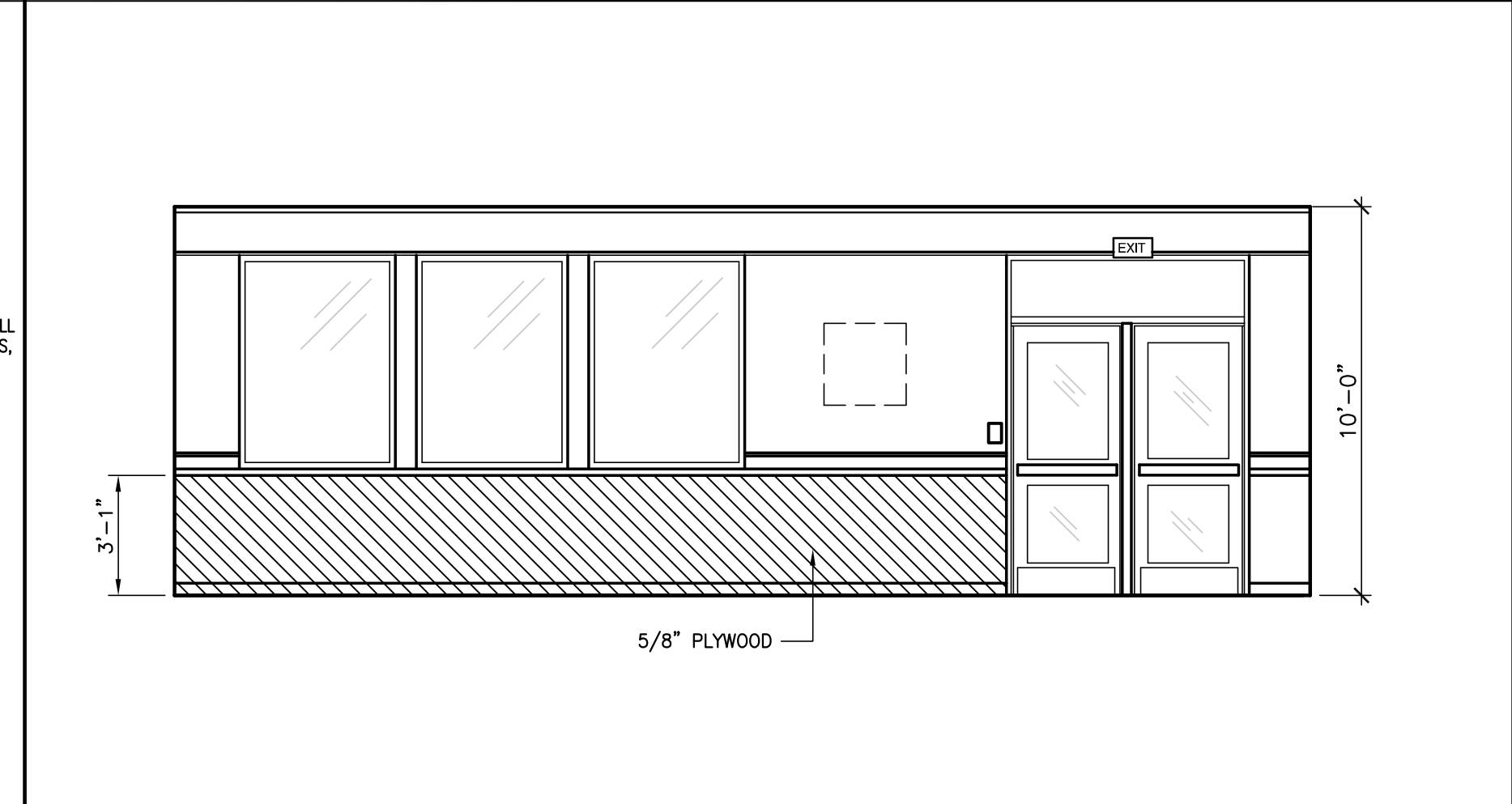
PLYWOOD SHEATHING PLAN SCALE: 1/4"=1'-0" 1



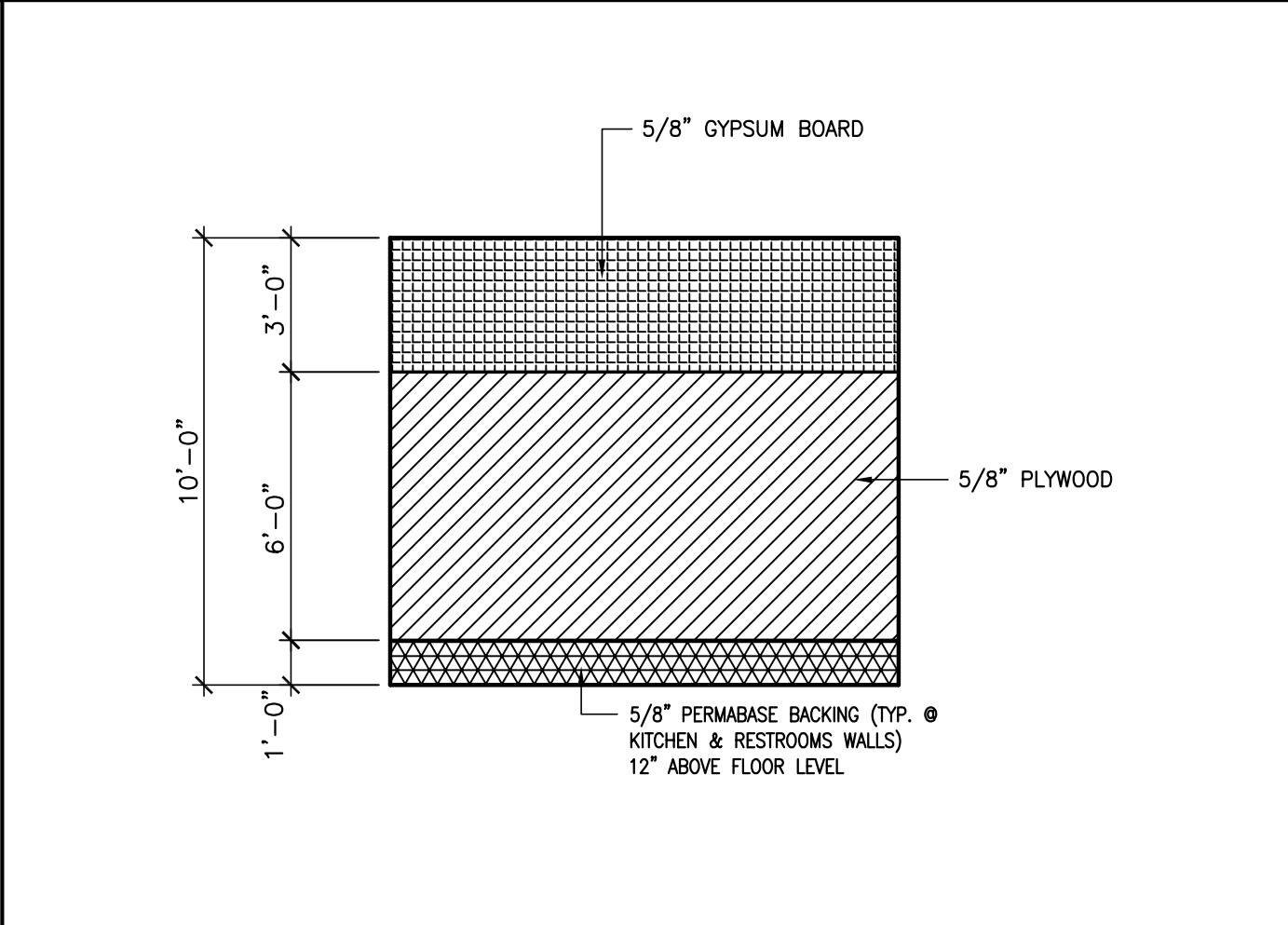
PLAZA WALL SECTION SCALE: 3/4"=1'-0" 6



LAVATORY BACKING SCALE: 3"=1'-0" 4



TYPICAL WALL - PUBLIC AREA SCALE: 1/4"=1'-0" 3



TYPICAL WALL - KITCHEN AND EMPLOYEE AREA SCALE: 1/4"=1'-0" 2

FILE NAME:

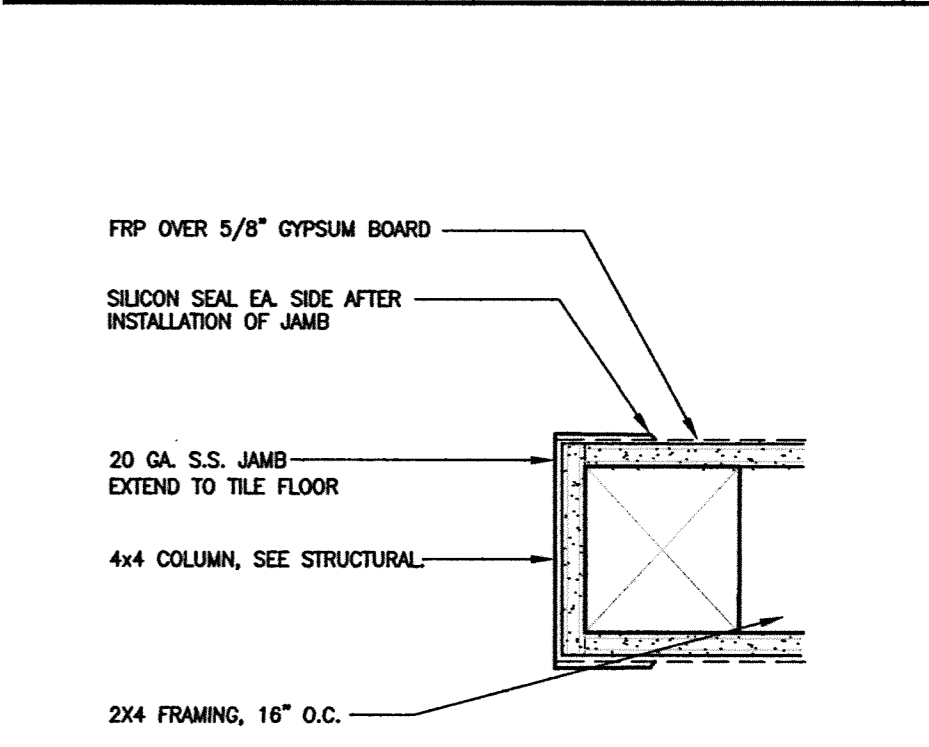
KKG Foods CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

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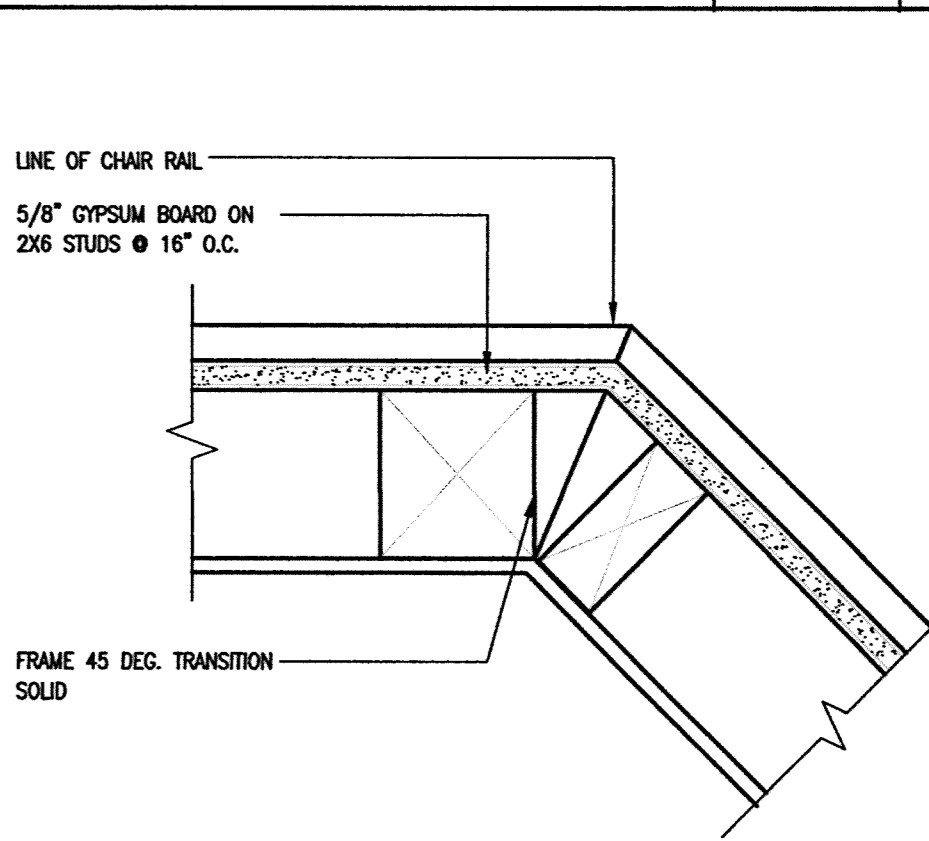


- REFER TO "K" DRAWINGS FOR KITCHEN AND SEATING EQUIPMENT.
- REFER TO STRUCTURAL DRAWINGS FOR ALL POSTS INDICATED IN FRAMING WALLS AND FOR LOCATION OF ALL SHEAR PANELS.
- ALL EXTERIOR WALLS AND CEILING SPACES TO RECEIVE THERMAL INSULATION. INTERIOR WALLS AROUND RESTROOMS AND RESTROOM VEST. TO RECEIVE SOUND INSULATION. RE: SPEC.'S
- FOR LOCATION OF FLOOR SINKS AND FLOOR DRAINS SEE SHEET P3.2.
- ALL FRAMING DIMENSIONS ARE TO ROUGH FRAMING, AND DO NOT INCLUDE ANY FINISH MATERIALS UNLESS OTHERWISE NOTED. SEE SHEET A2.0 FOR FRAMING PLAN.
- ALL EXTERIOR WALLS SHALL BE OF 2x6 STUDS. ALL INTERIOR WALLS SHALL BE OF 2x4 STUDS UNLESS OTHERWISE NOTED.
- APPLY BORIC ACID ON TOP OF THE MUD SILL OR BOTTOM PLATE, PRIOR TO CLOSING ALL WALLS.
- PROVIDE BACKING FOR RESTROOM ACCESSORIES, DOOR STOPS, EXP. JOINTS, CABINETS, ETC. SEE SHEET A3.1 FOR LOCATION OF PLYWOOD BACKING.
- INSTALL OCCUPANT LOAD SIGN PER 18/A11.0
- PROVIDE MASTIC OR APPROVED SEALANT UNDER ALL EXT. PLATES
- XX REFER A13.0 FOR INTERIOR FINISH SCHEDULE.
- X REFER A13.0 FOR WINDOW SCHEDULE.
- 1 REFER A13.0 FOR DOOR SCHEDULE.

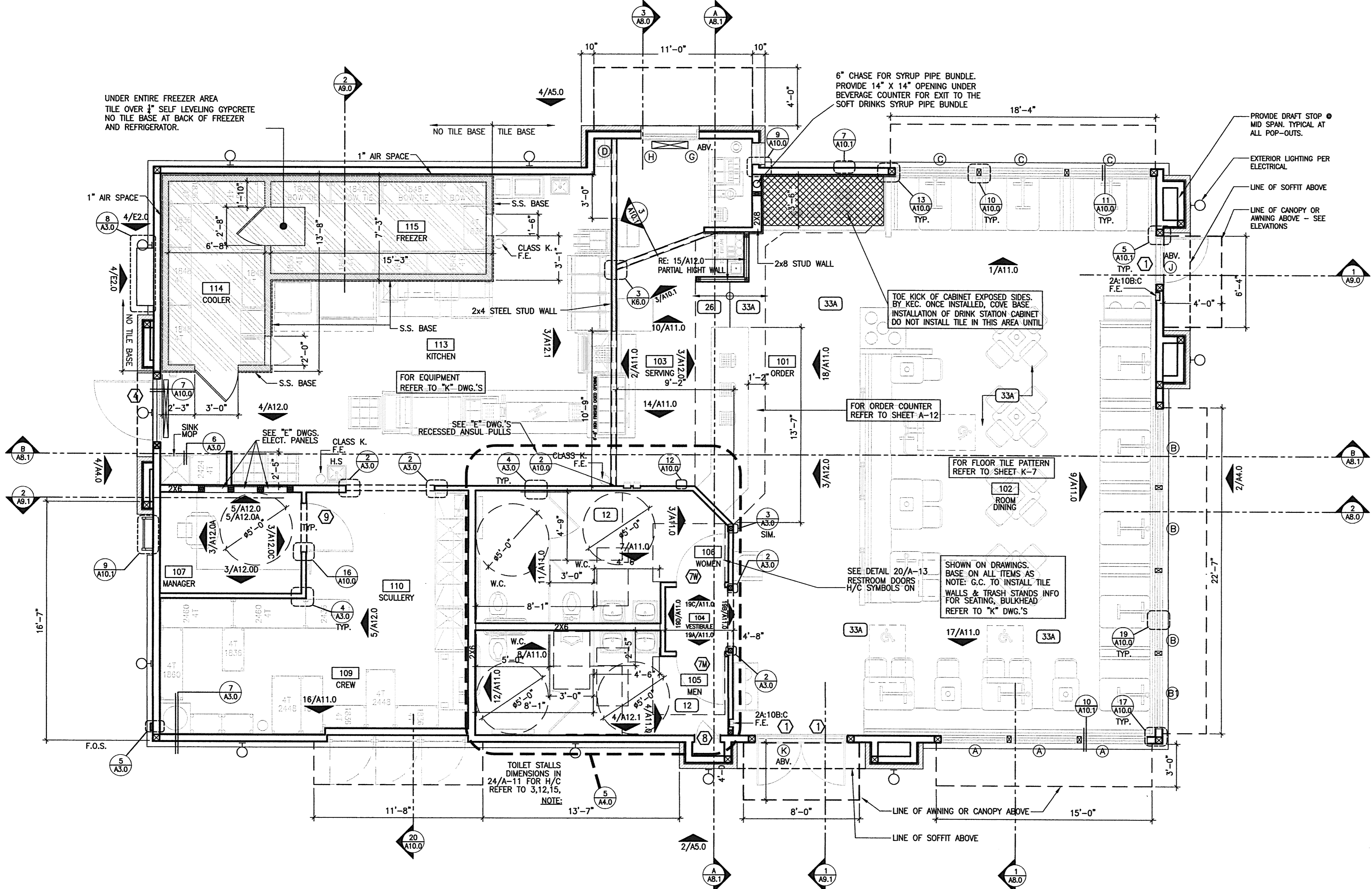
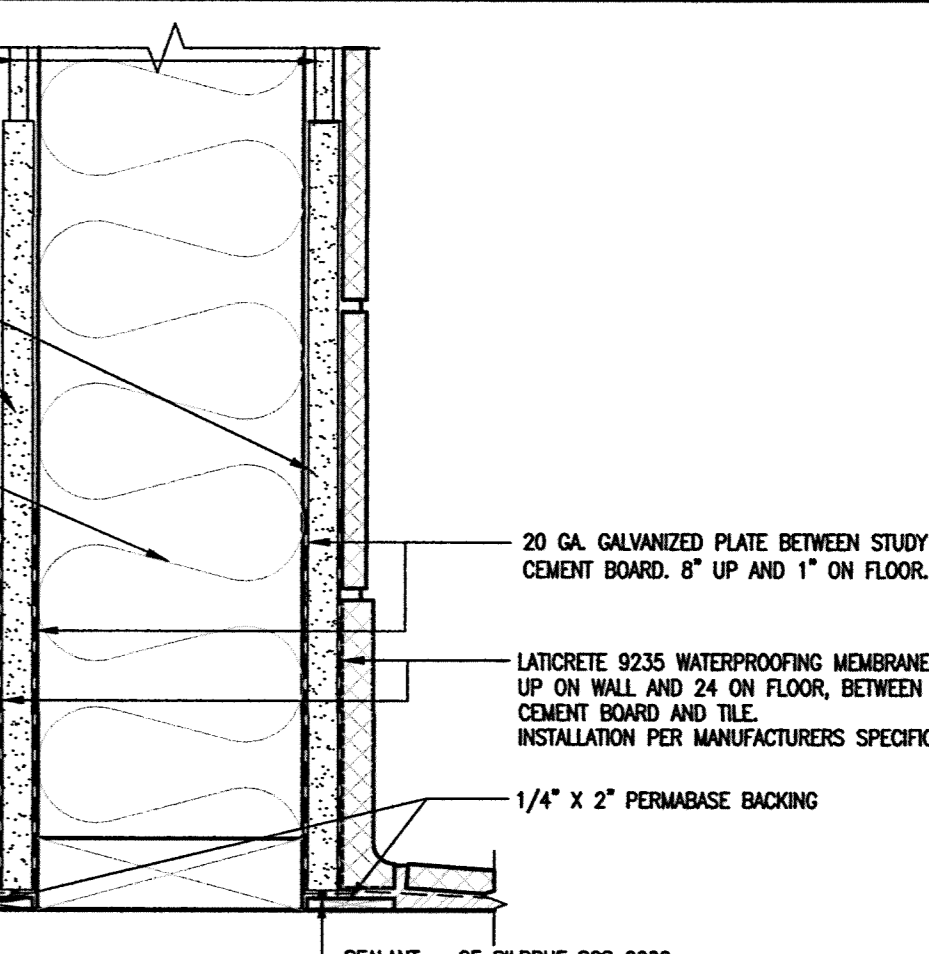
**NOTES**



**KITCHEN WALL END CAP**

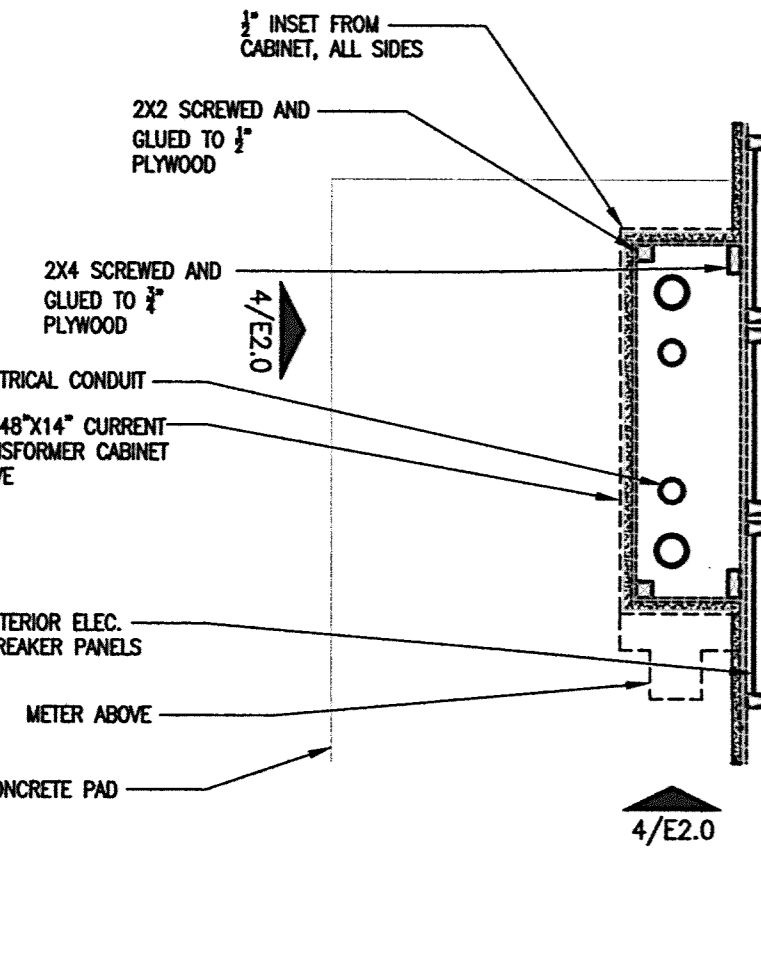


**CHAIR RAIL CORNER**



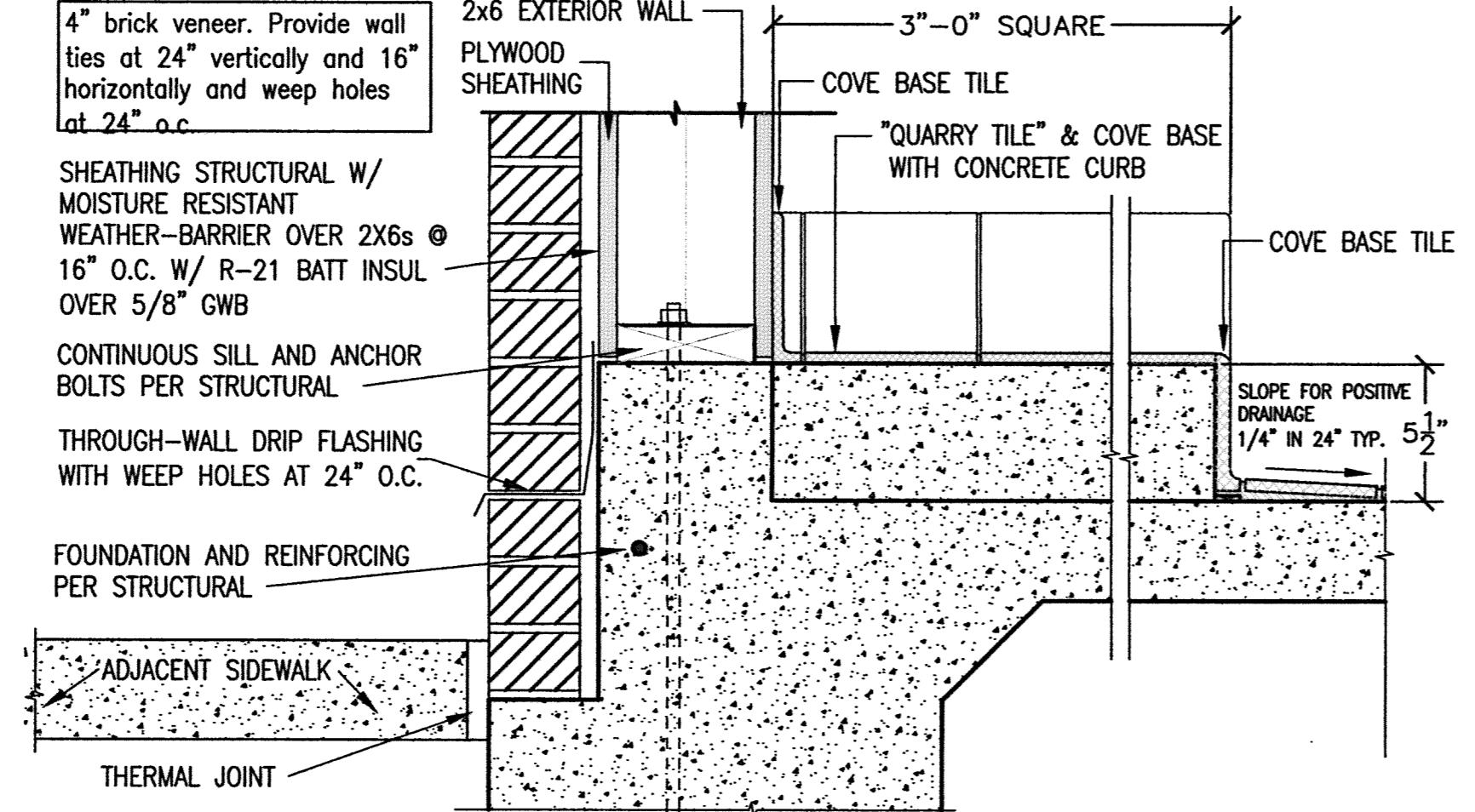
**FLOOR PLAN**

SCALE: 1/4"=1'-0" 7

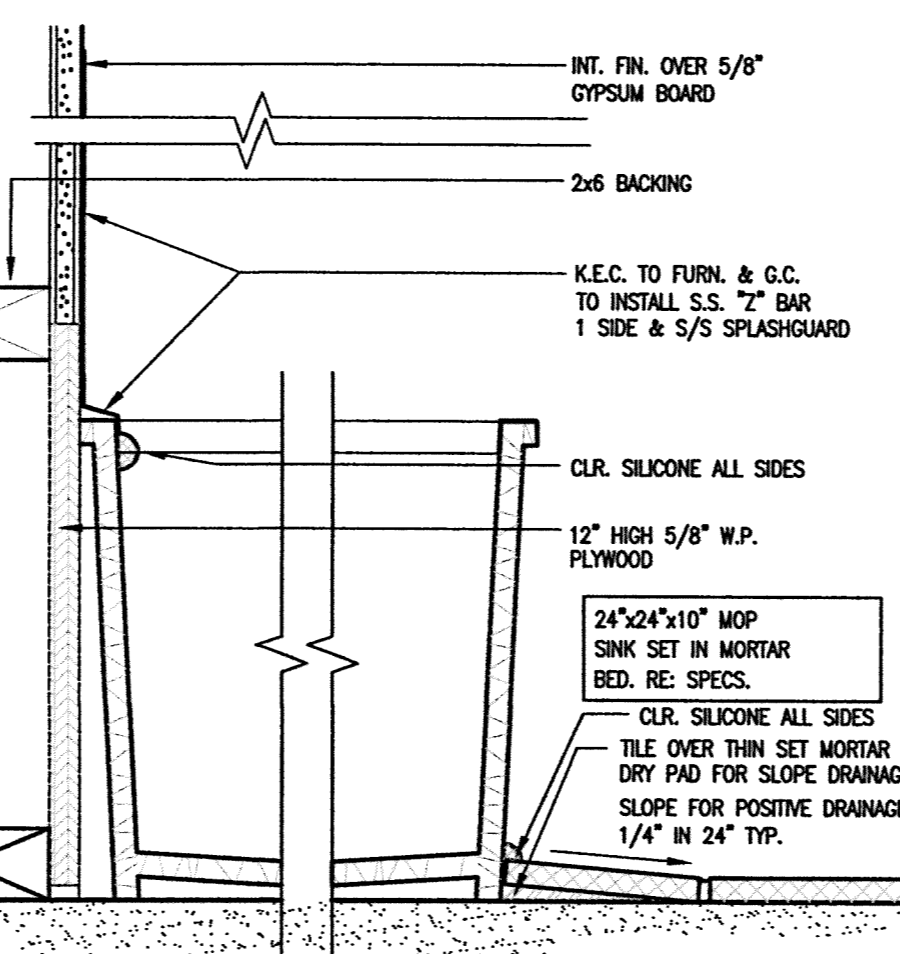


**CURRENT TRANSFORMER CABINET DETAIL**

SCALE: 1/2" = 1'-0" 8

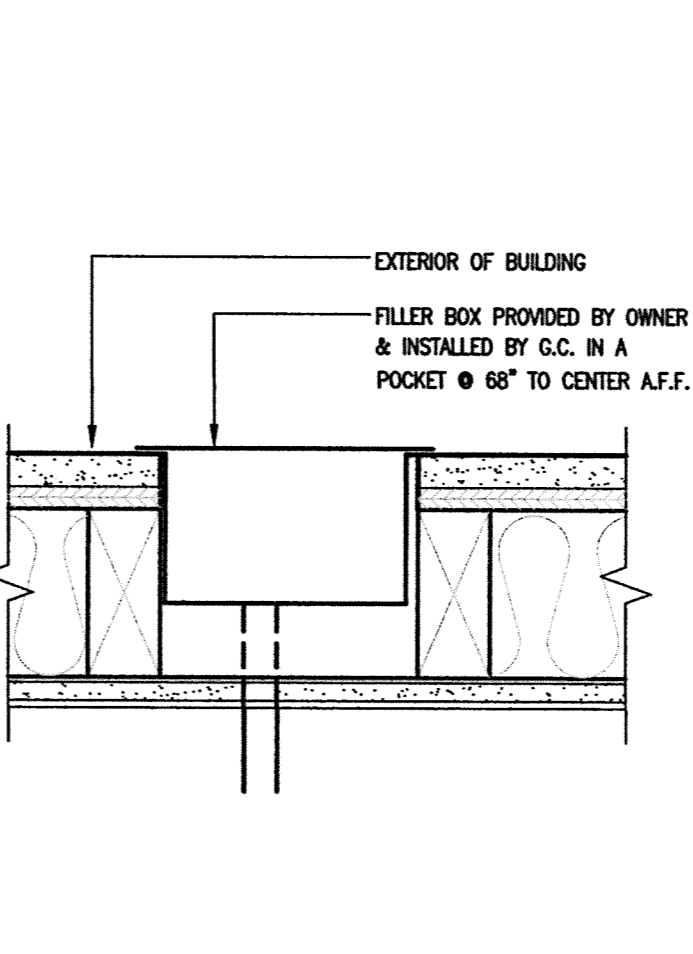


**CURB DETAIL**



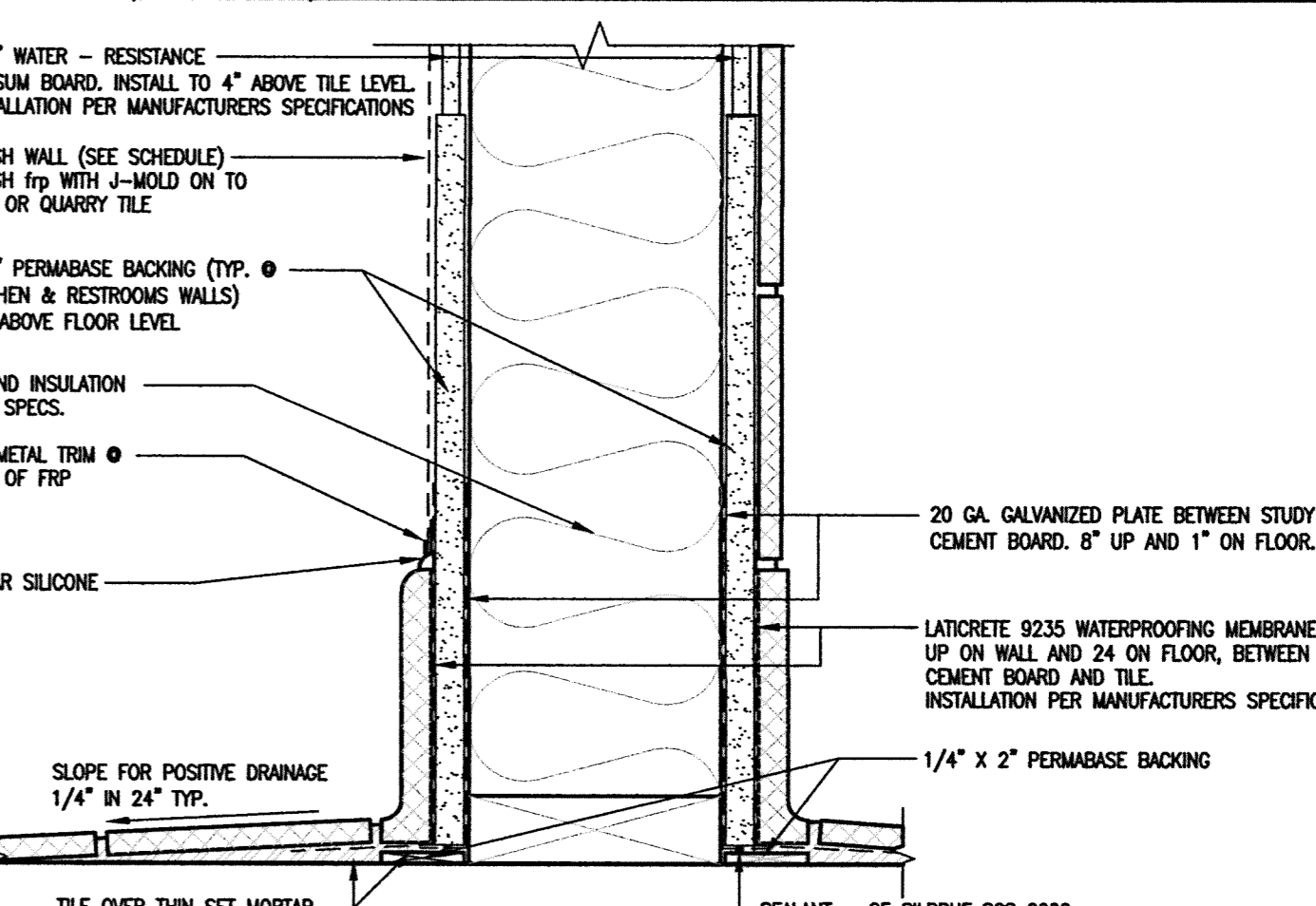
**MOP SINK**

SCALE: 3"=1'-0" 6



**CO2 FILLER BOX**

SCALE: 3"=1'-0" 5



**TILE BASE IN KITCHEN / RESTROOMS**

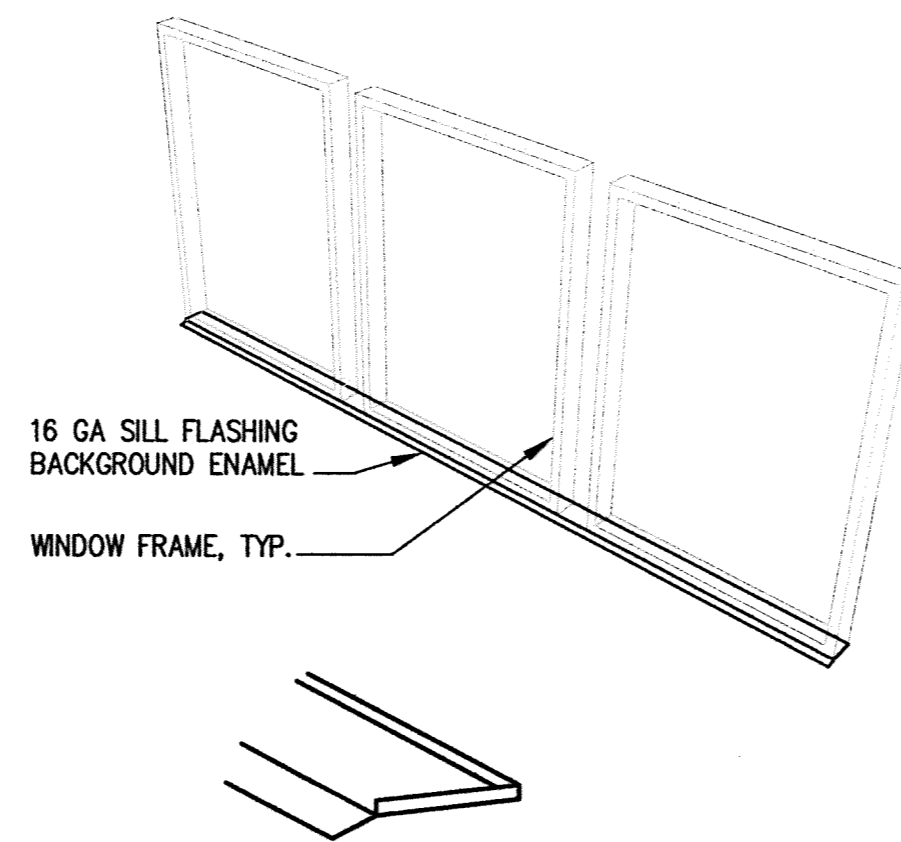
SCALE: 3"=1'-0" 4



1. ALL EXTERIOR NAILING SHALL BE GALVANIZED FINISH NAILS DRIVEN FLUSH WITH SURFACE ON ALL WOOD TRIM & SIDING WHERE OCCURS.
2. ALL EXTERIOR TRIM SHALL BE AS CALLED OUT IN SPECIFICATIONS.
3. ADDRESS NUMBERS TO BE INSTALLED BY G.C. ADDRESS NUMBERS SHALL BE ARABIC NUMERALS OR ALPHABET LETTERS. NUMBERS SHALL BE A MINIMUM OF 4 INCHES HIGH WITH A MINIMUM STROKE WIDTH OF 0.5 INCH. CONTACT FIRE DEPARTMENT TO VERIFY LOCATION.
4. RE. A13.0 (EXTERIOR FINISH SCHEDULE) FOR ALL COLORS & FINISH MATERIALS. X

SCALE: 6  
N.T.S.

SCALE: 3  
N.T.S.



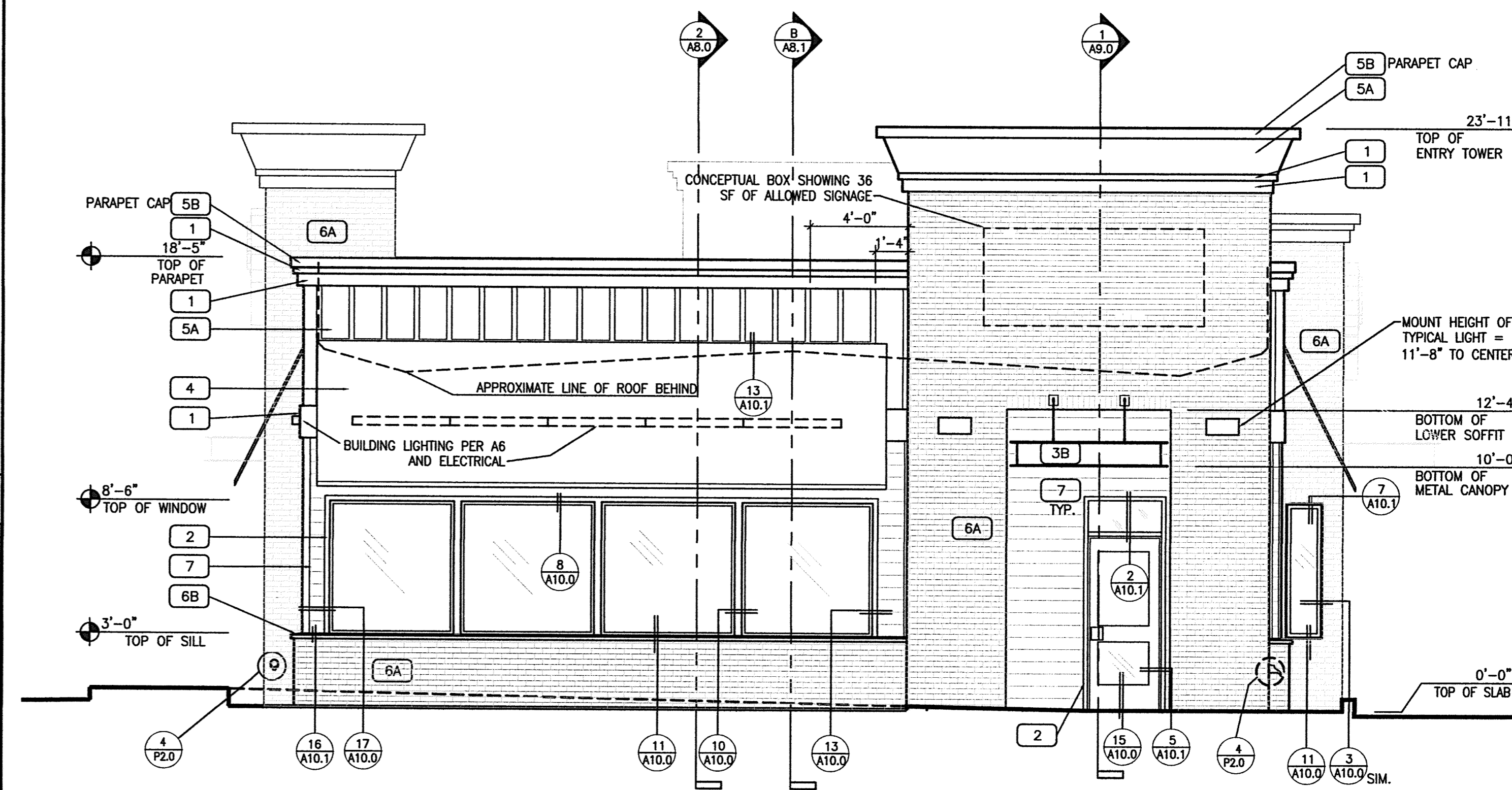
SCALE: 7  
N.T.S.

WINDOW FLASHING DETAIL

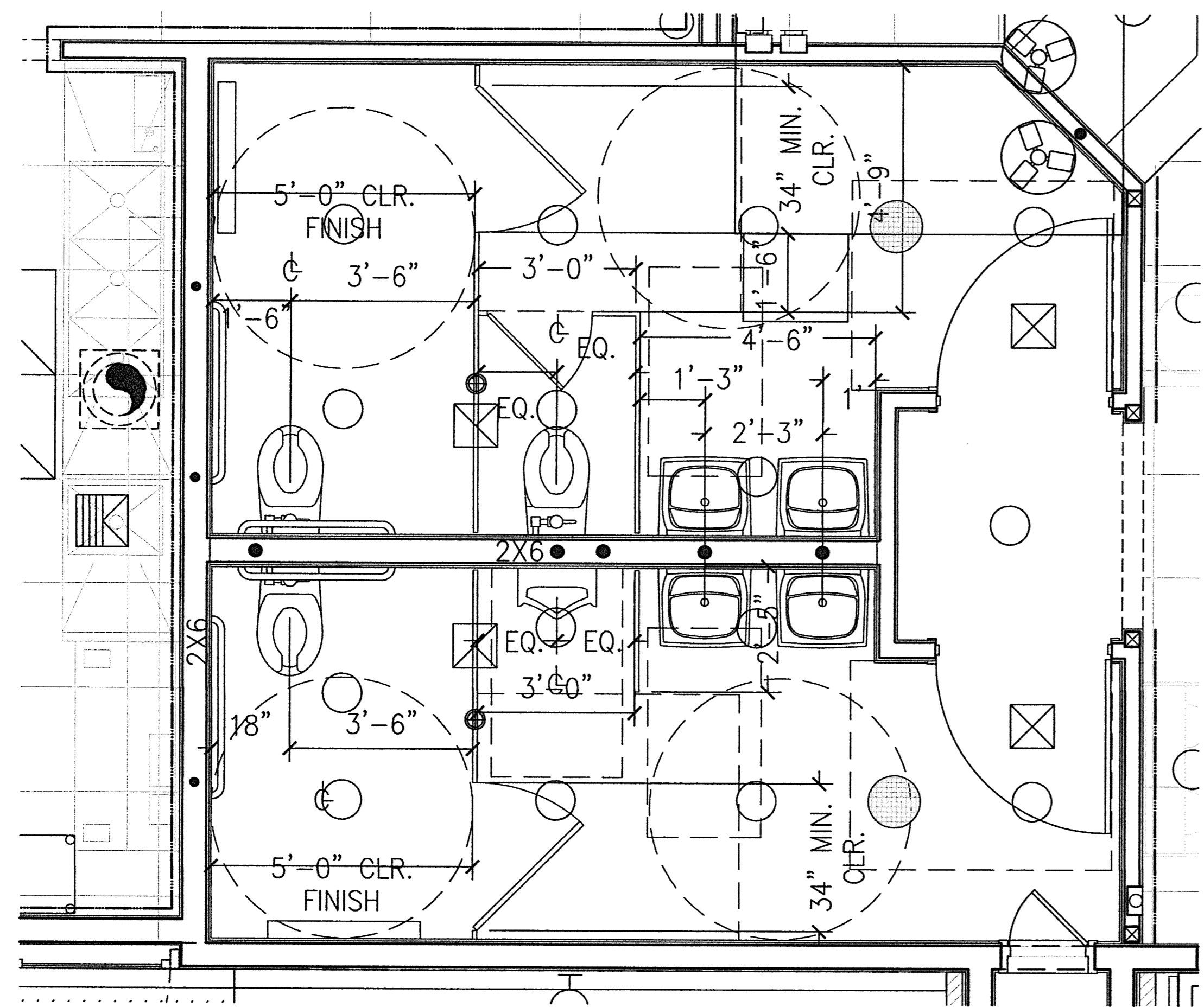
SCALE: 4  
N.T.S.

NORTH ELEVATION

SCALE: 1  
1/4"=1'-0"



SEE A-11 FOR LOCATION OF RESTROOM ACCESSORIES



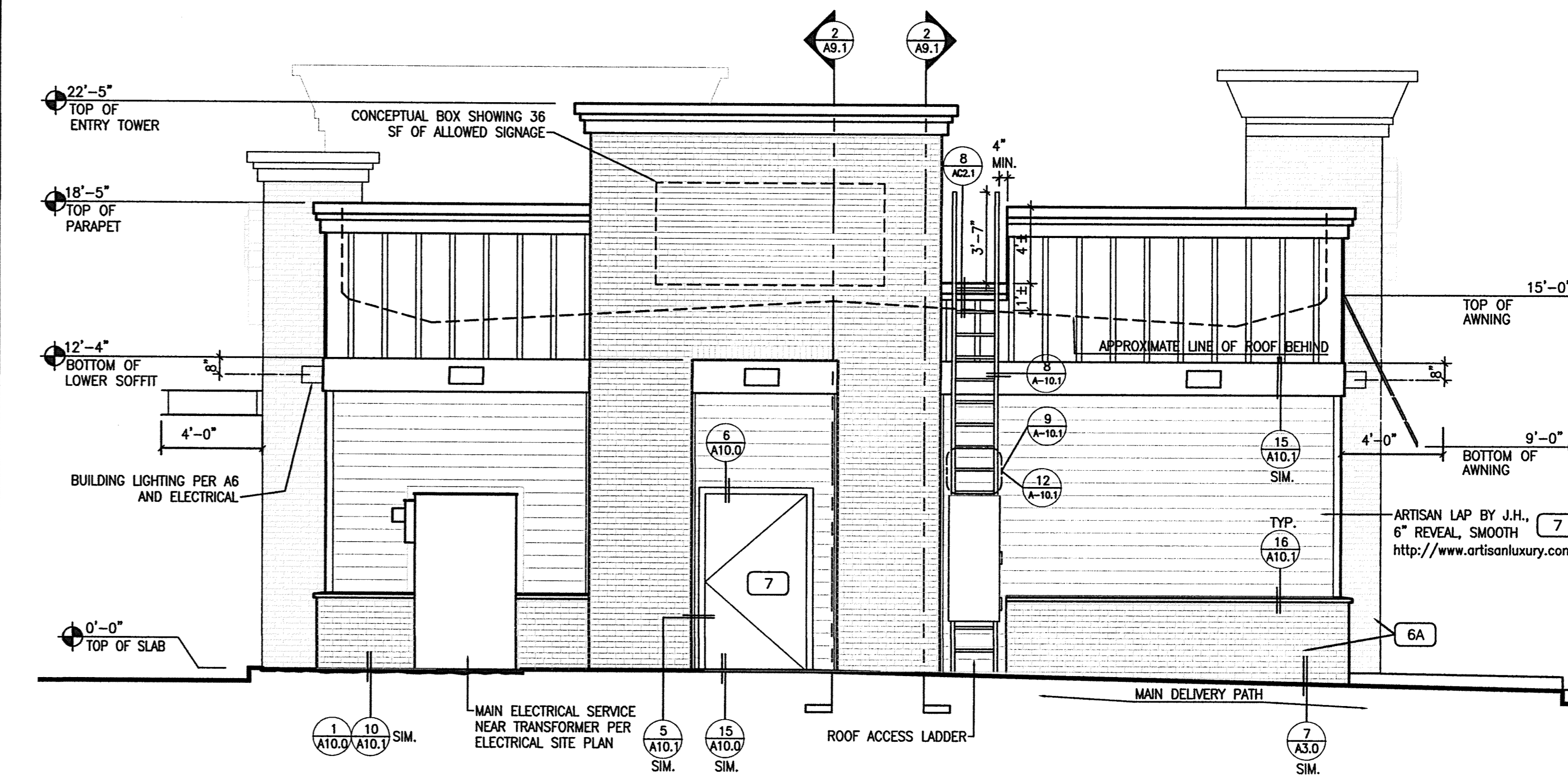
SCALE: 5  
1/2"=1'-0"

MENS #105 / WOMENS #106 RESTROOMS

SCALE: 2  
1/4"=1'-0"

SOUTH ELEVATION

PLEASE NOTE THAT THE SIGN AREAS SHOWN ARE JUST CONCEPTUAL - ALL SIGNAGE IS UNDER SEPARATE PERMIT AND ACCORDING TO THE MASTER SIGN PERMIT



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**CB ANDERSON ARCHITECTS PLLC**  
 7209 Greenwood Avenue N  
 Seattle, Washington 98103  
 206-782-2911  
 Fax 782-5624

CLIENT  
**Wilsonville Devco LLC**  
 c/o Intersect Investment Partners  
 908 Box 816  
 Portland, OR 97207  
 (503) 688-9600

PROJECT  
**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE  
**EXTERIOR ELEVATIONS & DETAILS**

REVISIONS

1	
2	
3	
4	
5	

STAMP  
**REGISTERED ARCHITECT**  
 CRAIG B. ANDERSON  
 10000 1st Ave N  
 SEATTLE, WA 98147  
 Exp 6-30-13  
 STATE OF OREGON

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATING AGENCIES

DATE: Jan-2-2013  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③  
 BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE: AS SHOWN  
 SHEET

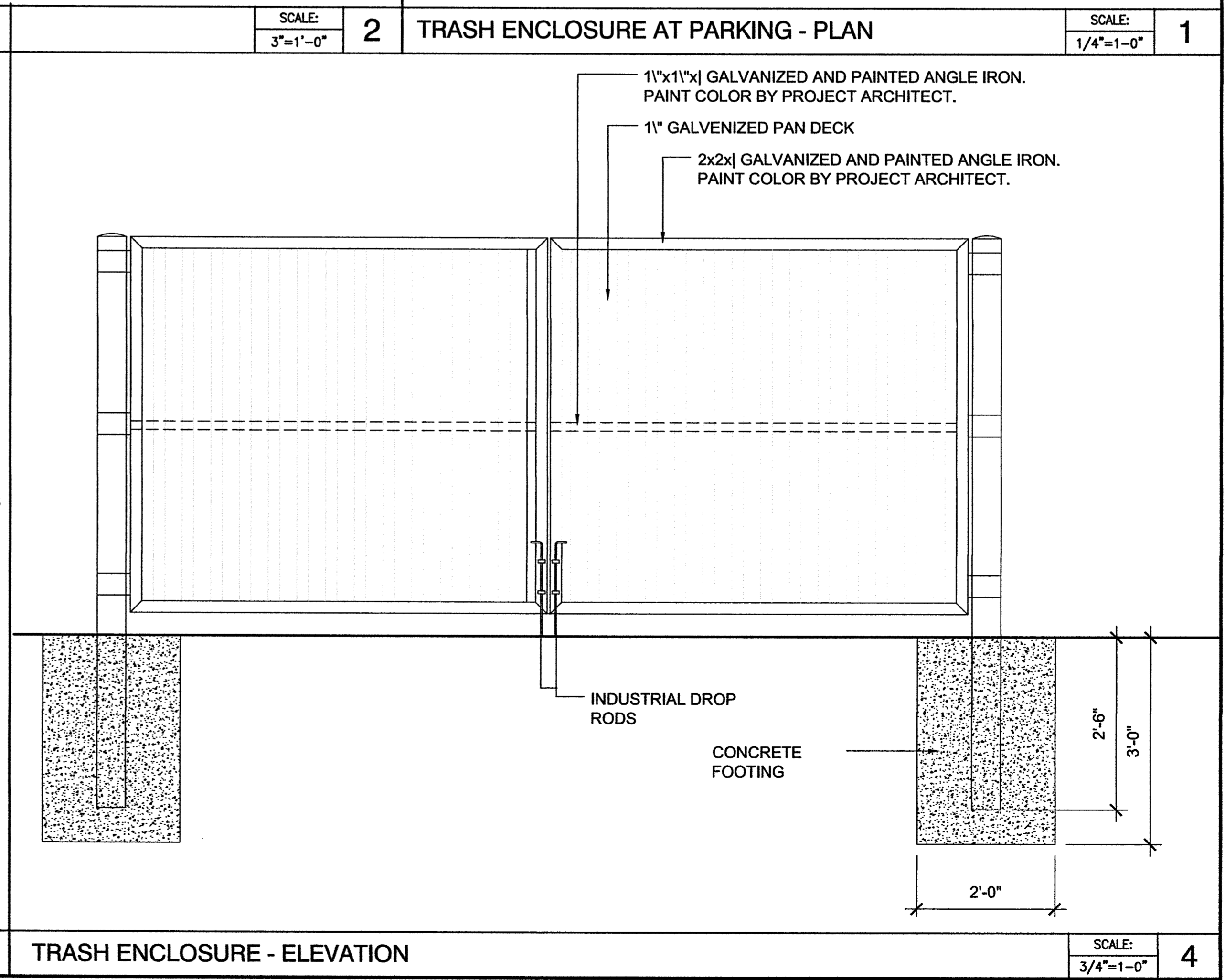
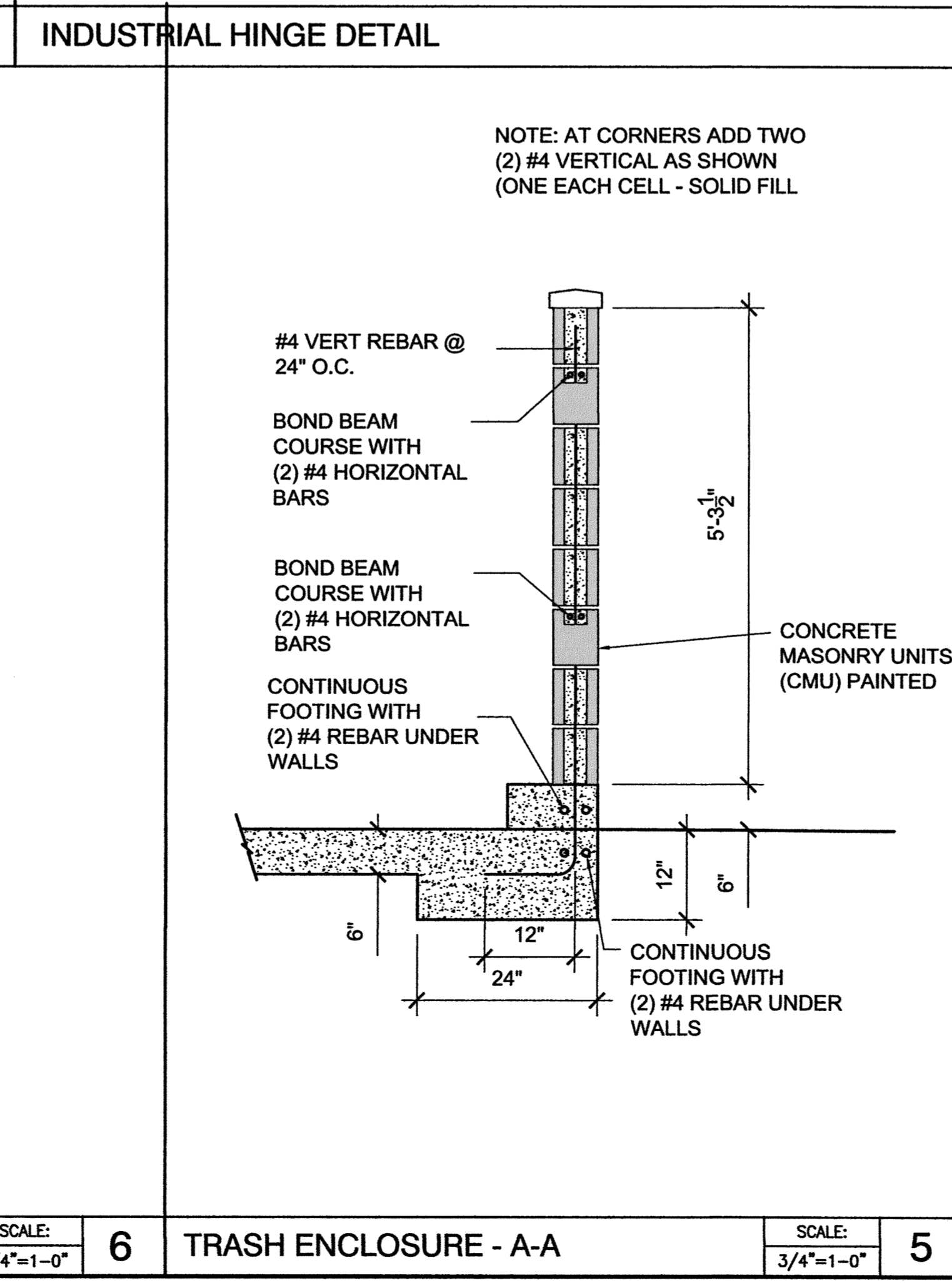
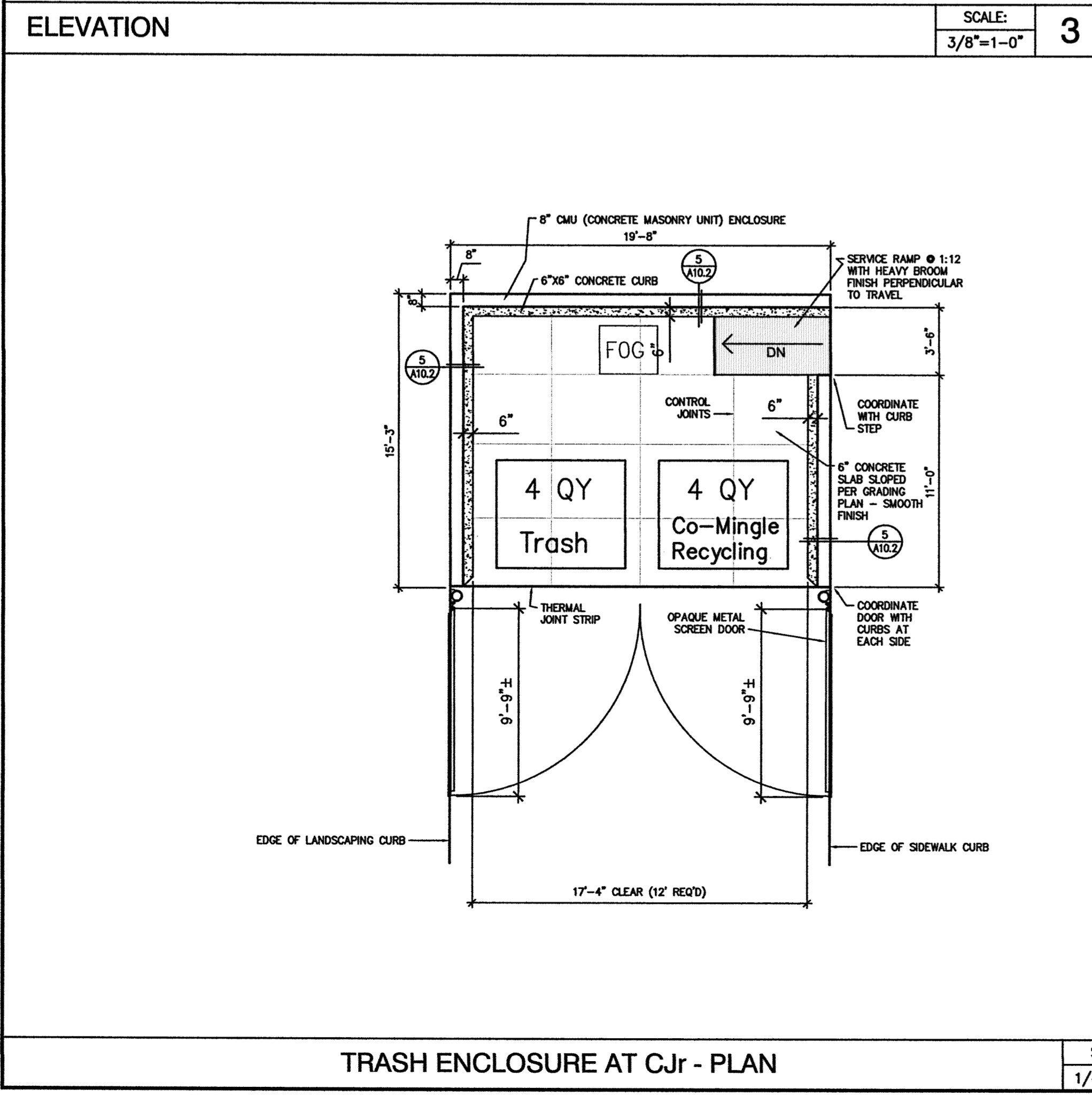
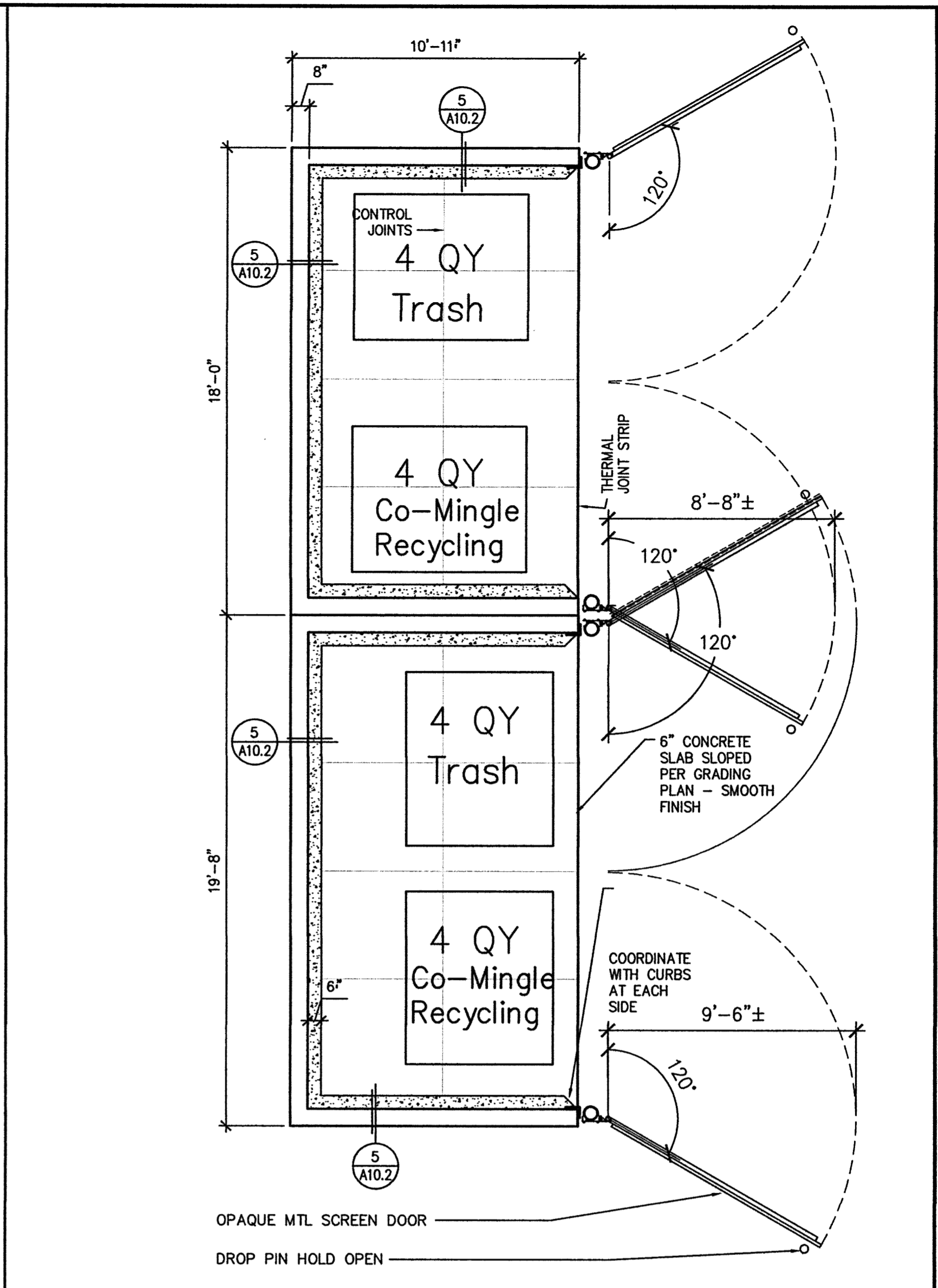
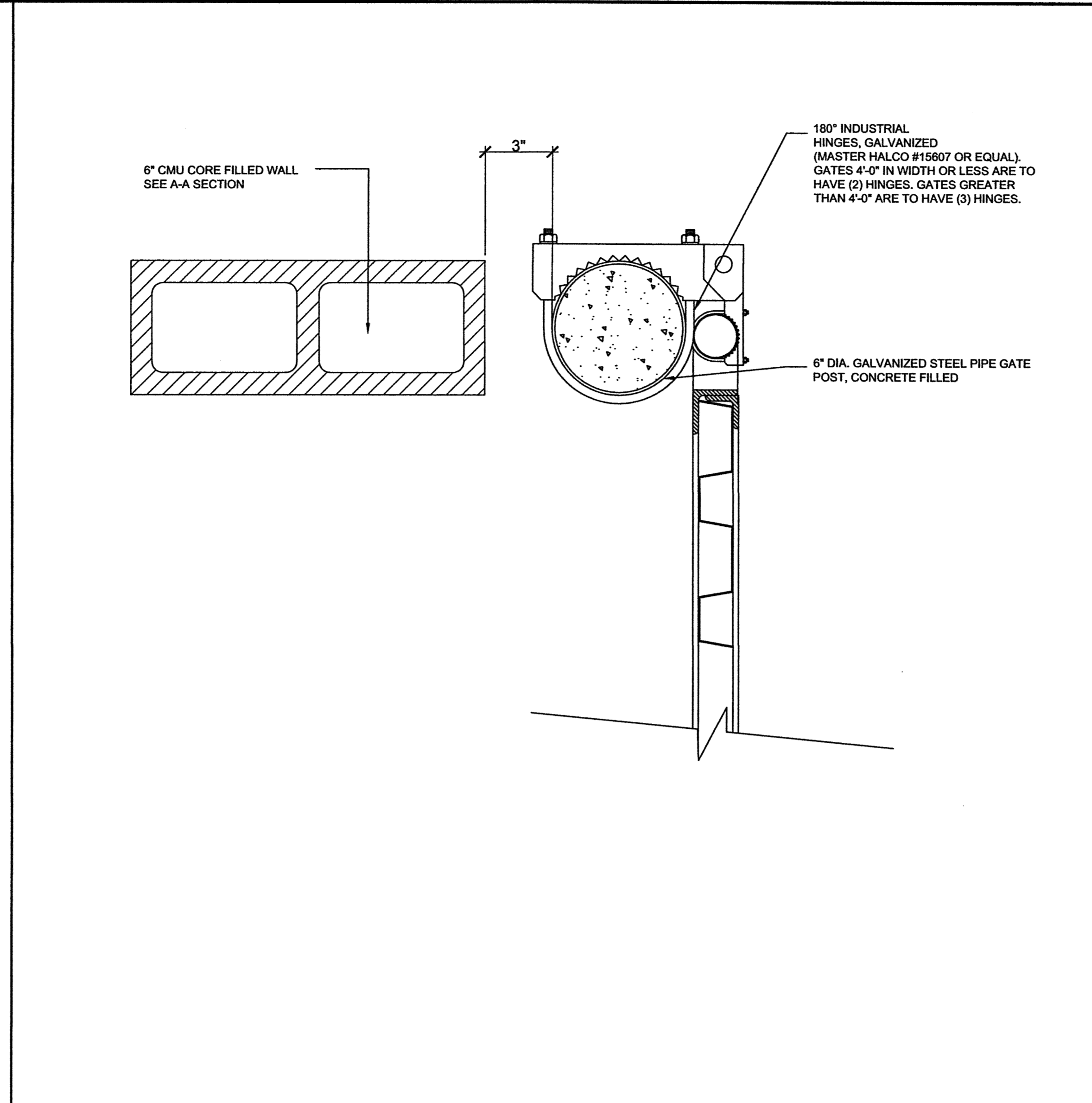
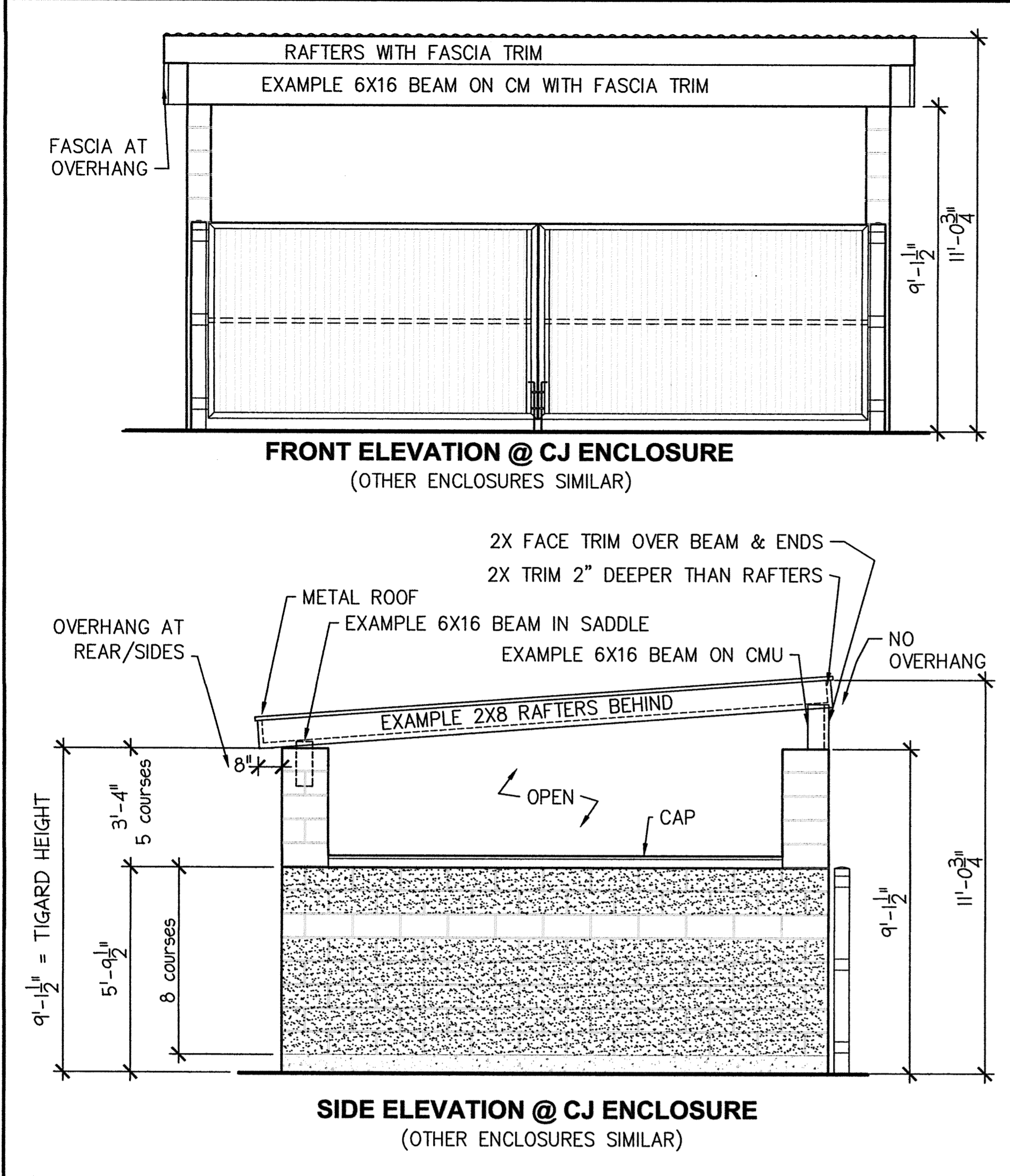
**A4.0**

FILE NAME:









FILE NAME:

K&K Foods Conversion Based on CKE Prototype Version 22-14

**ARCHITECTS**

**CB ANDERSON ARCHITECTS PLLC**

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Fax 782-5622

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**Wilsonville Devco LLC**  
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Portland, OR 97207  
(503) 688-9600

PROJECT

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE

**TRASH ENCLOSURE**

REVISIONS

STAMP

**REGISTERED ARCHITECT**  
CRAIG B. ANDERSON  
JAN 14, 2011  
SEATTLE, WA  
46-17  
Exp 6-30-13  
STATE OF OREGON

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DATE: Jan-2-2013  
BLDG. SUBMITTAL DATE: ①  
DATE: ②  
DATE: ③  
BID DATE:  
CKE NO.:  
ARCH. NO.: 1199  
SCALE: AS SHOWN  
SHEET

**A10.2**









CB ANDERSON ARCHITECTS PLLC

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Seattle, Washington 98103  
206-782-2911  
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CLIENT  
**Wilsonville Devco LLC**  
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PROJECT  
**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE  
**OCCUPANCY & EXITING DIAGRAM**

REVISIONS
△
△
△
△
△



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DATE: Jan-2-2013

BLDG. SUBMITTAL DATE: ①

DATE: ②

DATE: ③

BID DATE:

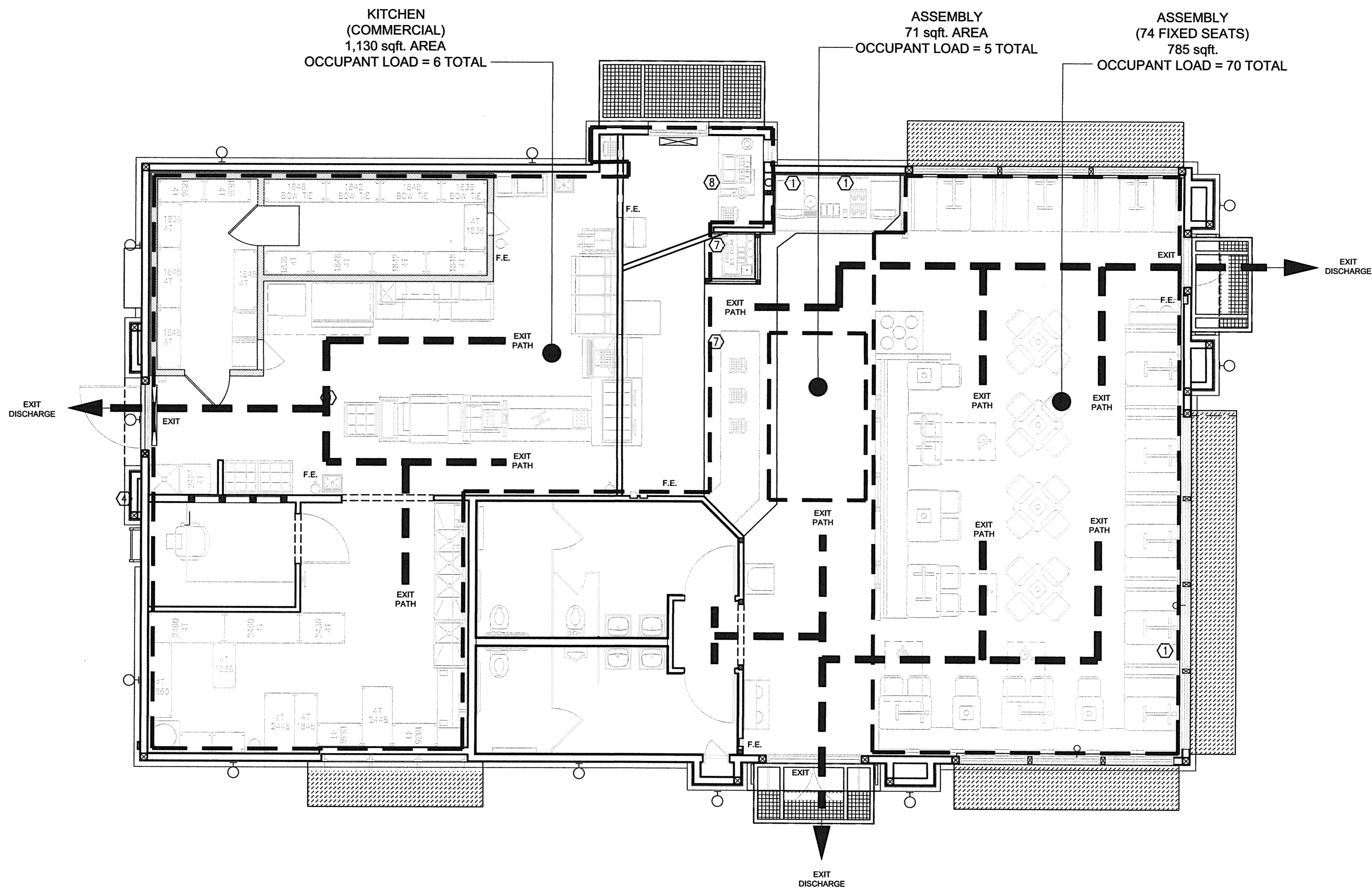
CKE NO.:

ARCH. NO.: 1199

SCALE: AS SHOWN

SHEET

**T2.0**



**GENERAL DIAGRAM NOTES**

- 1) REFER TO FIRE PROTECTION PLAN & DETAILS (SHEET K-3) FOR ADDITIONAL INFORMATION.
- 2) REFER TO ELECTRICAL DRAWINGS (SHEET E-5) FOR EXIT SIGNAGE.
- 3) REFER TO ELECTRICAL DRAWINGS (SHEET E-5) FOR EMERGENCY EGRESS LIGHTING.
- 4) REFER TO DOOR SCHEDULE (SHEET A-13) FOR DOOR/HARDWARE INFORMATION.



FILE NAME:

OCCUPANT LOAD & EXITING DIAGRAM

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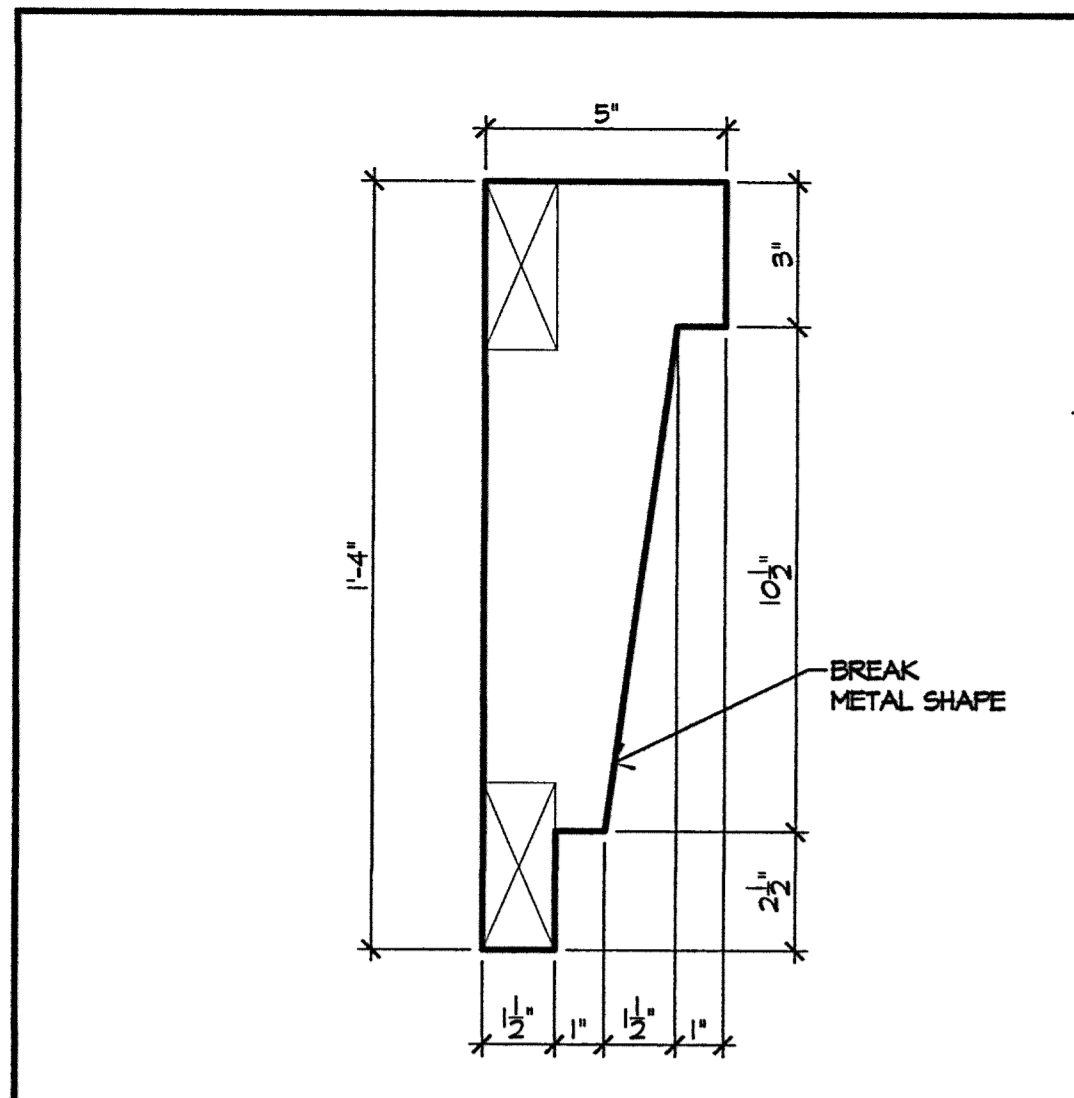
KGK Foods Conversion Based on CKE Prototype Version 22-14





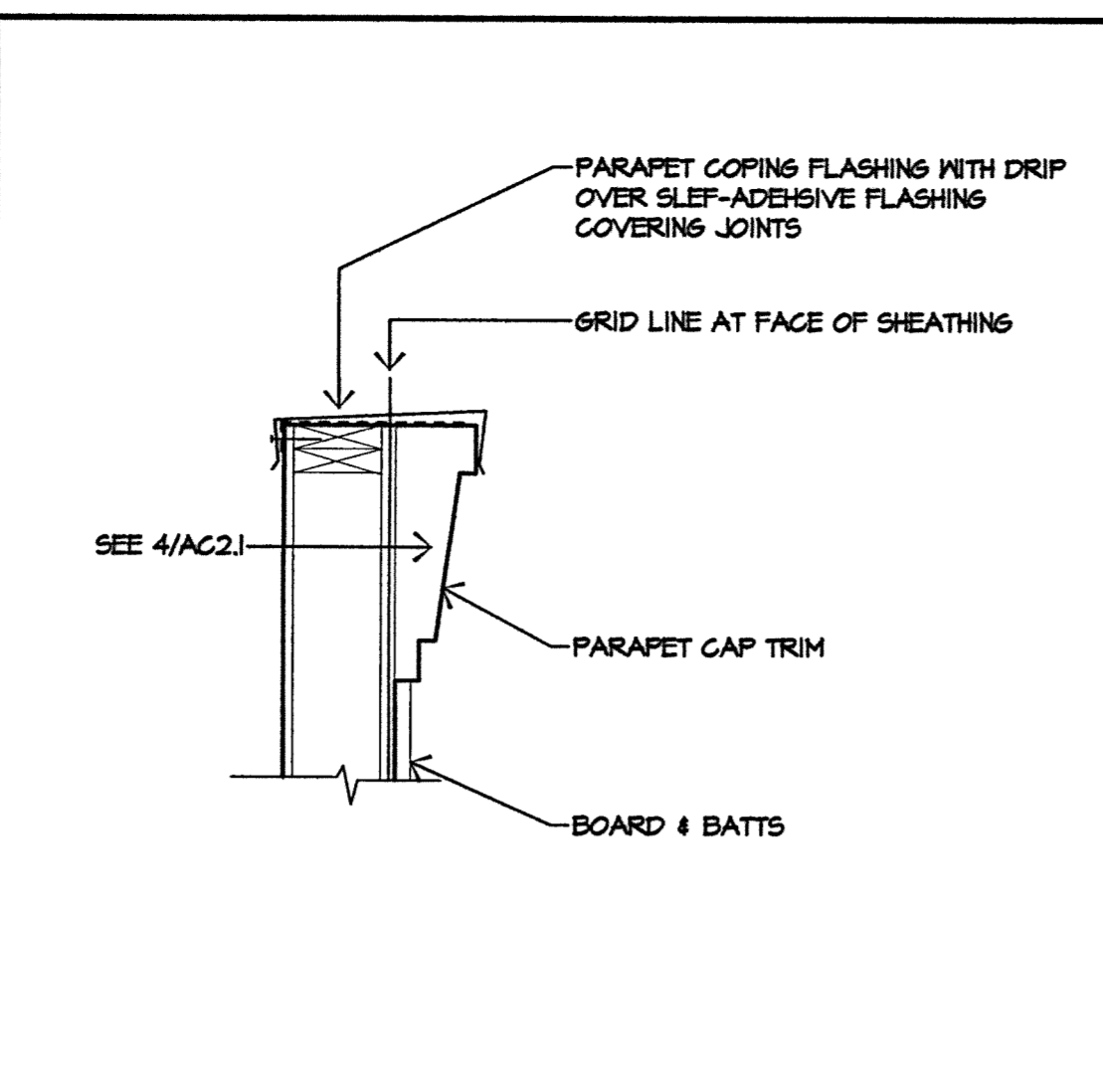






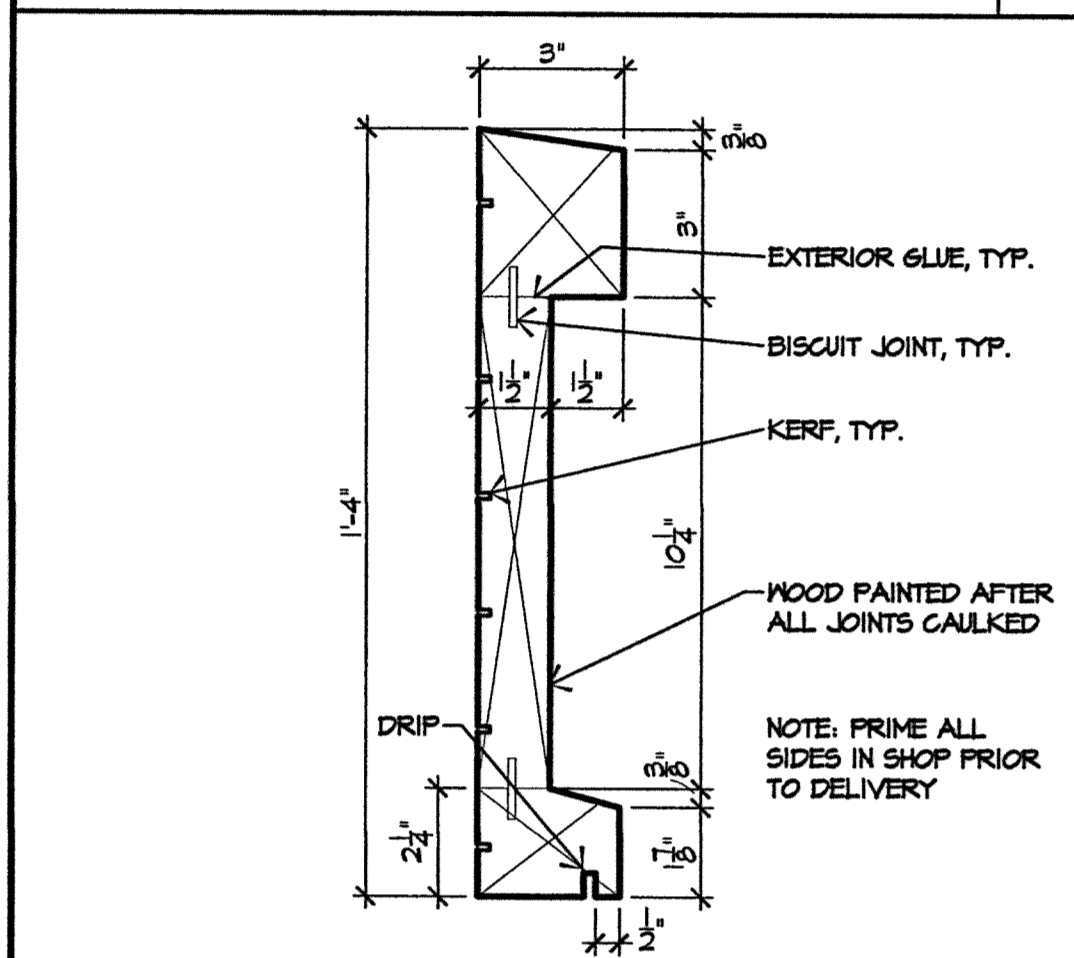
UPPER TRIM BAND

4



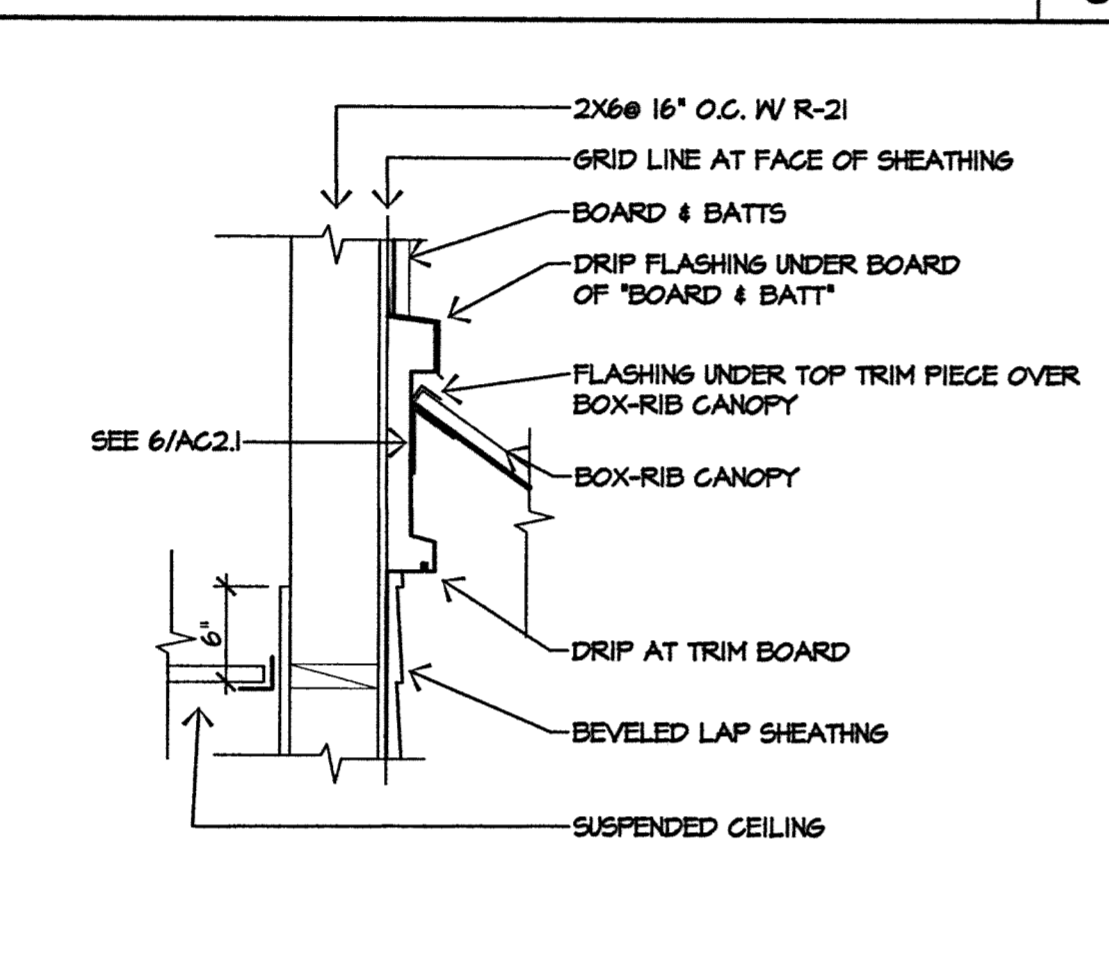
TOP OF BOX-RIB CANOPY

3



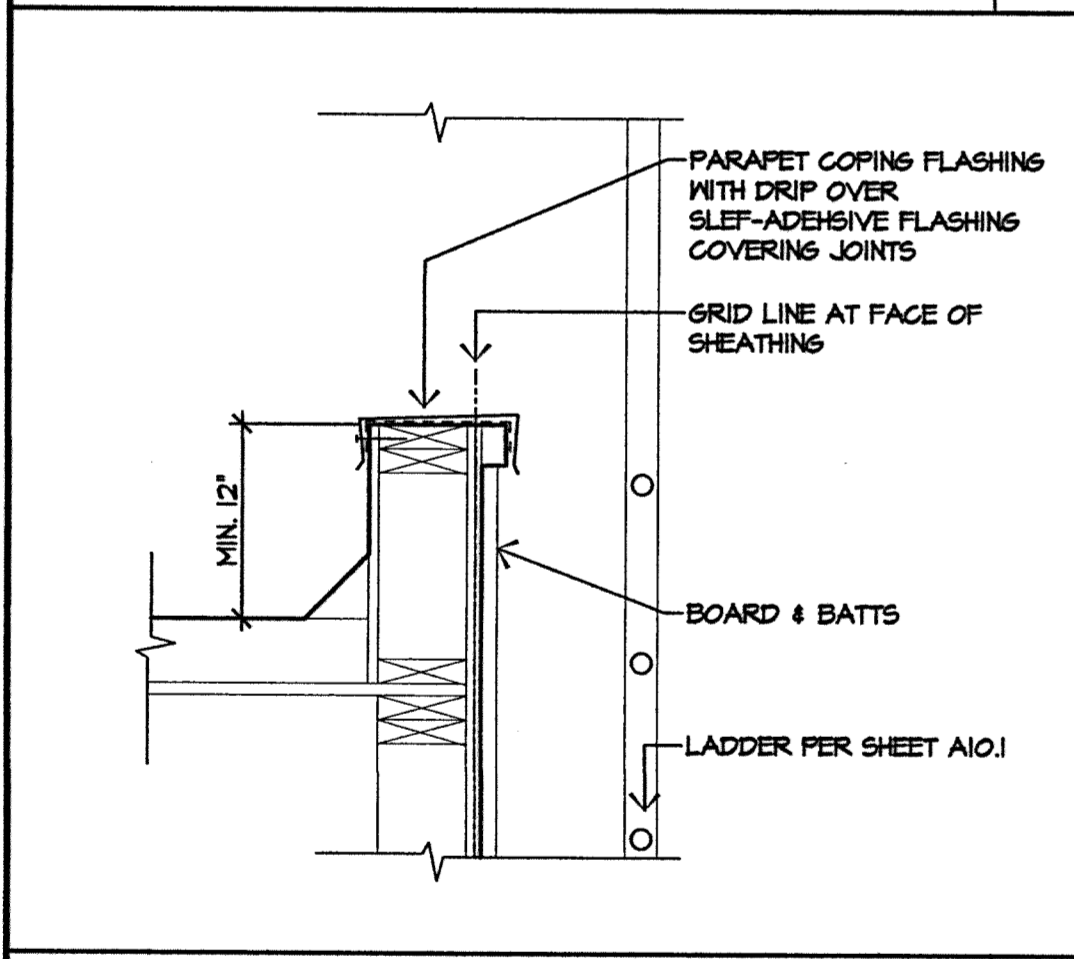
UPPER TRIM BAND

6



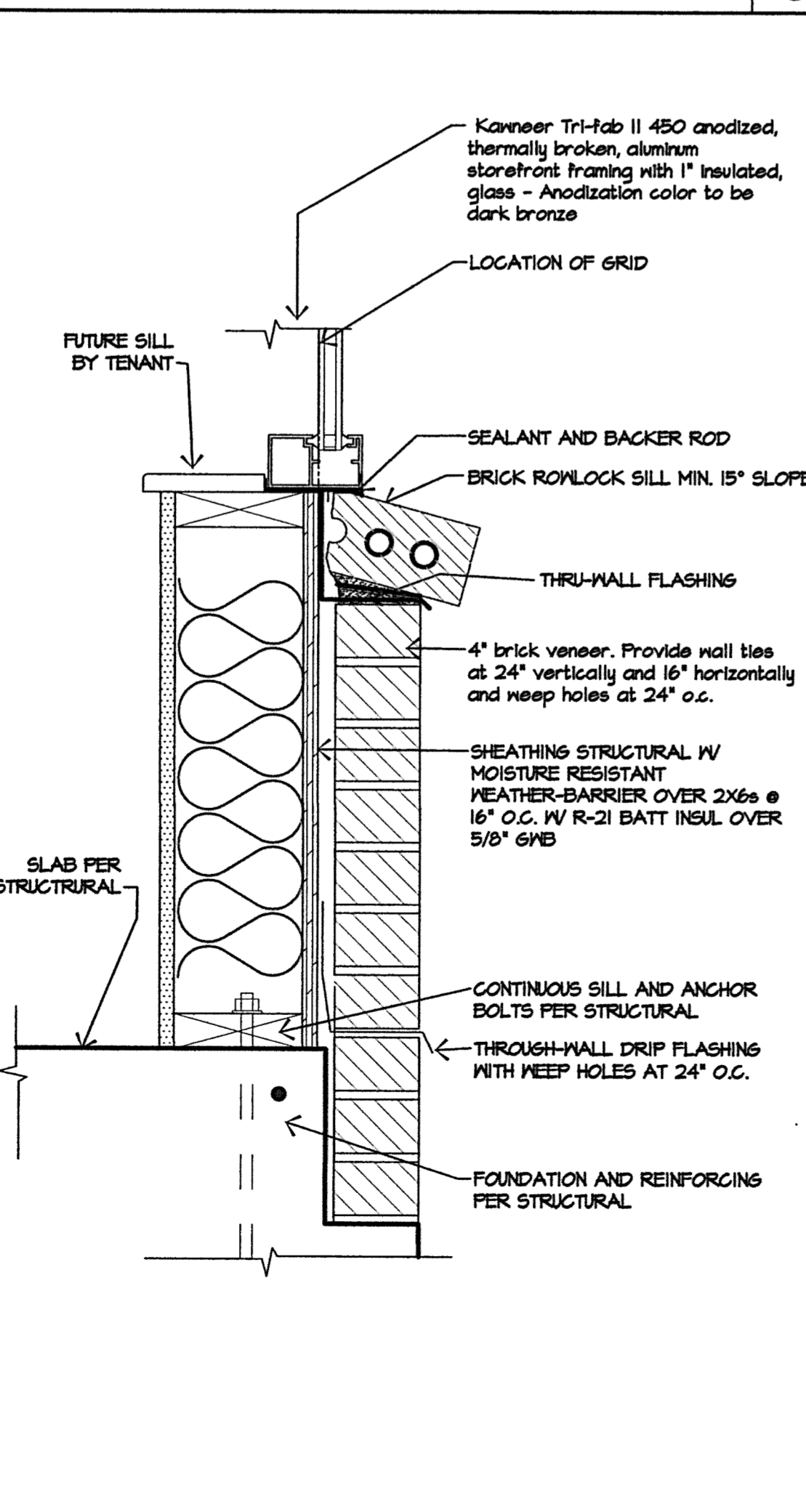
TOP OF BOX-RIB CANOPY

5



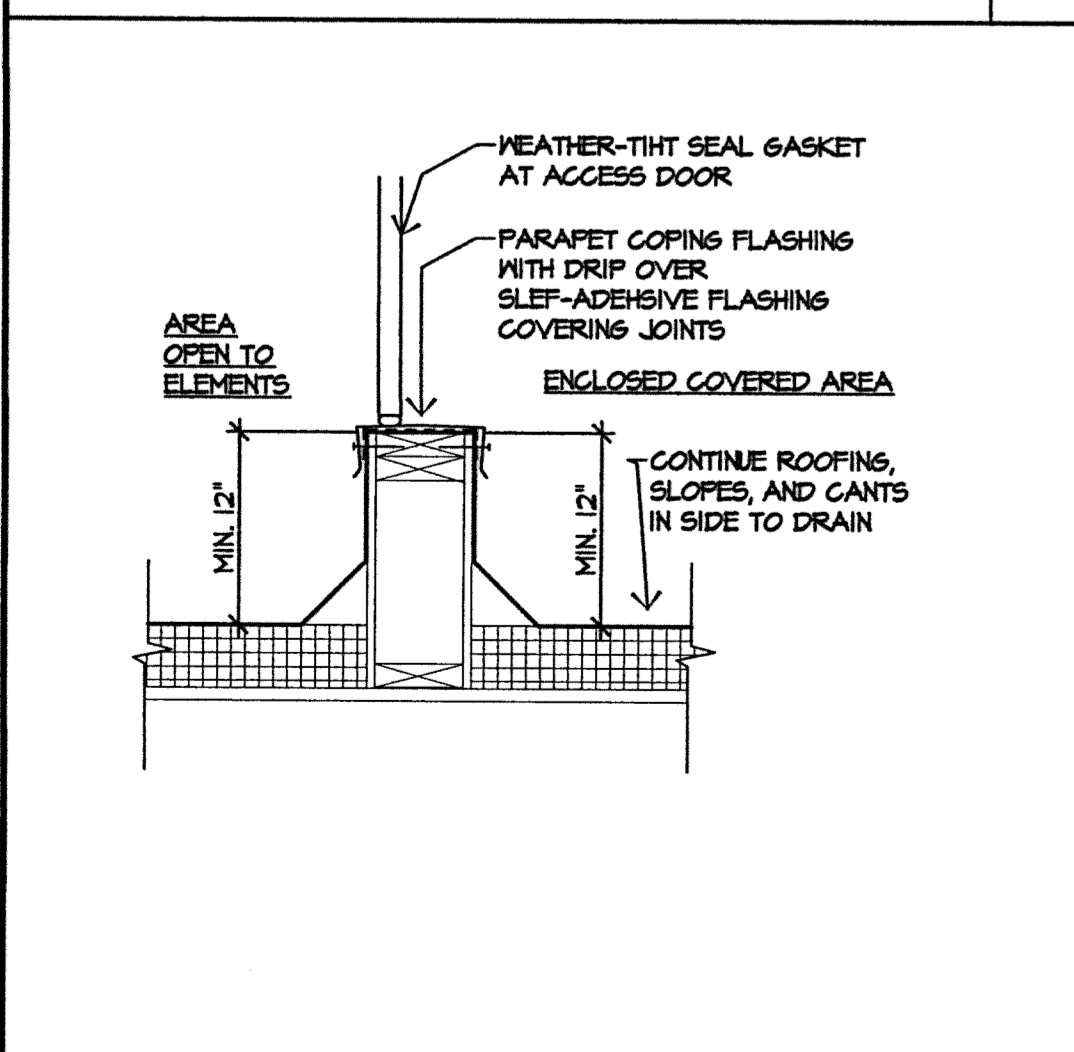
PARAPET AT LADDER, TYPICAL

8



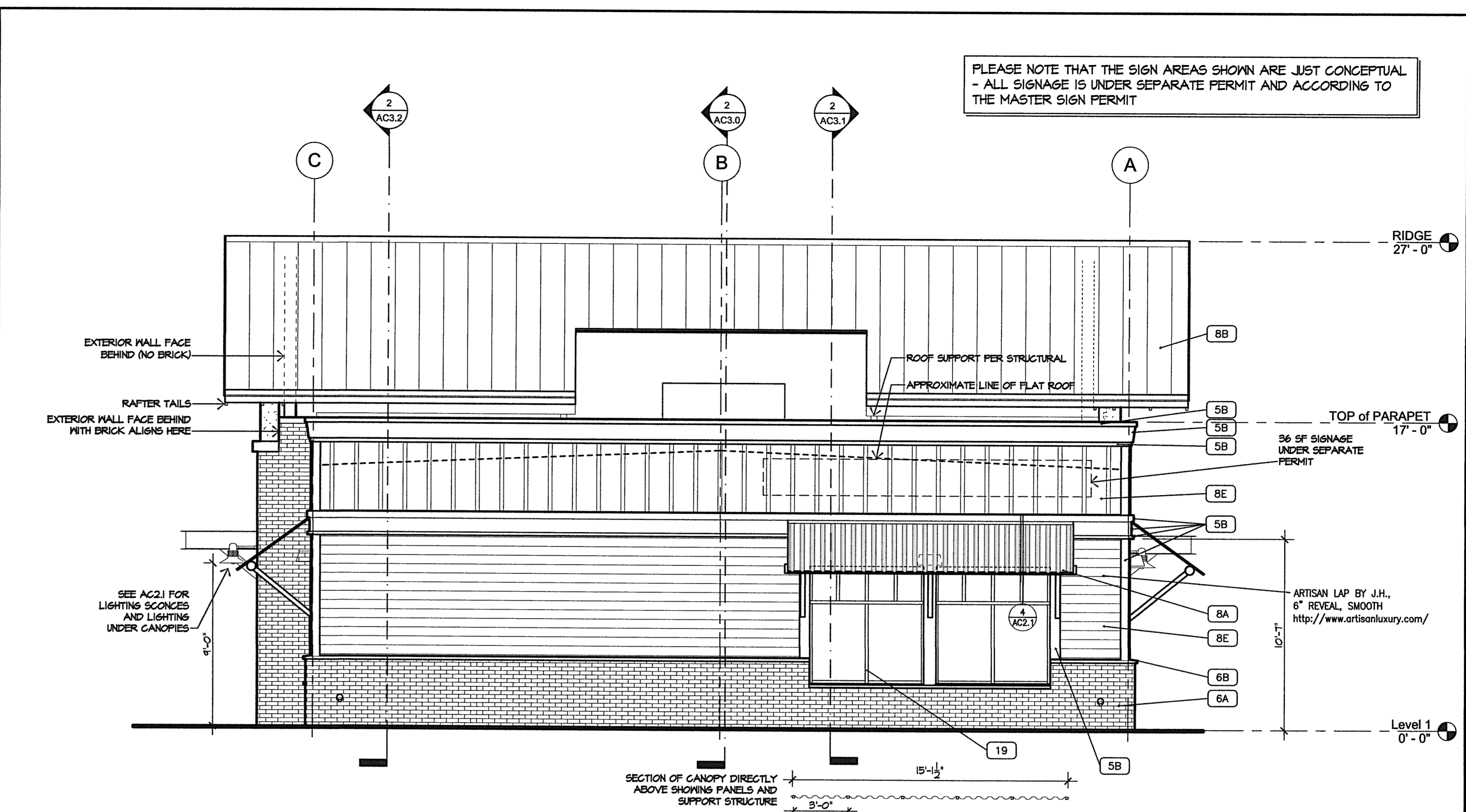
SILL AND BRICK WAINSCOT

7



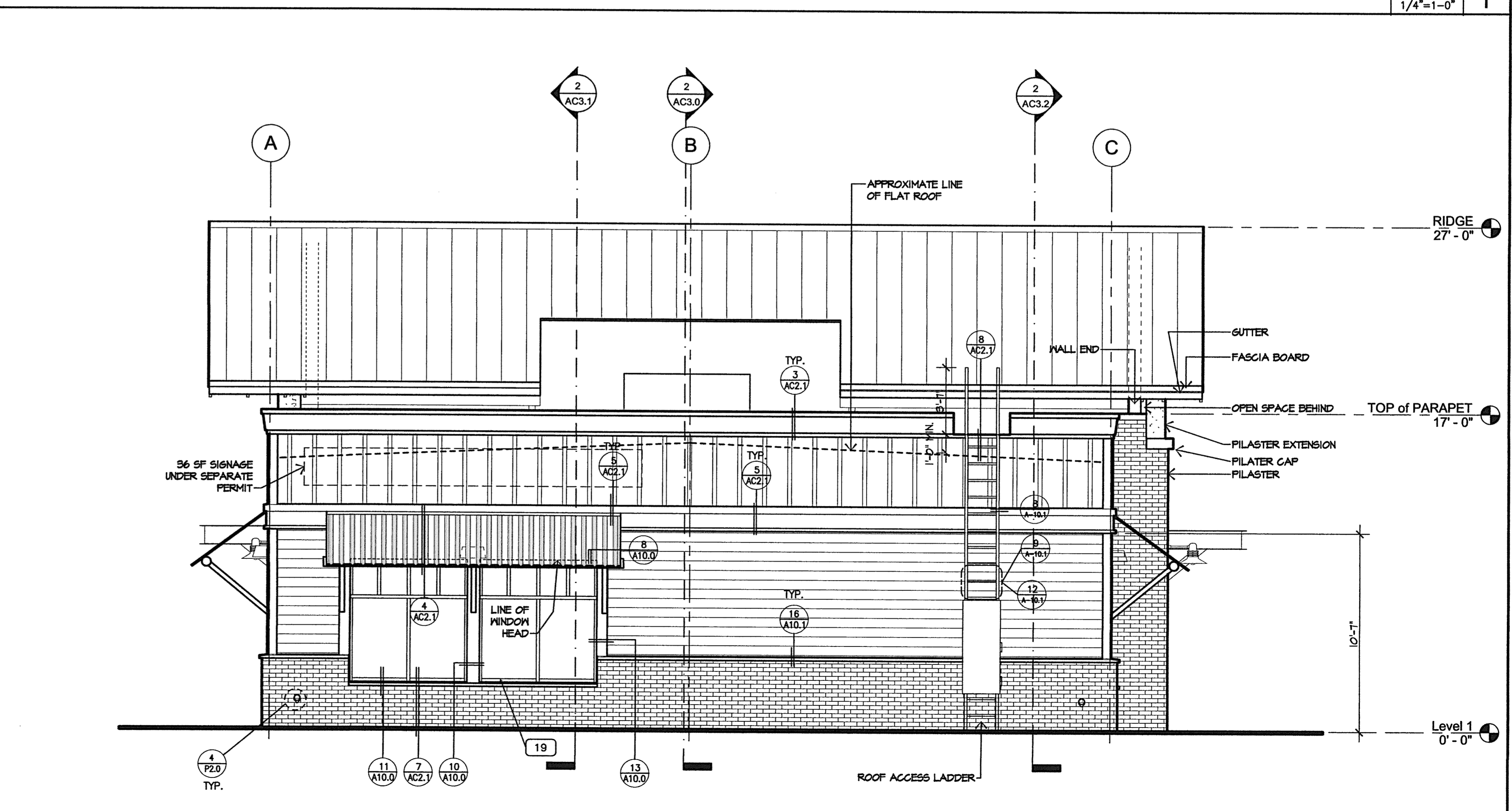
CURB AT ROOF DOOR

9



RETAIL BUILDING: WEST ELEVATION

SCALE: 1/4"=1'-0" 1



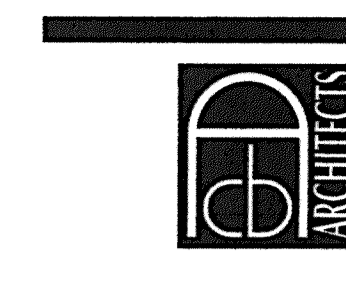
RETAIL BUILDING: EAST ELEVATION

SCALE: 1/4"=1'-0" 2

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CLIENT

Wilsonville Devco LLC  
c/o Integrated Investment Partners  
PO Box 316  
Portland, OR 97207  
(503) 688-9600

PROJECT

Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE

RETAIL BUILDING:  
ELEVATIONS  
(EAST & WEST)

REVISIONS

STAMP



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DATE: Jan-2-2013

BLDG. SUBMITTAL DATE: ①

DATE: ②

DATE: ③

BID DATE:

CKE NO.:

ARCH. NO.: 1199

SCALE:

SHEET

AC2.1



**CURVE TABLE**

CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD
C1	135.83'	145.50'	53°29'12"	N26°54'00"E	130.95'
C2	18.43'	26.50'	39°50'40"	N73°33'56"E	18.06'
C3	34.69'	61.00'	32°34'45"	S49°48'42"E	34.22'
C4	28.05'	863.64'	1°51'38"	S27°48'35"E	28.04'

**LINE TABLE**

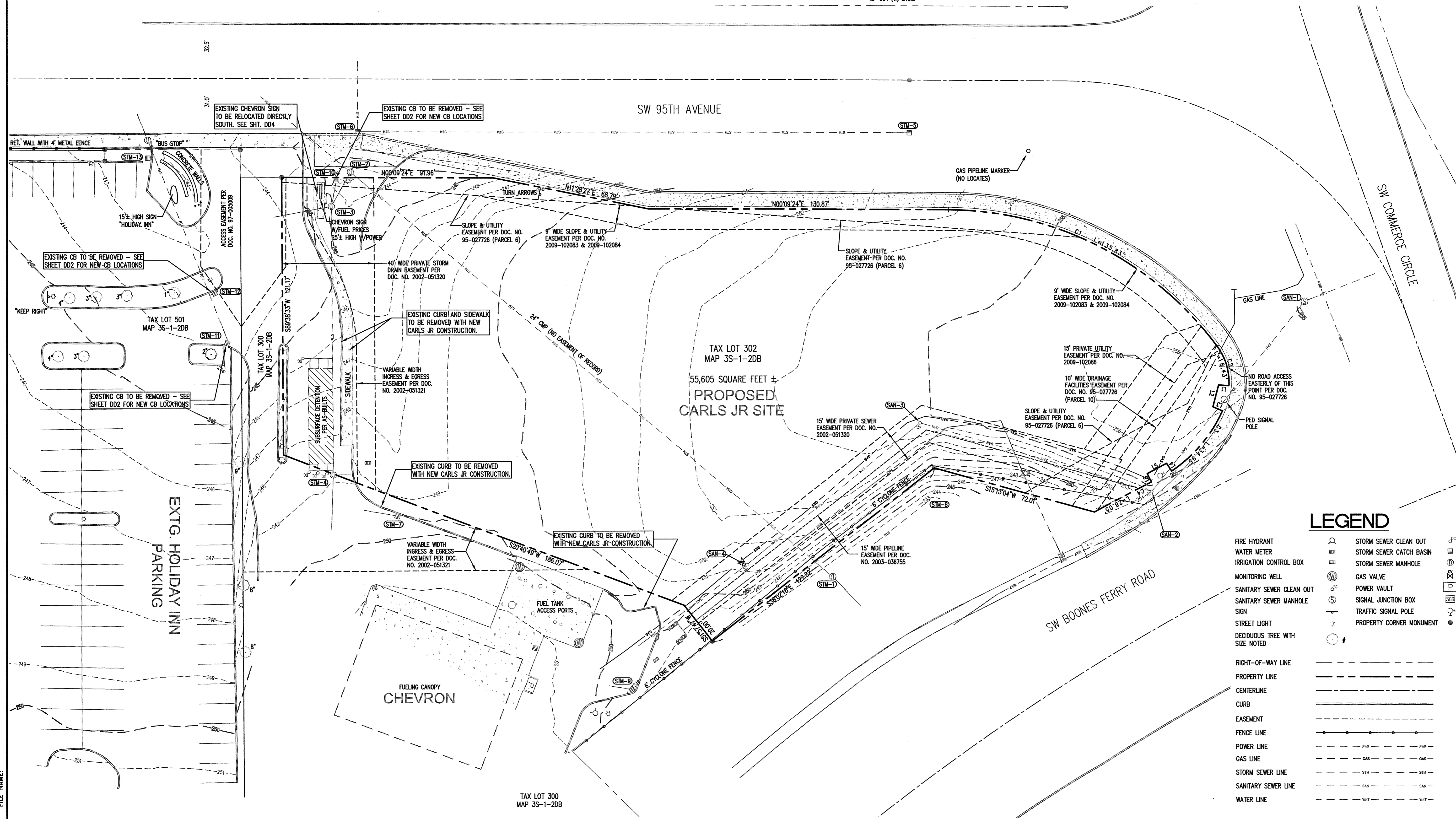
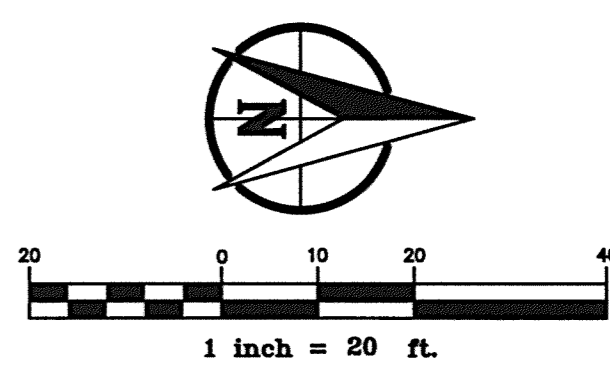
LINE	BEARING	LENGTH
L1	S03°28'28"W	4.50
L2	S75°40'24"E	9.87
L3	N23°53'53"E	4.52
L4	S56°28'43"W	4.04
L5	S28°07'38"E	9.52
L6	N63°28'08"E	4.00

**NOTES**

- FIELD WORK WAS COMPLETED DURING MARCH 2012. THERE IS CONSTRUCTION WORK SCHEDULED TO COMMENCE IN THE NEAR FUTURE ALONG THE STREETS ADJACENT TO THE PROPERTY SO CONDITIONS WITHIN THE RIGHT-OF-WAY WILL LIKELY CHANGE.
- ELEVATIONS ARE BASED ON WASHINGTON COUNTY BENCHMARK NO. 455. THE BENCHMARK IS LOCATED IN THE NORTHWEST CORNER OF THE BRIDGE CROSSING OVER INTERSTATE 5 WITH AN ELEVATION OF 281.55 (NGVD 1929 DATUM).
- EASEMENTS ARE PER PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY OF OREGON WITH AN EFFECTIVE DATE OF JANUARY 10, 2012 AND ORDER NO. 47251249586201-C150.
- ADDITIONAL FIELD WORK TO INCLUDE THE NORTH END OF THE PARKING LOT FOR THE HOLIDAY INN WAS COMPLETED ON MAY 29, 2012.

**STORM & SANITARY SEWER INFORMATION**

- |  |   |   |  |
|--|---|---|--|
| <p>SAN-1 MANHOLE<br/>RIM 255.45'<br/>12" IN (SE) 239.6'<br/>12" OUT (N) 239.4'</p> <p>SAN-2 CLEANOUT<br/>RIM 4"± ABOVE GRADE<br/>IE 244.4"</p> <p>SAN-3 CLEANOUT<br/>RIM 4"± ABOVE GRADE<br/>IE 245.8"</p> <p>SAN-4 CLEANOUT<br/>RIM 4"± ABOVE GRADE<br/>IE 246.8"</p> <p>* RECORD INFORMATION PER LAPONT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)</p> | <p>STM-1 MANHOLE<br/>RIM 246.88'<br/>24" OUT (SW) 237.3'</p> <p>STM-2 MANHOLE<br/>RIM 244.70'<br/>12" IN (SE) 233.4'<br/>24" OUT (NE) 232.6'<br/>24" OUT (SW) 232.6'</p> <p>STM-3 MANHOLE<br/>RIM 243.52'<br/>8" IN (S) 238.3"<br/>8" IN (W) 238.3"<br/>12" IN (E) 238.3"<br/>12" OUT (NW) 238.3"<br/>(MH BURIED, UNABLE TO REMOVE LD)</p> <p>STM-4 MANHOLE<br/>RIM 248.88'<br/>8" IN (N) 243.4' (STUB?)<br/>10" IN (E) 241.5'<br/>12" OUT (W) 240.2'</p> | <p>STM-6 CATCH BASIN<br/>RIM 245.44'<br/>(HAS FILTER FABRIC, UNABLE TO MEAS. INVERT, NO AS-BUILT INFO AVAIL.)</p> <p>STM-7 CATCH BASIN<br/>RIM 249.55'<br/>10" OUT (S) 246.6"<br/>(HAS TRAP)</p> <p>STM-8 CULVERT<br/>18" CMP (SW) 242.5'</p> <p>STM-9 CATCH BASIN<br/>RIM 248.94'<br/>8" OUT (S) 247.0"</p> <p>STM-10 DITCH INLET<br/>BOTTOM RIM 242.87'<br/>8" OUT (E) 240.0"</p> | <p>STM-11 CATCH BASIN<br/>RIM 243.45'<br/>WITH TRAP - OUT (W)</p> <p>STM-12 CATCH BASIN<br/>RIM 243.05'<br/>WITH TRAP - OUT (S)</p> <p>STM-13 CATCH BASIN<br/>RIM 242.16'<br/>WITH TRAP - OUT (N)</p> <p>STM-14 MANHOLE<br/>RIM 243.31'<br/>12" IN (E) 236.9'<br/>15" OUT (W) 236.8"</p> <p>* RECORD INFORMATION PER LAPONT CENTER CONSTRUCTION PLANS (NOT FIELD VERIFIED)</p> |
|--|---|---|--|



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ALOHA, OR 97007  
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(503) 688-9600

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**EXISTING CONDITIONS PLAN**

REVISIONS




THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATING AGENCIES

DATE: 01-07-2013  
BLDG. SUBMITTAL DATE: ①  
DATE: ②  
DATE: ③  
BID DATE:    
CKE NO.:    
ARCH. NO.: 1199  
SCALE:    
SHEET

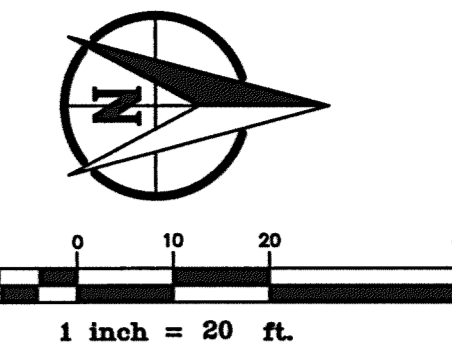
**DD1**

FILE NAME

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14



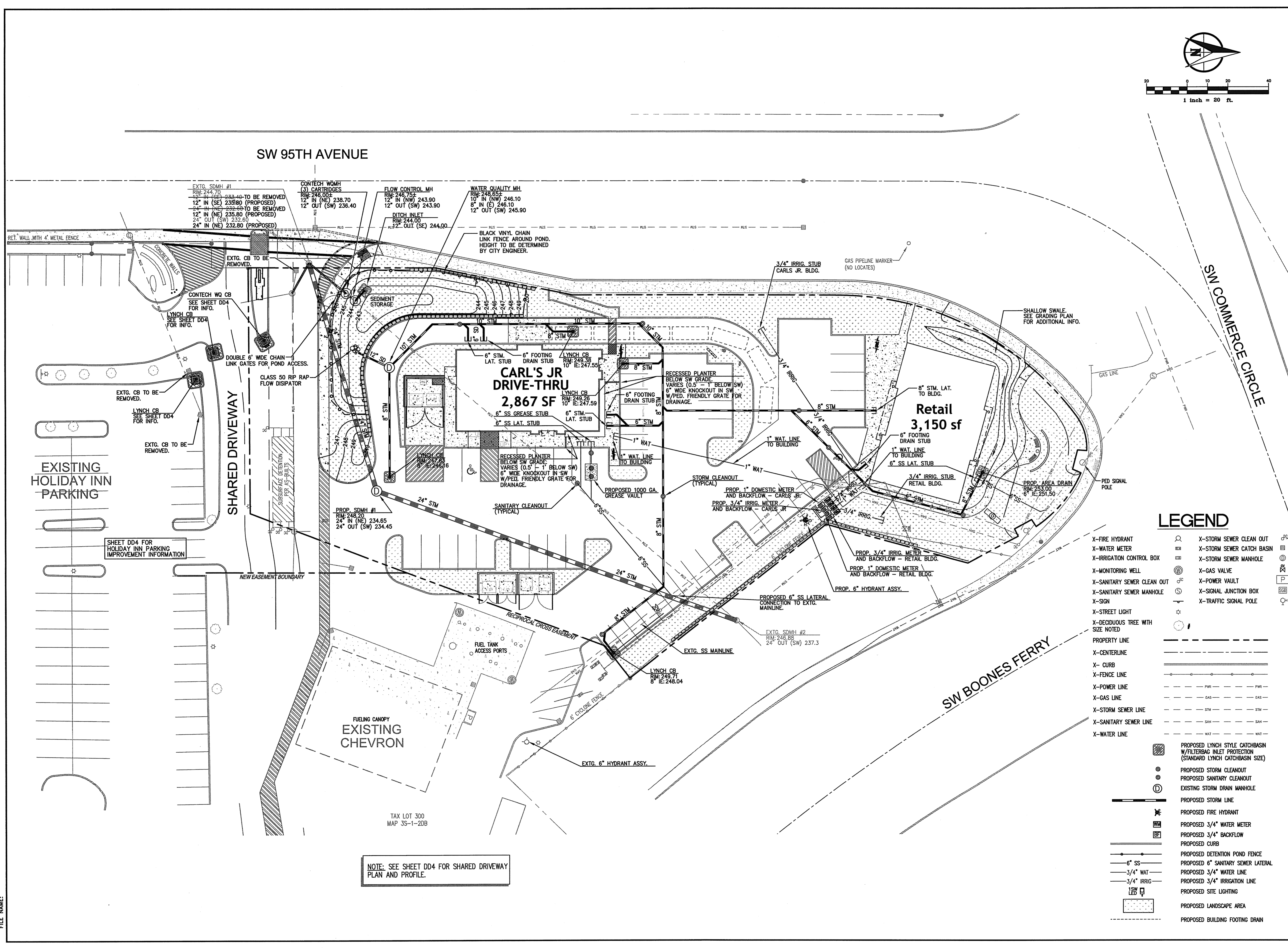
FILE NAME:



SW 95TH AVENUE

SW COMMERCE CIRCLE

SW BOONES FERRY



EXTG. SDMH #1  
RIM: 244.70  
12" IN (SE) 235.80 (PROPOSED)  
12" IN (NE) 235.80 (PROPOSED)  
24" OUT (SW) 232.65  
24" IN (NE) 232.80 (PROPOSED)

CONTECH WQMH  
(3) CARTRIDGES  
RIM: 246.00  
12" IN (NE) 238.70  
12" OUT (SW) 236.40

FLOW CONTROL MH  
RIM: 246.75  
12" IN (NW) 243.90  
12" OUT (SW) 243.90

WATER QUALITY MH  
RIM: 248.65  
10" IN (NW) 246.10  
8" IN (E) 246.10  
12" OUT (SW) 245.90

**CARL'S JR  
DRIVE-THRU  
2,867 SF**

6" STIM LAT. STUB  
6" FOOTING DRAIN STUB  
LYNCH CB  
RIM: 249.36  
10" IE: 247.59

6" SS GREASE STUB  
6" STIM LAT. STUB

**Retail  
3,150 sf**

8" STIM LAT. TO BLDG.  
6" FOOTING DRAIN STUB  
1" WAT. LINE TO BUILDING  
6" SS LAT. STUB

FUELING CANOPY  
**EXISTING CHEVRON**

TAX LOT 300  
MAP 3S-1-20B

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

**LEGEND**

- X-FIRE HYDRANT
  - X-WATER METER
  - X-IRRIGATION CONTROL BOX
  - X-MONITORING WELL
  - X-SANITARY SEWER CLEAN OUT
  - X-SANITARY SEWER MANHOLE
  - X-SIGN
  - X-STREET LIGHT
  - X-DECIDUOUS TREE WITH SIZE NOTED
  - PROPERTY LINE
  - X-CENTERLINE
  - X-CURB
  - X-FENCE LINE
  - X-POWER LINE
  - X-GAS LINE
  - X-STORM SEWER LINE
  - X-SANITARY SEWER LINE
  - X-WATER LINE
- 
- X-STORM SEWER CLEAN OUT
  - X-STORM SEWER CATCH BASIN
  - X-STORM SEWER MANHOLE
  - X-GAS VALVE
  - X-POWER VAULT
  - X-SIGNAL JUNCTION BOX
  - X-TRAFFIC SIGNAL POLE
- 
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
  - PROPOSED STORM CLEANOUT
  - PROPOSED SANITARY CLEANOUT
  - EXISTING STORM DRAIN MANHOLE
  - PROPOSED STORM LINE
  - PROPOSED FIRE HYDRANT
  - PROPOSED 3/4" WATER METER
  - PROPOSED 3/4" BACKFLOW
  - PROPOSED CURB
  - PROPOSED DETENTION POND FENCE
  - PROPOSED 6" SANITARY SEWER LATERAL
  - PROPOSED 3/4" WATER LINE
  - PROPOSED 3/4" IRRIGATION LINE
  - PROPOSED SITE LIGHTING
  - PROPOSED LANDSCAPE AREA
  - PROPOSED BUILDING FOOTING DRAIN

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KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

**KITTRIDGE ENGINEERS, LLC**  
 CLIENT  
**Wilsonville Devco LLC**  
 c/o Integrated Investment Partners  
 PO Box 916  
 Portland, OR 97207  
 (503) 688-9600

PROJECT  
**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE  
**PRELIMINARY UTILITY PLAN**

REVISIONS


STAMP

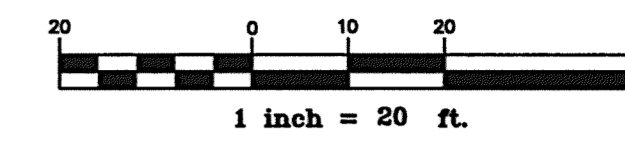
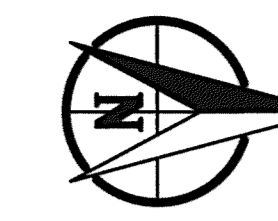
REGISTERED PROFESSIONAL ENGINEER  
**PRELIMINARY**  
 NOT FOR CONSTRUCTION  
 C. S. KITTRIDGE  
 RENEWAL: 06/30/13

THESE DRAWINGS ARE NOT FOR CONSTRUCTION UNLESS THEY BEAR THE APPROVAL STAMP OF THE CITY OF WILSONVILLE OR OTHER REQUIRED REGULATING AGENCIES

DATE: 01-07-2013  
 BLDG. SUBMITTAL DATE: ①  
 DATE: ②  
 DATE: ③  
 BID DATE: \_\_\_\_\_  
 CKE NO.: \_\_\_\_\_  
 ARCH. NO.: 1199  
 SCALE: \_\_\_\_\_  
 SHEET

**DD2**

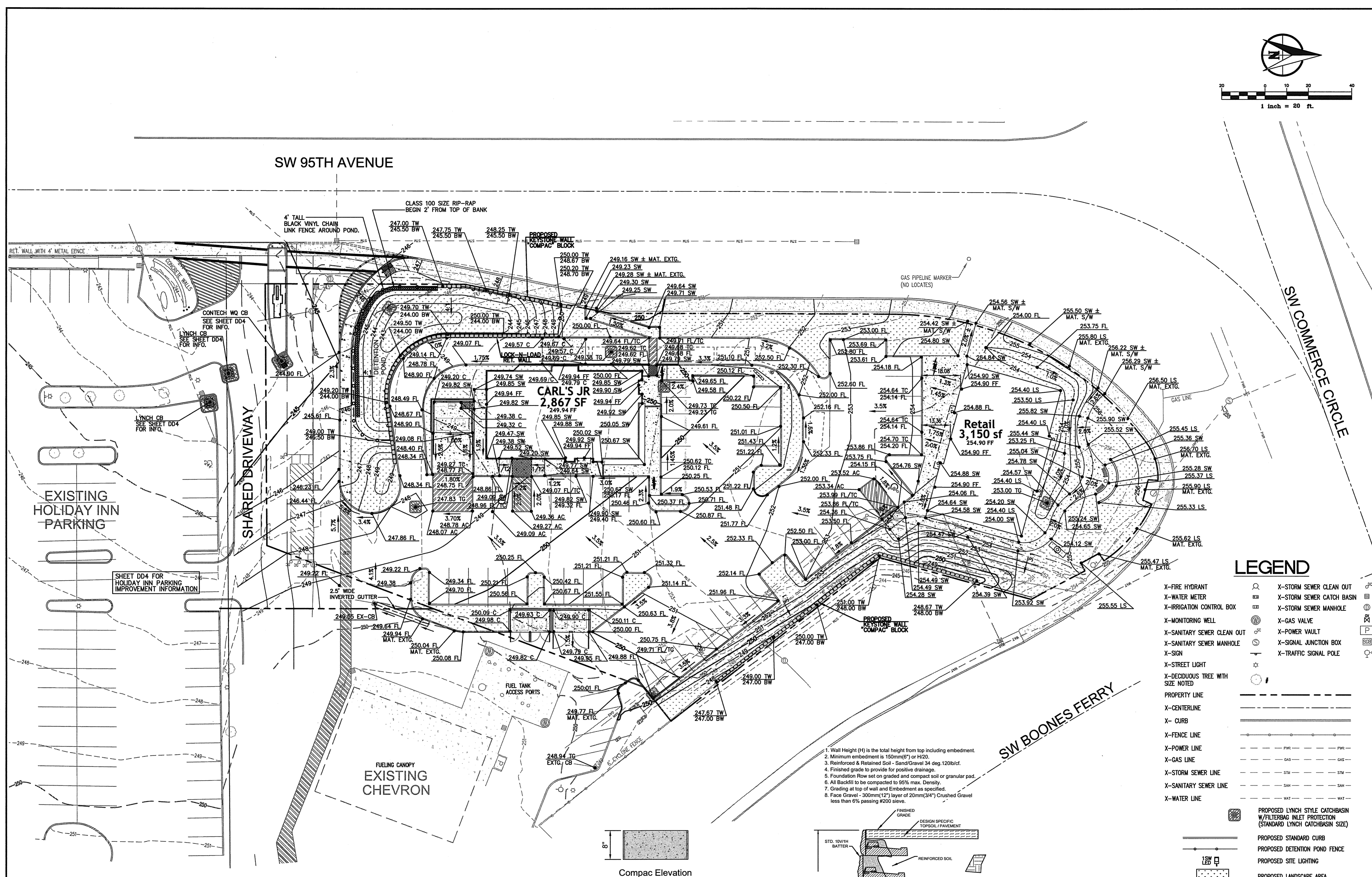




SW 95TH AVENUE

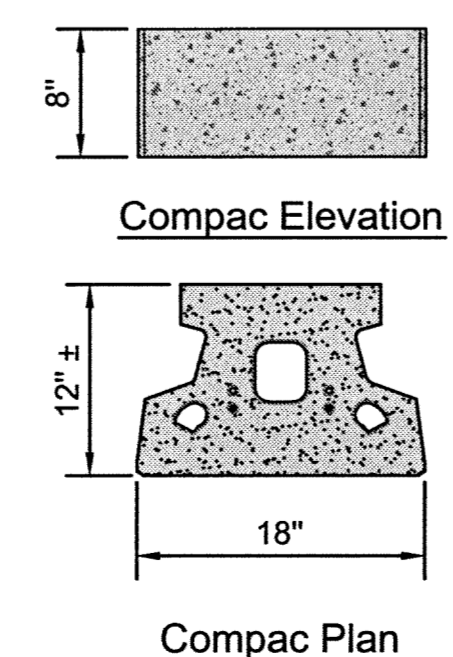
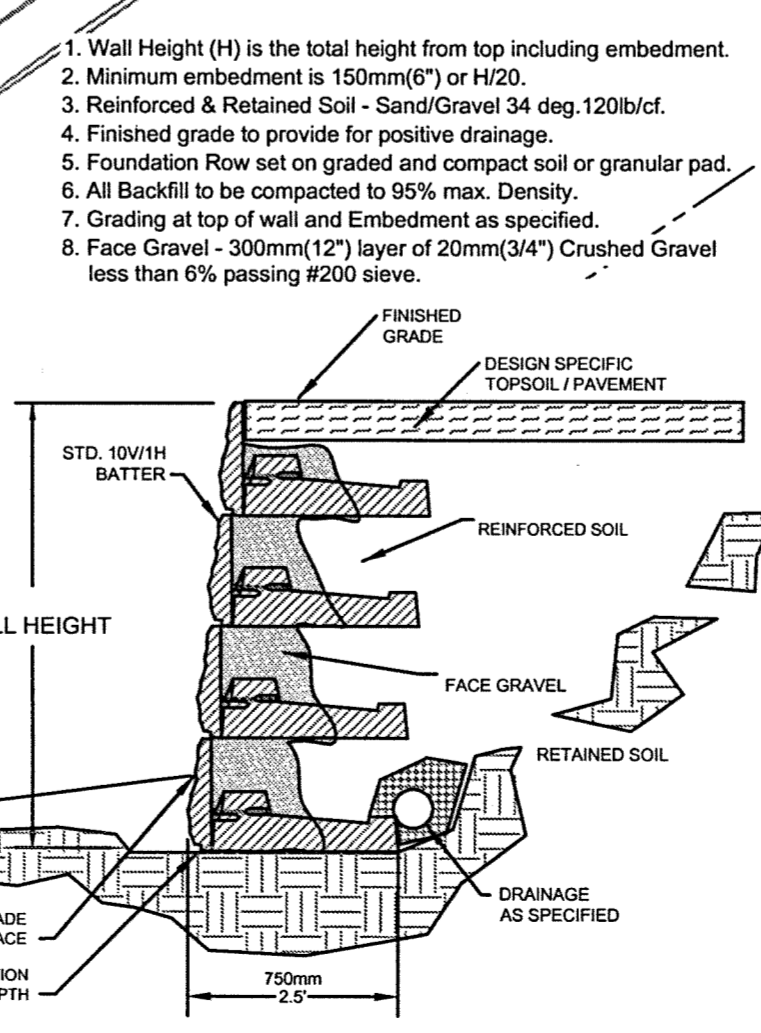
SW COMMERCE CIRCLE

SW BOONES FERRY



### LEGEND

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
- PROPERTY LINE
- X-CENTERLINE
- X-CURB
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- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
- X-STORM SEWER MANHOLE
- X-GAS VALVE
- X-POWER VAULT
- X-SIGNAL JUNCTION BOX
- X-TRAFFIC SIGNAL POLE
- PROPOSED LYNCH STYLE CATCHBASIN W/FILTERBAG INLET PROTECTION (STANDARD LYNCH CATCHBASIN SIZE)
- PROPOSED STANDARD CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA
- SPOT ELEVATION - TOP OF CURB
- SPOT ELEVATION - FLOW LINE, GUTTER
- SPOT ELEVATION - TOP OF WALL
- SPOT ELEVATION - BOTTOM OF WALL
- SPOT ELEVATION - FINISH FLOOR
- SPOT ELEVATION - TOP OF GRATE
- SPOT ELEVATION - CONCRETE PAD
- SPOT ELEVATION - LANDSCAPE



Compac Keystone Wall Unit  
NOT TO SCALE - WEST POND WALL

Lock-n-Load Wall Section  
NOT TO SCALE - EAST POND WALL

NOTE: SEE SHEET DD4 FOR SHARED DRIVEWAY PLAN AND PROFILE.

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

**KITTRIDGE ENGINEERS, LLC**  
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TEL: (503) 708-3942

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PROJECT  
**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE  
**PRELIMINARY GRADING PLAN**

REVISIONS	
1	
2	
3	



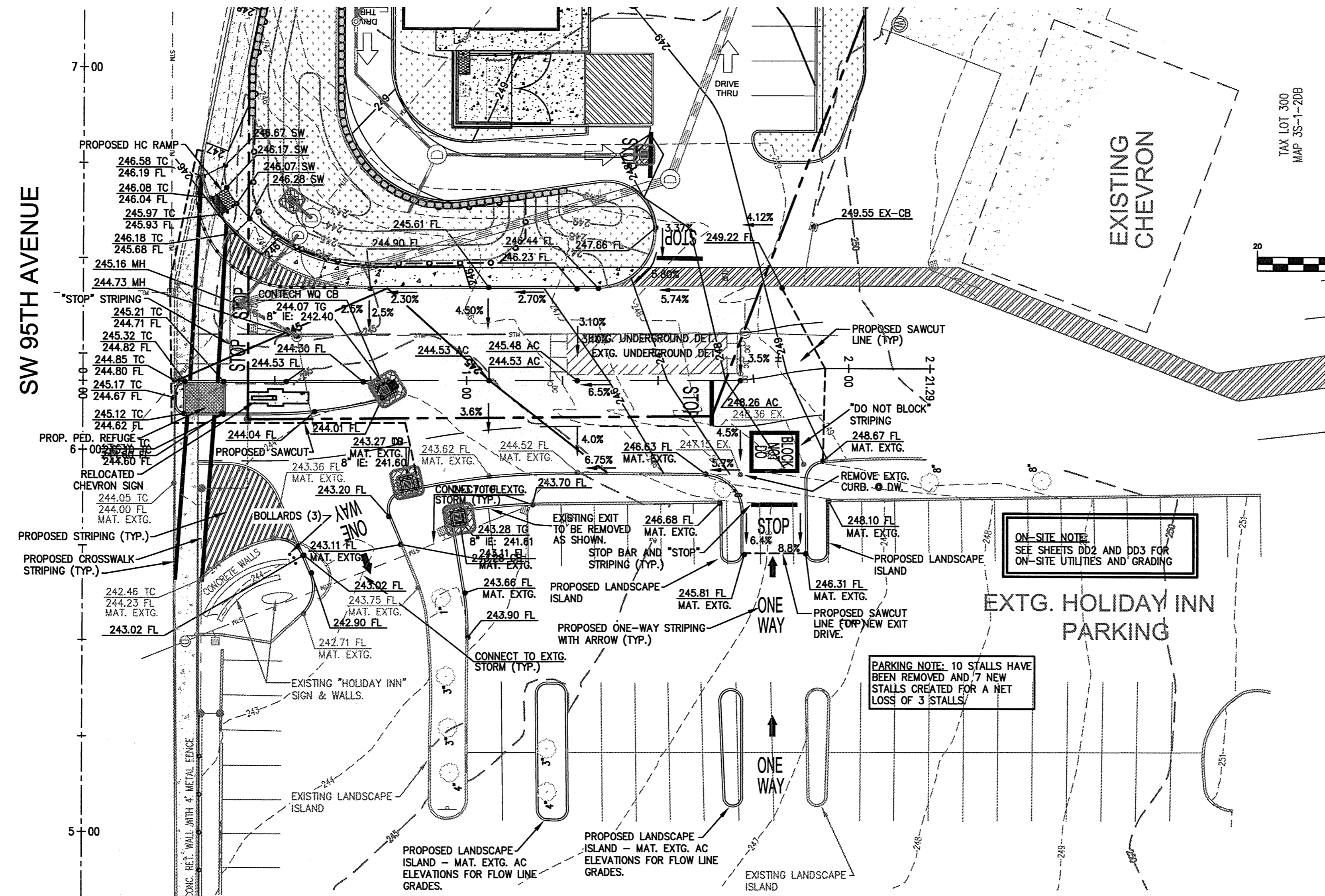
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DATE: 01-07-2013  
BLDG. SUBMITTAL DATE: ①  
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CKE NO.:  
ARCH. NO.: 1199  
SCALE:  
SHEET

**DD3**

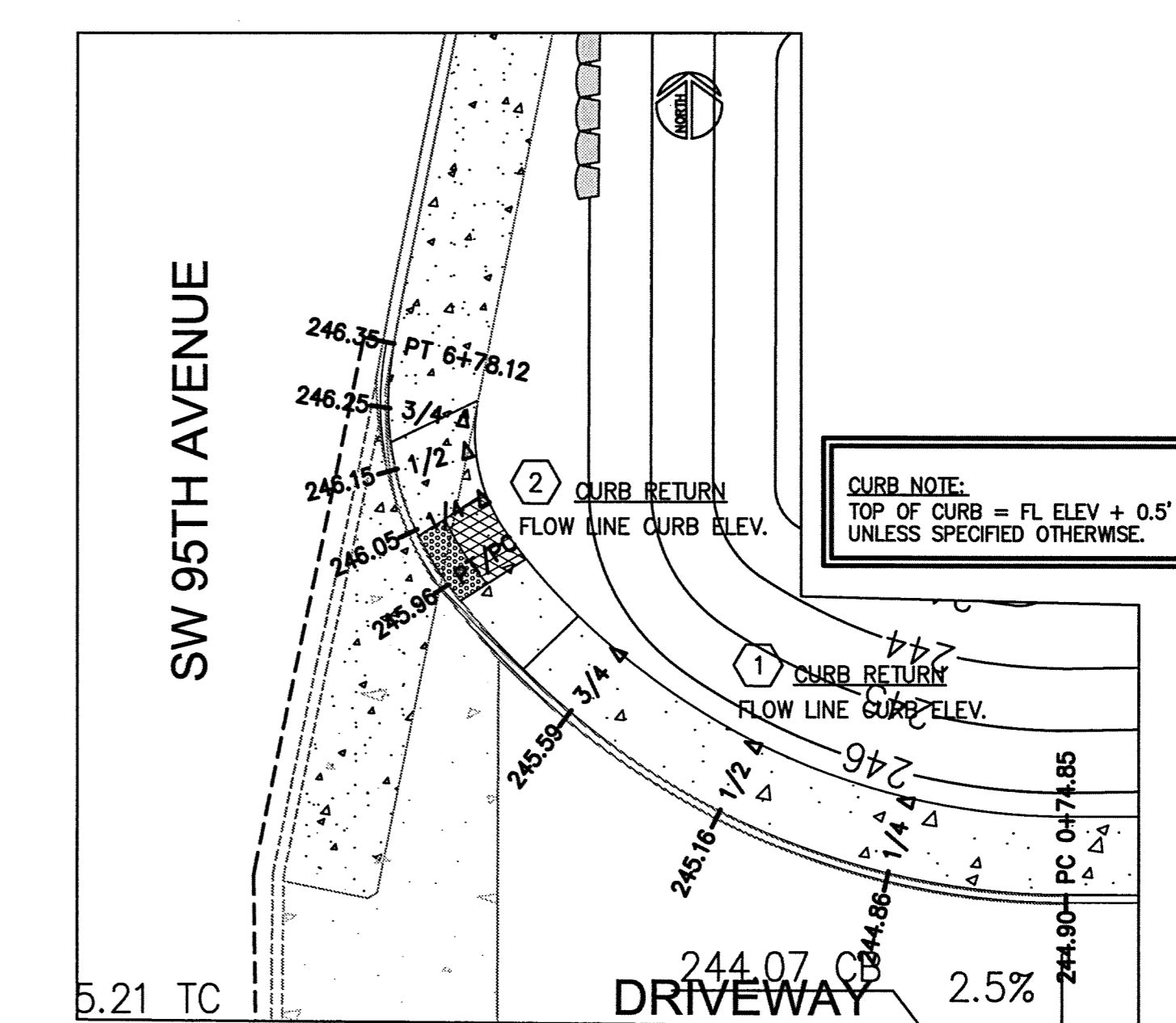
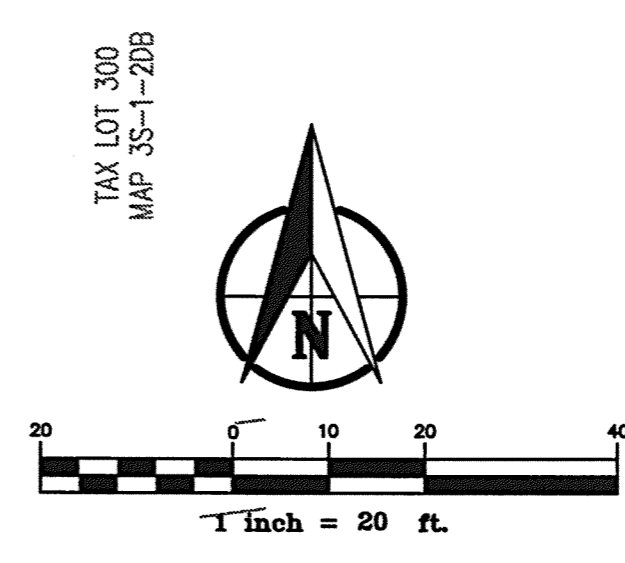
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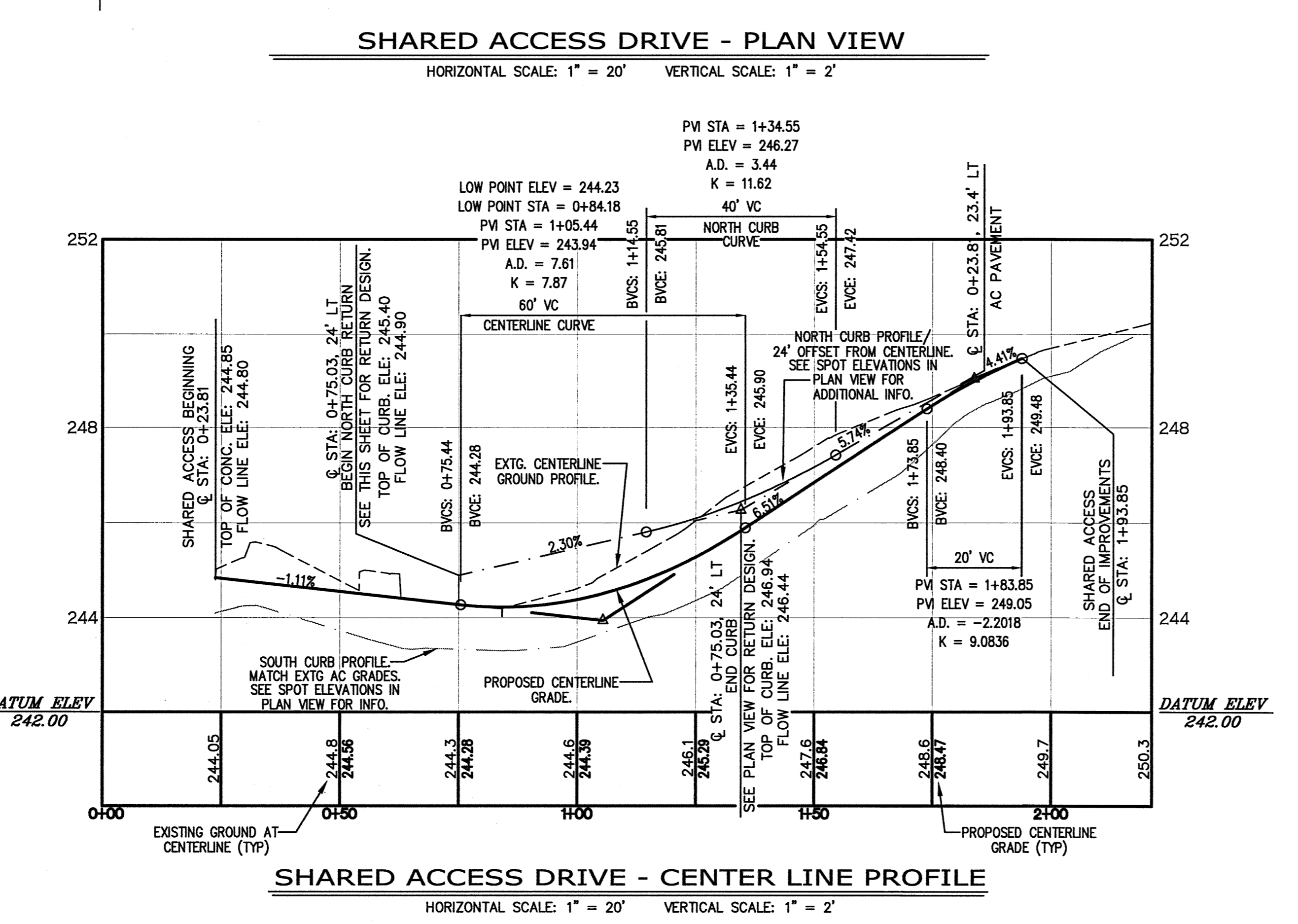


CURB RETURN NO. 1 DATA	
CURVE DATA	STREET STATION
①	P.C. = 0+74.85
$\Delta = 53^{\circ}00'00''$	P.T. = 0+34.68
R = 50.00'	DRIVEWAY - EAST
T = 25.18'	P.T. = 0+34.68
L = 46.65'	DRIVEWAY - WEST

CURB RETURN NO. 2 DATA	
CURVE DATA	STREET STATION
②	P.C. = 6+78.12
$\Delta = 48^{\circ}00'00''$	P.T. = 6+78.12
R = 20.00'	DRIVEWAY - WEST
T = 8.91'	P.T. = 6+78.12
L = 16.76'	NW 95TH AVENUE



**CURB RETURN DATA**  
SCALE: 1" = 10'



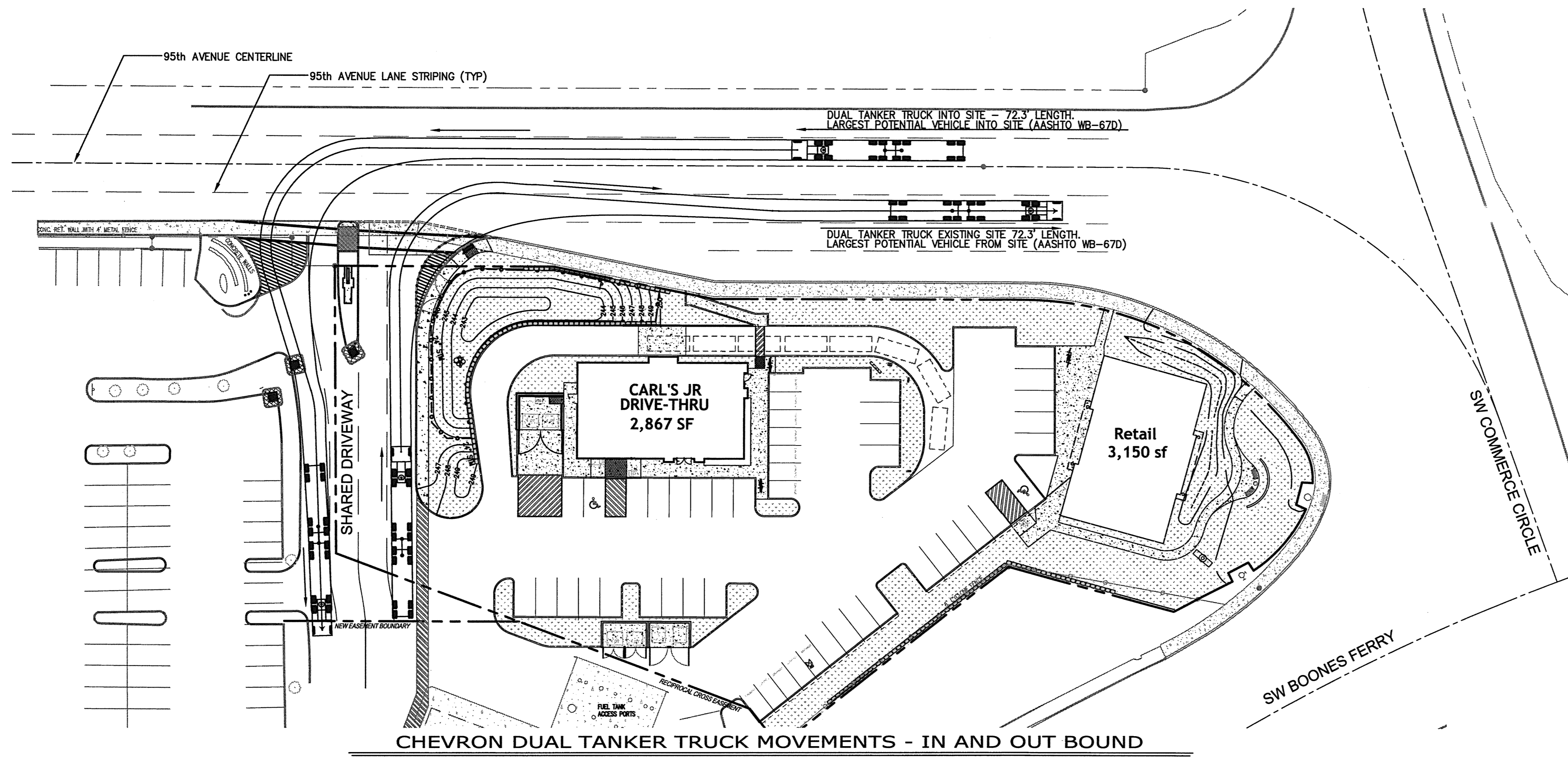
**LEGEND**

- X-FIRE HYDRANT
- X-WATER METER
- X-IRRIGATION CONTROL BOX
- X-MONITORING WELL
- X-SANITARY SEWER CLEAN OUT
- X-SANITARY SEWER MANHOLE
- X-SIGN
- X-STREET LIGHT
- X-DECIDUOUS TREE WITH SIZE NOTED
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- X-CENTERLINE
- X-CURB
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- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- X-STORM SEWER CLEAN OUT
- X-STORM SEWER CATCH BASIN
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- PROPOSED CURB
- PROPOSED DETENTION POND FENCE
- PROPOSED SITE LIGHTING
- PROPOSED LANDSCAPE AREA

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

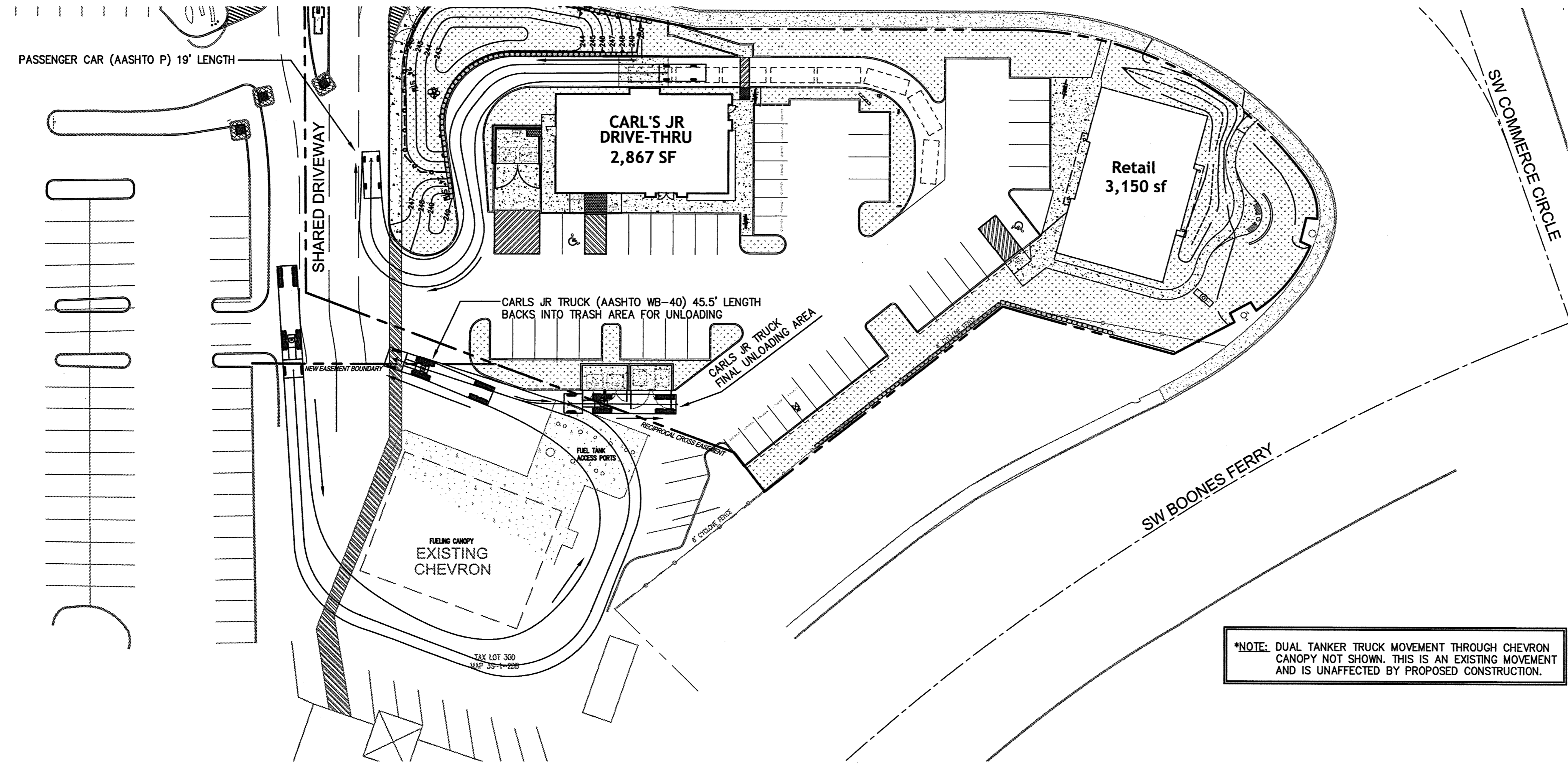
FILE NAME:   
 PROJECT: **Wilsonville Devco LLC**   
 c/o Integrated Investment Partners   
 PO Box 916   
 Portland, OR 97207   
 (503) 688-9400   
 CLIENT: **KITTRIDGE ENGINEERS, LLC**   
 6565 SW 207TH AVENUE   
 ALAHA, OR 97007   
 TEL: (503) 708-3942   
 DRAWING TITLE: **SHARED DRIVEWAY AND PROFILE & HOLIDAY INN PARKING IMPROVEMENTS**   
 REVISIONS:   
 STAMP: **REGISTERED PROFESSIONAL ENGINEER**   
**PRELIMINARY**   
**NOT FOR CONSTRUCTION**   
 P. KITTRIDGE   
 RENEWAL 08/30/13   
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 DATE: 01-07-2013   
 BLDG. SUBMITTAL DATE:   
 DATE:   
 BID DATE:   
 CKE NO.:   
 ARCH. NO.: 1199   
 SCALE:   
 SHEET: **DD4**





**CHEVRON DUAL TANKER TRUCK MOVEMENTS - IN AND OUT BOUND**

SCALE: 1" = 30'



**PASSENGER CAR AND SERVICE TRUCK TURNING MOVEMENTS - CARLS JR.**

SCALE: 1" = 30'

**LEGEND**

- PROPERTY LINE
- X-CENTERLINE
- X-CURB
- X-FENCE LINE
- X-POWER LINE
- X-GAS LINE
- X-STORM SEWER LINE
- X-SANITARY SEWER LINE
- X-WATER LINE
- PROPOSED STANDARD CURB
- PROPOSED LANDSCAPE AREA

\*NOTE: DUAL TANKER TRUCK MOVEMENT THROUGH CHEVRON CANOPY NOT SHOWN. THIS IS AN EXISTING MOVEMENT AND IS UNAFFECTED BY PROPOSED CONSTRUCTION.

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PROJECT  
**Boones Ferry Pointe**  
 SW 95TH AVE & COMMERCE CIRCLE  
 WILSONVILLE, OREGON

DRAWING TITLE  
**PROPOSED TRUCK TURNING MOVEMENTS**

REVISIONS



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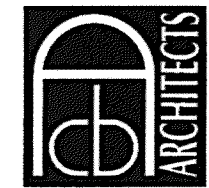
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 BID DATE:  
 CKE NO.:  
 ARCH. NO.: 1199  
 SCALE:  
 SHEET

**DD5**

FILE NAME:

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14





CB ANDERSON ARCHITECTS PLLC

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2911  
Fax 782-5624

CLIENT

Wilsonville Devco LLC  
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90 Box 916  
Portland, OR 97207  
(503) 688-9600

PROJECT

Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

DRAWING TITLE

REVISIONS

PER PLANING  
01/02/2013

STAMP

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DATE: 11/09/2012

BLDG. SUBMITTAL DATE: ①  
DATE: ②  
DATE: ③

BD DATE:

CKE NO.:

ARCH. NO.: 1199

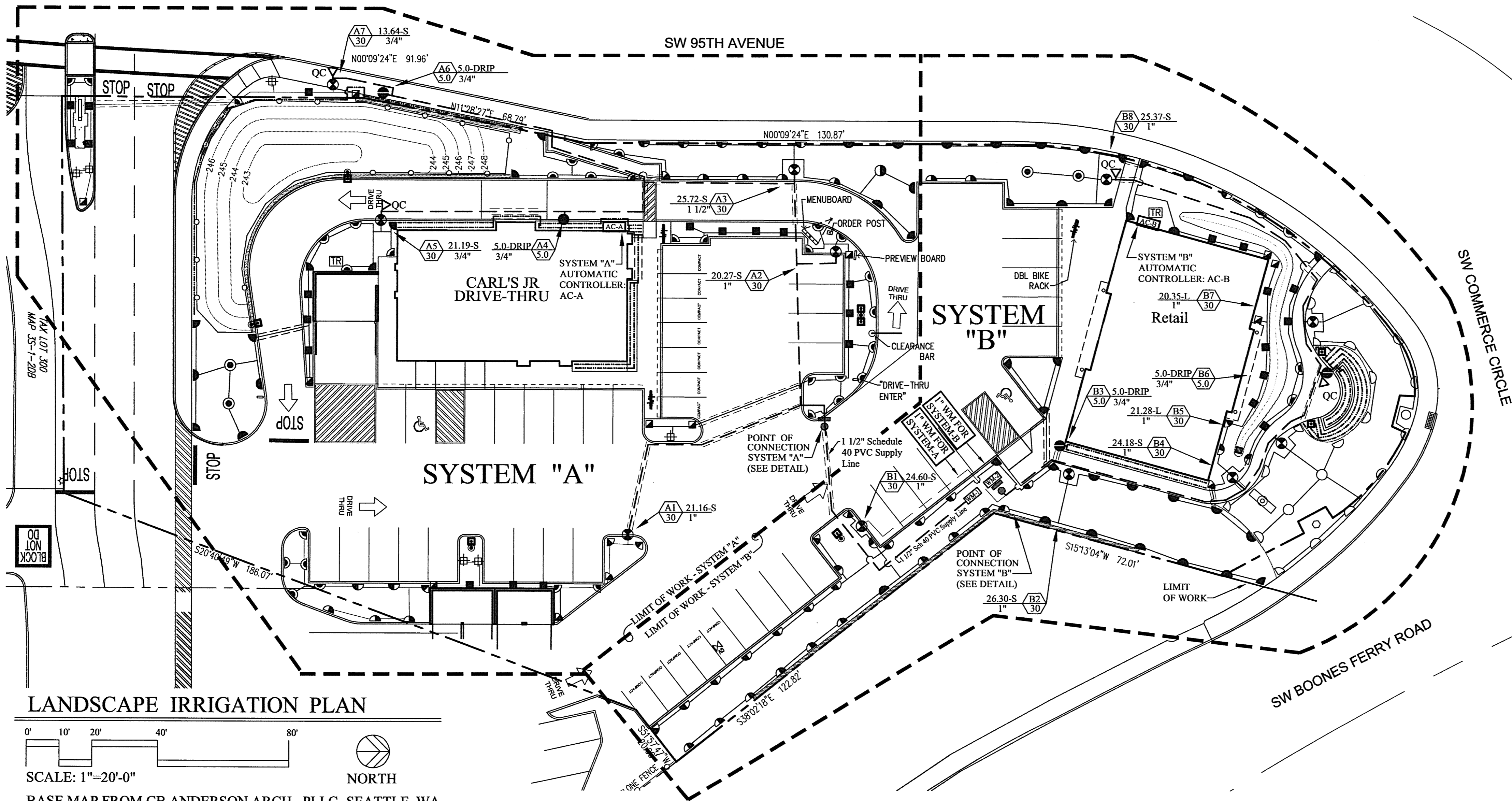
SCALE:

SHEET LANDSCAPE PLAN

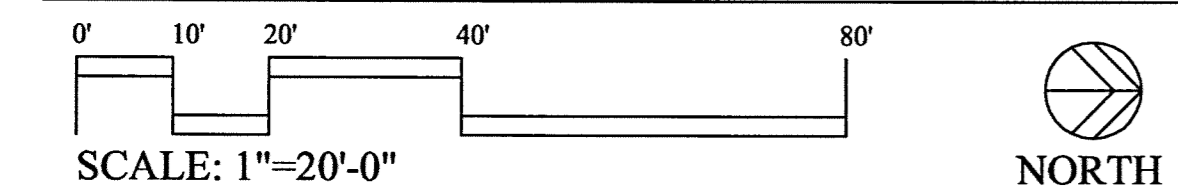
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K&K Foods CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14



LANDSCAPE IRRIGATION PLAN



SCALE: 1"=20'-0"

BASE MAP FROM CB ANDERSON ARCH., PLLC.-SEATTLE, WA

IRRIGATION LINE AND SLEEVE SIZING

CLASS 200 PVC LATERAL LINE		SCHEDULE 40 PVC MAIN LINE	
3/4" LINE	UP TO 10 GPM	3/4" VALVE	UP TO 17 GPM
1" LINE	BETWEEN 11-15 GPM	1" VALVE	BETWEEN 18-25 GPM
1 1/4" LINE	BETWEEN 16-25 GPM	1 1/2" VALVE	BETWEEN 26-64 GPM
1 1/2" LINE	BETWEEN 26-35 GPM	2" VALVE	OVER 65 GPM
2" LINE	BETWEEN 35-60 GPM		
2 1/2" LINE	OVER 61 GPM		

IRRIGATION NOTES

- Irrigation design and layout assumes minimum 40 lbs. static water pressure 35 GPM at point of connection. Verify PSI and GPM, if discrepancy exists report to Landscape Architect and owner before proceeding.
- The Landscape Contractor shall inspect the site and verify existing conditions and dimensions prior to bidding and construction.
- Landscape Architect to have 48 hour notice to review pressure test for project. Provide owner with backflow certification for review and approval.
- Irrigation plan is diagrammatic. Adjust pipe, valves and irrigation heads according to field conditions and plant locations. Report any discrepancies to Landscape Architect.
- Coordinate irrigation points of connection and location of automatic controller with General Contractor. Provide two (2) 1" Water Meter by General Contractor. Coordinate all work with other trades involved.
- All valves will be placed in valve boxes in a manner which facilitates access for maintenance. Locate valves in shrub and ground cover beds unless approved by Landscape Architect. No valve boxes in lawn areas.
- All components of irrigation system shall be installed and adjusted to provide adequate coverage and no over spray onto buildings, windows, or paved areas. Contractor is responsible for providing a complete working irrigation system.
- Contractor is responsible for any damages to building and infrastructure during landscape construction.
- Bury control wires below mainline and tape to mainline every 10' o.c.
- Flush mainline before installing valves. Flush zone lines before installing nozzles. Test entire system before back filling trenches.
- Electric Valves and Backflow prevention assembly to be placed in below grade valve boxes and installed in shrub beds. Verify that valve boxes are not placed where plant materials are to be installed. Provide sleeving under all paved surfaces.
- Provide full coverage to all lawn and shrub beds. Install additional irrigation heads as necessary to achieve full coverage and water to all plantings.
- 120 AC Electrical power source at controller locations shall be provided by General Contractor. The Landscape Contractor shall make the final connection from the electrical source to the controller. All wiring per L & C Codes.
- All irrigation heads to be installed perpendicular to finish grade unless otherwise specified. Place all lawn heads in lawn areas, 4" from edge of paving or bedline. Place all shrub heads in shrub beds, min. 4" from bedline and min. 10" from all structures. CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS.
- Winterizing the system: The irrigation contractor shall install a manual drain valve assembly. Draining the mainline shall be accomplished by introducing compressed air into the system through the quick coupler valve at the point of connection.
- Demonstrate working system to building maintenance manager upon completion and acceptance.
- Locate all utilities before digging. Call 1-(800) 424-5555.
- Install irrigation system in accordance with all applicable codes and ordinances.
- Mount Rain Sensor near top of building. Place in approved location with Landscape Architect.
- From controller install one spare yellow wire to each valve box. All wire splices to be for wire connections: DBY-Direct Bury Splice by 3M.
- Contractor to provide reproducible PDF As-built drawings to Landscape Architect prior to close-out of job.
- Electrical control wires to be spliced in control boxes only.
- Asphalt and/or concrete to be removed 2" from behind extruded curb or sidewalk in planter.
- Provide one (1) year warranty for parts, materials and workmanship upon final acceptance date from owner for work completed per Irrigation Plan.
- All mainlines to be buried min. 18" depth below grade; lateral lines to be buried min. 12" depth below grade.
- Sprinkler heads adjacent to head in parking, to be set on parking lot stripe.

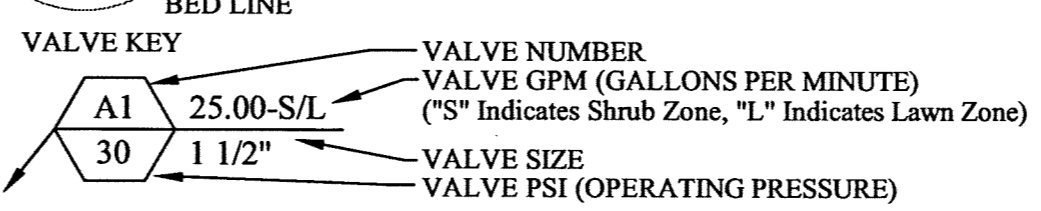
IRRIGATION LEGEND

SYMBOL	MANUFACTURER	PSI	RADIUS	GPM	LOCATION	SYMBOL	MANUFACTURER
○	RAIN BIRD 1804 W/15F-LA MPR PLASTIC NOZZLE	30	15'	3.70	LAWN	△	ACA
○	RAIN BIRD 1804 W/15H-LA MPR PLASTIC NOZZLE	30	15'	1.85	LAWN	△	ACA-B
○	RAIN BIRD 1804 W/15Q-LA MPR PLASTIC NOZZLE	30	15'	0.93	LAWN	↑	GLEN HILTON MC MINI CLICK II RAIN SENSOR WITH BY-PASS SWITCH. INSTALL PER OWNER.
○	RAIN BIRD 1804 W/12F MPR PLASTIC NOZZLE	30	12'	2.60	LAWN	1 1/4" MAIN	SCHEDULE 40 PVC MAIN LINE, SIZE PER PLAN, MIN. DEPTH 18" BELOW FINISHED GRADE.
○	RAIN BIRD 1804 W/12H MPR PLASTIC NOZZLE	30	12'	1.30	LAWN	3/4"	CLASS 200 PVC LATERAL LINE, SIZE PER PLAN, MIN. DEPTH 12" BELOW FINISHED GRADE.
○	RAIN BIRD 1804 W/12Q MPR PLASTIC NOZZLE	30	12'	0.65	LAWN	2" SL	SCHEDULE 40 PVC SLEEVING, SIZE PER PLAN, MIN. DEPTH 18" BELOW FINISHED GRADE FOR MAINLINE. MINIMUM DEPTH 12" BELOW FINISHED GRADE FOR LATERAL LINES.
○	RAIN BIRD 1804 W/10F MPR PLASTIC NOZZLE	30	10'	1.57	LAWN	△	OC
○	RAIN BIRD 1804 W/10H MPR PLASTIC NOZZLE	30	10'	0.78	LAWN	△	ACA
○	RAIN BIRD 1804 W/10Q MPR PLASTIC NOZZLE	30	10'	0.39	LAWN	△	ACA-B
○	RAIN BIRD 1804 W/9SST MPR PLASTIC NOZZLE	30	9x18"	1.73	LAWN	↑	GLEN HILTON MC MINI CLICK II RAIN SENSOR WITH BY-PASS SWITCH. INSTALL PER OWNER.
○	RAIN BIRD 1804 W/15SST MPR PLASTIC NOZZLE	30	4x30"	1.21	LAWN	1 1/4" MAIN	SCHEDULE 40 PVC MAIN LINE, SIZE PER PLAN, MIN. DEPTH 18" BELOW FINISHED GRADE.
○	RAIN BIRD 1804 W/15EST MPR PLASTIC NOZZLE	30	4x15"	0.61	LAWN	3/4"	CLASS 200 PVC LATERAL LINE, SIZE PER PLAN, MIN. DEPTH 12" BELOW FINISHED GRADE.

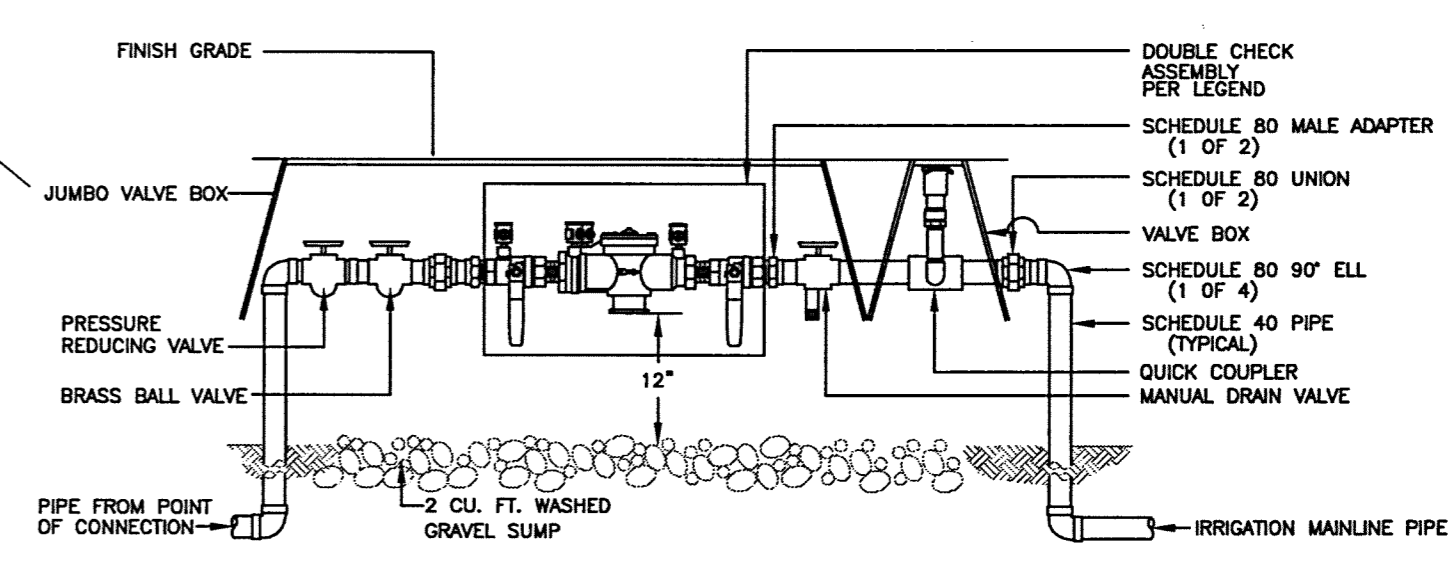
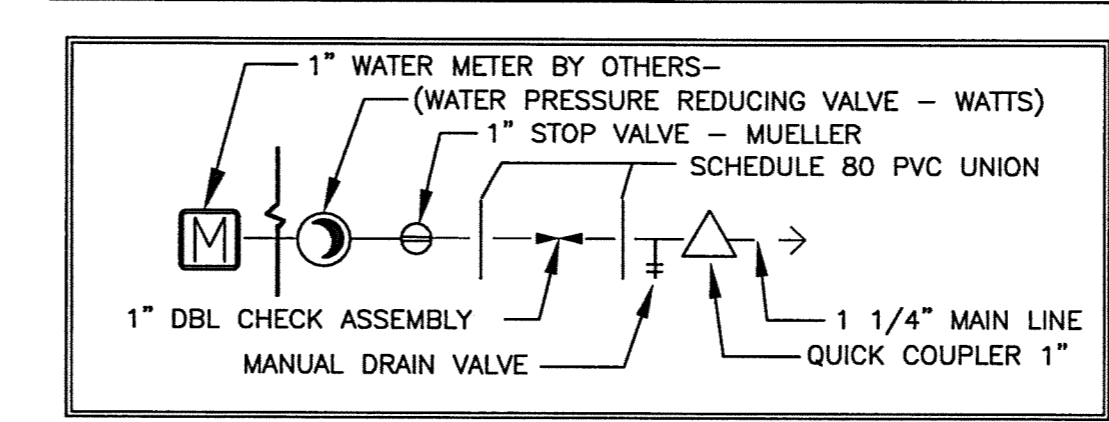
- NOTE: PLACE ALL LAWN HEADS IN LAWN, MIN. 3" AWAY FROM PAVING OR BEDLINE.
- NOTE: USE 12" HEIGHT POP-UP HEADS IN ALL OTHER LOCATIONS. CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS. USE 6" POP-UP HEIGHT HEADS ADJACENT TO HEAD IN PARKING STALLS.
- RAIN BIRD 1812 W/15F MPR PLASTIC NOZZLE 30 15' 3.70 SHRUB
  - RAIN BIRD 1812 W/15H MPR PLASTIC NOZZLE 30 15' 1.85 SHRUB
  - RAIN BIRD 1812 W/15Q MPR PLASTIC NOZZLE 30 15' 0.93 SHRUB
  - RAIN BIRD 1812 W/12F MPR PLASTIC NOZZLE 30 12' 2.60 SHRUB
  - RAIN BIRD 1812 W/12H MPR PLASTIC NOZZLE 30 12' 1.30 SHRUB
  - RAIN BIRD 1812 W/12Q MPR PLASTIC NOZZLE 30 12' 0.65 SHRUB
  - RAIN BIRD 1812 W/10F MPR PLASTIC NOZZLE 30 10' 1.57 SHRUB
  - RAIN BIRD 1812 W/10H MPR PLASTIC NOZZLE 30 10' 0.78 SHRUB
  - RAIN BIRD 1812 W/10Q MPR PLASTIC NOZZLE 30 10' 0.39 SHRUB
  - RAIN BIRD 1812 W/9SST MPR PLASTIC NOZZLE 30 9x18" 1.73 SHRUB
  - RAIN BIRD 1812 W/15SST MPR PLASTIC NOZZLE 30 4x30" 1.21 SHRUB
  - RAIN BIRD 1812 W/15EST MPR PLASTIC NOZZLE 30 4x15" 0.61 SHRUB

- DRIP LINE: MAXIMUM 18" WIDTH BETWEEN DRIP LINES
- RAIN BIRD DRIP LINE AND REQUIRED AIR/VACUUM RELIEF 30 TBD 1.50 SHRUB
- CONNECT INTO BOTTOM INLET OF 12" POP-UP HEADS. REQUIRED EQUIPMENT TO PROVIDE COMPLETE COVERAGE OF PLANT MATERIAL.
- RAINBIRD PEB SERIES AUTOMATIC CONTROL VALVE, SIZE PER PLAN AND PROVIDE AMTEX CONTROL BOX & MIN. 2 CU. FT. PEA GRAVEL.
  - RAINBIRD PEB SERIES AUTOMATIC CONTROL VALVE, SIZE PER PLAN AND RAINBIRD DRIP LINE VALVE - PER MANUFACTURE SPEC. PROVIDE AMTEX CONTROL BOX & MIN. 2 CU. FT. PEA GRAVEL.
  - WATER PRESSURE REDUCING VALVE (IF NEEDED) - WATTS SERIES US, SIZE PER P.O.C. DETAIL.
  - MUELLER BRASS ISOLATION VALVE, SIZE PER P.O.C. DETAIL, INSTALL IN ROUND 6" AMTEX CONTROL BOX
  - FEBCO DBL CHECK VALVE, SIZE PER P.O.C. DETAIL, INSTALL IN LARGE AMTEX CONTROL BOX PER DETAIL.

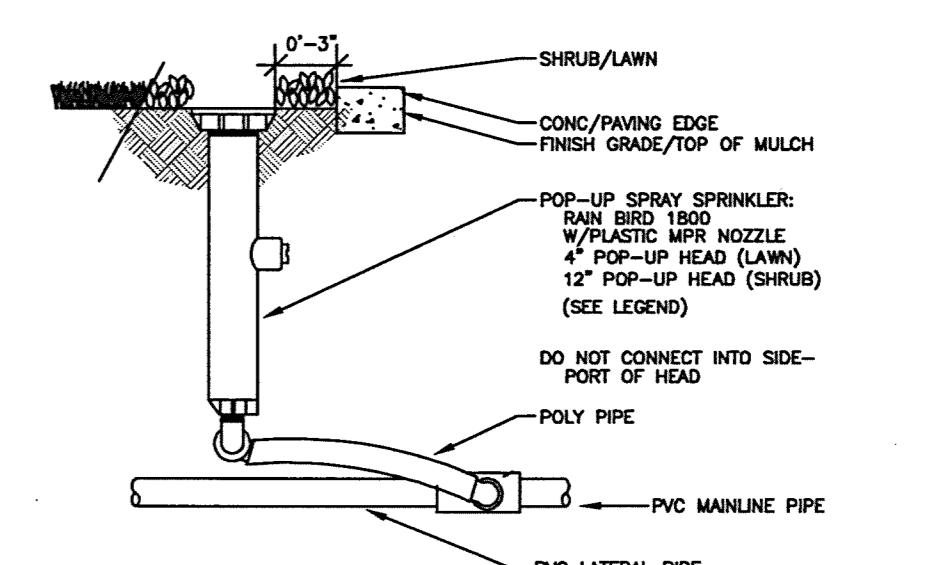
- RAINBIRD QUICK COUPLER VALVE 44RC, ASSUME 1" UNLESS OTHERWISE NOTED ON PLAN. PROVIDE HOSE SWIVEL. INSTALL PER DETAIL.
- SYSTEM "A" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.
- SYSTEM "B" - RAINBIRD ESP-LX MODULAR SERIES-8 STATION AUTOMATIC CONTROLLER. EXTERIOR MOUNT ON BUILDING, COORDINATE WITH OWNER. PROVIDE CONDUIT TO POWER SOURCE AND CONNECT TO POWER.



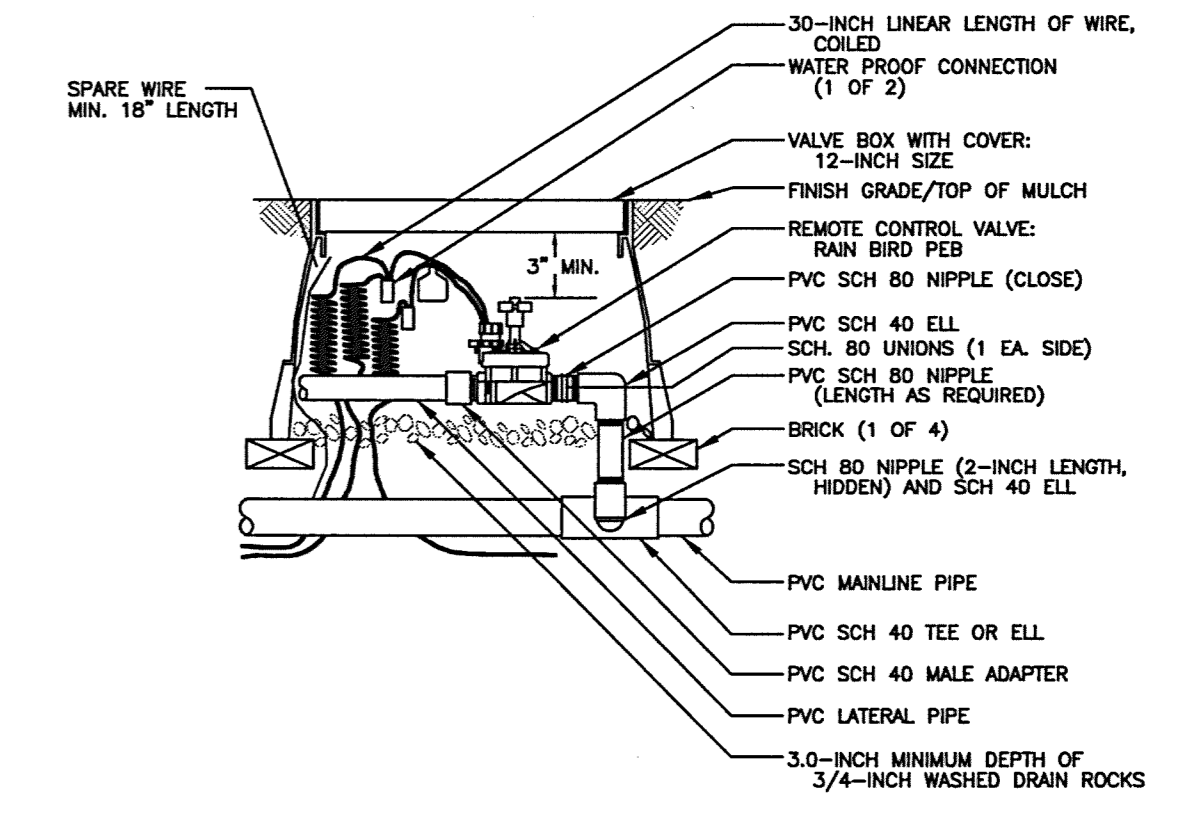
SYSTEM "A" AND SYSTEM "B"- POINT OF CONNECTION DETAIL



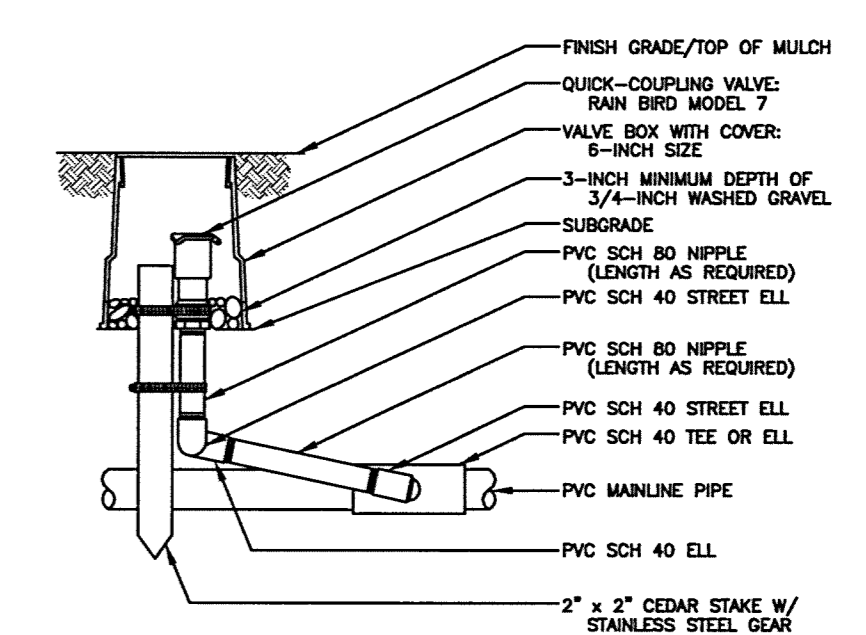
DETAIL: BACKFLOW PREVENTER  
NO SCALE



DETAIL: POP-UP SPRAY SPRINKLER  
NO SCALE



DETAIL: REMOTE CONTROL VALVE  
NO SCALE



DETAIL: QUICK-COUPLING VALVE  
NO SCALE

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SECTION 02800  
LANDSCAPE WORK

PART 1 - GENERAL

- 1.1 SUMMARY
A. Section includes preparation of subsoil and topsoil; topsoil bedding; new or relocated trees, plants, seeded grass, sod lawn, ground cover, mulch, amendments, fertilizer and maintenance.
B. When specifications contained in this Section conflict with the Landscape Drawings, the Landscape Drawings take precedent.
C. Related Sections:
1. Section 02311 - Grading
2. Section 02320 - Backfill: Rough grading of site.
3. Section 02324 - Trenching: Rough grading over trench cut.
4. Section 02811 - Landscape Irrigation.
1.2 REFERENCES
A. American National Standards Institute:
1. ANSI Z60.1 - Nursery Stock.
B. International Society of Arborists:
1. ISA - Certification Documentation for Tree Pruning Qualifications.
C. National Arborist Association:
1. NAA - Certification Documentation for Tree Pruning Qualifications.
1.3 DEFINITIONS
A. Weeds: Any plant life not specified for a given area or scheduled.
B. Plants: Living trees, plants, grass, and ground cover specified in this Section and described in ANSI Z60.1.
1.4 SUBMITTALS
A. Section 01330 - Submittal Procedures: Requirements for submittals.
B. Product Data: Submit list of plant material sources, data for amendments, fertilizer and other items required plant source list including nurseries and contact numbers 30 days after award of contract.
1.5 CLOSE-OUT SUBMITTALS
A. Section 01700 - Execution Requirements: Close-out procedures.
B. Operation and Maintenance Data: types, application frequency, and recommended coverage of fertilizer.
1.6 SUBSTITUTIONS
A. No material substitution shall be made without the written approval of the Landscape Architect or the Owner's Representative.
1.7 QUALIFICATIONS
A. Nursery: Company specializing in growing and cultivating the plants with five (5) years experience.
B. Installer: Company specializing in installing and planting the plants with five (5) years experience and approved by nursery. Installer must be a licensed and bonded landscaping Contractor.
C. Tree Pruner: Company specializing in pruning trees with proof of ISA Arborist Certification or NAA Certified Tree Care Specialist.
D. Maintenance Services: Performed by the installer.
1.8 DELIVERY, STORAGE, AND HANDLING
A. Section 01600 - Product Requirements: Product storage and handling requirements.
B. Deliver fertilizer in waterproof bags showing weight, chemical analysis, and name of manufacturer.
C. Protect and maintain plant life until planted.
D. Deliver plant life materials immediately prior to placement. Keep plants moist.
E. Plant material which has been damaged by reason of delivery, storage or handling will be rejected.
1.9 ENVIRONMENTAL REQUIREMENTS
A. Do not install plant life when ambient temperatures may drop below 35 degrees F or rise above 90 degrees F.
1.10 COORDINATION
A. Section 01300 - Administrative Requirements: Coordination and project conditions.
B. Install plant life after and coordinate with installation of underground irrigation system piping and watering heads specified in Section 02811.
1.11 GUARANTEE AND REPLACEMENT
A. Section 01700 - Execution Requirements: Product warranties and product bonds.
B. Beginning at time of final acceptance, all materials and workmanship shall be guaranteed for a time period of one (1) year. During time of guarantee all plant material shall be in a healthy, thriving condition.
C. Provide one (1) year manufacturer's guarantee for trees, plants, seeded grass, sod, and ground cover.
D. Replace any seeded or sodded areas which fail to show vigorous growth after three (3) weeks of seeding. Fill and seed all areas which settle, as specified. At the end of the guarantee period, the seeded and sodded areas shall be a flourishing, vigorous, uniform, thick stand of specified grasses with no bare spots and no weeds. Remove and replace immediately and dead, diseased, dying or missing plant materials at no cost to the Owner during the guarantee period. Replacement plants shall be as specified and guaranteed until completion of the guarantee period.
E. Lawns that have been hydroseeded shall be allowed to become established. Landscape Contractor to cut straight edge between hydroseeded lawn and mulch planter. Remove all lawn over spray in mulch planter, dispose of debris off-site.
1.12 MAINTENANCE SERVICE
A. Section 01700 - Execution Requirements: Maintenance service.
B. Beginning at the first installation of plant material, maintain plant life through final acceptance.
C. Continue maintenance until final acceptance.
D. Maintenance to include:
1. Cultivation and weeding plant beds and tree pits.
2. Applying herbicides for weed control. Remedy damage resulting from use of herbicides.
3. Remedy damage from use of insecticides.
4. Irrigating sufficient to saturate root system.
5. Pruning, including removal of dead or broken branches.
6. Disease control.
7. Maintaining wrapping, gurns, turnbuckles and stakes. Adjust turnbuckles to keep guy wires tight. Repair or replace accessories when required.
8. Fertilizer.
9. Removal and replacement of dead or damaged plants.
10. Mowing and edging lawn.

PART 2 - PRODUCTS

- 2.1 TREES, PLANTS, AND GROUND COVER
A. Planting Stock:
1. All plants shall be true to type and name in accordance with the latest edition of Standardized Plant Names, official code of the American Joint Committee on Horticulture Nomenclature, and each bundle or each plant shall be labeled properly.
2. All plants shall have a well-branched, vigorous and balanced root and exhibiting normal habit of growth and unless otherwise specified, shall be No 1 Grade conforming to "American Standard for Nursery Stock" of the American Association of Nurserymen (A.A.N). They shall be free from disease, injurious insects, insect eggs and larvae, mechanical wounds, broken branches, rot, decay or any other defect. Trees shall have reasonable straight trunks with well balanced tops and a single leader.
B. Trees, Plants, and Ground Cover:
1. Species and size identifiable in plant schedule, grown in climatic conditions similar to those in locality of the Work.
2.2 SOD LAWN
A. Sod shall be supplied by local supplier per approval of Landscape Architect.
B. Sod shall be premium quality, uniform green color, free of all weeds, pests, poa annua, and disease. Sod shall be a minimum of twelve (12) months old, but not more than thirty (30) months old, and shall be delivered to the project the site within twenty-four (24) hours of cutting.
2.3 SOIL MATERIALS
A. Topsoil: Imported topsoil from local supplier. Topsoil to have a 60/40 mix. Landscape Contractor to supply soil results and soil sample to Landscape Architect.
B. Topsoil: Fertile, agricultural soil, typical for locality, capable of sustaining vigorous plant growth, taken from drained site; free of subsoil, clay or impurities, plants, weeds and roots; minimum pH value of 5.4 and maximum pH value of 7.0, organic matter to exceed 1.5 percent, magnesium to exceed 100 units; phosphorus to exceed 150 units; potassium to Exceed 20 units; soluble salts/conductivity not to exceed 900 ppm/0.9 mmhos/cm in soil.

PART 2 - PRODUCTS (Continued)

- 2.4 SOIL AMENDMENT MATERIALS
A. If soil tests indicate soil amendment, apply soil conditioners/fertilizers to amend soil to specified conditions.
1. Tree Fertilizer: Containing fifty percent of the elements derived from organic sources; of proportion necessary to eliminate any deficiencies of topsoil as indicated by analysis.
B. Peat Moss: Shredded, loose sphagnum moss; free of lumps roots, inorganic material or acidic materials; minimum of 85 percent organic material measured by oven dry weight.
C. Bone Meal: Raw, granular, commercial grade, minimum of 3 percent nitrogen and 20 percent phosphorous.
D. Lime: Round limestone, dolomite type, minimum 85 percent carbonates with a minimum of 30 percent magnesium carbonates.
E. Water: Clean, fresh, potable and free of substances or matter which could inhibit vigorous growth of plants.
F. Herbicide: As allowed by local codes and regulations.
G. Pesticide: As allowed by local codes and regulations.
H. Osmacote slow release per plan.
2.5 MULCH MATERIALS
A. Mulching Material: Mulch shall consist of fine ground (1/4 inch minus) fir or hemlock bark supplied from fresh water mill, uniform in color and free from weeds, seeds, or other materials harmful to plant growth.
2.6 ACCESSORIES
A. Wrapping Materials: Tree-wrap, not less than 4 inch wide tape, designed to prevent bore damage and winter freezing.
B. Stakes: Lodge pole, 2" diameter with pointed end, min. 8' height.
C. Cable, Wire, Eye Bolts and Turnbuckles: Non-corrosive, of sufficient strength to withstand wind pressure and resulting movement of plant life.
D. Plant Protectors: Rubber sleeves over cable to protect plant stems, trunks, and branches.
E. Grates: Cast iron design, square, sized to resist pedestrian loads; where indicated on the Drawings.
F. Decorative Cover: Fir bark chips supplied from fresh water mill.
G. Tree Protectors: Plastic with galvanized rings.
2.7 SOURCE QUALITY CONTROL AND TESTS
A. Section 01400 - Quality Requirements: Testing and Inspections Services. Provide quality control and testing.
B. Testing is not required if recent tests are available within 12 months of installation for imported topsoil. Submit these test results to the testing laboratory. Indicate, by test results, information necessary to determine suitability.

PART 3 - EXECUTION

- 3.1 EXAMINATION
A. Section 01300 - Administrative Requirements: Coordination and project conditions.
B. Verify that prepared subsoil and planters are ready to receive Work.
C. Saturate soil with water to test drainage.
D. Verify that required underground utilities are available, in proper location, and ready for use.
3.2 PREPARATION OF SUBSOIL
A. Prepare subsoil to eliminate uneven areas. Maintain profiles and contours. Make changes in grade gradual. Blend slopes into level areas.
B. Remove foreign materials, weeds and undesirable plants and their roots. Remove contaminated subsoil.
C. Remove foreign materials larger than 1 inch in diameter prior to placement of topsoil.
D. Scarify subsoil to a depth of 6 inches where plants are to be placed. Repeat cultivation in areas where equipment used for hauling and spreading topsoil has compacted subsoil.
E. Dig pits and beds three (3) times wider than plant root system.
3.3 PLACING TOPSOIL
A. Spread topsoil over scarified subgrade. In lawn areas, place minimum 4" depth of approved topsoil over subgrade. In shrub planters, place minimum 6" depth of approved topsoil over subgrade. Rake smooth.
B. Place topsoil during dry weather and dry unfrozen subgrade.
C. Remove vegetable matter and foreign non-organic material from topsoil while spreading.
D. Grade topsoil to eliminate rough, low or soft areas, and to ensure positive drainage.
E. Planting holes intended for shrub material root balls to have minimum 8" depth of approved topsoil in bottom of pit. Planting holes intended for tree root balls to have minimum 12" depth of approved topsoil in bottom of pit.
F. Finish elevation shall be defined as 2 inches below curbs, walks, and/or other adjacent hardscape for all sod lawn areas, and flush with curbs, walks and/or other hardscape for all fine lawn hydroseeded areas. Finish elevation refers to grades prior to installation of mulch or lawn.
3.4 FERTILIZING
A. Apply after initial raking to topsoil.
B. Mix thoroughly into upper 2 inches of topsoil.
C. Lightly water to aid the dissipation of fertilizer.
3.5 PLANTING
A. Place plants for best appearance.
B. Set plants vertical.
C. Remove non-biodegradable root containers.
D. Set plants in pits or beds, partly filled with prepared plant mix, at a minimum depth as indicated on Drawings under each plant. Remove burlap, ropes and wires, from the top half of root ball.
E. Place bare root plant materials so roots lie in a natural position. Backfill soil mixture in 6 inch layers. Maintain plant life in vertical position.
F. Saturate soil with water when the pit or bed is half full of topsoil and again when full.
3.6 PLANT RELOCATION AND RE-PLANTING
A. Relocate plants as indicated on Drawings (as applicable).
B. Ball or pot removed plants if temporary relocation is required.
C. Place bare root plant materials so roots lie in a natural position. Backfill soil mixture in 6 inch layers. Maintain plant materials in vertical position.
D. Saturate soil with water when the pit or bed is half full of topsoil and again when the pit or bed is full of topsoil.
3.7 PLANT SUPPORT
A. Brace plants vertically with plant protector wrapped guy wires and stakes to the following:
Tree Caliper Tree Support Method
1 inch One (1) stake with one (1) tie.
1 - 1 3/4 inches Two (2) stakes with two (2) ties.
2 - 3 inches Three (3) stakes with three (3) ties, bolts and turn buckles.
Over 3 inches Four (4) guy wires with eye bolts and turn buckles.
3.8 FIELD QUALITY CONTROL
A. Section 01400 - Quality Requirements: Testing and inspection services.
B. Plants will be rejected if a ball of earth surrounding roots has been disturbed or damaged prior to or during planting.
3.9 CLEAN-UP
A. A general clean-up shall be made immediately after and as part of all Work done in the area. The clean-up shall include the entire area under this Contract. Adjacent areas shall be cleaned to the extent that the Work done under the Contract may scatter litter. Such clean-up shall include pick-up and removal from the area of Work all clippings, trimmings, leaves, and all other litter and debris originating from any source. Remove flag labels from all planting material.
3.10 SCHEDULE - PLANT LIST
A. See Landscape Materials Schedule on Landscape Drawings.

END OF SECTION

SECTION 02811  
UNDERGROUND IRRIGATION SYSTEM

PART 1 - GENERAL

- 1.1 SUMMARY
A. Section includes trenching, pipe and fittings, valves, sprinkler heads, accessories and controls for automatic landscape irrigation system.
B. Related Sections:
1. Section 02320 - Backfill: Backfilling utility structures.
2. Section 02324 - Trenching: Excavating and backfilling for irrigation piping.
3. Section 02800 - Landscape Work: Landscape plant and materials.
4. Section 16000 - Electrical Work - Bidder Design.
1.2 REFERENCES
A. American Society for Testing and Materials:
1. ASTM D2235 - Standard Specification for Solvent Cement for Acrylonitrile-Butadiene-Styrene (ABS) Plastic Pipe and Fittings.
2. ASTM D2241 - Standard Specification for Poly (Vinyl Chloride) (PVC) Pressure-Rated Pipe (SDR Series).
3. ASTM D2282 - Standard Specification for Acrylonitrile-Butadiene-Styrene (ABS) Plastic Pipe (SDR-PR).
4. ASTM D2564 - Standard Specification for Solvent Cements for Poly (Vinyl Chloride) (PVC) Plastic Piping Systems.
B. National Electrical Manufacturers Association:
1. NEMA 250 - Enclosures for Electrical Equipment (1000 Volts Maximum).
1.3 SYSTEM DESCRIPTION
A. Electronic solenoid or Hydraulic controlled underground automatic irrigation system, with low point or self pressure bowload drain, as indicated.
B. Source Power: 120 volts AC, single phase.
C. Low Voltage Controls: 24 volts.
1.4 SUBMITTALS
A. Section 01330 - Submittal Procedures:
B. Shop Drawings: Indicate piping layout to water source, location of sleeves under pavement, location and coverage of sprinkler heads, components, plant and landscaping features, site structures, schedule of outlets and fittings to be used.
C. Product Data: Submit manufacturer's technical data; and component and control system, and wiring diagrams.
1.5 CLOSEOUT SUBMITTALS
A. Section 01700 - Execution Requirements: Closeout procedures.
B. Project Record Documents: Record actual locations of all concealed components, piping system and conduit.
C. Operation and Maintenance Data:
1. Provide instructions for operation and maintenance of system and controls, seasonal activation and shutdown, and manufacturer's parts catalog.
2. Provide schedule indicating length of time each valve is required to be open to provide a determined amount of water.
1.6 QUALIFICATIONS
A. Manufacturer: Provide underground automatic irrigation as a complete unit produced by a single acceptable manufacturer, with minimum five (5) years experience, specializing in manufacturing products specified in this Section, including heads, valves, piping circuits, controls, and accessories.
B. Installer: Company specializing in performing Work of this Section with minimum five (5) years experience approved by manufacturer. Installer must be a licensed and bonded underground Irrigation Contractor.
1.7 FIELD MEASUREMENTS
A. Verify pressure, flow and volum prior to installing irrigation system. If discrepancy, report to Landscape Architect.
B. Verify that field conditions and field measurements are as indicated.
C. Verify landscape plant material staking and layout.
1.8 COORDINATION
A. Section 01300 - Administrative Requirements: Coordination and project coordination.
B. Coordinate the Work with site backfilling, landscape grading and delivery of plant life.
1.9 EXTRA MATERIALS
A. Section 01700 - Execution Requirements: Spare parts and maintenance products.
1. Two (2) sprinkler heads of each type and size.
2. Two (2) valve keys for manual valves.
3. Two (2) valve box keys.
4. Two (2) wrenches for each type head core and for removing and installing each type head.

PART 2 - PRODUCTS

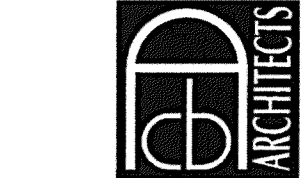
- 2.1 PIPE MATERIALS
A. PVC Pipe: ASTM D2241; Schedule 40, size as required; solvent-weld sockets.
B. Fittings: Type and style of connection to match pipe.
C. Solvent Cement: ASTM D2564 for PVC pipe fittings.
D. Sleeve Material: PVC.
2.2 OUTLETS
A. Manufacturers:
1. Rainbird Company.
2. Substitutions: Section 01600 - Product Requirements.
B. Outlets: Brass or bronze construction.
C. Rotary Type Sprinkler Head: Pop-up type without screens; fully adjustable for flow and pressure; size as indicated; with letter or symbol designating degree or arc and arrow indicating center of spray pattern.
D. Spray Type Sprinkler Heads: Fixed surface head: pop-up head with full circle, half circle, third circle, or quarter circle pattern.
E. Emitters: Adjustable outlet, non-clogging, with two (2) trickle tubes.
2.3 MANUAL VALVES
A. Globe Valves: Bronze construction, non-rising stem.
B. Backflow Preventers: Bronze body construction, double check valve type, as required by governing Authority Having Jurisdiction.
C. Valve Box and Cover: AMETEK, 12 inch standard box with locking lid.
2.4 CONTROLS AND CONTROL VALVES
A. Controller: Automatic controller, microprocessor solid state control with visible readout display and motor driven time for a multiple station system, programmable for seven (7) days in quarter (1/4) hour increments, with automatic start and shutdown.
B. Controller Housing: Weatherproof, watertight, with lockable access door.
C. Valves: Hydraulic or Electric Solenoid as indicated on Drawings, normally closed, including required fittings and accessories.
D. Wire Conductors: Color coded, copper conductor, direct burial type. Bundle grouped control wires together.

PART 3 - EXECUTION

- 3.1 EXAMINATION
A. Section 01300 - Administrative Requirements: Coordination and project conditions.
B. Verify location of existing utilities.
C. Verify that required utilities are available, in proper location, and ready for use.
3.2 PREPARATION
A. Comply with requirements of Uniform Plumbing Code.
B. Piping layout indicated is diagrammatic only. Route piping to avoid plants, ground cover, utilities, and structures.
C. Layout and stake locations of system components.
D. Review layout requirements with other affected Work. Coordinate locations of sleeves under paving to accommodate system.
3.3 TRENCHING
A. Trench and backfill in accordance with Section 02320 and Section 2324.
B. Sleeves to be a minimum twice (2 x) the IPS diameter of insert pipe, and shall extend a minimum of 12 inches beyond edge of paving, walks, and walls. Plug ends of pipe to prevent soil from entering ends, if backfill is required prior to use.
C. Valve Box and Cover: AMETEK, 12 inch standard box with locking lid.
3.4 SLEEVING
A. Install sleeves under all paved and surfaced areas, through walls, and as required to facilitate a smooth construction sequence. Extend sleeves as shown on Drawings. Coordinate with other trades as required.
B. Sleeves to be a minimum twice (2 x) the IPS diameter of insert pipe, and shall extend a minimum of 12 inches beyond edge of paving, walks, and walls. Plug ends of pipe to prevent soil from entering ends, if backfill is required prior to use.
C. Valve Box and Cover: AMETEK, 12 inch standard box with locking lid.
3.5 INSTALLATION
A. Connect to utilities.
B. Set outlets and box covers at finish grade elevations.
C. Install control wiring in accordance with National Electric Code. Provide 10 inch expansion coil at each valve to which controls are connected, and at 100 foot intervals. Bury wire beside pipe. Mark valves with neoprene valve markers containing locking device. Set valve markers in 160 psi PVC pipe risers exiting from top of valve to finish grade.
D. After piping is installed, but before outlets are installed and backfilling commences, open valves and flush system with full head of water.
E. Install lawn heads at manufacturer's recommended heights. Install shrubby heads at heights indicated. Located part-circle heads to maintain a minimum distance of 4 inches from walls and 2 inches from other boundaries, unless otherwise indicated.
F. Use dielectric fittings at connections where materials of dissimilar metal are joined.
3.6 FIELD QUALITY CONTROL
A. Section 01400 - Quality Requirements: Testing and inspection services 01700 - Execution Requirements: Testing, adjusting, and balancing.
B. Prior to backfilling, test system for leakage for whole system to maintain 100 psi pressure for one (1) hour. Piping may be tested in sections to expedite Work. Coordinate with Landscape Architect w/48 hours notice.
C. System is acceptable if no leakage or loss of pressure occurs and system self drains during test period. Remove and repair piping, connections, valves which do not pass hydrostatic testing, at no cost to Owner.
D. Provide one (1) complete spring start-up and a fall shutdown.
3.7 BACKFILLING
A. Provide 18 inches minimum cover over top of piping. Trench and backfill to subgrade elevation as specified in Section 02324 - Trenching. Protect piping from displacement.
3.8 ADJUSTING
A. Section 01700 - Execution Requirements: Testing, adjusting, and balancing.
B. Adjust control system to achieve time cycles required.
C. Set controller for water times suitable for the time of year and plan a written schedule on water, and place schedule adjacent to the controller.
D. Change or adjust head types for full water coverage as directed.
3.9 DEMONSTRATION AND TRAINING
A. Instruct Owner's personnel in operation and maintenance of system, including adjusting of sprinkler heads. Use operation and maintenance material as basis demonstration.

END OF SECTION

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PROJECT

Boones Ferry Pointe
SW 95TH AVE & COMMERCE CIRCLE
WILSONVILLE, OREGON

DRAWING TITLE

REVISIONS

Table with 2 columns: Description, Date. Row 1: PER PLANNING, 01/02/2013.

STAMP

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DATE: 11/09/2012

BLDG. SUBMITTAL DATE: 1

2

3

BID DATE:

CHE NO.:

ARCH. NO.: 1199

SCALE:

SHEET SPECIFICATIONS: IRRIGATION & PLANTING PLANS

L 3.0

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FILE NAME:

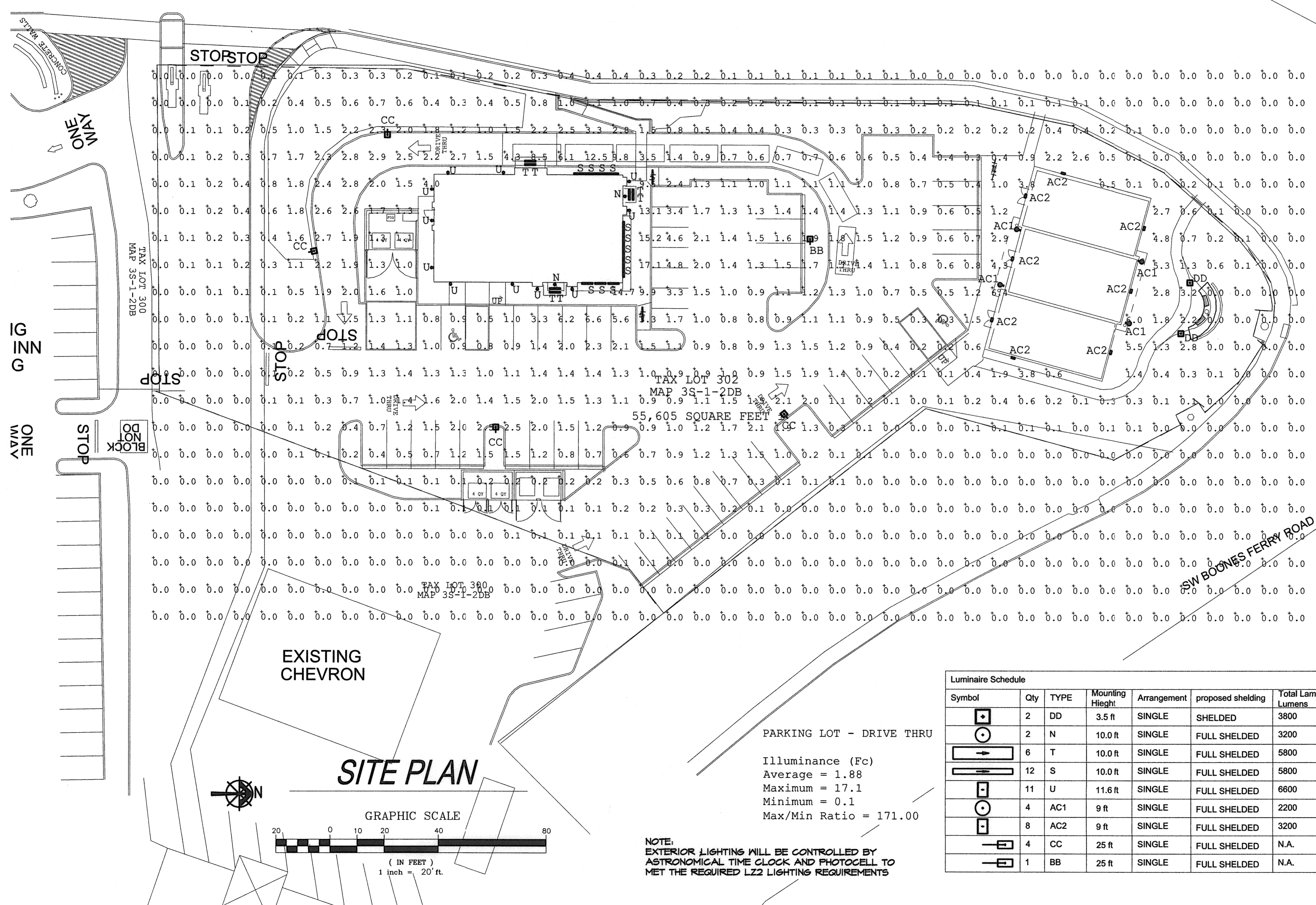
CGK Foods CONVERSION BASED ON CGK PROTOTYPE VERSION 22-14

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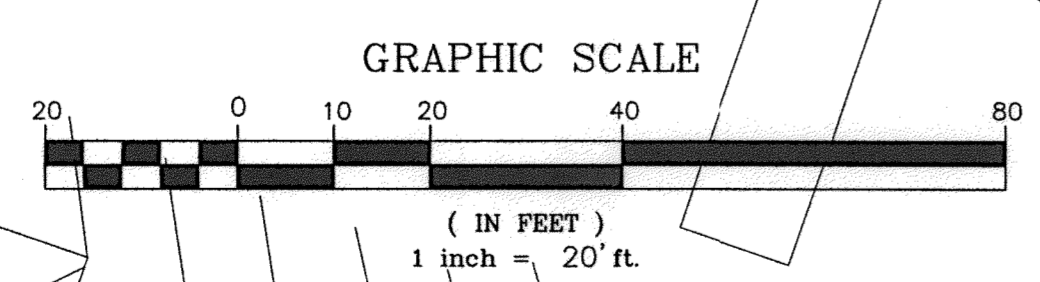
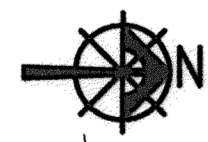


FILE NAME:

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### SITE PLAN



PARKING LOT - DRIVE THRU  
Illuminance (Fc)  
Average = 1.88  
Maximum = 17.1  
Minimum = 0.1  
Max/Min Ratio = 171.00

NOTE:  
EXTERIOR LIGHTING WILL BE CONTROLLED BY  
ASTRONOMICAL TIME CLOCK AND PHOTOCELL TO  
MET THE REQUIRED LZ2 LIGHTING REQUIREMENTS

Symbol	Qty	TYPE	Mounting Height	Arrangement	proposed shielding	Total Lamp Lumens	LLF	CATALOG #	Proposed Wattage	Max. wattage per light zone LZ2
	2	DD	3.5 ft	SINGLE	SHELDED	3800	0.600	BRL-36-50-MP	32	35
	2	N	10.0 ft	SINGLE	FULL SHELDED	3200	0.700	1101F2642U	42	100
	6	T	10.0 ft	SINGLE	FULL SHELDED	5800	0.750	FSRW-64	(2) 42	100
	12	S	10.0 ft	SINGLE	FULL SHELDED	5800	0.750	SSF-232	(2) 42	100
	11	U	11.6 ft	SINGLE	FULL SHELDED	6600	0.600	WB200-100PMH	100	100
	4	AC1	9 ft	SINGLE	FULL SHELDED	2200	0.700	H91514-100MH	100	100
	8	AC2	9 ft	SINGLE	FULL SHELDED	3200	0.700	TWSL-242TRF-WD	(2) 42	100
	4	CC	25 ft	SINGLE	FULL SHELDED	N.A.	0.900	ATBA20LED70-R2	N.A.	100
	1	BB	25 ft	SINGLE	FULL SHELDED	N.A.	0.900	ATBA20LED70-R2	N.A.	100

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

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**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**PHOTOMETRIC SITE PLAN**

REVISIONS

STAMP

DATE: December 5, 2012  
BLDG. SUBMITTAL DATE: ①  
DATE: ②  
DATE: ③  
BID DATE:  
CKE NO.:  
ARCH. NO.: 1189  
SCALE:  
SHEET

**SE1.0**

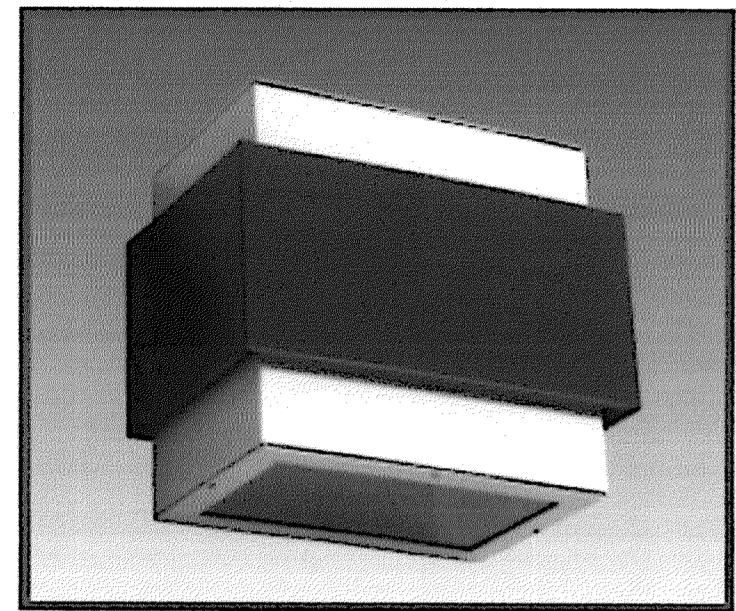






# EXTERIOR WALL SCONCE

TYPE U



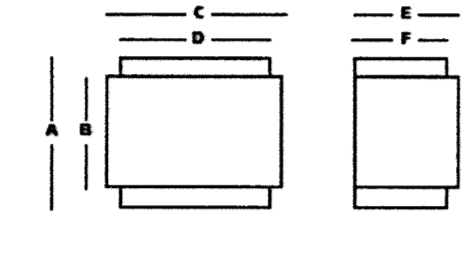
The WB2000 Series is a very versatile wall sconce that would be aesthetically pleasing in any architectural building design. Illuminate or complement building areas with any combination of uplighting and downlighting along with a variety of standard and custom color finishes in solid or contrasting colors.

## Fixture Specifications

### FEATURES

- All aluminum construction
- Luminaire finished in weatherproof powder-coat paint
- UL Wet location listed
- Tempered clear glass lenses at top and bottom openings
- Lensed openings under outside band wash front and sides of main fixture housing with uniform illumination
- Contrasting color band available

### DIMENSIONS



### ORDERING INFORMATION

SAMPLE CATALOG NUMBER

WB2000 XXXXXX XX XX XXX

Series Waiver/Source Finish Distribution Voltage

WB2000 Exterior Wall Sconce

WB White

WB White w/ bronze band

WB White w/ red band

WB White w/ grey band

WB White w/ black band

WB White w/ silver band

WB White w/ gold band

WB White w/ copper band

WB White w/ bronze band

WB White w/ red band

WB White w/ grey band

WB White w/ black band

WB White w/ silver band

WB White w/ gold band

WB White w/ copper band

WB White w/ bronze band

WB White w/ red band

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WB White w/ silver band

WB White w/ gold band

WB White w/ copper band

WB White w/ bronze band

WB White w/ red band

WB White w/ grey band

WB White w/ black band

WB White w/ silver band

WB White w/ gold band

# COOPER LIGHTING - METALUX

## DESCRIPTION

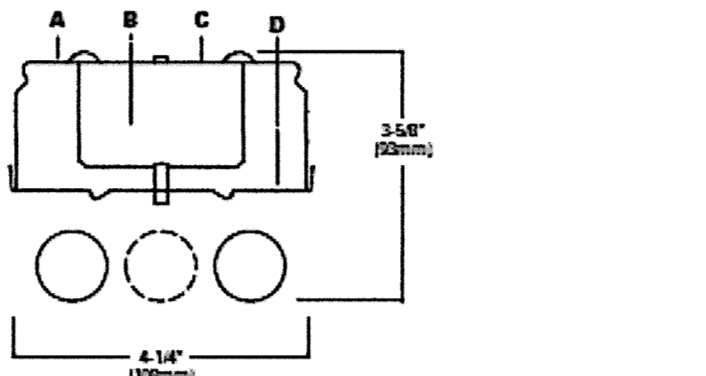
The SSF Series is a functional and multi-purpose standard strip family that incorporates premium performance and construction durability. Designed with our easy-to-use Flip-Up socket design, the SSF significantly reduces installation time. The performance and application versatility of this series can be increased by incorporating asymmetrical or asymmetrical reflectors. The SSF Series can be installed using various mounting methods and numerous options and accessories are available. The SSF Series can be the illumination solution in commercial, industrial, retail and residential applications. Fixtures can be used in storage/utility areas, covers, display cases, shops, task and general area lighting.

## SPECIFICATION FEATURES

- A...Construction**  
Channel die cold rolled steel with numerous KOs for ease of installation. Grooves for Tong Hanger. End plate quickly converts to snap-in channel connector for continuous row alignment. Lamp holder bracket flips in place. Channel/wireway cover secured with quarter-turn fasteners.
- B...Electrical\***  
Ballasts are CBM/ETL Class "P" and are positively secured by mounting bolts. Rotor Lock lampholders, UL/CUL listed. Suitable for damp locations.
- C...Finish**  
Multistage iron phosphate pretreatment ensures maximum bonding and rust inhibitor. Lighting upgrade, baked white enamel finish. Pre-painted material is standard, RAL optional.
- D...Channel/Wireway Cover**  
Die formed heavy gauge steel. Tight fit for ease of maintenance. Easily removed without use of tools. Optional reflector available incorporating silver technology enhances energy efficiency. Consult Pre Sales Technical Support.

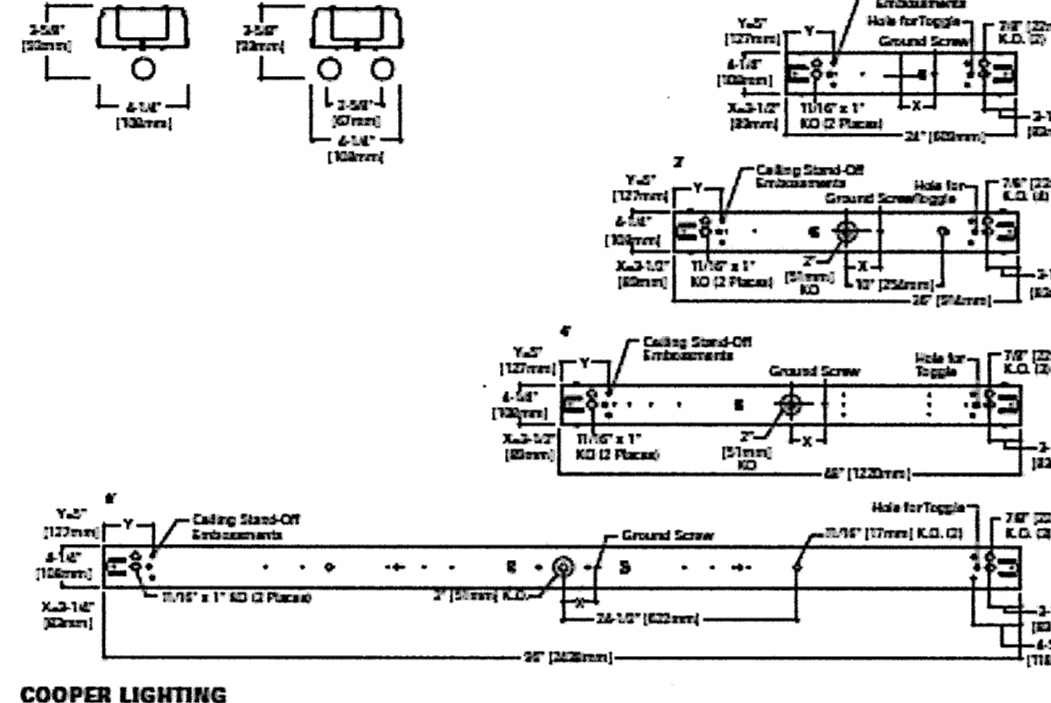
Category #	Type
Project	U
Comments	Date
Prepared by	

SSF 117, 125, 128T8, 132, 217, 225, 228T8, 232, 328T8, 332  
2' 3' OR 4' STRIP  
1, 2 OR 3 TB LAMPS  
Standard Striplite



## LAMP CONFIGURATIONS

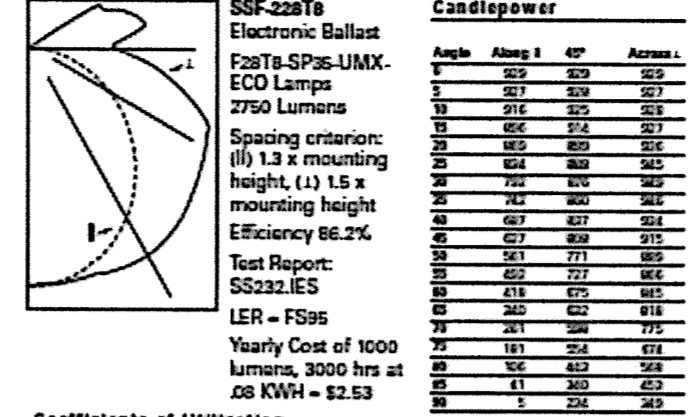
## MOUNTING DATA



## ENERGY DATA

Input Watts	Ballast	Options*	Ballast Type**
117 (20), 117 (24), 125 (20), 125 (24), 128T8 (20), 128T8 (24), 132 (20), 132 (24), 217 (20), 217 (24), 225 (20), 225 (24), 228T8 (20), 228T8 (24), 232 (20), 232 (24), 328T8 (20), 328T8 (24), 332 (20), 332 (24)	117 (20), 117 (24), 125 (20), 125 (24), 128T8 (20), 128T8 (24), 132 (20), 132 (24), 217 (20), 217 (24), 225 (20), 225 (24), 228T8 (20), 228T8 (24), 232 (20), 232 (24), 328T8 (20), 328T8 (24), 332 (20), 332 (24)	117 (20), 117 (24), 125 (20), 125 (24), 128T8 (20), 128T8 (24), 132 (20), 132 (24), 217 (20), 217 (24), 225 (20), 225 (24), 228T8 (20), 228T8 (24), 232 (20), 232 (24), 328T8 (20), 328T8 (24), 332 (20), 332 (24)	117 (20), 117 (24), 125 (20), 125 (24), 128T8 (20), 128T8 (24), 132 (20), 132 (24), 217 (20), 217 (24), 225 (20), 225 (24), 228T8 (20), 228T8 (24), 232 (20), 232 (24), 328T8 (20), 328T8 (24), 332 (20), 332 (24)

## PHOTOMETRICS



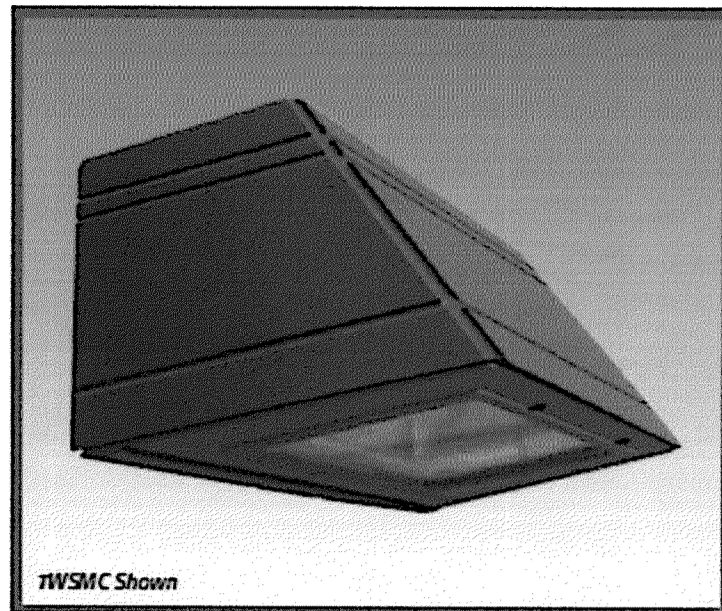
Distance from center of fixture	Beam diameter	Beam area	Beam diameter	Beam area
0	0	0	0	0
1	1.2	1.1	1.2	1.1
2	2.4	4.5	2.4	4.5
3	3.6	10.2	3.6	10.2
4	4.8	18.1	4.8	18.1
5	6.0	28.3	6.0	28.3
6	7.2	40.8	7.2	40.8
7	8.4	54.7	8.4	54.7
8	9.6	70.0	9.6	70.0
9	10.8	86.7	10.8	86.7
10	12.0	104.7	12.0	104.7
11	13.2	124.0	13.2	124.0
12	14.4	144.5	14.4	144.5
13	15.6	166.2	15.6	166.2
14	16.8	189.1	16.8	189.1
15	18.0	213.2	18.0	213.2
16	19.2	238.5	19.2	238.5
17	20.4	265.0	20.4	265.0
18	21.6	292.7	21.6	292.7
19	22.8	321.6	22.8	321.6
20	24.0	351.7	24.0	351.7
21	25.2	383.0	25.2	383.0
22	26.4	415.5	26.4	415.5
23	27.6	449.2	27.6	449.2
24	28.8	484.1	28.8	484.1
25	30.0	520.2	30.0	520.2
26	31.2	557.5	31.2	557.5
27	32.4	596.0	32.4	596.0
28	33.6	635.7	33.6	635.7
29	34.8	676.6	34.8	676.6
30	36.0	718.7	36.0	718.7
31	37.2	762.0	37.2	762.0
32	38.4	806.5	38.4	806.5
33	39.6	852.2	39.6	852.2
34	40.8	899.1	40.8	899.1
35	42.0	947.2	42.0	947.2
36	43.2	996.5	43.2	996.5
37	44.4	1047.0	44.4	1047.0
38	45.6	1098.7	45.6	1098.7
39	46.8	1151.6	46.8	1151.6
40	48.0	1205.7	48.0	1205.7
41	49.2	1261.0	49.2	1261.0
42	50.4	1317.5	50.4	1317.5
43	51.6	1375.2	51.6	1375.2
44	52.8	1434.1	52.8	1434.1
45	54.0	1494.2	54.0	1494.2
46	55.2	1555.5	55.2	1555.5
47	56.4	1618.0	56.4	1618.0
48	57.6	1681.7	57.6	1681.7
49	58.8	1746.6	58.8	1746.6
50	60.0	1812.7	60.0	1812.7
51	61.2	1880.0	61.2	1880.0
52	62.4	1948.5	62.4	1948.5
53	63.6	2018.2	63.6	2018.2
54	64.8	2089.1	64.8	2089.1
55	66.0	2161.2	66.0	2161.2
56	67.2	2234.5	67.2	2234.5
57	68.4	2309.0	68.4	2309.0
58	69.6	2384.7	69.6	2384.7
59	70.8	2461.6	70.8	2461.6
60	72.0	2539.7	72.0	2539.7
61	73.2	2619.0	73.2	2619.0
62	74.4	2700.5	74.4	2700.5
63	75.6	2783.2	75.6	2783.2
64	76.8	2867.1	76.8	2867.1
65	78.0	2952.2	78.0	2952.2
66	79.2	3038.5	79.2	3038.5
67	80.4	3126.0	80.4	3126.0
68	81.6	3214.7	81.6	3214.7
69	82.8	3304.6	82.8	3304.6
70	84.0	3395.7	84.0	3395.7
71	85.2	3488.0	85.2	3488.0
72	86.4	3581.5	86.4	3581.5
73	87.6	3676.2	87.6	3676.2
74	88.8	3772.1	88.8	3772.1
75	90.0	3869.2	90.0	3869.2
76	91.2	3967.5	91.2	3967.5
77	92.4	4067.0	92.4	4067.0
78	93.6	4167.7	93.6	4167.7
79	94.8	4269.6	94.8	4269.6
80	96.0	4372.7	96.0	4372.7
81	97.2	4477.0	97.2	4477.0
82	98.4	4582.5	98.4	4582.5
83	99.6	4689.2	99.6	4689.2
84	100.8	4797.1	100.8	4797.1
85	102.0	4906.2	102.0	4906.2
86	103.2	5016.5	103.2	5016.5
87	104.4	5128.0	104.4	5128.0
88	105.6	5240.7	105.6	5240.7
89	106.8	5354.6	106.8	5354.6
90	108.0	5469.7	108.0	5469.7
91	109.2	5586.0	109.2	5586.0
92	110.4	5703.5	110.4	5703.5
93	111.6	5822.2	111.6	5822.2
94	112.8	5942.1	112.8	5942.1
95	114.0	6063.2	114.0	6063.2
96	115.2	6185.5	115.2	6185.5
97	116.4	6309.0	116.4	6309.0
98	117.6	6433.7	117.6	6433.7
99	118.8	6559.6	118.8	6559.6
100	120.0	6686.7	120.0	6686.7

Zone	Lumen	Wattage	Efficiency
1	117	11.7</	



# TRAPEZOIDAL WALL SCONCE

TYPE AC-1



The TWS Series trapezoidal wall sconce offers unique styling and maximum versatility with two sizes, multiple light sources, and a variety of finishes. The downlight-only design features a maximum cutoff distribution with exceptional horizontal illumination.

## Fixture Specifications

- FEATURES**
- Durable cast aluminum housing\*
  - Integrated design eliminates high angle brightness
  - Luminaire finishes in weatherproof powder-coat paint
  - Completely sealed, flat tempered glass lens suitable for use in wet location
  - Snips complete with lamp
- DIMENSIONS**
- 
- | Finish | A      | B       | C       |
|--------|--------|---------|---------|
| TWSK   | 7 1/2" | 15 1/2" | 9 1/2"  |
| TWSL   | 9 1/2" | 24 1/2" | 13 1/2" |

## ORDERING INFORMATION

SAMPLE CATALOG NUMBER

TWSK XXXXXX W/D XX XXX

Series	Wattage/Source	Description	Finish	Voltage
TWSK	70W	70 watt pulse start metal halide	Black	120V
TWSL	100W	100 watt pulse start metal halide	Black	120V
TWSK	100W	100 watt pulse start metal halide	Black	277V
TWSL	150W	150 watt pulse start metal halide	Black	277V
TWSK	150W	150 watt high pressure sodium	Black	120V
TWSL	210W	210 watt high pressure sodium	Black	277V
TWSK	210W	210 watt high pressure sodium	Black	277V
TWSL	25W	25 watt quad tube fluorescent	Black	120V
TWSK	32W	32 watt tube fluorescent	Black	120V
TWSL	42W	42 watt tube fluorescent	Black	120V
TWSK	2x26W	2x26 watt tube fluorescent	Black	120V
TWSL	2x32W	2x32 watt tube fluorescent	Black	120V
TWSK	30W	30 watt LED	Black	120V

Series	Wattage/Source	Description	Finish	Voltage
TWSK	70W	70 watt pulse start metal halide	Black	120V
TWSL	100W	100 watt pulse start metal halide	Black	120V
TWSK	100W	100 watt pulse start metal halide	Black	277V
TWSL	150W	150 watt pulse start metal halide	Black	277V
TWSK	150W	150 watt high pressure sodium	Black	120V
TWSL	210W	210 watt high pressure sodium	Black	277V
TWSK	210W	210 watt high pressure sodium	Black	277V
TWSL	25W	25 watt quad tube fluorescent	Black	120V
TWSK	32W	32 watt tube fluorescent	Black	120V
TWSL	42W	42 watt tube fluorescent	Black	120V
TWSK	2x26W	2x26 watt tube fluorescent	Black	120V
TWSL	2x32W	2x32 watt tube fluorescent	Black	120V
TWSK	30W	30 watt LED	Black	120V

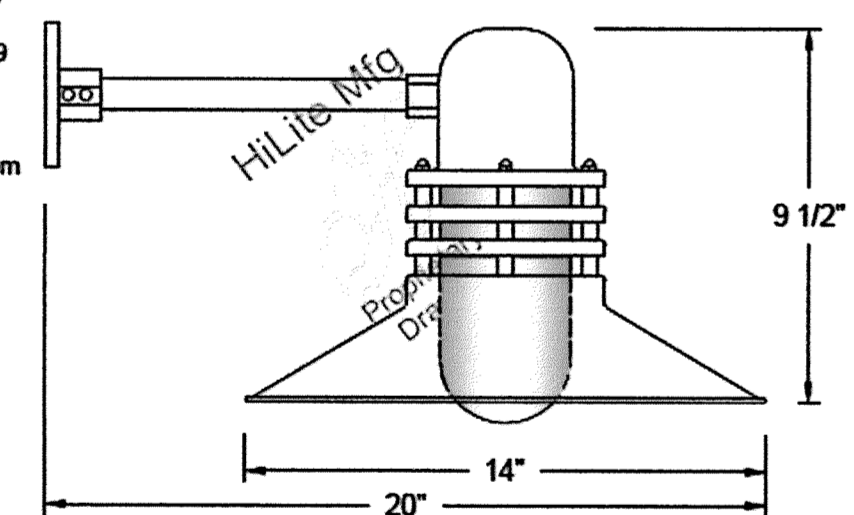


Performance Designed Lighting Products  
1085 Johnson Drive • Buffalo Grove, IL 60089 • TOLL-FREE: 800-544-4848 • PHONE: 847-279-0627 • FAX: 847-279-0642



# H-91514-B Mars Collection

Job Name:  
Type: TYPE AC-2  
Quantity:



**FINISH** - Offered in exceptional finishes, comprised of polyester/powdered powder coat, baking enamel liquid, raw metal, or galvanized finishes.  
Standard Finishes are: 91(Black), 93(White), 95(Dark Green), 96(Galvanized), 97(Powder Coat Rust), BK01(Black Texture), GN20(Powder Coat Patina).  
Upgraded Finishes are: 29, 66, 82, 90, 92, 94, 97, 99, 100, 103, 104, 105, 110, 112, 113, 114, 115, 117, 118, 119, 120, 121, 122, 123, 133, 134, 135, 136, 98, 101, 102, 137, 138, 139, 140, 121, 122, 123, 124, 125, 126, 24, 44, 48, 49.

For interior finish of fixture refer to color chart on pages 344-348.

**MOUNTING** - Wall Mount.

**ACCESSORIES** - CAL(Clear Acrylic Layer), BAL(Blue Acrylic Layer), RAL(Red Acrylic Layer) and GNAL(Green Acrylic Layer) available.

**REFLECTOR** - Heavy duty, spun shade, aluminum 6061-T6 and/or 1100-T6, galvanized 22 gauge, steel 20/22 gauge, copper 032/040 and brass 032/040 construction. Dependant on finish.

**SOCKETS/LAMPS** - Available in:  
Incandescent  
- rated 100 watt max/120 volt, medium base.  
Compact Fluorescent(CFL)  
- rated 13/18/26/32 watt max/120/277 volt, GX24Q base.  
Metal Halide(MH)  
- rated 35/50/70/100 watt max/120/208/240/277 volt, medium base, 4KV socket.  
High Pressure Sodium(HPS)  
- rated 50/70/100 watt max/120/277 volt, medium base.  
Light-Emitted Diode (LED)  
- rated 20w LED max. See LED specifications for complete part number.

**LENS OPTION** - CLR(Clear), RIB(Clear Ribbed), FR(Frost), RD(Red), BL(Blue), GRN(Green), AM(Amber) and AMC(Amber Crackle) available.



MADE IN THE U.S.A. Suitable for wet location.

## ORDERING INFORMATION

Sample Number: BSL-100-MP-120-00-35-8K-4GL

Product Family	Lamp Wattage	Voltage *	Options	Optional System	Optional Luminaires	Accessories **
BSL-100-MP-120-00-35-8K-4GL	100W	120V	35-8K-4GL	BSL-100-MP-120-00-35-8K-4GL	BSL-100-MP-120-00-35-8K-4GL	BSL-100-MP-120-00-35-8K-4GL

- Notes:  
1. All MP lamps are 120V and feature a 120V base. All TWS series are 120V and feature a 120V base.  
2. All 20/22/24/26/32W CFL lamps feature a 6-pin base. Available in CFL and CFL distribution only.  
3. Dual compact fluorescent lamps.  
4. Nominal MSL lamp length of 27.5" for CFL, not to exceed 7".  
5. All halogen lamps are T4 and feature a 19mm base.  
6. High/low voltage lamps available in 120V only. Requires use of VAS201 secondary. Three-way CFL of ballasts are 120 through 277V. Specify with UVH voltage designation.  
7. Product also available in 120V/277V and 277V for international markets. Contact factory for availability and ordering information.  
8. Double or 150/277V and 277V.  
9. Multi-step is 120/208/240/277V and 277V.  
10. Tri-step is 120/277V and 277V.  
11. 120 through 277V only. Electronic ballast standard with all CFL lamps. ES available with 35/50/70/100 MP lamps. 120V requires and is supplied with VAS201 Three-way. Not available with GBL or EMAB system.  
12. Custom and RAL color matching available upon request. Consult your NVLE Lighting Systems Representative for further information.  
13. Add as suffix in the order status.  
14. Dual switching requires dual 20, 22 or 40W Compact Fluorescent lamps. Allow independent switching control of each lamp through use of two (2) electronic ballasts. Allow 50% power reduction when dual ballast are independently used and controlled.  
15. VAS201 and emergency ballast options to be requested being determined only. Cannot be used for primary "light only" applications.  
16. CFL lamps only. Emergency ballast provides 80 minutes of supplemental light, maximum operating temperature at 32°F (0°C), 40W maximum.  
17. For use in clean lighting applications only.  
18. Forward secondary line provided standard on 35/51 and CFL distribution.  
19. Other secondary, reverse 30 with color suffix.



NOTE: Specifications and dimensions subject to change without notice.  
Visit our web site at www.cooperlighting.com  
Customer First Center 1121 Highway 24 South Peachtree City, GA 30269 770.468.4800 FAX 770.468.4801

ADH9249 2p  
2010-08-22 10:28:29  
2010-07-22 13:07:21

## DESCRIPTION

The McGraw-Edison BSL/BRL Bollard Series, available in heights from 24" to 42", has crisp, clean lines which blend with any architectural setting. Constructed of seamless, heavy-duty aluminum and finished with a tough polyester powder coat finish, the McGraw-Edison Louvered Bollard Series is gasketed to seal out external contaminants. U.L. 1598 listed and CSA certified for wet locations.

Bollards are designed for walkways, entrances, drives and other small-area lighting applications where low mounting heights are desirable.

## SPECIFICATION FEATURES

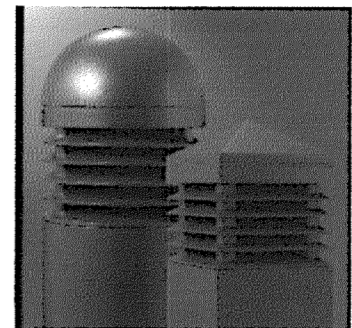
**Construction**  
Rugged, minimum 5/32" thick cast aluminum top cap secured via a concealed stainless steel allen screw with twist removal mechanism for lamp access. Flow through ventilation assure cool to the touch top. LOUVERS: Cast Aluminum Louver blades provide sharp cutoff delivering no direct light above 90°. Louvers are secured to the shaft via tamper stainless steel rods and fasteners. LOWER HOUSING: Nominal 1/8" thick aluminum extruded housing. Bollard housing is secured to the base with flathead, counter sunk screws for smooth, uncluttered appearance. BASE: Rugged cast aluminum. Completely concealed.

**Electrical**  
HID High Power Factor ballast for 20°F starting. CFL Electronic ballast for OF starting. Product is factory mounted to the base. Quick disconnects provided between lamp and electrical assembly. Metal Halide and High Pressure Sodium lamp sources up to 100W and up to 40W Compact Fluorescent sources.

**Optical**  
LAMP ENCLOSURE: One piece tempered glass with internal flutes for even distribution of light. Illumination. Decorative colored glass optional. Globe is fully gasketed with EPDM material. Socket is porcelain, medium-base for HID lamp sources and

## McGraw-Edison®

Category #	Type
Project	DD
Comments	Date
Prepared by	



## BSL/BRL BOLLARD

26 - 100W

Pulse Start Metal Halide

High Pressure Sodium

Compact Fluorescent

Incandescent

PATHWAY LUMINAIRE



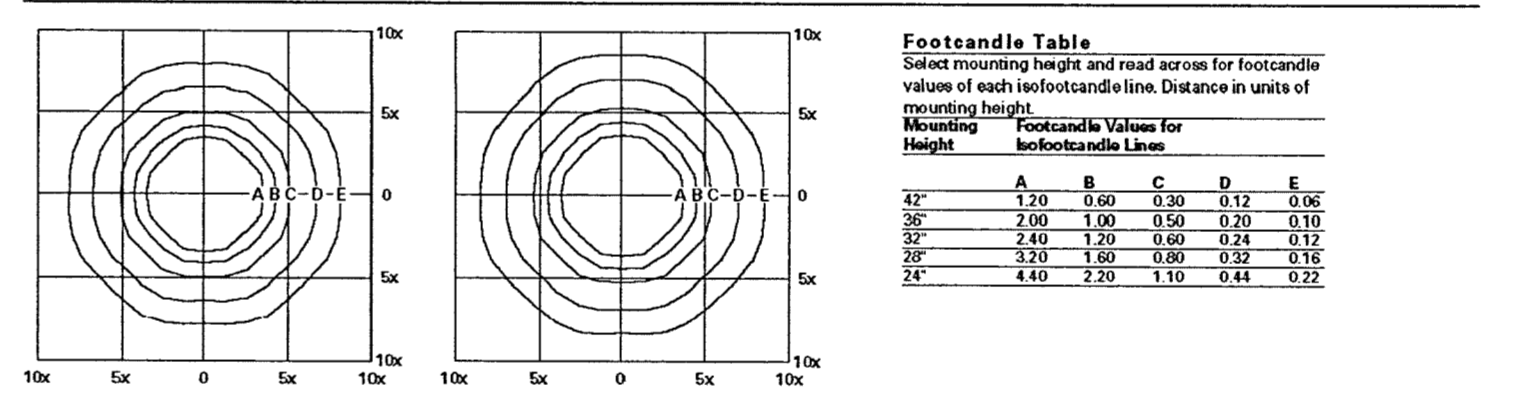
NOTE: Refer to fixture only.

Lamp Type	Wattage
Pulse Start Metal Halide (MP)	50, 70, 100W
High Pressure Sodium (HPS)	35, 50, 70, 100W
Compact Fluorescent (CF)	11, 26, 32, 40, 42W
Incandescent (I)	100W

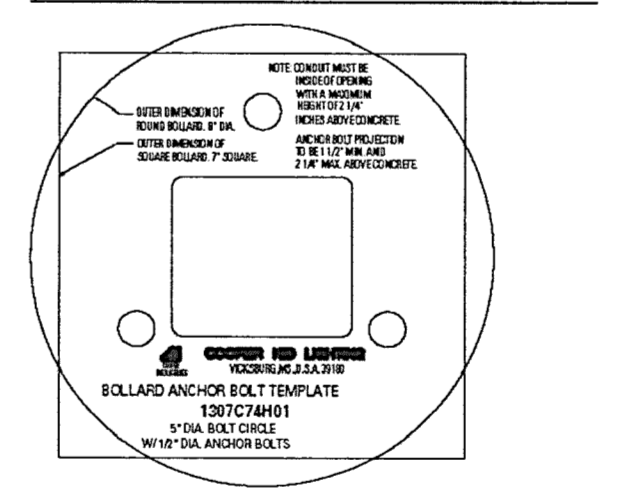
High Reactance Ballast Input Watts  
50W HPS HFF (60 Watts)  
50W MP HFF (60 Watts)  
70W HPS HFF (80 Watts)  
70W MP HFF (80 Watts)  
100W HPS HFF (110 Watts)  
100W MP HFF (110 Watts)  
150W HPS HFF (170 Watts)  
150W MP HFF (170 Watts)

SHIPPING DATA  
Approximate Net Weight:  
26 lbs. (12 kg)

## PHOTOMETRICS



## ANCHOR BOLT TEMPLATE (NOT TO SCALE)



## ORDERING INFORMATION

Product Family	Feature Height	Lamp * Wattage	Lamp Type	Voltage *	Color *	Options *
BSL-Square Bollard with Clear Lens and Pyramidal Top	24"-24"	120-120W	MP-Pulse Start Metal Halide	120-120V	Black	1=Single Flare (120, 277 or 347V) 2=Double Flare (208, 240 or 480V) L=Lamp Included
BRL-Round Bollard with Stacked Louvers and Crown Top	30"-30" 42"-42"	30-30W 42-42W	CF-Compact Fluorescent HPS HPS 50-50W 70-70W 100-100W I 26- (1)26W 32- (1)32W 42- (1)42W I 100-100W	277-277V 240-240V 277-277V 240-240V 277-277V 240-240V 120-277V	Black White Dark Platinum Graphite Metallic	R1=Provisional cut for CFL Receptacle R2=Incandescent CFL Receptacle CFEM=Emergency Battery Back AB=Aqua Marine Blue Glass Jar HG=Hunter Green Glass Jar CG=Cherry Glass Jar BB=Royal Blue Glass Jar RE=Red Glass Jar

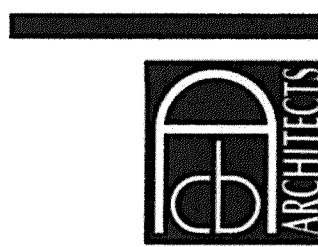
- Notes:  
1. HID lamps are medium base. 120W MP is available for most U.S. markets only.  
2. 30W HPS available in 120V only.  
3. Products also available in non-US voltages and 50Hz for international markets. Consult factory for availability and ordering information.  
4. All MP lamps are 120V and feature a 120V base. All TWS series are 120V and feature a 120V base.  
5. Compact Fluorescent only. Electronic ballast universal voltage 120-277V.  
6. Other finish colors available, including a full range of RAL color matches. Consult your Cooper Lighting Representative.  
7. Add as suffix in the order status.  
8. Custom lamp stands & Must specify clear lamp if desired.  
9. Location of H and R suffix on housing subject to height of luminaire.  
10. CFL lamps only, rated maximum operating temperature 32°F (0°C).



NOTE: Specifications and dimensions subject to change without notice.  
Visit our web site at www.cooperlighting.com  
Customer First Center 1121 Highway 24 South Peachtree City, GA 30269 770.468.4800 FAX 770.468.4801

ADH9249 2p  
2010-08-22 10:28:29  
2010-07-22 13:07:21

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CB ANDERSON ARCHITECTS PLLC

7209 Greenwood Avenue N  
Seattle, Washington 98103  
206-728-2911  
Fax 782-5624

## CLIENT

Wilsonville Devco LLC  
c/o Integrated Investment Partners  
PO Box 919  
Portland, OR 97207  
(503) 688-5600

## PROJECT

Boones Ferry Pointe  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

## DRAWING TITLE

SITE LIGHTING SPECIFICATIONS

## REVISIONS


## STAMP

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DATE: December 5, 2012

BLDG. SUBMITTAL DATE: ①

DATE: ②

DATE: ③

BID DATE: ④

CKE NO.: ⑤

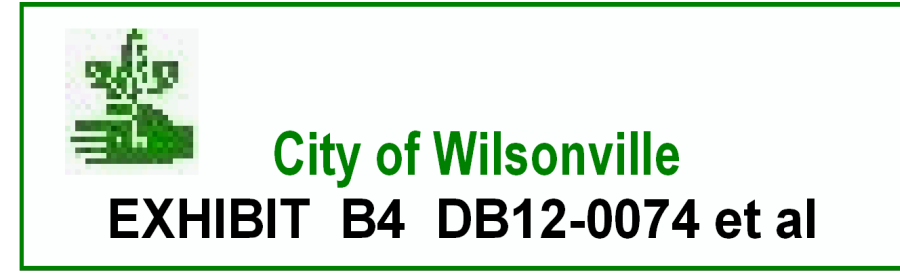
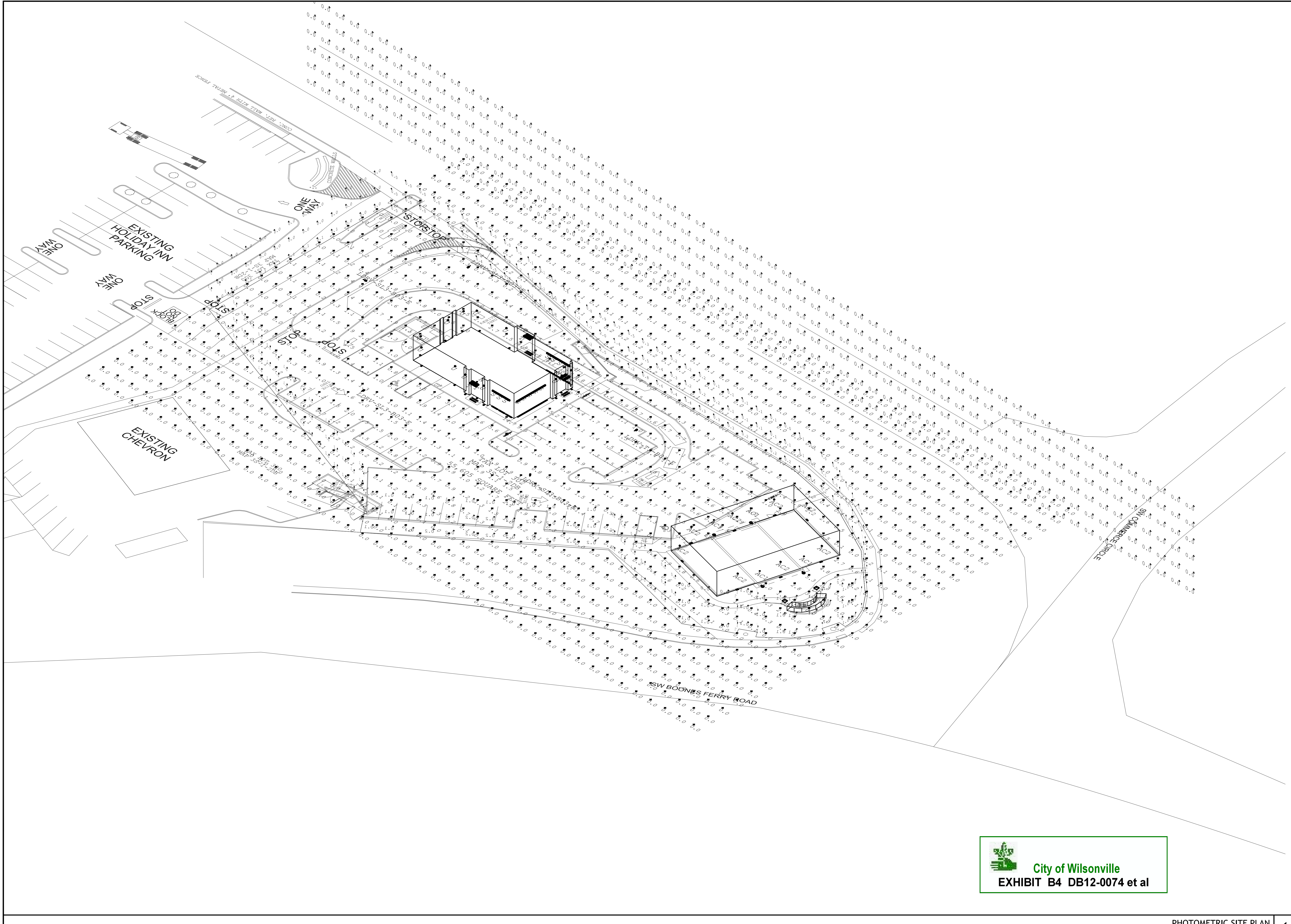
ARCH. NO.: 1199

SCALE: SHEET

SE1.3



FILE NAME:



PHOTOMETRIC SITE PLAN  
SCALE: 1" = 20'-0"

KWKG CONVERSION BASED ON CKE PROTOTYPE VERSION 22-14

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DATE: December 5, 2012

BLDG. SUBMITTAL DATE: ①  
DATE: ②  
DATE: ③

BID DATE:

CKE NO.:

ARCH. NO.: 1199

SCALE:

SHEET

**SE1.4**

REVISIONS

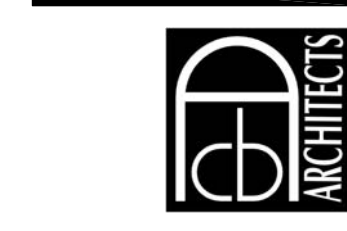
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PHOTOMETRIC SITE PLAN  
ISOMETRIC VIEW

**Boones Ferry Pointe**  
SW 95TH AVE & COMMERCE CIRCLE  
WILSONVILLE, OREGON

**Wilsonville Devco LLC**  
C/O INTEGRATED INVESTMENT PARTNERS  
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**VIII. Board Member Communications:**

**A. Agenda Results from the February 25, 2013 DRB  
Panel B meeting**



# City of Wilsonville

## Development Review Board Panel B Meeting Meeting Results

<b>DATE:</b>	FEBRUARY 25, 2013	
<b>LOCATION:</b>	29799 SW TOWN CENTER LOOP EAST, WILSONVILLE, OR	
<b>TIME START:</b>	6:30 P.M.	<b>TIME END:</b> 8:49 P.M.

### ATTENDANCE LOG

BOARD MEMBERS	STAFF
Andrew Karr, Chair	Blaise Edmonds
Dianne Knight	Barbara Jacobson
Jhuma Chaudhuri	Daniel Pauly
Aaron Woods	Amanda Hoffman
Cheryl Dorman, Vice Chair, was absent.	Chris Neamtzu

### AGENDA RESULTS

AGENDA	ACTIONS
CITIZENS' INPUT	None
CONSENT AGENDA	
A. Approval of January 28, 2013 Minutes	Unanimously approved as submitted
PUBLIC HEARING	
<p><b>A. Resolution No. 245. Les Bois Row Homes: Polygon Northwest Company – applicant.</b> The applicant is requesting approval of Final Development Plan (FDP) for PDP - 1 Central (Les Bois Row Homes) for detached row houses and duplexes. The site includes Tax Lots 14300 – 1440 and 14600 – 15200 in Section 15DB, T3S, R1W, Clackamas County, Oregon. Staff: Blaise Edmonds</p> <p>Case File: DB12-0083 – Final Development Plan</p>	<p>A. Resolution No. 245 unanimously continued to March 25, 2013.</p>
<p><b>B. Resolution No. 246. Toulouse Street Detached Row Homes: Stacy Connery, Pacific Community Design for Polygon Northwest Company – applicant.</b> The applicant is requesting approval of a Preliminary Development Plan (PDP) Refinement and Amendment with Specific Area Plan Refinement, Tentative Subdivision Re-plat and Final Development Plan (FDP) for PDP - 1 Central (Toulouse St. Row Homes) for detached row houses. The site includes Tax Lots 8200, 8300, 8400 and 8500, Section 15DB, T3S, R1W, Clackamas County, Oregon. Staff: Amanda Hoffman</p> <p>Case Files: DB12-0077 - Preliminary Development Plan Refinement and Amendment with Specific Area Plan Refinement DB12-0078 - Tentative Subdivision Re-plat DB12-0079 - Final Development Plan</p>	<p>B. Resolution No. 246 unanimously adopted with corrections to the Staff report.</p>

<p><b>C. Resolution 247. Costa Circle West Detached Row Homes: Stacy Connery, Pacific Community Design for Polygon Northwest Company – applicant.</b> The applicant is requesting approval of a Preliminary Development Plan (PDP) Refinement and Amendment with Specific Area Plan Refinement, Tentative Subdivision Re-plat and Final Development Plan (FDP) for PDP - 2 Central (Costa Circle West Row Homes) for detached row houses. The site includes Tax Lots 400, 500, 600, 700, 800, 900, 1000, 1100, 1200, 1300, 1400, 1500, 1600, 1700 and 1800 in Section 15AD, T3S, R1W, Clackamas County, Oregon. Staff: Daniel Pauly</p> <p>Case Files: DB12-0080 - Preliminary Development Plan Refinement and Amendment with Specific Area Plan Refinement  DB12-0081 - Tentative Subdivision Re-plat  DB12-0082 - Final Development Plan</p>	<p>C. Resolution No 247 unanimously adopted with added Condition PDC 13 which addressed the placement of rainwater planters.</p>
<p><b>BOARD MEMBER COMUNICATIONS</b></p>	
<p>A. Results of the February 11, 2013 DRB Panel A meeting</p>	<p>None</p>
<p><b>STAFF COMMUNICATIONS</b></p>	
<p></p>	<p>None</p>

**RECORDED BY:**